

MOTION:

**November 17, 2015
Regular Meeting
Res. No. 15-**

SECOND:

**RE: REZONING #PLN2014-00040, BLACKBURN – GAINESVILLE
MAGISTERIAL DISTRICT**

ACTION:

WHEREAS, this is a request to rezone ± 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and ± 51.7 acres from A-1, Agricultural to PMD, Planned Mixed Use District; and

WHEREAS, the site is located on the north and south sides of Balls Ford Road, approximately $\frac{1}{2}$ mile west of the intersection of Balls Ford Road and Sudley Road. The site is identified on County maps as GPINs 7597-92-1061, 7597-82-7607 and 7597-82-4068. The site is designated REC, Regional Employment Center, and SRH, Suburban Residential High, in the Comprehensive Plan; and

WHEREAS, staff has reviewed the subject application and recommends denial as stated in the staff report; and

WHEREAS, the Planning Commission, at its public hearing on April 1, 2015, continued Rezoning #PLN2014-00040, Blackburn, to date certain of April 15, 2015 as stated in Planning Commission Resolution Number 15-028; and

WHEREAS, the Planning Commission, at its public hearing on April 15, 2015, recommended approval of Rezoning #PLN2014-00040, Blackburn, as stated in Planning Commission Resolution Numbers 15-034, 15-037 and 15-038; and

WHEREAS, a Board of County Supervisors' public hearing, duly advertised in a local newspaper for a period of two weeks was held on May 12, 2015, and interested citizens were heard; and

WHEREAS, the Board of County Supervisors closed the public hearing and deferred Rezoning #PLN2014-00040, Blackburn, to date uncertain, as stated in Resolution Number 15-352; and

WHEREAS, there have been no changes made to the case subsequently; and

WHEREAS, the Prince William County Board of County Supervisors believes that general welfare and good zoning practices are served by the denial of this request;

NOW, THEREFORE, BE IT RESOLVED that the Prince William Board of County Supervisors does hereby deny Rezoning #PLN2014-00040, Blackburn.

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ATTACHMENT: Proffer Statement dated April 24, 2015

Votes:

Ayes:

Nays:

Absent from Vote:

Absent from Meeting:

For Information:

Planning Director

Jay Du Von
Walsh, Colucci, Lubeley & Walsh, PC
4310 Prince William Parkway, Suite 300
Woodbridge, VA 22192

ATTEST: _____
Clerk to the Board

PROFFER STATEMENT

RE: REZ #PLN2014-00040, Blackburn
Record Owners: Estate of Marion C. Wheeler
Property: G.P.I.Ns.: 7597-92-1061, 7597-82-7607 and 7597-82-4068 (the "Property")
Acreage: 128.3502 acres, Gainesville Magisterial District
Current Zoning: A-1, Agricultural
Proposed Zoning: PMR, Planned Mixed Residential and
PMD, Planned Mixed Use District
Date: April 24, 2015

The undersigned hereby proffers that the use and development of the subject Property shall be in strict conformance with the following conditions. In the event the above-referenced rezoning is not granted as applied for by the Applicant, these proffers shall be withdrawn and are null and void. The headings of the proffers set forth below have been prepared for convenience or reference only and shall not control or affect the meaning or be taken as an interpretation of any provision of the proffers. Any improvements proffered herein below shall be provided at the time of development of the portion of the site served by the improvement, unless otherwise specified. The terms "Applicant" and "Developer" shall include all future owners and successors in interest.

For purposes of reference in this Proffer Statement, the Master Zoning Plan (the "MZP") shall be the plan prepared by Urban, Ltd., entitled "Master Zoning Plan, Planned Mix Residential District and Planned Mix Use District, Blackburn" dated April, 2013, revised November 21, 2014 and the landscape plan shall be the plan prepared by Studio 39 entitled "Blackburn, MZP- Landscape Set," dated August 8, 2014, revised December 3, 2014 (the "Landscape Plan").

USES AND DEVELOPMENT

1. Uses:

- A. PMR Zoning District (Land Bays 1 and 2): approximately 76.677 acres of land, as identified on the MZP, shall be developed with no more than one hundred sixty (160) single family detached units and one hundred seventy-seven (177) townhouse units.
- B. PMD Zoning District (Land Bays 3, 4 and 5): approximately 51.6725 acres of land, as identified on the MZP, shall be developed with no more than seventy-eight (78) townhouse units, one million gross square feet of office, pursuant to the O(H) zoning district and 115,000 gross square feet of uses allowed in the B-1

zoning district. The office district referenced above may contain retail uses in accordance with the O(H) Zoning District.

2. Development of the Property shall be in substantial accordance with the MZP, however, the internal road alignment, building, parking, lot lines, lot shape, community amenities, trails (onsite or off-site) and on-site or off-site stormwater management improvements and on-site or off-site road improvements may undergo alterations in accordance with final engineering considerations at the time of site/subdivision plan review.
3. Notwithstanding the layout shown for Land Bay 3 or 4, the Applicant reserves the right to make modifications to the internal layout and design of the land bays in order to provide for future B-1 uses located in Land Bay 4 and the final design of the regional storm pond that is located in Land Bays 3 and 4.
4. Drive through uses shall be prohibited in Land Bays 1, 2, 3 and 5. One drive through may be permitted in Land Bay 4 for either a financial institution or pharmacy.

COMMUNITY DESIGN

5. Landscaping and landscape buffers shall be provided substantially as shown the Landscape Plan, using drought tolerant indigenous species where supplemental planting area is required, subject to final engineering considerations at the time of site/subdivision plan review and subject to minimal, necessary utility crossings and fencing. Where existing vegetation is proposed to remain undisturbed within said landscaping and landscape buffers, said existing vegetation shall be credited against the plant unit requirements identified in the Design and Construction Standards Manual ("DCSM") and where the existing vegetation is sufficient may satisfy said requirements.
6. In the event adjacent properties are rezoned or developed for compatible uses, the landscape buffers as shown on the MZP may be reduced in accordance with the DCSM and, if applicable, the review and approval of a site plan revision.
7. A monument style sign feature identifying the project shall be permitted in locations along Balls Ford Road, Ashton Avenue, Hollis Drive extended, Coverstone Road and Bethlehem Road, generally as shown on the MZP. The design and materials used for the monument signs shall be in general accordance with the sign elevation sheet attached hereto as Attachment A, entitled "Entry Features," prepared by Studio 39, dated August 11, 2014, and in accordance with the Design Guidelines identified below and/or subject to approval by the Planning Director or his/her designee. The size of said monument style signs may vary but, they shall not exceed the dimensions otherwise shown on Attachment A, unless approved by the Planning Director or his/her designee.

Such signage shall require sign permit approval. Landscaping around the monument style signs shall include a mixture of evergreens, shrubs, perennials and ornamental grasses.

8. Development within the Land Bays will be general accordance with the design guidelines entitled "Blackburn, Virginia," dated May 27, 2014, (the "Design Guidelines"). Said Design Guidelines will provide for the orderly and unified development of the Property.
9. Architecture within the Land Bays shall be subject to the Design Guidelines and shall be subject to the following:
 - A. For purposes of this proffer, building fronts are defined as that elevation where the main entrance is located, building sides are defined as any elevation between the front and rear and building rears are defined as the elevation opposite the front.
 - B. The exterior of the townhouse units located in Land Bays 2 and 3 shall be designed in general accordance with the single family attached (SFA) guiding principles found in the Design Guidelines.

Compliance with this proffer shall be evidenced with the submission of building elevations for review and approval at least two weeks prior to the issuance of the building permit release letter. Any substantive changes to the design and/or materials shall be submitted to the Planning Director for review and approval. Such approval shall be based on a determination that the changes result in a building of similar or greater quality.

- C. The exterior of the single family detached units located in Land Bay 1 shall be shall be designed in general accordance with the single family detached (SFD) guiding principles found in the Design Guidelines.:

Compliance with this proffer shall be evidenced with the submission of building elevations for review and approval at least two weeks prior to the issuance of the building permit release letter. Any substantive changes to the design and/or materials shall be submitted to the Planning Director for review and approval. Such approval shall be based on a determination that the changes result in a building of similar or greater quality.

- D. The exterior of the buildings located on Land Bays 4 and 5 shall be shall be designed in general accordance with the guiding principles found in the Design Guidelines.

- i) The overall architectural style of the office buildings located in Land Bay 5 shall be in general conformance with one of the design concept alternatives prepared by Bignell Watkins Hasser Architects P.C., attached hereto as Attachment B, entitled "Blackburn Office Park, Design Concepts," alternatives 1-4, dated December, 17, 2013, recognizing that individual features of the buildings may vary from the design concept and from building to building to add architectural variety.
 - ii) Building heights within Land Bay 5 will be graduated with buildings closest to Balls Ford Road containing a minimum of three (3) stories. The four interior buildings shall contain a minimum of four stories and a maximum of six (6) stories. Buildings located within Land Bay 5 shall not exceed a building height of seventy feet (70'), including roof top mechanicals.
1. Compliance with this proffer shall be evidenced with the submission of building elevations for review and approval at least two weeks prior to the issuance of the building permit release letter. Any substantive changes to the design and/or materials shall be submitted to the Planning Director for review and approval. Such approval shall be based on a determination that the changes result in a building of similar or greater quality.
10. Residential units located in Land Bays 1, 2 and 3 shall be designed and constructed as ENERGY STAR ® qualified homes or equivalent rated home. Compliance shall be demonstrated by a certification statement provided prior to each request for occupancy permit issuance.
11. Land Bay 1 shall include a meandering nature/beaten path generally as shown on Sheet 4 of the MZP. The path alignment shall be determined during construction to minimize impact on existing trees and in the location approximately shown on the final site plan for Land Bay 1 and shall be located outside of any wetlands.. The path will be a minimum five foot (5') wide meandering natural surface walking path cleared of saplings and mulched at Applicant's discretion. Small signs designating the path will be installed every 500 feet. The HOA will be responsible for maintenance of the path and associated signage.
12. Development of Land Bays 3 and 4 shall include an integrated sidewalk network to facilitate pedestrian connectivity between the two land bays. Said integrated sidewalk shall be shown on the site/subdivision plan for Land Bays 3 and 4.
13. Exterior lighting within Land Bay 5 shall be directed downward and inward to minimize off-site bleeding, with the exception of security lighting.

2. CULTURAL RESOURCES

14. As a condition of the site/subdivision plan approval for Land Bays 1, 4 and 5, the Applicant shall retain a qualified professional to conduct a Phase I Metal Detector Survey of that portion of the land bay for which metal detection is recommended. Said Metal Detection Survey work shall be done in accordance with the scope of work dated April 22, 2013 attached hereto as Attachment C. Three (3) copies of the draft report and one electronic copy documenting the results and recommendations of the Phase I Metal Detector Survey shall be submitted to the Planning Office for review, comment, and approval, prior to plan approval. In the event the findings of the Phase I Metal Detection Survey indicate that a Phase II archaeological evaluation is warranted, the Applicant shall conduct such Phase II archaeological evaluation on those sites and resources that are located in areas of the land bay(s) that will be disturbed. All Phase II scopes of work shall be approved by the Planning Director or his designee. Three (3) copies and one electronic copy of the Phase II draft report documenting the results and recommendations of the archaeological evaluation shall be submitted to the Planning Office for review, comment, and approval prior to final site/subdivision plan approval for each Land Bay 1, 4 and 5. The qualified professional, the archaeological testing, and the reports shall meet the standards set forth in the current version of the Virginia Department of Historic Resources (VDHR) *Guidelines for Conducting Historic Resources Survey in Virginia*.
15. In the event the Phase II evaluations find an archaeology site significant and the site will be disturbed by construction, the Applicant will, prior to issuance of a land disturbance permit for that area containing the significant site, either initiate mitigation of the archaeology site through Section 106 of the National Historic Preservation Act (NHPA), with the County as a consulting party; or, if Section 106 review is not required, the Applicant will prepare a mitigation plan for approval by the Planning Director or his designee. If a County mitigation plan is required, its stipulations shall be completed prior to issuance of the land disturbance permit. The mitigation plan shall meet the standards set forth in the current version of the VDHR's *Guidelines for Conducting Historic Resources Survey in Virginia* and also the Advisory Council on Historic Preservation's (ACHP) *Recommended Approach for Consultation on Recovery of Significant Information from Archeological Sites* (<http://www.achp.gov/archguide.html#supp>). Final reports shall be submitted in quantities, formats and media as requested by the County Archaeologist after the report(s) have been approved by the County.
16. As a condition of first site/subdivision plan approval for Land Bays 1, 4 and 5, the Applicant shall curate with the County, artifacts, field records, laboratory records, photographic records, computerized data and other historical records the Applicant possesses as recovered as a result of its cultural resources investigations. All artifacts and records submitted for curation shall meet current professional standards and The Secretary of the Interior's Standards and Guidelines for Archeology and Historic Preservation. A curation fee equal to VDHR's curation fee (not to exceed \$350.00/box)

will be paid by the Applicant at the time of delivery to the County. Ownership of all records submitted for curation shall be transferred to the County with a letter of gift.

ENVIRONMENTAL

17. The Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$75.00 per acre for water quality monitoring, drainage improvements and/or stream restoration projects. Said contribution shall be made prior to and as a condition of site/subdivision plan approval for each phase of the project with the amount to be based on the acreage reflected on that site/subdivision plan.
18. Stormwater Management and/or Best Management Practices shall be provided on-site and/or off-site in accordance with the DCSM. The on-site facilities shall be provided in the approximate location(s) shown on the MZP, subject to minor changes as may be approved by the County in connection with site plan review and in accordance with the DCSM and DEQ permitting. Facilities will be constructed along with development of each phase of the project.
19. As a condition of site/subdivision plan approval for each Land Bay, the Applicant shall utilize, to the extent feasible due to existing soil types on the Land Bay, low impact development (LID) techniques in combination with traditional stormwater management techniques to manage stormwater runoff as part of its proposed stormwater management for the Property, as permitted by the DCSM. Said techniques may include, but shall not be limited to, bioretention areas, rain gardens, filter strips and/or other practices that can be integrated into landscaping areas and/or other common areas as shown on the MZP. The final specific techniques to be utilized shall be determined at the time of site/subdivision plan review for the each Land Bay, in consultation with the Department of Public Works, Watershed Management Division.
20. No clearing or improvements shall be made outside of the limits of clearing and grading shown on the MZP without County approval, with the exception of: (a) the installation of plant materials, if needed to supplement existing vegetation, within buffer areas shown on the MZP; installation of landscaping; (b) the removal of noxious vegetation, such as poison ivy, poison oak, etc., as well as dead, dying or hazardous trees; (c) minor engineering considerations and minor utility improvements.
21. The limits of the Resource Protection Area ("RPA") located within Land Bay 3 and 4 shall be determined at the final design of the Regional Storm Pond, which will be located in Land Bays 3 and 4. As a condition of approval for the regional storm pond, the Applicant shall reforest within the RPA, utilizing a planting rate of 700 trees per acre, in accordance with DCSM Section 802.21.E, or applicable sections of the DCSM, or as approved by the regional storm pond plan.

FIRE & RESCUE

22. As a condition of each residential occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$720.00 for a townhouse unit and \$749.00 for a single family detached unit to be constructed, to be used for fire and rescue purposes.
23. As a condition of each nonresidential building permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$0.61 per square foot of non-residential gross floor area (gfa), excluding parking structures as approved by said building permit, to be used for fire and rescue purposes.
24. In the event that side yard setbacks of single family detached homes are less than ten feet (10'), each dwelling unit shall have a fire sprinkler system to comply with Section 32-306.12.3(a) of the Prince William County Zoning Ordinance. Compliance shall be demonstrated on each lot grading permit application and building plan application.
25. The Applicant shall work with the County during the design and construction of the buildings and parking structures located on Land Bay 5 to provide, if required, adequate communication coverage within the said structures (e.g., "leaky wire" radio system or equivalent) in the event of an emergency. Compliance with this proffer shall be satisfied with certification/documentation submitted with the building plans.

OWNERS ASSOCIATION

26. A separate Owners Association(s) or another identifiable single entity (i.e., owner) may be formed prior to approval of the first site plan for any nonresidential building in Land Bays 4 and/or 5 to oversee the on-going management and maintenance of the common areas of Land Bays 4 and/or 5.
27. Prior to the issuance of the first occupancy permit, the Applicant shall create a homeowners' association ("HOA") for Land Bays 1, 2 and 3 which shall be responsible for the maintenance of any private streets, common space, buffers, entrance features, recreational amenities, walking path and associated monument signage.
28. The Applicant shall incorporate into the HOA documents a provision which shall require plowing of snow on emergency vehicle access areas, in addition to all private streets and alleys, and on all public streets prior to acceptance into the state maintenance system, all as identified on the MZP.

29. The Applicant shall incorporate into the HOA documents and budget, reserve funding budget items supported by a reserve study for the long term viability of all HOA maintained infrastructure. The HOA documents and budget shall be provided to each purchaser in the project.
30. The Applicant/Owner shall provide a certified nutrient management plan upon final, permanent stabilization of the site, but no later than final inspection of the last developed residential Land Bay 1, 2 or 3 and prior to release of the final performance bond for the last developed residential Land Bay 1, 2 or 3. The plan will cover all commonly held turf areas to be maintained by the Homeowner or Property Owner Association (HOA). It will not apply to private lots. The plan will be prepared by a Virginia Certified Nutrient Management Planner per the State requirements for urban nutrient management plans, and subsequently updated as required by the State. A copy of the plan shall also be provided to the Prince William County Watershed Management Branch. The HOA will be responsible for subsequent submissions of updated plans. An annual report documenting implementation of the plan will be provided to Watershed Management by the Applicant and then the HOA.

HOUSING

31. As a condition of each residential occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$250.00 for each unit. Said contribution is to be utilized by the County's Housing Preservation & Development Fund.

LIBRARIES

32. As a condition of each residential occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$601.00 for a townhouse unit and \$610.00 for a single family detached unit to be used for library purposes.

PARKS AND RECREATION

33. As a condition of first site/subdivision plan approval, the Applicant shall meet with the Director of Planning and the Director of Parks and Recreation (or their designees) (collectively the County Representatives) to determine whether the Applicant or the Parks & Recreation Department will construct a field at Ellis Barron Park generally in the area shown on Attachment O attached hereto. The construction shall consist of: one artificial turf field with final dimensions not to exceed 360' x 225', design and permitting

for the field and associated parking, grading/earthwork, appropriate storm water improvements for the field, and an expanded parking area, (collectively "the Turf Field") subject to County approval. The Applicant shall either:

- A. As a condition of each occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$3,725.00 for a townhouse unit and \$3,972.00 for a single family detached unit to be used for the design and construction of an artificial turf field to be located on Ellis Barron Park with any residual funds to be used for parks and recreation purposes.
 - B. Construct the artificial turf field subject to obtaining all necessary easements and permits. The Applicant shall commence construction and diligently pursue said construction prior to or as a condition of the issuance of the 351st residential building permit. The Applicant shall provide evidence of the design and construction costs for the Turf Field to the Department of Parks and Recreation. The evidence of costs shall include but not be limited to all costs for, design, permitting, hard and soft costs, (including bonding) for the Turf Field (the "Applicant's Costs"). In the event the Applicant's Costs to design and construct the Turf Field do not equal the total level of service for parks and recreation, the Applicant shall contribute the monetary balance upon completion of the Turf Field. The Applicant's Costs shall not exceed the monetary level of service payment identified in Proffer # 33.A above.
34. The Applicant shall provide community recreation amenities for the residential units on the Property. Amenities shall include, a community club house with bathroom/changing rooms and with a six-lane, 25 meter pool, walking trails and tot lots (which shall contain a bench, shade tree/or structure, trash receptacle and play structures), located as generally shown on Sheets 4 and 5 of the MZP.
35. As a condition of final site plan approval for Land Bay 2, the Applicant shall incorporate a tot lot within Land Bay 2 in an area of the land bay south of Coverstone Drive. The final location of the tot lot shall be determined at final site plan.

PHASING

36. The Applicant may develop the Property in phases but not necessarily in the order of the Land Bay designations.
37. The Applicant shall submit a site plan for the first building in Land Bay 5 prior to building permit issuance of the 300th residential unit.

38. As a condition of issuance of the 401st occupancy permit, the Applicant shall commence construction on the first office building and associated surface parking located in Land Bay 5. Said building shall be a minimum of three (3) stories with a minimum of 100,000 gross square feet and shall be located along the Balls Ford Road frontage.
39. Development of Land Bay 5 may be phased such that any building may be built first and the Applicant reserves the right to utilize surface parking in Land Bay 5 until such time as structured parking is required in order to meet the parking standards set forth in the Design Construction and Standards Manual (DCSM) and/or the Prince William County Zoning Ordinance.
40. At the time the Balls Ford Road improvements east of Ashton Avenue are constructed, (as further described below), the Applicant shall install water and sewer stubs, to Land Bay 5.
41. At the time the Balls Ford Road improvements east of Ashton Avenue are constructed, (as further described below), the Applicant shall design and install the signal heads to accommodate the full intersection, including signal heads for Land Bay 5, subject to County and VDOT approval.
42. As a condition of issuance of the 100th residential occupancy permit, the Applicant shall commence construction of 25,000 square feet of commercial space in Land Bay 4.
43. Construction of the pool and clubhouse amenities located in Land Bay 1 shall be commenced after occupancy permit of the 200th residential unit and shall be substantially completed by the issuance of the building permit for the 275th residential unit.
44. Construction of amenities located in Land Bays 2 and 3 shall be constructed at the time the Land Bay is being developed.

SCHOOLS

45. As a condition of each residential occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$11,685.00 per townhouse unit and \$20,694.00 per single family detached unit, to be used for school purposes.

TRANSPORTATION

46. As a condition of each residential occupancy permit issuance, the Applicant shall make a one time monetary contribution to the Prince William Board of County Supervisors in the amount of \$15,196.00 per townhouse unit and \$17,962.00 per single family detached unit to be used for transportation improvements. The contributions shall be applied to capital improvement projects that are identified in the CIP, 6-year road plan or any other capital improvement projects in the County. Said contributions to be reduced by credits for transportation improvements made by Applicant in Proffers 48.a., 48.b. and 48.c.. However, the total amount of all such credits cannot exceed the total value of the transportation level of service monetary contributions proffered in this condition.
47. Access to the Land Bays shall be provided as shown on the MZP, subject to final engineering.
48. Balls Ford Road Improvements. Prior to approval of the first site/subdivision plan for any Land Bay the Applicant shall submit a Public Improvement Plan to Prince William County Transportation and VDOT for the Balls Ford Road improvements shown on Sheets 8 and 9 of the MZP. Construction and bonding shall be completed in phases in accordance with the approved Public Improvement Plan as follows:
 - A. Balls Ford Road improvements east of Ashton Avenue. The Applicant shall construct Balls Ford Road improvements east of Ashton Avenue as generally shown on Sheet 9 of the MZP and as further engineered in the approved Public Improvement Plan described in Proffer 48 above prior to any occupancy permits being issued in Land Bays 1, 3, 4 or 5. This phase of improvements shall include Balls Ford Road/Ashton Avenue intersection improvements including signalization, the entrance to Land Bay 5 and construction of a modified four-lane divided MA-1 road section running from the east side of Ashton Avenue eastward along Balls Ford Road, to the existing entrance of GPIN 7697-15-1568 (10850 Balls Ford Road). The Applicant shall receive credit against the transportation level of service contribution identified in Proffer 46 for all costs of any off-site improvements, including but not limited to appraisal fees, right-of-way acquisition costs, easements, design and construction costs. Said offsite improvements are shown on Attachment D attached hereto entitled "Offsite Road Improvement Exhibit – Balls Ford Road Blackburn," revised May 23, 2014, prepared by Urban, Ltd ("Attachment D"). The Applicant shall provide evidence of these amounts to the County Department of Transportation for review and approval.
 - B. Balls Ford Road improvements west of Ashton Avenue. The Applicant shall begin the construction of the Balls Ford Road improvements west of Ashton

Avenue, as generally shown on Sheet 8 of the MZP and as further engineered in the approved Public Improvement Plan described in Proffer 48 above prior to any site/subdivision plan approval for Land Bay 5 or prior to building permit issuance of the 300th residential unit, whichever comes first. This phase of improvements shall include, if not already constructed pursuant to Proffer 48.a. above, Balls Ford Road/Ashton Avenue intersection improvements and construction of a modified four-lane divided MA-1 road section running from the west side of Ashton Avenue westward along Balls Ford Road, to the western property line of GPIN 7597-73-9556 (7501 Bethlehem Rd). The Applicant shall receive credit against the transportation level of service contribution identified in Proffer 46 for all costs of any off-site improvements, including but not limited to appraisal fees, right-of-way acquisition costs, easements, design and construction costs. Said offsite improvements are shown on Attachment D attached hereto. The Applicant shall provide evidence of these amounts to the County Department of Transportation for review and approval.

- C. Class I Trail on Balls Ford Road. As part of the Balls Ford Road Public Improvement Plan identified in Proffer 48 above, the Applicant shall design a ten (10) foot wide Class I asphalt trail on the south side of Balls Ford Road approximately from station 14+37 at the western property of GPIN 7597-73-9556 (7501 Bethlehem Rd) to station 50+50 at the eastern boundary of the Property, as generally shown on Sheets 8 and 9 of the MZP (unless the County/VDOT approves a waiver request for a narrower width). Said trail shall be located within Balls Ford Road right-of-way. The Applicant shall receive a credit against the transportation level of service contribution identified in Proffer 46 for the value of the trail located off the Property frontage, with said credit to include appraisal fees, right-of-way acquisition costs, easements, design and construction costs of said trail. For purposes of this proffer, the "off-site" portion includes trail segments located on GPINs 7597-73-9556, 7597-83-1724, 7597-94-9277 and 7597-94-8644 (respectively, 7501 Bethlehem Road, 7505 Bethlehem Road, 11171 Balls Ford Road and 11201 Balls Ford Road). The Applicant shall provide evidence of these amounts to the County's Department of Transportation for review and approval to evidence the value of such credit. The trail shall be constructed with the Ball Ford Road improvements noted above.
- D. Transportation Credits. Transportation proffer credits identified in Proffers 48.a., 48.b., and 48.c. shall be allowed based on estimates of the total costs of off-site improvements in that phase, as shown on the approved Public Improvement Plan and as such estimates are approved by Prince William County Department of Transportation ("PWCDOT"). Once the road construction is completed, the Applicant shall provide receipts or invoices for all costs which shall be reconciled

against the estimates and any further credits due/allowed or, if the actual costs are less than the estimates, the difference shall be paid to Prince William County.

49. Coverstone Road and Bethlehem Road Improvements. As a condition of site/subdivision plan approval for Land Bay 2, the Applicant shall design the Coverstone Road and Bethlehem Road improvements as generally shown on Sheets 11 and 12 of the MZP. The improvements will be constructed with construction of Land Bay 2.
50. Ashton Avenue Improvements. As a condition of site/subdivision plan approval for Land Bays 1 or 3, Applicant shall design Ashton Avenue improvements as generally shown on Sheet 10 of the MZP. The improvements shall include a five foot (5') wide side walk on the east side of Ashton Avenue running from Balls Ford Road to the southern property line of Land Bay 3, tying into the existing sidewalk.
51. Hollis Drive Extended. As a condition of site/subdivision plan approval for Land Bay 1, the Applicant shall design the extension of Hollis Drive to the cul de sac as generally shown on Sheet 13 of the MZP. Hollis Drive will be constructed with construction of Land Bay 1.
52. In the event the Applicant is not able to acquire off-site right-of-way or easements required in order to provide the Balls Ford Road improvements, in accordance with Proffers 48.a., 48.b. and 48.c, including any easements required for utilities, utility relocation and stormwater management for the road improvements, the Applicant shall request the County to acquire the right of way and/or easements by means of its condemnation powers at Applicant's expense. Applicant's request shall be in writing and shall comply in all respects with the County's Eminent Domain Policy. Said request shall be made to the appropriate County agency and shall be accompanied by the following:
 - A. The names of the record owners, the property addresses, tax map parcel numbers and GPIN numbers for each landowner from whom such right of way and/or easements are sought.
 - B. Plats, plans and profiles showing the necessary right of way and/or easements to be acquired and showing the details of the proposed transportation improvements to be located on each such property.
 - C. Pursuant to Virginia Code § 25.1-417, a determination of the value of the Property will be based on the following:
 - i) if the assessed value is less than \$25,000, then the value shall be determined by assessment records or other objective evidence; or

- ii) if the assessed value is greater than \$25,000 an independent appraisal of the value of the right of way and easements to be acquired, and any and all damages to the residue of the involved property, said appraisal to be performed by an appraiser licensed in Virginia and approved by the County.
 - D. A 60 year title search of each involved property.
 - E. Documentation demonstrating to the County's satisfaction Applicant's good faith, best efforts to acquire the right of way and/or easements, at a cost of at least the appraised value of the involved property interests.
 - F. A letter of credit acceptable to the County, cash or equivalent (from a financial institution acceptable to the County) in an amount equal to the appraised value of the property to be acquired, and all damages to the residue, together with an amount representing the County's estimate of its cost of condemnation proceedings, in a form permitting the County to draw upon the same as necessary to effectuate the purposes hereof.
 - G. An Agreement signed by Applicant's representative and approved by the County Attorney whereby Applicant agrees to pay all costs of the condemnation, including expert witness fees, court costs, exhibit costs, court reporter fees, attorney fees for the Office of the County Attorney, and all other costs associated with the litigation, including appeals. The Agreement shall specifically provide that in the event the property owner is awarded in the condemnation suit more than the appraised value estimated by Applicant's appraiser, Applicant shall pay to the County the amount of the award in excess of the amount represented by the letter of credit or cash deposit within 15 days of the award.
53. A bus shelter shall be provided in Land Bay 5 on the north side of Balls Ford Road generally as shown on Sheet 5 of the MZP, subject to County, VDOT and PRTC approval. The shelter will be shown on the first site plan for Land Bay 5.
54. The Applicant shall provide interparcel access within Land Bay 5 in the general locations as shown on the MZP and said interparcel access shall be constructed as respective phases of Land Bay 5 are developed.
55. The Applicant shall reserve land for one access easement in Land Bay 4 to provide an interparcel connection in the location generally shown on Sheets 5 of the MZP. The exact location of said access easement shall be determined at the time of site plan approval for that portion of the Land Bay. The access easement reservation shall be shown on the subdivision plan and said easement so reserved shall be granted in the event

the adjacent property is developed and a reciprocal access easement is granted on the adjacent parcel.

56. At the time of first site plan submission for Land Bay 5, the Applicant shall study the Balls Ford Road/ Ashton Avenue signal timing and, if warranted, shall suggest timing modifications subject to County and VDOT approval.
57. At the time of first site/subdivision plan for the Property, the Applicant shall submit to the County and VDOT a study of the Balls Ford Road/ Sudley Manor Drive (Route 234) intersection to determine if restriping within the existing right of way for a third eastbound left turn lane on Balls Ford Road, onto northbound Sudley Manor Drive is feasible. Studied improvements will be limited to restriping, pavement markings (including lane identification such as I-66 only lanes, with arrows) and ground mounted directional signage, if required. The study will be limited to improvements that may be made within the existing right of way and shall be subject to VDOT and County Approval.
58. Prior to occupancy permit for the 250,000 square foot of office space located in Land Bay 5, the Applicant shall establish a Travel Demand Management ("TDM") program in consultation with Prince William County Department of Transportation to determine appropriate TDM measures.

WATER AND SEWER

59. The Property shall be served by public sanitary sewer and water, and the Applicant shall be responsible for those improvements required in order to provide such service for the demand generated by the development of the Property.

MISCELLANEOUS

60. As a condition of issuance of the 100th residential occupancy permit, the Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$50,000.00 to be used by the Prince William County Department of Economic Development for the promotion of business and corporate development along I-66 corridor.
61. The Applicant shall promptly remove any graffiti from the Property. Graffiti shall be deemed any inscription or marking on walls, buildings or structures not permitted by the sign regulations in Section 32-25.20 et al. seq. of the Zoning Ordinance. Any graffiti is to be reported to the Prince William County Police Department before removal.

62. As a condition of site/subdivision plan approval for Land Bay 1, the Applicant shall construct/install a six (6') foot high ornamental metal fence along the Land Bay 1 boundary line as generally shown on the MZP and Landscape Plan but, shall not be required in areas along this boundary where fencing is already in place. Fencing installed by the Applicant shall be maintained by the HOA.
63. As a condition of first site/subdivision plan approval, the Applicant shall install/erect a sign, not to exceed 10 feet in height in Land Bay 5 near the Balls Ford Road frontage. Said sign shall identify this portion of the Property as the future site of Blackburn Business Park. The Applicant shall maintain the sign until such time the site is developed or it is no longer deemed necessary by the Gainesville District Supervisor or the Planning Director.
64. In the event the monetary contributions set forth in the Proffer Statement are paid to the Prince William County Board of County Supervisors within 18 months of the approval of this rezoning, as applied for by the Applicant, said contributions shall be in the amounts as stated herein. Any monetary contributions set forth in the Proffer Statement which are paid to the Board after 18 months following the approval of this rezoning shall be adjusted in accordance with the Urban Consumer Price Index ("CPI-U") published by the United States Department of Labor, such that at the time contributions are paid, they shall be adjusted by the percentage change in the CPI-U from that date 18 months after the approval of this rezoning to the most recently available CPI-U to the date the contributions are paid, subject to a cap of 6% per year, non-compounded.
65. For purposes of this Proffer Statement, "final rezoning" shall be defined as that zoning which is in effect on the day following the last day upon which the Prince William Board of County Supervisors' decision granting the rezoning may be contested in the appropriate court or, if contested, the day following entry of a final court order affirming the decision of the Board of Supervisors which has not been appealed, or if appealed, the day following which the decision has been affirmed on appeal.

PMR and PMD MODIFICATIONS

66. The following waivers and modifications to the requirements of the Zoning Ordinance and DCSM shall be deemed granted and approved as provided by Section 32-700.25 of the Zoning Ordinance.
 - A. Modification of Section 32-306.10.1.(b) and Section 32-306.12.6.C and F of the Zoning Ordinance to allow a minimum of one modified unit type and to allow a modified housing type with yards and setbacks as shown on the MZP and further identified on Attachment E attached hereto, subject to compliance with Section 32-306.12.3 of the Zoning Ordinance.

- B. Waiver of Section 306.21 of the Zoning Ordinance to allow the elimination of the nonresidential secondary use requirement within the PMR District.
- C. Modification of Section 32-405.04 of the Zoning Ordinance to allow perimeter buffer around the PMD zoned property to be in accordance with those buffers shown on the Landscape Plan.
- D. Modification of Section 32-405.04.1 to allow PMD area to be based on contiguous and non-contiguous portions of the Property.
- E. Modification of Section 32-401.14.3 and 4, and Section 32-402.24.3 and 4 of the Zoning Ordinance to allow for the calculation of density (FAR) and open space in Land Bays 4 and 5 to be based on the entire individual Land Bay rather than on a lot by lot basis if/when the Land Bays are subdivided.
- F. Modification of Section 32-250.24 of the Zoning Ordinance to allow signs in conformance with Attachment A and the Design Guidelines identified herein.
- G. Modification of Detail 650.05 RM-2, of the DCSM to allow 11' lane with curb, gutter and sidewalk and right-of-way dedication on Bethlehem Road as shown on Sheets 7 and 11 of the MZP and pursuant to approved waiver #WAI 2015-00013 attached hereto as Attachment F.
- H. Modification of Section 602.07.A of the DCSM to allow a commercial entrance on Ashton Avenue as shown on Sheet 10 of the MZP and pursuant to approved waiver #WAI 2015-00014 attached hereto as Attachment G.
- I. Modification of Section 601.04.C of the DCSM to allow single family detached lots to have frontage on private streets pursuant to approved waiver #WAI 2015-00015 attached hereto as Attachment H.
- J. Modification of Detail 650.11 MA-1, of the DCSM to allow modifications to Balls Ford Road in accordance with improvements shown on Sheets 7, 8 and 9 of the MZP pursuant to approved waiver #WAI 2015-00016 attached hereto as Attachment I.
- K. Modification of Detail 650.06 TS-1, of the DCSM to allow modifications to road width in the townhouse residential sections pursuant to approved waiver #WAI 2015-00017 attached hereto as Attachment J.
- L. Modification of Detail 650.08 CI-1, of the DCSM to allow modification of Hollis Drive extended as shown on Sheets 7 and 13 of the MZP pursuant to approved waiver #WAI 2015-00018 attached hereto as Attachment K.

- M. Modification of Detail 650.08 CI-1, of the DCSM to allow modification of Coverstone Drive as shown on Sheets 7 and 12 of the MZP, pursuant to approved waiver #WAI 2015-00019 attached hereto as Attachment L.
- N. Modification of Detail 650.09 MC-1, of the DCSM to allow for the Ashton Avenue ½ section (east side), as shown on Sheets 7 and 10 of the MZP, pursuant to approved waiver #WAI 2015-00020 attached hereto as Attachment M.
- O. Modification of Detail 650.04 RL-2, of the DCSM to allow modifications to street standards and to allow the sidewalk in the single family residential section to be constructed outside the public street right-of-way pursuant to approved waiver #WAI 2015-00021 attached hereto as Attachment N.

SIGNATURE PAGE

ESTATE OF MARION C. WHEELER

By: _____

Name: Claude T. Compton

Title: Executor

Date: _____

ATTACHMENTS

Attachment A	Blackburn Entry Features, revised August 11, 2014
Attachment B	Blackburn Office Park Design Concepts (sheets 1-4), dated Dec. 17, 2013
Attachment C	Proposed Scope of Work for Conducting Military Sites (Intensive Metal Detector) Survey of the Wheeler (A.K.A Blackburn) Property, April 22, 2013
Attachment D	Offsite Road Improvement Exhibit – Balls Ford Road Blackburn, revised May 23, 2014
Attachment E	Housing unit modification graphics, revised August, 2014
Attachment F	Approved Waiver #WAI 2015-00013, DCSM Detail 650.05. RM-2
Attachment G	Approved Waiver #WAI 2015-00014, DCSM Section 602.07.A
Attachment H	Approved Waiver #WAI 2015-00015, DCSM Section 601.04.C
Attachment I	Approved Waiver #WAI 2015-00016, DCSM Detail 650.11 MA-1
Attachment J	Approved Waiver #WAI 2015-00017, DCSM Detail 650.06 TS-1
Attachment K	Approved Waiver #WAI 2015-00018, DCSM Detail 650.08 CI-1 (Hollis Drive)
Attachment L	Approved Waiver #WAI 2015-00019, DCSM Detail 650.08 CI-1 (Coverstone Dr)
Attachment M	Approved Waiver #WAI 2015-00020, DCSM Detail 650.09 MC-1
Attachment N	Approved Waiver #WAI 2015-00021, DCSM Detail 650.04 RL-2
Attachment O	Soccer Field Exhibit, February 2, 2015



Melissa S. Peacor
County Executive

COUNTY OF PRINCE WILLIAM

OFFICE OF EXECUTIVE MANAGEMENT

1 County Complex Court, Prince William, Virginia 22192-9201
(703) 792-6600 Metro 631-1703 FAX: (703) 792-7484

BOARD OF COUNTY SUPERVISORS

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Maureen S. Caddigan, Vice Chairman
Pete Candland
John D. Jenkins
Jeanine M. Lawson
Michael C. May
Martin E. Nohe
Frank J. Principi

October 30, 2015

TO: Board of County Supervisors

FROM: Christopher M. Price, AICP *CMP*
Director of Planning

THRU: Melissa S. Peacor
County Executive

RE: Rezoning #PLN2014-00040, Blackburn
Gainesville Magisterial District

A public hearing for the rezoning was opened and public testimony was heard at the May 12, 2015 Board of County Supervisors meeting. The Board closed the public hearing and deferred action of the case to a date uncertain in order to allow additional time for the applicant to consider increasing the phasing of nonresidential commitments and improving the balance of the mix of uses for project.

Since the Board hearing in May, the applicant has not made any changes to the proposal. As such, the attached staff report, dated April 24, 2015, and staff's recommendation remain unchanged.

Staff continues to recommend denial.



Melissa S. Peacor
County Executive

COUNTY OF PRINCE WILLIAM

OFFICE OF EXECUTIVE MANAGEMENT

1 County Complex Court, Prince William, Virginia 22192-9201
(703) 792-6600 METRO 631-1703 FAX: (703) 792-7484

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April 24, 2015

TO: Board of County Supervisors

FROM: Christopher M. Price, AICP *CH*
Director of Planning

THRU: Melissa S. Peacor
County Executive

RE: Rezoning #PLN2014-00040, Blackburn
Gainesville Magisterial District

I. Background is as follows:

- A. Request - This is a request to rezone +/- 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and +/-51.7 acres from A-1 to PMD, Planned Mixed Use District, to allow for the development of a maximum of 415 residential units (160 single family detached & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development.

Blackburn Total Project – 128.4 acres	Acres – Total Project area designated REC - 105.5	Gross Floor Area (SF) – Total 1,673,500**
Proposed Development in Regional Employment Center Nonresidential vs. residential comparison using both acreage and floor area Comprehensive Plan recommends that residential development not exceed 25% of the gross floor area	Nonresidential +/-41.1 ac. or 38% of project	Nonresidential Total = 1,115,000 SF or 67% of project
	Residential +/-64.4 ac. or 62% of project	Residential Total - 415 units or 558,500* SF or 33% of project

* Based on estimated single family detached dwelling unit floor area of 2,500 SF and townhouse floor area of 2,000 SF.

**Figure includes the overall nonresidential square footage proposed, but has no phasing proffered for timing of construction.

At the issuance of an occupancy permit for the 401st residential unit, the applicant shall commence construction of the first office building in Landbay 5, having a minimum of 100,000 SF.

Uses / Development Features	Existing / Permitted Development	Proposed Development (requires rezoning)
Zoning	A-1, Agricultural	PMR & PMD
Density	1 SFD per 10 acres	4.8 dwelling units per acre (residential landbays 86.2 acres)
Use	Agricultural / Residential	Residential, Commercial/retail & office/employment
Residential	Up to 12 SFD (10 acre lots)	Up to 415 units (160 SFD & 255 townhouses)
Nonresidential	N/A – use not permitted (zoned A-1)	Up to 1,115,000 SF Commercial/retail & office/employment

- B. Site Location - The site is located on the north and south sides of Balls Ford Rd. approximately ½ mile west of the intersection of Balls Ford Rd. and Sudley Rd. (see maps in Attachment A). The site is identified on County maps as GPINs 7597-92-1061, 7597-82-7607 & 7597-82-4068.
- C. Comprehensive Plan - The site is designated REC, Regional Employment Center, and SRH, Suburban Residential High, in the Comprehensive Plan.
- D. Zoning –The site is zoned A-1, Agricultural, without proffers.
- E. Surrounding Land Uses - The property is bound by Interstate 66 to the north. To the south and east, townhouses and multifamily development make up the majority of the uses at a medium and high density. On the north side of Balls Ford Rd., the uses are nonresidential including office and lodging. On the east side of the property, the uses include motor vehicle sales/rental unlimited, financial institution, warehousing, and a shopping center. To the west of the northern portion of the project is area planned REC, consisting of vacant land and office and lodging uses. Further to the west, the land uses are industrial including light and heavy industrial uses. To the west of the southern portion of the project, where the residential development is proposed, the uses consist of light and heavy industrial uses. The majority of the area to the west of the site is designated FEC, Flexible Use Employment Center and EI, Industrial Employment, with a small area of REC. Immediately to the west of the site, there is a +/-12 acre area that is designated as REC. This +/-12 acre area is located between light and heavy industrial uses to the west and the proposed suburban residential low density development within Landbay 1.

II. Current Situation is as follows:

- A. Planning Commission Recommendation – The Planning Commission heard this rezoning at the April 1, 2015 public hearing and continued the hearing to April 15, 2015. The case was continued in order to allow time to work with the applicant to explore including a proffered trigger that ensures commercial and employment development for the site. The Planning Commission recommends approval of REZ #PLN2014-00040, Blackburn, subject to the proffers dated March 18, 2015, with amended proffers that include;

- A proffer for the phasing of construction for the first office building within landbay 5.
- Revision to proffer #24 to ensure that single family residential units with side yard setbacks of less than 10 feet shall have a fire sprinkler system.

See the attached proffers with revision date of April 20, 2015.

- B. Planning Office Recommendation: The Planning Office continues to recommend denial for the following reasons:

- The applicant's revised proffers include construction phasing for the timing of the proposed office/employment development; however, the trigger for construction is very late in the build out of the residential development. The total residential unit cap for the project is 415 units, and the phasing proffer requires construction to commence at the 401st building permit. The majority of the project area is designated REC, and the County's land use policy expressly states the need for nonresidential phasing in order to ensure that employment development always remains the primary use within this planned district.
- The purpose of the Regional Employment Center classification is to provide for areas located close to and/or with good access from an interstate highway where intensive regional employment uses are to be located.
 - a) This property is situated with high visibility from, and excellent access to, Interstate 66. Due to the limited availability of employment planned areas along I-66, the +/-64.4 acres proposed for residential development should instead continue to provide opportunities for future intensive regional employment investment.
 - b) This project, without earlier project construction phasing of the employment component, is primarily a residential development project with a minimum 25,000 SF of retail development (to be constructed at the 300th residential occupancy permit). The project is not consistent with the development guidance provided in the Comprehensive Plan for the REC.
- The applicant's residential proposal consists of single family detached units and townhouses. Neither unit type is envisioned in the REC designation. In addition,

per the Comprehensive Plan, residential uses shall represent no greater than 25 percent of the total REC gross floor area of the project. The table on page 1 shows that the proposed residential floor area in the area planned REC is estimated to be 558,500 SF and the residential area is 33% of the project floor area and 62% of the project/land area. Also, within each landbay, uses are not well integrated, each containing a single use, which does not provide walkability.

- The proposal contains no active recreation open space area within the project area. The open space shown along Balls Ford Rd. is a stormwater management dry pond. Other areas shown to contain open space are wetlands. It was strongly recommended that the applicant provide usable open space on-site, particularly in the townhouse section of landbay 2 where no amenities have been provided.
- The Comprehensive Plan Future Transit Alternatives Map identifies a future Metro station location at Sudley Rd. and I-66. In addition, VDOT is actively seeking to locate a major park and ride facility in this location. This further highlights the planned intent for employment development, density and a mix uses in this area.

See the revised proffers dated April 20, 2015, and Attachment B for the staff analysis.

- C. Board of County Supervisors Public Hearing - A public hearing before the Board of County Supervisors has been advertised for May 12, 2015.

III. Issues in order of importance are as follows:

A. Comprehensive Plan

- Long-Range Land Use - Is the proposed use consistent with those uses intended by the REC & SRH designations?
- Level of Service (LOS) - How does the proposal address the Policy Guide for Monetary Contributions?

B. Community Input - Have members of the community raised any issues?

C. Other Jurisdictional Comments - Have other jurisdictions raised any issues?

D. Legal Uses of the Property - What uses are allowed on the property? How are legal issues resulting from the Board of County Supervisors action addressed?

E. Timing - When must the Board of County Supervisors take action on this application?

IV. Alternatives beginning with the staff recommendation are as follows:

A. Deny Rezoning #PLN2014-00040, Blackburn.

- Comprehensive Plan

- a) Long-Range Land Use - If the application is denied, the land use designation of the site would remain REC and SRH, and the zoning classification would remain A-1. The REC is appropriately located with visibility and access to I-66 to allow for the development of intensive regional employment uses. Residential development in the REC shall be considered secondary uses; however, the proposed development exceeds the 25% of the total REC gross floor area of the project. The portion of the project that is designated SRH is proposed for the development of 177 townhouses at a density of 7.7 du/ac. The density range in SRH projects is 10-16 du/ac. The proposed single family detached units and townhouses are housing types that are not envisioned in the REC or SRH.
 - b) Level of Service - Denial would not have any impact on the existing level of service.
2. Community Input - The application has been transmitted to adjacent property owners within 200 feet. As of the date of this report, the Planning Office has received seven emails in support that reference road improvements, benefits to local businesses, and benefits to schools as reasons for their support. A total of 25 citizens spoke at the Planning Commission public hearing with all but one speaking in favor of the request. The Planning Office has also received three emails of opposition expressing concerns regarding traffic impacts, use compatibility, open space and environmental concerns.
3. Other Jurisdictional Comments – Due to the proximity to the Manassas National Battlefield Park, the United States National Park Service was sent the proposal for review and comment. The National Park Service responded expressing concern that the rezoning may result in “significant visual impacts upon the park.” A balloon test was conducted on-site and it was determined that structures of 100 feet or more could be seen from multiple places in the park. In response, the applicant capped the building height at 70 feet. In addition, the Park Service raised concerns regarding added traffic congestion to the road network in and around the park. No other jurisdictions were sent the proposal for review.
4. Legal Uses of the Property – The rezoning site could be developed with uses permitted in the A-1 zoning district. Legal issues resulting from Board of County Supervisors action are appropriately addressed by the County Attorney’s office.
5. Timing - The Board of County Supervisors generally has one year from the date of acceptance to take action on a rezoning request. The one year time frame ended on July 30, 2014; however, additional time was needed for continued improvement efforts intended to reduce the project’s inconsistencies with the Comprehensive Plan, in particular, the Long Range Land Use and Environmental chapters.

- B. Approve REZ #PLN2014-00040, Blackburn, subject to the proffers dated April 24, 2015.

1. Comprehensive Plan Consistency Analysis:

- a) Long-Range Land Use - The site is classified REC. In addition to the 415 residential units that are proposed, the request would allow for the potential development of up to 1,000,000 square feet of office/employment development and 115,000 square feet of commercial/retail development. The portion of the project that is designated SRH, Suburban Residential High is proposed for the development of 177 townhouses at a density of 7.7 du/ac. In addition, the proposed design guidelines and building elevations for the office development outlines the various elements of quality office design consistent with the Comprehensive Plan office development design goals.
- b) Level of Service (LOS) - The level of service impacts related to the request would be mitigated by the proffered monetary contributions as follows:

Category: Monetary Proffers	2006 or 2014 LOS Contribution Proffered	Single family 160 units	Townhouse 255 units	Nonresidential 1,115,000 SF	Total
Fire and Rescue	2006	\$749 per unit = \$119,840	\$720 per unit = \$183,600	\$0.61/sq. ft. = \$680,150	\$983,590
Libraries	2006	\$610 per unit = \$97,600	\$601.00 per unit = \$153,255	N/A	\$250,855
Housing	N/A	\$250 per unit = \$40,000	\$250.00 per unit = \$63,750	N/A	\$103,750
Parks and Recreation	2006	\$3,972 per unit = \$635,520	\$3,725 per unit = \$949,875	N/A	\$1,585,395
Schools	2014 - SFD ONLY 2006 - Townhouses	\$20,694 per unit = \$3,311,040	\$11,685 per unit = \$2,979,675		\$6,290,715
Transportation*	2006	\$17,962 per unit = \$2,873,920	\$15,196 per unit = \$3,874,980	N/A	\$6,748,900
Water Quality	2014	\$75 per acre	128.4 acres	N/A	\$9,630
Total					\$15,972,835.00

*Transportation LOS contribution is proffered to be credited for transportation improvements

2. Community Input - The application has been transmitted to adjacent property owners within 200 feet. The application has been transmitted to adjacent

property owners within 200 feet. As of the date of this report, the Planning Office has received seven emails in support that reference road improvements, benefits to local businesses, and benefits to schools as reasons for their support. A total of 25 citizens spoke at the Planning Commission public hearing with all but one speaking in favor of the request. The Planning Office has also received three emails of opposition expressing concerns regarding traffic impacts, use compatibility, open space and environmental concerns.

3. Other Jurisdictional Comments - Due to the proximity to the Manassas National Battlefield Park, the United States National Park Service was sent the proposal for review and comment. The National Park Service responded expressing concern that the rezoning may result in "significant visual impacts upon the park." A balloon test was conducted on-site and it was determined that structures of 100 feet or more could be seen from multiple places in the park. In response, the applicant capped the building height at 70 feet. In addition, the Park Service raised concerns regarding added traffic congestion to the road network in and around the park. No other jurisdictions were sent the proposal for review.
4. Legal Uses of the Property - The site could be developed with a maximum of 415 residential units (160 single family detached & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development. Legal issues resulting from Board of County Supervisors action are appropriately addressed by the County Attorney's office.
5. Timing - The Board of County Supervisors generally has one year from the date of acceptance to take action on a rezoning request. The one year time frame ended on July 30, 2014; however, additional time was needed for continued improvement efforts intended to reduce the project's inconsistencies with the Comprehensive Plan, in particular, the Long Range Land Use and Environmental chapters.

V. **Recommendation** is that the Board of County Supervisors concurs with Alternative A and approves the attached resolution.

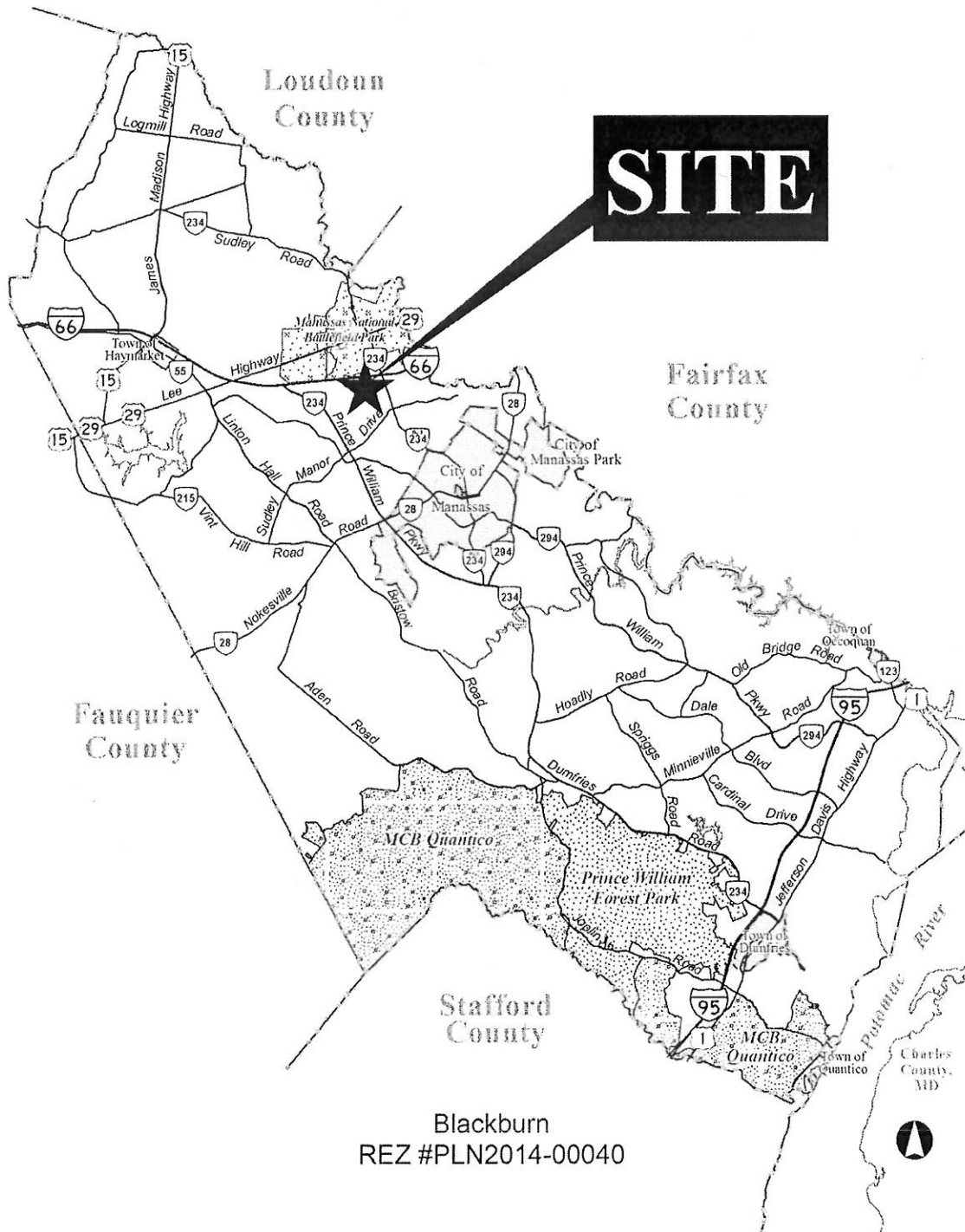
Staff: Stephen L. Donohoe, X5282

Attachments

- A. Area Maps
- B. Staff Analysis
- C. Master Zoning Plan
- D. Landscape Plan
- E. Design Guidelines
- F. Illustrative Plans
- G. Environmental Constraints Analysis
- H. Military Site Scope Work Exhibit
- I. Balls Ford Rd. Off-site Improvements

- J. Housing Unit Modifications
- K. Waivers
- L. Off-site Field Exhibit
- M. Historical Commission Resolution
- N. Planning Commission Resolutions

Attachment A – Maps
VICINITY MAP

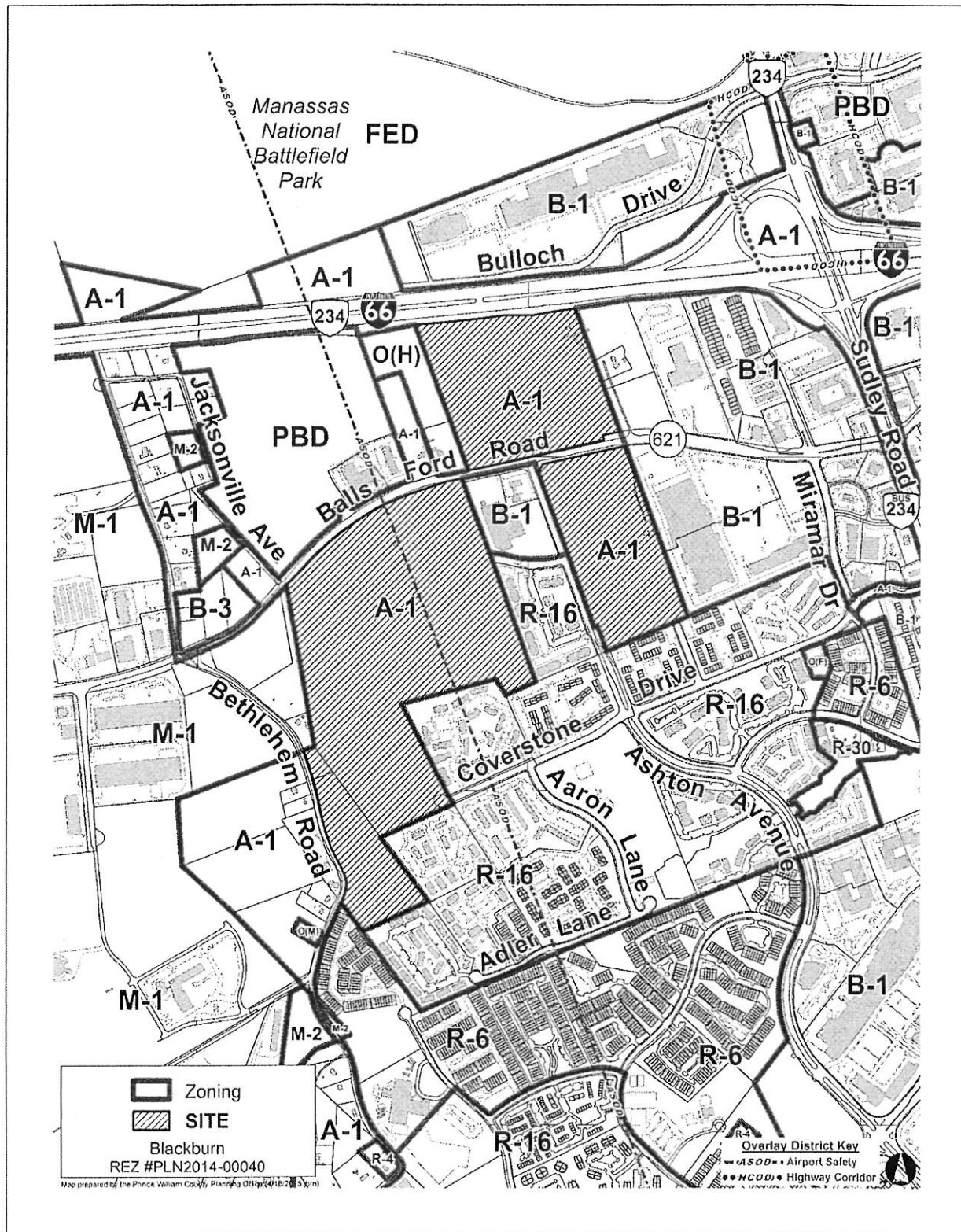


Blackburn
REZ #PLN2014-00040

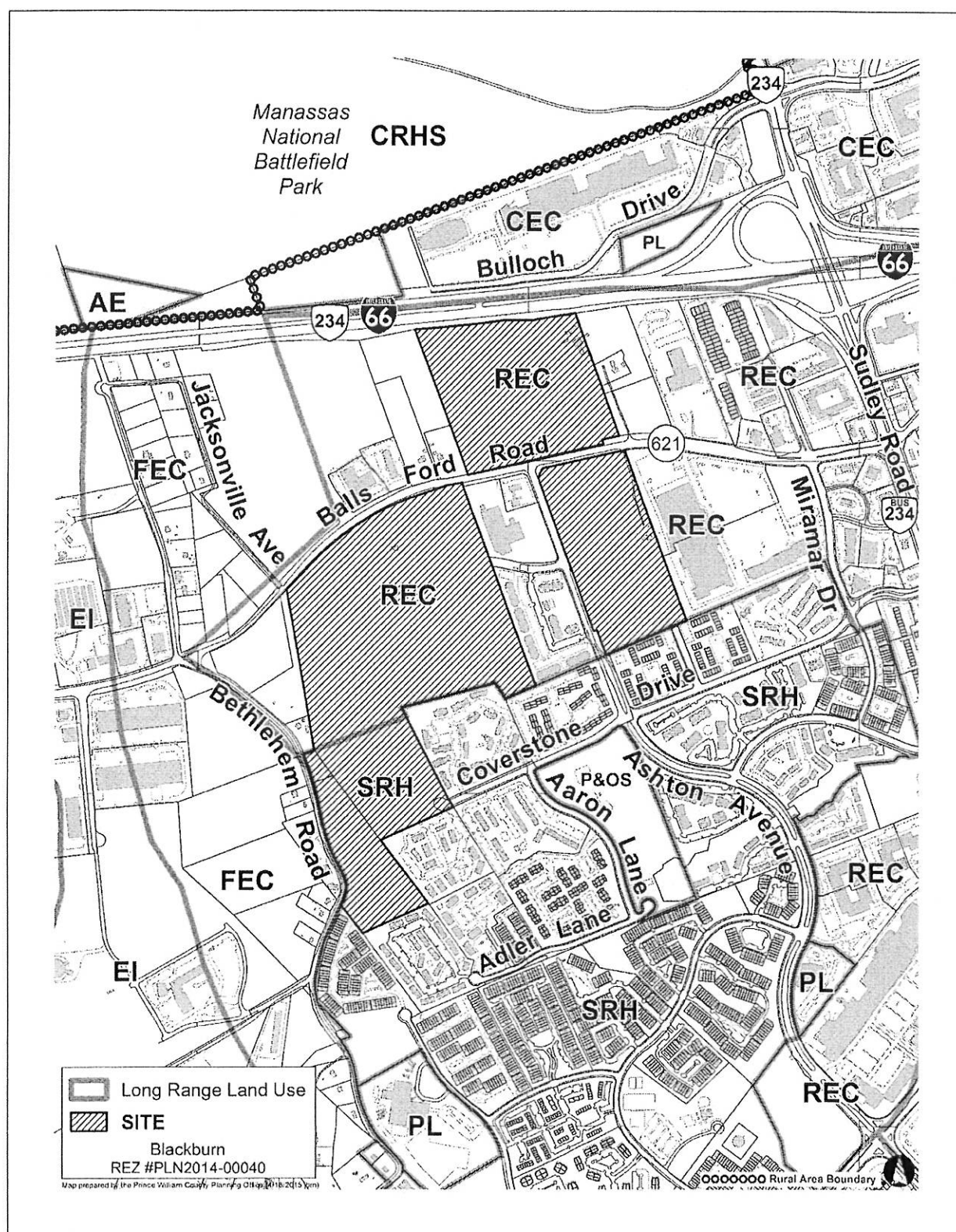
Attachment A – Maps
AERIAL MAP



Attachment A – Maps EXISTING ZONING MAP



Attachment A – Maps
LONG-RANGE LAND USE MAP



Part I. Summary of Comprehensive Plan Consistency
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Staff Recommendation: Denial

This analysis is based on the relevant Comprehensive Plan action strategies, goals, and policies. A complete analysis is provided in Part II of this report.

Comprehensive Plan Sections	Plan Consistency
Long-Range Land Use	No
Community Design	Yes
Cultural Resources	Yes
Environment	No
Fire and Rescue	Yes
Housing	Yes
Libraries	Yes
Parks, Open Space and Trails	Yes
Police	Yes
Potable Water	Yes
Sanitary Sewer	Yes
Schools	Yes
Transportation	Yes

Part II. Comprehensive Plan Consistency Analysis

The following table summarizes the area characteristics (see maps in Attachment A):

Direction	Land Use	Long Range Future Land Use Map Designation	Zoning
North	I-66 & Retail uses, Manassas National Battlefield	REC	B-1
South	Residential; multifamily & townhouse development	SRH & P&OS	R-6, R-30 & R-16
East	Motor vehicle sales/rental unlimited, financial institution, warehousing, shopping center, and other various B-1 uses	REC	B-1
West	Office & lodging	REC, FEC & EI	PBD, O(H) M-1 & A-1

Long-Range Land Use Plan Analysis

Through wise land use planning, the County ensures that landowners are provided a reasonable use of their land while the County is able to judiciously use its resources to provide the services for residents and employers' needs. The Long Range Land Use Plan sets out policies and action strategies that further the County's goal of concentrating population, jobs, and infrastructure within vibrant, walkable, mixed-use centers serviced by transit. In addition to delineating land uses on the Long Range Land Use Map, the plan includes smart growth principles that promote a countywide pattern of land use that encourages fiscally sound development and achieves a high-quality living environment; promotes distinct centers of commerce and centers of community; complements and respects our cultural and natural resources, and preserves historic landscapes and site-specific cultural resources; provides adequate recreational, park, open space and trail amenities that contribute to a high quality of life for county residents; and revitalizes, protects, and preserves existing neighborhoods.

Long-Range Land Use Plan Classification	Land Uses Intended
Regional Employment Center (REC)	The purpose of the Regional Employment Center classification is to provide for areas located close to and/or with good access from an interstate highway where intensive regional employment uses are to be located. REC projects* should be planned and developed in a comprehensive, coordinated manner. Primary uses in the REC are mid-rise and/or high-rise office (including government offices particularly those for Prince William County agencies), research and development facilities, lodging, and mixed-use projects. Residential uses shall represent no greater than 25 percent of the total REC gross floor area of the project. Drive-in/drive-through uses are discouraged. Shared/structured parking is encouraged. The acceptable housing type within any mixed-use REC project is multifamily, at a density of 16-30 dwelling units per gross acre, less the ER, Environmental Resource designated portion of a property. Development in REC projects shall occur according to an infrastructure implementation plan submitted at the time of rezoning. The intent of this plan is to ensure that critical infrastructure for office, employment and lodging uses is developed adequately for each phase of the project. Development shall also occur according to a phasing plan that must ensure that office, employment and lodging uses are always the primary uses within the area rezoned. Office development in REC areas is encouraged to be in accordance with the <i>Illustrative Guidelines for Office Development</i> , provided as a supplement to the Community Design Plan chapter of the Comprehensive Plan and available

* In all instances, a "project" or "project area" is defined as the boundary of a rezoning or special use permit request.

Attachment B - Staff Analysis

	from the Planning Office. A minimum office building height of 4-6 stories is preferred.
Suburban Residential High (SRH)	The purpose of the Suburban Residential High classification is to provide for areas of a variety of housing opportunities at the highest suburban density. The preferred housing type in this classification is multifamily (apartments and condominiums). The density range in SRH projects is 10-16 dwellings per acre, less the ER-, Environmental Resource-designated portion of a property.

This is a request to rezone approximately 128.4 acres from A-1, Agricultural to PMD, Planned Mixed Use District, and PMR, Planned Mixed Residential.

PMR Zoning (Landbays 1 & 2) – Approximately 76.68 acres shall be developed with no more than 337 residential units.

- Landbay 1 (52.76 acres) – To be designated PMR-LDR, Low Density Residential, for the development of 160 single-family detached dwelling units.
- Landbay 2 (22.92 acres) – To be designated PMR-HDR, High Density Residential, for the development of 177 townhouse units.

PMD Zoning (Landbays 3, 4,& 5) – Approximately 51.67 acres shall be developed with no more than 78 townhouse units, one million gross square feet of office & 115,000 square feet of uses.

- Landbay 3 (10.59 acres) – To be designated PMD-HDR, High Density Residential, for the development of 78 townhouses.
- Landbay 4 (11.81 acres) – To be designated PMD-B-1, General Business, to allow up to 115,000 gsf of uses permitted in the B-1 zoning district.
- Landbay 5 (29.27) – To be designated PMD-O(H), Office- High-Rise, for the development of up to 1,000,000 square feet of office development.

Proposal's Strengths

- Zoning – The PMD, Planned Mixed Use District zoning is considered compatible with the REC, Regional Employment Center Comprehensive Plan designation.
- Employment – The proposal allows for the potential development of up to 1,000,000 square feet of office/employment development.

Proposal's Weaknesses

- Nonresidential Construction Phasing - The applicant's proposal includes no phasing for the timing for construction of the proposed office/employment development. The majority of the project area is designated REC, Regional Employment Center, and the Board's land use policy expressly states the need for nonresidential phasing in order to ensure that employment development always remains the primary use within this planned district.
- Land Use Classification - The purpose of the Regional Employment Center classification is to provide for areas located close to and/or with good access from an interstate highway where intensive regional employment uses are to be located.
 - This property is situated with high visibility from, and excellent access to, Interstate 66. Due to the limited availability of employment planned areas along I-66, the +/- 64.4 acres proposed for residential development should instead continue to provide opportunities for future intensive regional employment investment.
 - This project, without construction phasing of the employment component, is a residential development project with a minimum 25,000 SF of retail development (to be constructed at the 300 residential occupancy permit). The project is not consistent with the development guidance provided in the Comprehensive Plan for the REC.
- Residential Development in REC - Residential uses shall represent no greater than 25 percent of the total REC gross floor area of the project. The residential square footage in the area planned REC is estimated to be 558,500 SF, which is 33% of the overall project floor area. The impacts on the amount of land designated for residential development should also be considered. The residential area proposed is 62% (64.4 acres) of the project/land area that is designated REC.
- Planned Mixed Use Intent – Although the project contains multiple uses, the project poorly integrates the uses proposed. The residential development is not well-integrated with the employment uses and does not provide walkability.
- Unit Type – The planned housing type within any mixed-use REC project or SRH project is multifamily. The applicant's residential proposal consists of single family detached units and townhouses. Neither unit type is envisioned in the REC designation. In addition, the residential density proposed is not consistent with either the REC or the SRH.
- Open Space - The proposal contains no active recreation open space area within the project area. The open space shown along Balls Ford Rd. is a stormwater management dry pond. Other areas shown to contain open space are wetlands. It was strongly recommended that the applicant provide usable open space on-site, particularly in the townhouse section of landbay 2 where no amenities have been provided.
- Drive-Throughs – Drive through uses are discouraged in the REC project areas. As proffered, a drive-through will be permitted in landbay 4 associated with a pharmacy or financial institution.

Attachment B - Staff Analysis

Future Transit Location - The Comprehensive Plan Future Transit Alternatives Map identifies a future Metro station location at Sudley Rd. and I-66. This further highlights the planned intent for employment development, density and a mix uses in this area.

On balance, this application is found to be inconsistent with the relevant components of the Long-Range Land Use Plan.

Community Design Plan Analysis

An attractive, well-designed County will attract quality development, instill civic pride, improve the visual character of the community and create a strong, positive image of Prince William County. The Community Design Plan sets out policies and action strategies that further the County's goals of providing quality development and a quality living environment for residents, businesses and visitors, and creating livable and attractive communities. The plan includes recommendations relating to building design, site layout, circulation, signage, access to transit, landscaping and streetscaping, community open spaces, natural and cultural amenities, stormwater management, and the preservation of environmental features.

Proposal's Strengths

- Site Layout - As proffered, the site will be developed in substantial conformance with the Master Zoning Plan.
- Landscape Plan – The applicant has provided a proffered landscape plan that provides detailed landscaping.
- Design Guidelines & Elevations – The applicant has proffered general conformance to the design guidelines and building elevations attached. Design Guidelines for Office Development, outlining the various elements of quality office design and providing examples of high quality building and site design. The design guidelines recommend streetscaping elements (architecture, landscaping, signage, lighting, street furnishings, and pedestrian connections) to be incorporated into project.
- Amenities – The applicant intends to provide community walking trails, tot lots, and a community recreation facility with a club house and 6-lane, 25 meter pool. Also, the applicant has proffered to construct an off-site turf ball field at the nearby Ellis Barron Park.

Proposal's Weaknesses

- Usable Open Space Area – The proposal contains no active recreation open space area within the project area. The open space shown along Balls Ford Rd. is a stormwater management dry pond. Other areas shown to contain open space are wetlands. It was strongly recommended that the applicant provided usable open space, particularly in the townhouse section of landbay 2 where no amenities have been provided.

Attachment B - Staff Analysis

- Modifications to Village House Design - The applicant is requesting multiple modifications to the performance standards of the Village House detail provided in the Zoning Ordinance. The minimum lot area is proposed to be reduced from 6,000 square feet to 5,000 square feet and the minimum lot width is proposed to be reduced from a minimum of 60 feet to 50 feet. The side yard setback will be reduced from a minimum of 10 feet, to 5 feet, and the corner lot side setback is to be reduced from 20 feet to 16 feet. Lastly, the rear lot line setback is being reduced from a minimum 25 feet, to 20 feet. In regards to fire safety, the proffers do not specifically require noncombustible materials for the sides of building subject to the modified side yard setbacks.

On balance, this application is found to be consistent with the relevant components of the Community Design Plan.

Cultural Resources Plan Analysis

Prince William County promotes the identification, evaluation, and protection of cultural resource sites throughout the County, as well as the tourism opportunities these sites present. The Cultural Resources Plan recommends identifying, preserving, and protecting Prince William County's significant historical, archaeological, architectural, and other cultural resources—including those significant to the County's minority communities—for the benefit of all of the County's citizens and visitors. To facilitate the identification and protection of known significant properties that have cultural resource values worthy of preservation, the land use classification County Registered Historic Site (CRHS) is used in the Comprehensive Plan. The plan includes areas of potentially significant known but ill-defined or suspected pre-historic sites, Civil War sites, historic viewsheds, landscapes or areas of potential impact to important historic sites, and encourages the identification, preservation, protection, and maintenance of all cemeteries and/or gravesites located within the County.

Phase I archaeological studies are generally required at submission of rezoning and special use permit applications where significant prehistoric or historic sites and cemeteries are known or suspected. Phase II evaluations and treatment plans studies may also be required. Records research is required of all applicants for rezoning, special use permit, comprehensive plan amendment, and public facility review applications.

A Phase I archaeological survey was completed and reviewed by the County Archaeologist. As proffered, the applicant will curate with the County, artifacts and records recovered during the Phase II evaluation.

Proposal's Strengths

- Archaeological Study - As proffered, the applicant will curate with the County, artifacts and records recovered during the Phase II evaluation.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Cultural Resources Plan.

Environment Plan Analysis

Prince William County has a diverse natural environment, extending from sea level to mountain crest. Sound environmental protection strategies will allow the natural environment to co-exist with a vibrant, growing economy. The Environment Plan sets out policies and action strategies that further the County's goal of preserving, protecting and enhancing significant environmental resources and features. The plan includes recommendations relating to the incorporation of environmentally sensitive development techniques, improvement of air quality, identification of problematic soil issues, preservation of native vegetation, enhancement of surface and groundwater quality, limitations on impervious surfaces, and the protection of significant viewsheds.

The applicant submitted an environmental constraint analysis. This property is comprised of 128 acres of predominantly mature hardwood forests with some areas of open field and regenerating young successional forest. There are perennial streams and associated Resource Protection Areas (RPA) (established per PFD #13-00172 and PASA #13-00173). This site is located in Subwatersheds #164 and #166 (approximately 95% of total rezoning area), in the Bull Run subwatershed, and Subwatershed #262, which flows into the Broad Run. Also, there are several isolated jurisdictional wetlands throughout the site. A water quality monitoring contribution is proffered.

Proposal's Strengths

- **Water Quality** – The applicant has proffered to make a monetary contribution in the sum of \$75.00 per acre to the Board of County Supervisors for the purpose of monitoring water quality, stream restoration projects, and/or drainage improvements.

Proposal's Weaknesses

- **Stormwater Management** – The location the stormwater management facility in landbay 3 and 4 is shown within the RPA. The applicant has not proffered design commitments associated with the SWM facility located in the RPA.
- **Chesapeake Bay RPA** – The applicant has not committed to preserving an undisturbed 100 foot wide buffer around the stormwater management facility that is located in the RPA in landbays 3 and 4.

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- Open Space – The quality of the open space provided is questionable. The open space shown along Balls Ford Rd. is a stormwater management dry pond. Other areas shown to contain open space are wetlands.
- Preservation – The applicant proposing to preserve only 4.5% of the total site area. This corresponds to 7.2% of the proposed PMR zoned area and 0% of the proposed PMD zoned area. The design does not minimize disturbance or fit the development into the natural features of the site.
- Specimen Trees – The applicant has located several specimen trees on the property; however, none of the trees are shown to be preserved.

On balance, this application is found to be inconsistent with the relevant components of the Environment Plan.

Fire and Rescue Plan Analysis

Quality fire and rescue services provide a measure of security and safety that both residents and businesses have come to expect from the County. The Fire and Rescue Plan sets out policies and action strategies that further the County's goal of protecting lives, property, and the environment through timely, professional, humanitarian services essential to the health, safety, and well-being of the community. The plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility for fire and rescue facilities. The plan also includes recommendations to supplement response time and reduce risk of injury or death to County residents, establishment of educational programs, such as cardio-pulmonary resuscitation (CPR) training, automatic external defibrillators (AED), and encourage installation of additional fire protection systems—such as sprinklers, smoke detectors, and other architectural modifications.

Proposal's Strengths

- Level of Service - The applicant has proffered to provide monetary contributions for the residential units in accordance with the 2006 policy guidelines. The monetary contribution for the nonresidential building area is also proffered.
- 4.0 Minute Response Time – The site is within the recommended 4.0-minute response time for fire suppression and basic life support.
- 8.0 Minute Response Time – The site is within the recommended 8.0-minute response time for advanced life support.
- Stonewall Jackson Fire & Rescue Station Workload – The responding fire and rescue station is currently operating at a level outside the recommended standard for capacity.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Fire and Rescue Plan.

Housing Plan Analysis

Prince William County is committed to clean, safe and attractive neighborhoods for all its residents, and the elimination of neighborhood blight and substandard housing. The Housing Plan sets out policies and action strategies that further the County's goal of identifying locations and criteria for the provision of diverse housing opportunities for all segments of our population and to promote economic development. The Plan includes recommendations relating to neighborhood preservation and improvement, affordable housing, special needs housing, and public/private partnerships to address housing needs.

As recommended in the Comprehensive Plan, to assist in the need for affordable housing in Prince William County, the applicant has proffered a monetary contribution of \$250 per residential unit to the Housing Preservation and Development Fund. The fund assists in developing housing options for those in the County who cannot afford to reside in the community.

Proposal's Strengths

- **Monetary Contribution** - The applicant has proffered to provide a monetary contribution to the Housing Preservation and Development Fund.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Housing Plan.

Library Plan Analysis

Access to a variety of information is a valuable service provided by the County. The Library Plan sets out policies and action strategies that further the County's goal of providing adequate library facilities and information resources to our residents. The Plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility.

The library nearest the proposed project is the Gainesville Neighborhood Library. It is estimated that the proposed development will generate a need for additional library space and volumes of books and periodicals.

Proposal's Strengths

- Level of Service - The applicant has proffered to provide monetary contributions for the residential units in accordance with the 2006 policy guidelines.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Library Plan.

Parks and Open Space Plan Analysis

The quality of life for residents of Prince William County is linked closely to the development and management of a well-maintained system of parks, trails, and open space. Prince William County contains a diversity of park, open space, and trail resources. These parklands, open spaces, and recreational facilities play a key role in shaping both the landscape and the quality of life of Prince William County residents through the conservation of natural and cultural resources, protection of environmental quality, and provision of recreational facilities. The Parks, Open Space and Trails Plan sets out policies and action strategies that further the County's goal of providing park lands and recreational facilities of a quantity, variety, and quality appropriate to meet the needs of the current and future residents of Prince William County. The plan includes recommendations to preserve existing protected open space, maintain high quality open space, expand the amount of protected open space within the County, and to plan and implement a comprehensive countywide network of trails.

The proposed residential development is near the following area parks and trails:

Type	Name
Neighborhood	None
Community	Ellis Barron Park, Mayhew Sports Complex
Regional	Ben Lomond Park
Linear and Resource-Based	Broad Run Park
Trails	Broad Run Trail

Development of the site as proposed will generate a minimum need for an additional 72.38 acres of parks accessible to the general public (70 acres/1,000 population), including 15.51 acres of County-owned parkland (15 acres/1,000 population).

Proposal's Strengths

- Level of Service - The applicant has proffered to provide monetary contributions for the residential units in accordance with the 2006 policy guidelines.
- Amenities – The applicant intends to provide community walking trails, tot lots, and a community recreation facility with a club house and 6-lane, 25 meter pool.
- Off-site Field Construction – As proffered, if determined in consultation with the Parks Department, the applicant shall construct an off-site turf ball field at the nearby Ellis Barron Park.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Parks, Open Space and Trails Plan.

Police Plan Analysis

Residents and businesses expect a high level of police service for their community. This service increases the sense of safety and protects community investments. The Police Plan is designed to promote Prince William County's public safety strategic goal to continue to be a safe community, reduce criminal activity, and prevent personal injury and loss of life and property, as well as to ensure effective and timely responses throughout the County. This plan encourages funding and locating future police facilities to maximize public accessibility and police visibility as well as to permit effective, timely response to citizen needs and concerns. The plan recommends educational initiatives, such as Neighborhood and Business Watch, and Crime Prevention through Environmental Design (CPTED), which encourages new development to be designed in a way that enhances crime prevention. The plan also encourages effective and reliable public safety communications linking emergency responders in the field with the Public Safety Communications Center.

Proposal's Strengths

- Safety and Security Measures - The Police Department has recommended the safety and security measures that should be incorporated into the design of the site. No significant impact to police services is anticipated.

Proposal's Weaknesses

- None identified.

Attachment B - Staff Analysis

On balance, this application is found to be consistent with the relevant components of the Police Plan.

Potable Water Plan Analysis

A safe, dependable drinking water source is a reasonable expectation of County residents and businesses. The Potable Water Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound drinking water system. The plan includes recommendations relating to system expansion, required connections to public water in the development area, and the use of private wells or public water in the rural area.

The property is within the Prince William County Service Authority service area. Public water is available but may require construction of off-site facilities and acquisition of additional easements. Water mains must be sized and configured for hydraulic capacity, reliability and water quality. Details as to the location, size, capacity of the water mains that will serve the site are provided in the comments provided by the Service Authority dated July 10, 2014.

Proposal's Strengths

- Water Connection - The applicant is required to comply with Zoning Ordinance Section 32-250.74, which mandates connection of the site to public water service. The proffers require the applicant to design and construct all on-site and off-site water utility improvements necessary to develop the subject use.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Potable Water Plan.

Sanitary Sewer Plan Analysis

Appropriate wastewater and sanitary facilities provide needed public health and environmental protections. The Sanitary Sewer Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound sanitary and stormwater sewer system. The plan includes recommendations relating to system expansion, required connections to public sewer in the development area, and the use of either private or public sewer systems in locations classified as Semi-Rural Residential (SRR), as well as the rural area.

The property is within the Prince William County Service Authority area. Public sewer is available from sewer mains located on adjacent parcels to landbays 1, 2, 3 & 4. A sewer study will be required to confirm the existing system is adequately sized to handle the additional wastewater generation from this proposed development. Extending sewer across Balls Ford Rd. will be needed to stay within the Bull Run sewershed.

Proposal's Strengths

- Sewer Connection - The applicant is required to comply with Zoning Ordinance Section 32-250.75, which mandates connection of the site to public sewer service. The proffers require the applicant to design and construct all on-site and off-site sewer utility improvements necessary to develop the subject use.

Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Sanitary Sewer Plan.

Schools Plan Analysis

A high-quality education system serves not only the students and their families, but the entire community by attracting employers who value educational opportunities for their employees. The Schools Plan sets out policies and action strategies that further the County's goal of providing quality public education to our school-aged population. The Plan includes recommendations relating to facility size and location, siting criteria, compatible uses, and community use of school facilities.

The Schools Plan establishes level of service (LOS) standards for schools Countywide, based on available or project program capabilities. The Schools Plan recommends that proposed developments offset impacts on County schools by providing land for future schools and/or monetary contributions for construction of schools.

As indicated in comments received from Prince William County Public Schools, based on annually updated county-wide student generation factors, the proposed 415 residential units request is projected to generate the following numbers of students:

	SF	TH	Total
Elementary School, grades K-5	47	72	119
Middle School, grades 6-8	25	32	57
High School, grades 9-12	33	39	72
TOTAL	107	143	248

Students in the proposed development will likely be assigned to the following schools:

Type	Name	Current Enrollment
Elementary School	Sinclair ES	801 (91.4% Capacity)
Middle School	Bull Run Middle	1,182 (95.9% Capacity)
High School	Stonewall Jackson High	2,439 (101.2% Capacity)

Proposal's Strengths

- SFD Level of Service – In addition to the 2006 level of service monetary contribution proffered for the proposed townhouses, the applicant has proffered monetary contributions for the single family detached dwelling units in accordance with the 2014 policy guidelines.

Proposal's Weaknesses

- Current High School Capacity – Stonewall Jackson High School is currently operating at 101.2% capacity.

On balance, this application is found to be consistent with the relevant components of the Schools Plan.

Transportation Plan Analysis

By providing a multi-modal approach to traffic circulation Prince William County promotes the safe and efficient movement of goods and people throughout the County and surrounding jurisdictions. The Transportation Plan sets out policies and action strategies that further the County's goal of creating and sustaining an environmentally friendly, multi-modal transportation system that meets the demands for intra- and inter-county trips, is integrated with existing and planned development, and provides a network of safe, efficient, and accessible modes of travel. The plan includes recommendations addressing safety, minimizing conflicts with environmental and cultural resources, maximizing cost effectiveness, increasing accessibility of all travel modes, minimizing projected trip demand, and providing sufficient network capacity. Projects should include strategies that result in a level of service (LOS) of "D" or better on all roadway corridors and intersections, reduce traffic demand through transportation demand management strategies, dedicate planned rights-of-way, provide and/or fund transit infrastructure, pedestrian and bicycle pathways, and improved and coordinated access to transit facilities.

The project includes improvements to Balls Ford Rd., Ashton Ave., Bethlehem Rd., Coverstone Dr., and Hollis Dr. The applicant has proffered to make a monetary contribution according to the policy guidelines, but will be seeking a credit for the transportation improvements, not to exceed the amount of the LOS contribution. A traffic impact analysis (TIA) was required for this application. The following table provides information concerning the daily volumes and levels of services of roadways important to this development:

Attachment B - Staff Analysis

Roadway Name	Number of Lanes	Current VDOT Count	Existing Daily LOS
Sudley Rd.	6	47,000 vpd	D (south of Balls Ford Road)/E (north of Balls Ford Road)
Balls Ford Rd.	4	16,000-25,000 vpd	E
Ashton Ave.	4	4,400 vpd	C
Coverstone Dr.	2	6,800-7,400 vpd	E
Bethlehem Rd.	2	1,200 vpd	C
Prince William Parkway	4	46,700 vpd	C (south of Balls Ford Road)/D (north of Balls Ford Road)

Proposal's Strengths

- Level of Service - The applicant has proffered to provide monetary contributions in accordance with the policy guidelines.
- Balls Ford Road Improvements – In addition to the improvements to Ball Ford Rd. along the property frontage, the applicant has proffered off-site improvement to Balls Ford Rd. to the east and west of the site. The Balls Ford Rd. improvements will include the construction of a Class I trail.
- Coverstone Rd. & Hollis Dr. Extended - The improvements include extending Coverstone Dr. westward to intersect with Bethlehem Rd. and extending Hollis Dr. into the project area. Currently, both roads terminate at the edge of the property.
- Bus Shelter – As proffered, the applicant shall construct a bus shelter along the north side of Balls Ford Rd. subject to County, VDOT and PRTC approval.

Proposal's Weaknesses

- Sudley Rd. & Balls Ford Rd. Intersection – As indicated in the applicant's TIA, the intersection is currently operating at a level of service D. However, in the 2018 post development scenario, the level of service drops to LOS E during the AM peak hour and to LOS F during the PM peak hour. This intersection is located in the direction that the TIA attributes 70% of the movement of the project's generated vehicle trips.

On balance, this application is found to be consistent with the relevant components of the Transportation Plan.

Materially Relevant Issues

This section of the report is intended to identify issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The materially relevant issues in this case are as follows:

- None identified.

Minimum Design Criteria

Staff and other agencies that have reviewed the proposal noted the following minimum design criteria. The development proposal will be reviewed for compliance with all minimum standards at the time that the applicant submits detailed site development information prior to the issuance of construction permits. The listing of these issues is provided to ensure that these concerns are a part of the development record. Such issues are more appropriately addressed during the site plan review.

Waivers / Modifications:

- Modification of Section 32-306.10.1.(b) and Section 32-306.12.6.C and F of the Zoning Ordinance to allow a minimum of one modified unit type and to allow a modified housing type with yards and setbacks as shown on the MZP and further identified on Attachment E attached hereto, subject to compliance with Section 32-306.12.3 of the Zoning Ordinance.
- Waiver of Section 306.21 of the Zoning Ordinance to allow the elimination of the nonresidential secondary use requirement within the PMR District.
- Modification of Section 32-405.04 of the Zoning Ordinance to allow perimeter buffer around the PMD zoned property to be in accordance with those buffers shown on the Landscape Plan.
- Modification of Section 32-405.04.1 to allow PMD area to be based on contiguous and non-contiguous portions of the Property.
- Modification of Section 32-401.14.3 and 4, and Section 32-402.24.3 and 4 of the Zoning Ordinance to allow for the calculation of density (FAR) and open space in Land Bays 4 and 5 to be based on the entire individual Land Bay rather than on a lot by lot basis if/when the Land Bays are subdivided.
- Modification of Section 32-250.24 of the Zoning Ordinance to allow signs in conformance with Attachment A and the Design Guidelines identified herein.
- Modification of Detail 650.05 RM-2, of the DCSM to allow 11' lane with curb, gutter and sidewalk and right-of-way dedication on Bethlehem Road as shown on Sheets 7 and 11 of

Attachment B - Staff Analysis

the MZP and pursuant to approved waiver #WAI 2015-00013 attached hereto as Attachment E.

- Modification of Section 602.07.A of the DCSM to allow a commercial entrance on Ashton Avenue as shown on Sheet 10 of the MZP and pursuant to approved waiver #WAI 2015-00014 attached hereto as Attachment G.
- Modification of Section 601.04.C of the DCSM to allow single family detached lots to have frontage on private streets pursuant to approved waiver #WAI 2015-00015 attached hereto as Attachment H.
- Modification of Detail 650.11 MA-1, of the DCSM to allow modifications to Balls Ford Road in accordance with improvements shown on Sheets 7, 8 and 9 of the MZP pursuant to approved waiver #WAI 2015-00016 attached hereto as Attachment I.
- Modification of Detail 650.06 TS-1, of the DCSM to allow modifications to road width in the townhouse residential sections pursuant to approved waiver #WAI 2015-00017 attached hereto as Attachment J.
- Modification of Detail 650.08 CI-1, of the DCSM to allow modification of Hollis Drive extended as shown on Sheets 7 and 13 of the MZP pursuant to approved waiver #WAI 2015-00018 attached hereto as Attachment K.
- Modification of Detail 650.08 CI-1, of the DCSM to allow modification of Coverstone Drive as shown on Sheets 7 and 12 of the MZP, pursuant to approved waiver #WAI 2015-00019 attached hereto as Attachment L.
- Modification of Detail 650.09 MC-1, of the DCSM to allow for the Ashton Avenue ½ section (east side), as shown on Sheets 7 and 10 of the MZP, pursuant to approved waiver #WAI 2015-00020 attached hereto as Attachment M.
- Modification of Detail 650.04 RL-2, of the DCSM to allow modifications to street standards and to allow the sidewalk in the single family residential section to be constructed outside the public street right-of-way pursuant to approved waiver #WAI 2015-00021 attached hereto as Attachment N.

Agency Comments

The following agencies have reviewed the proposal and their comments have been summarized in relevant Comprehensive Plan chapters of this report. Individual comments are in the case file in the Planning Office:

County Archaeologist
DPW- Environmental Services
Fire and Rescue
Housing and Community Development
Library
Parks Authority
Planning Office, Case Manager and Zoning Administration
Police Department
PWC Historical Commission
PWC Service Authority
PWC Transportation
Schools
VDOT

MASTER ZONING PLAN

PLANNED MIX RESIDENTIAL DISTRICT (PMR)


PLANNED MIX USE DISTRICT (PMD)

BLACKBURN


GAINESVILLE MAGISTERIAL DISTRICT

PRINCE WILLIAM COUNTY, VIRGINIA

REZ # PLN 2014-00040





VICINITY MAP
SCALE: 1"=2000'

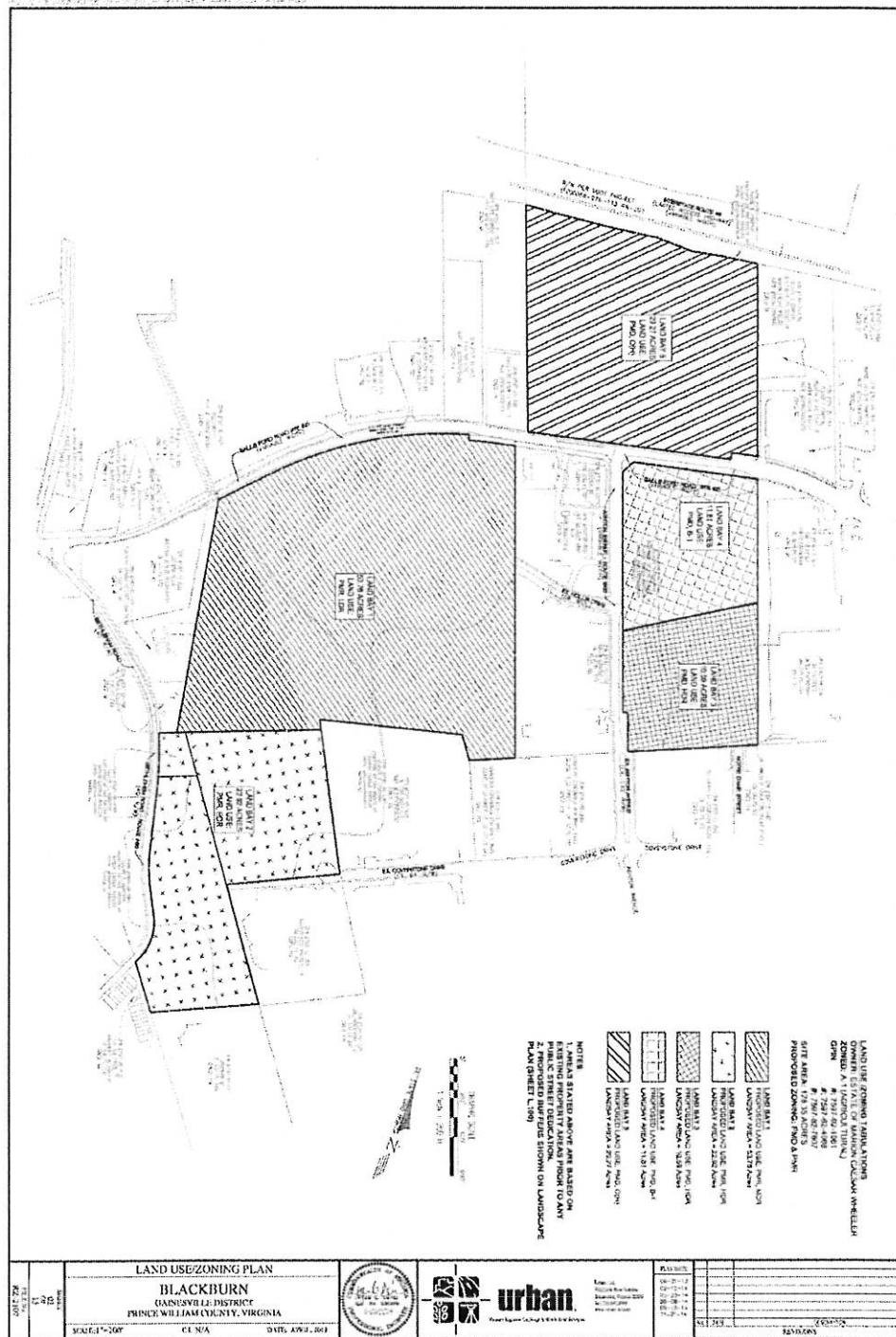


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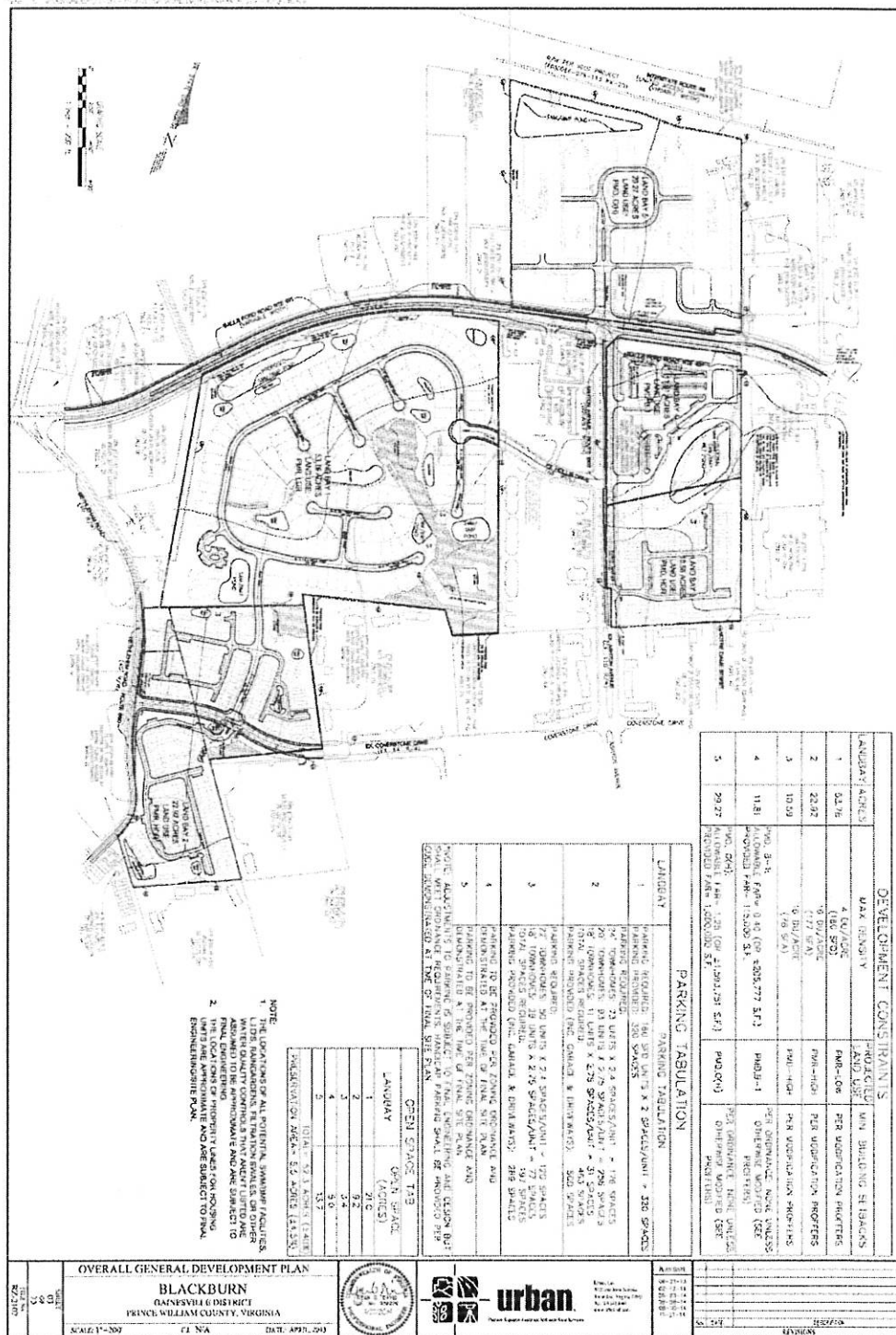
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ANNAPOLIS, MD 21403
CONTACT: CLAUDE@RENT.LLC

LAND USE ATTORNEY
WALSH, COLLIER, LUBELEY, EMBICH
& WALSH, P.C.
420 PRINCE WILLIAM PARKWAY
PRINCE WILLIAM, VA 22192
CONTACT: JAY@WLE.COM

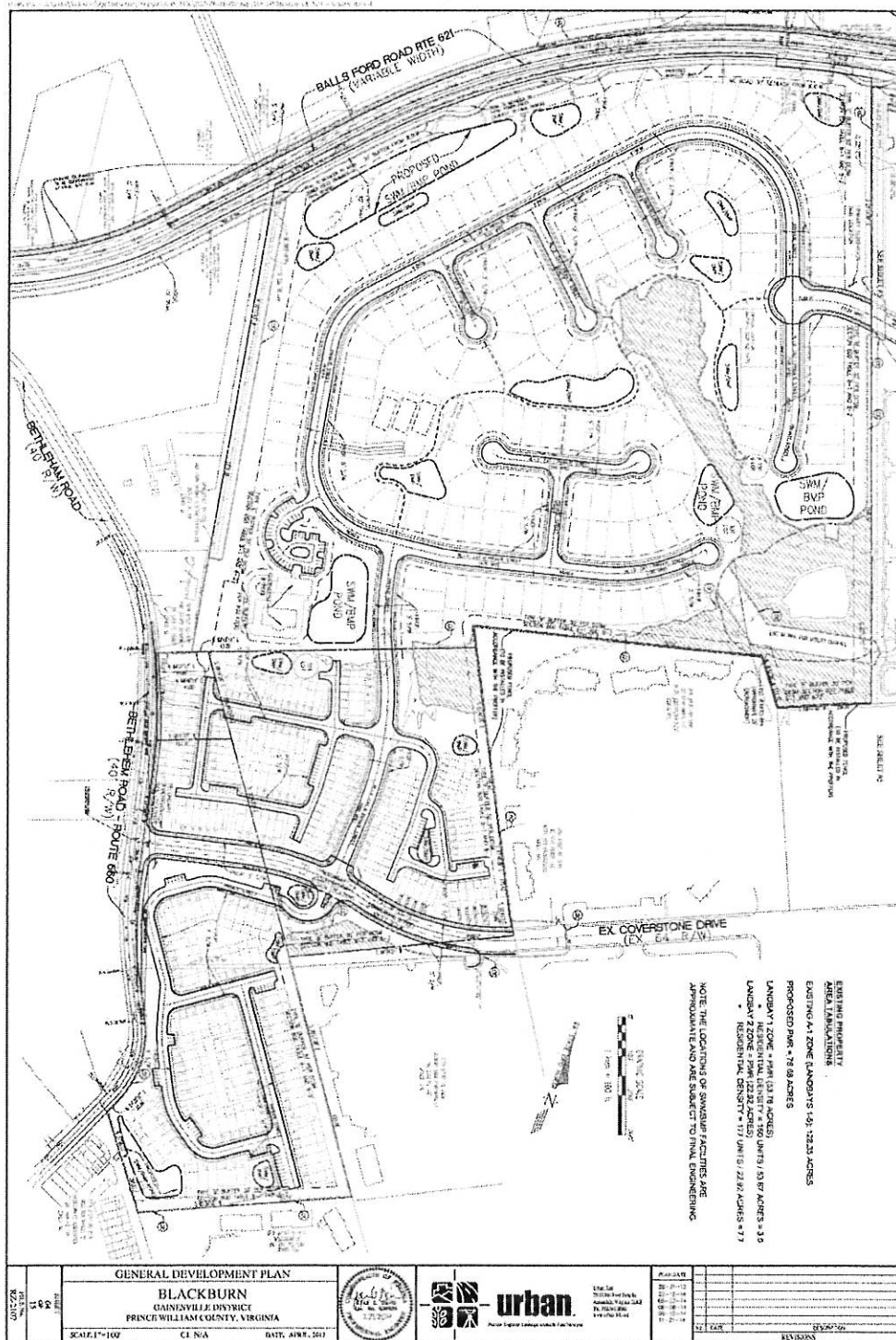
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BLACKBURN	GAINESVILLE DISTRICT				
PRINCE WILLIAM COUNTY, VIRGINIA					

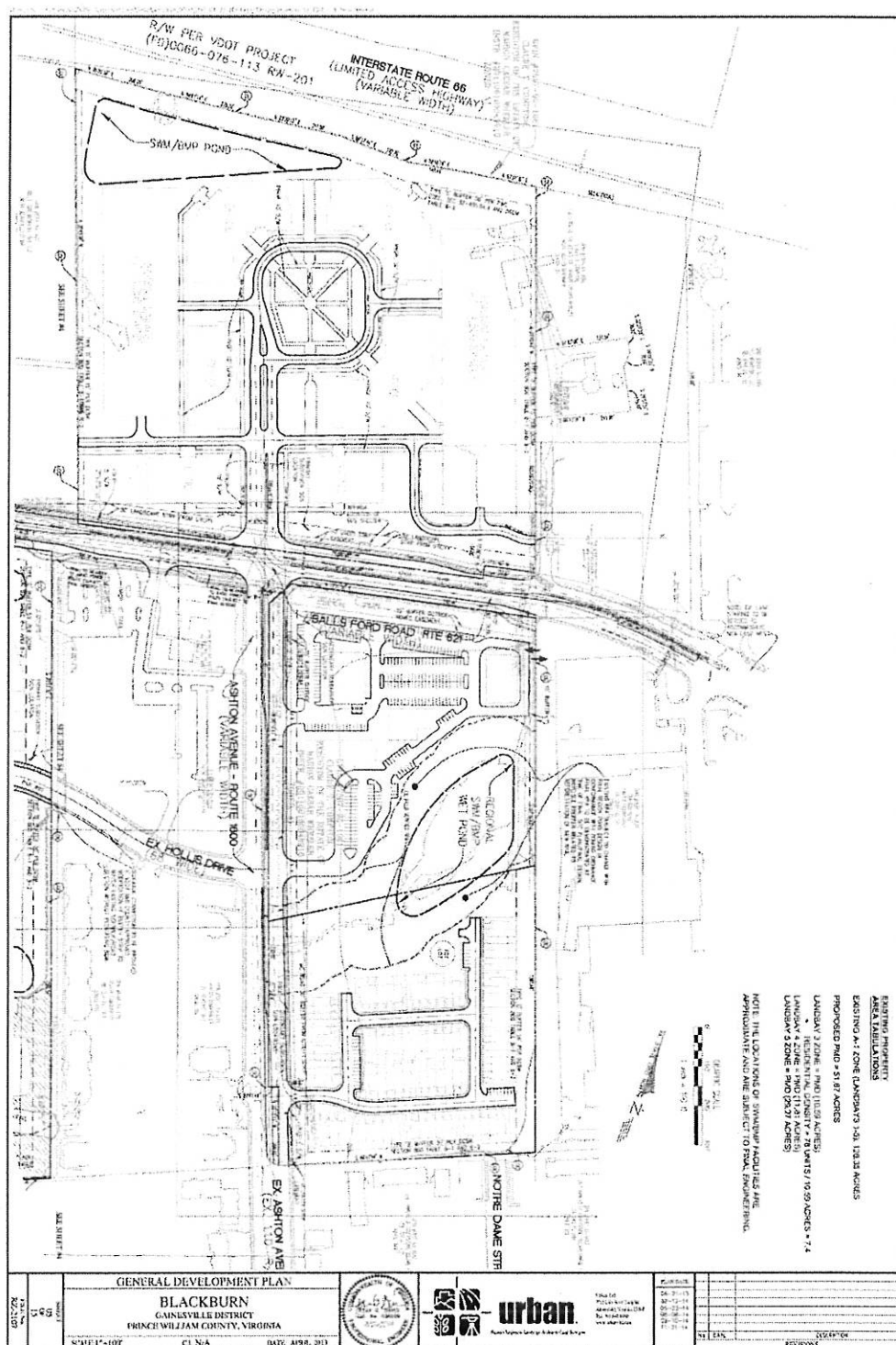


Attachment C – Master Zoning Plan

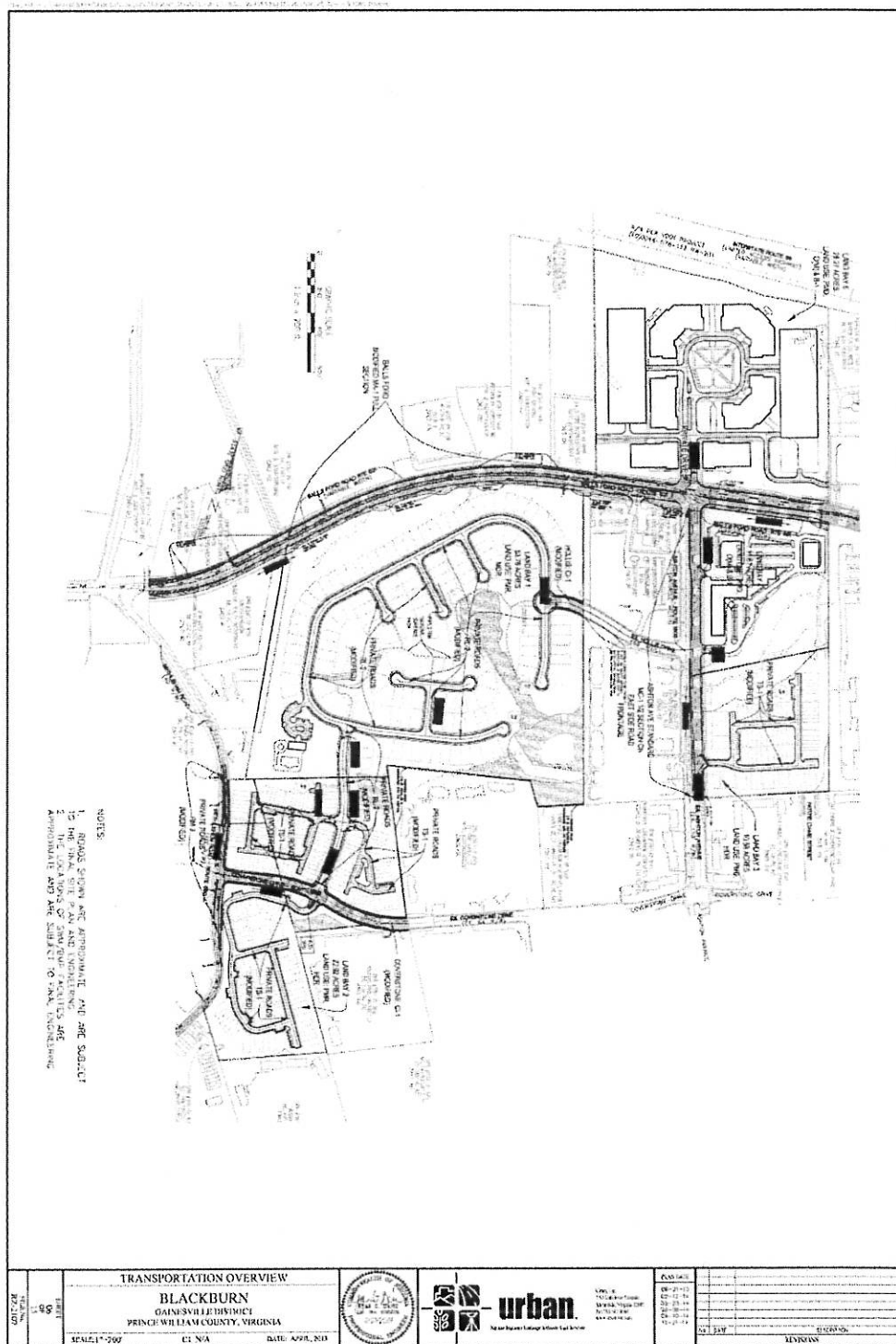


Attachment C – Master Zoning Plan



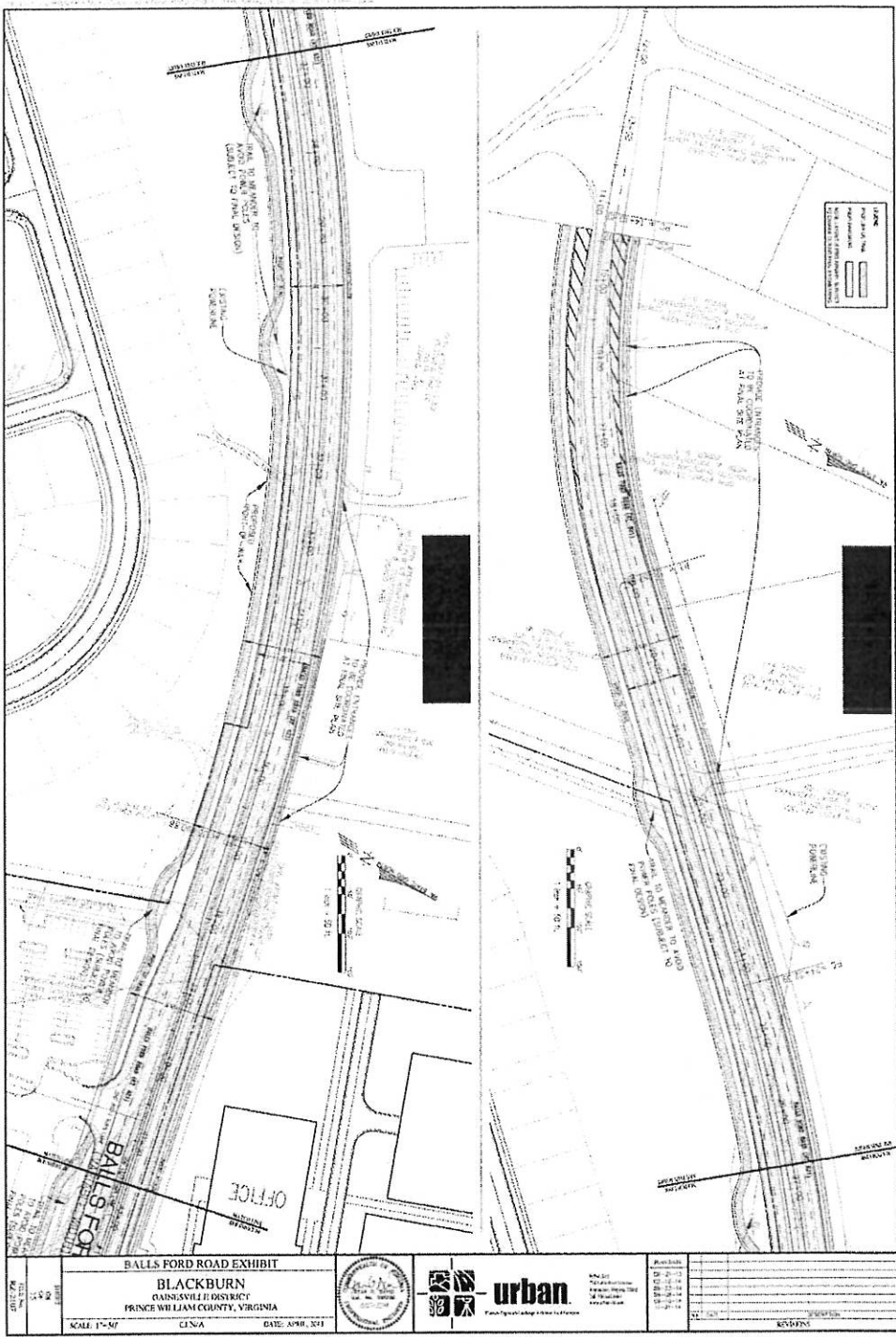


Attachment C – Master Zoning Plan



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Attachment C – Master Zoning Plan



BALLS FORD ROAD EXHIBIT
BLACKBURN
 GARDNERVILLE DISTRICT
 PRINCE WILLIAM COUNTY, VIRGINIA

SCALE: 1"=50'
 DATE: APRIL 2012

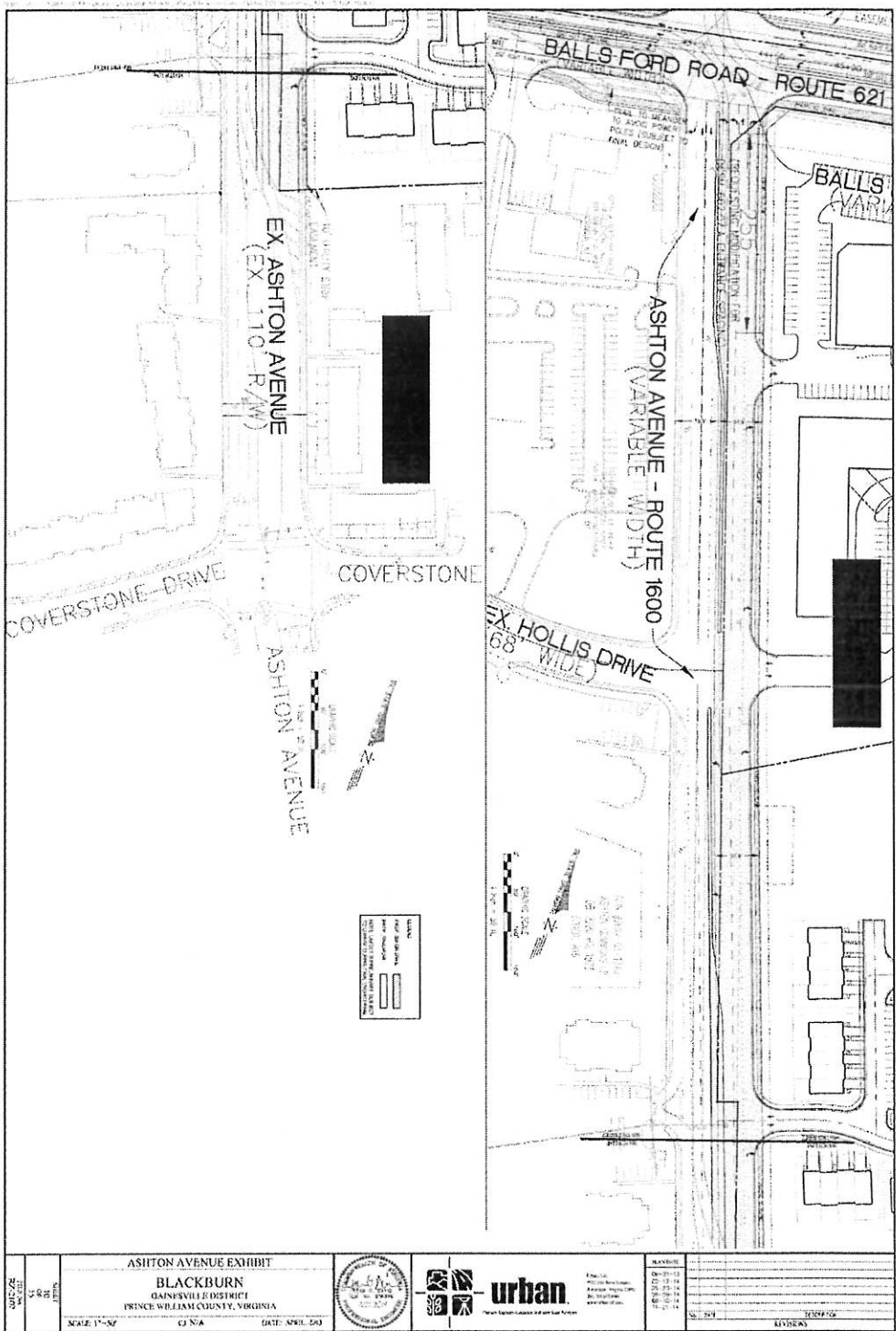
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 PROPOSED WIDENING
 EXISTING ROADWAY
 EXISTING UTILITY LINES
 EXISTING PROPERTY LINES

Notes:
 1. THE PROPOSED WIDENING OF BALLS FORD ROAD IS SHOWN IN THE EXHIBIT.
 2. THE EXISTING ROADWAY IS SHOWN IN THE EXHIBIT.
 3. THE EXISTING UTILITY LINES ARE SHOWN IN THE EXHIBIT.
 4. THE EXISTING PROPERTY LINES ARE SHOWN IN THE EXHIBIT.

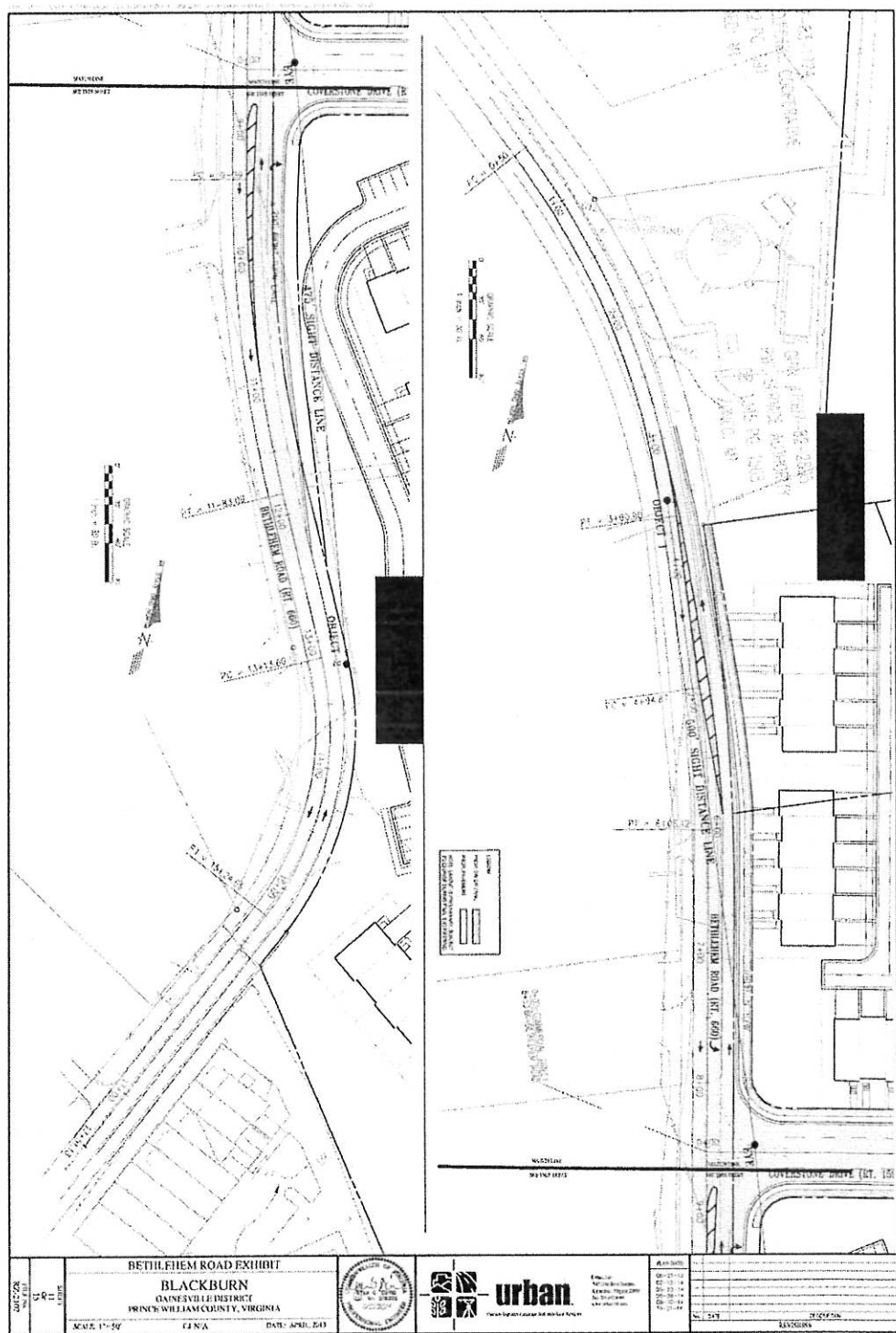
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 No. 145
 Revision
 Description

Urban Engineering, Inc.
 10000 BALLS FORD ROAD, BLACKBURN, VA 22006
 Phone: 703.444.1111
 Fax: 703.444.1112
 Email: info@urbaneng.com
 Website: www.urbaneng.com

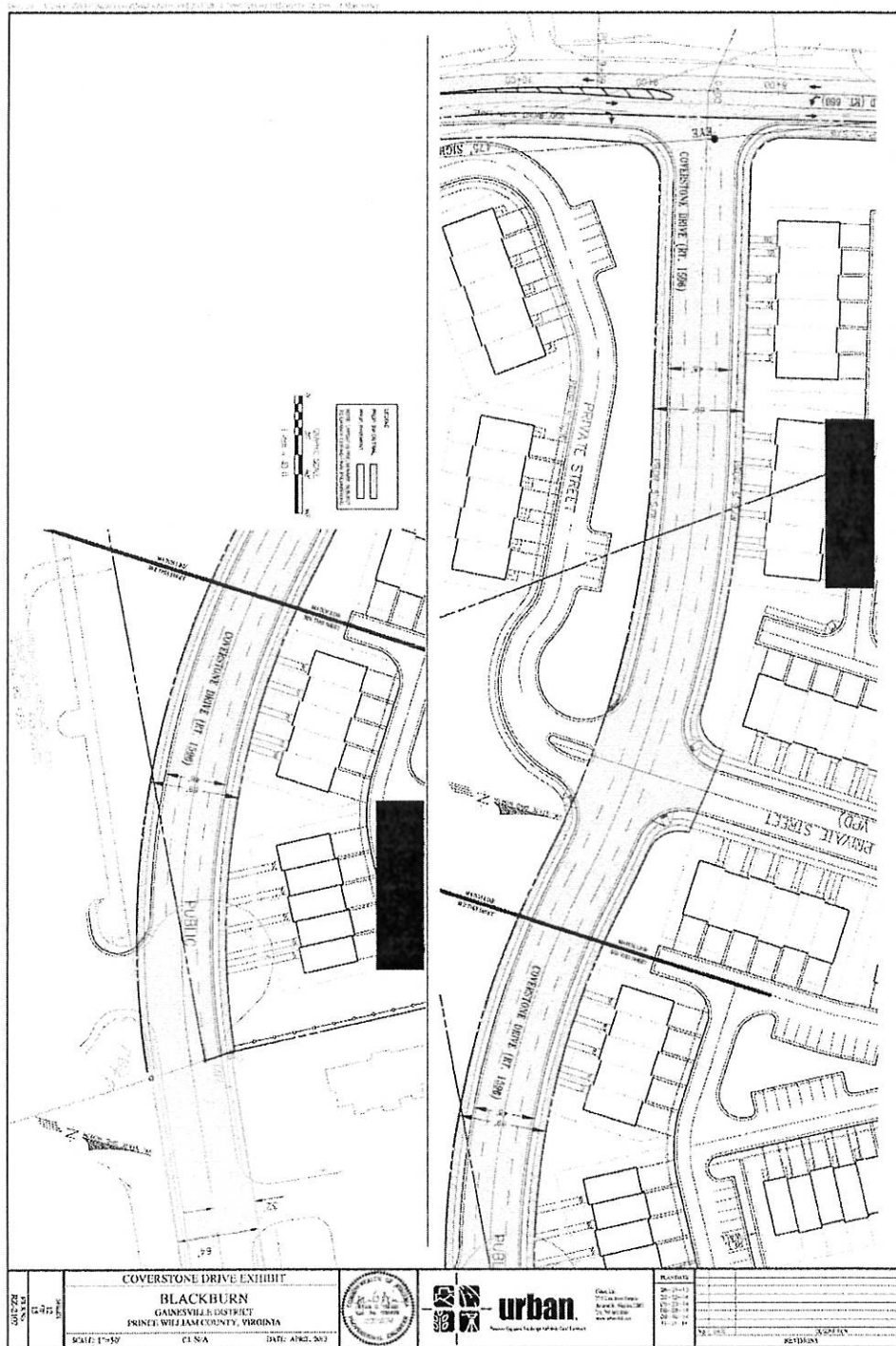
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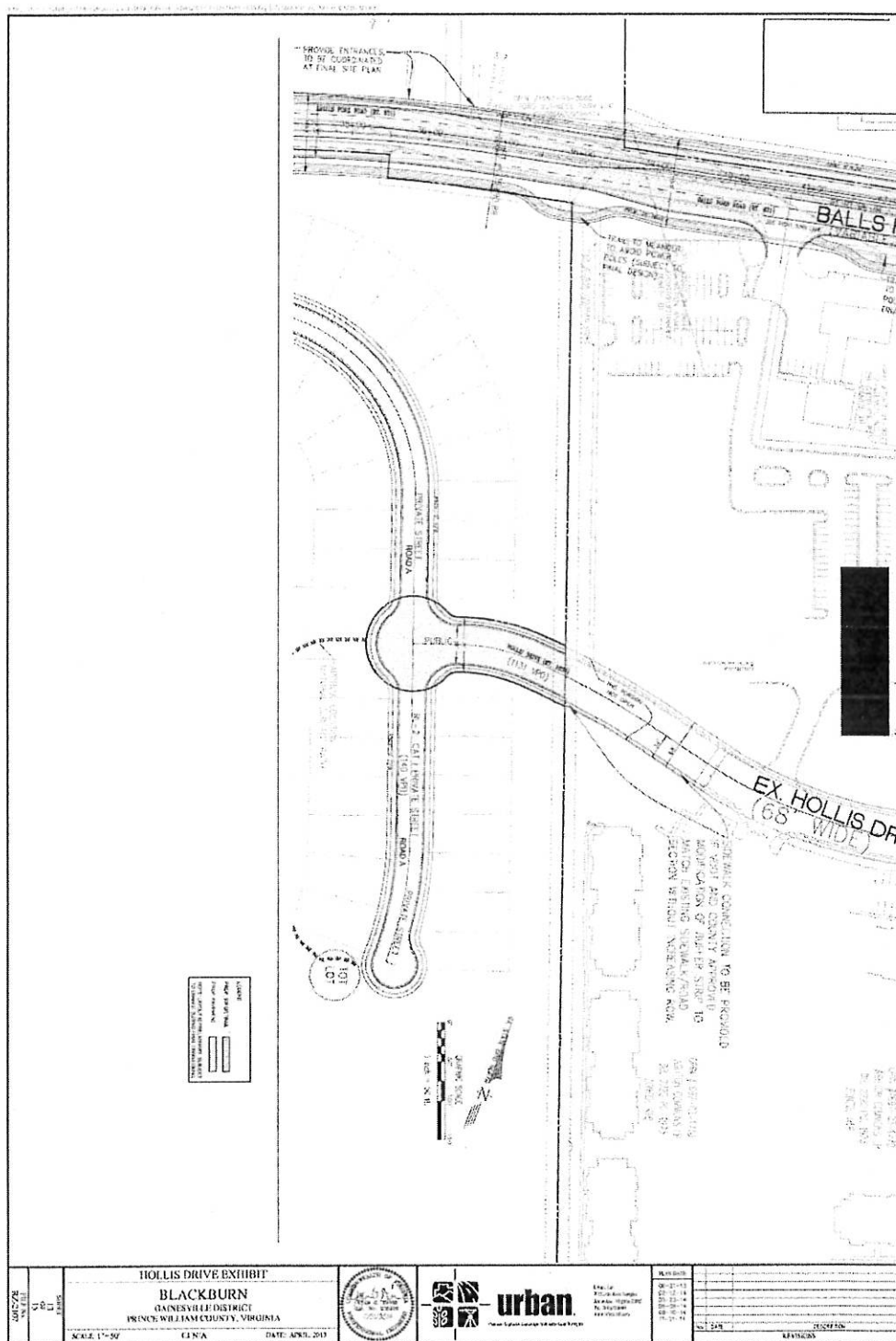
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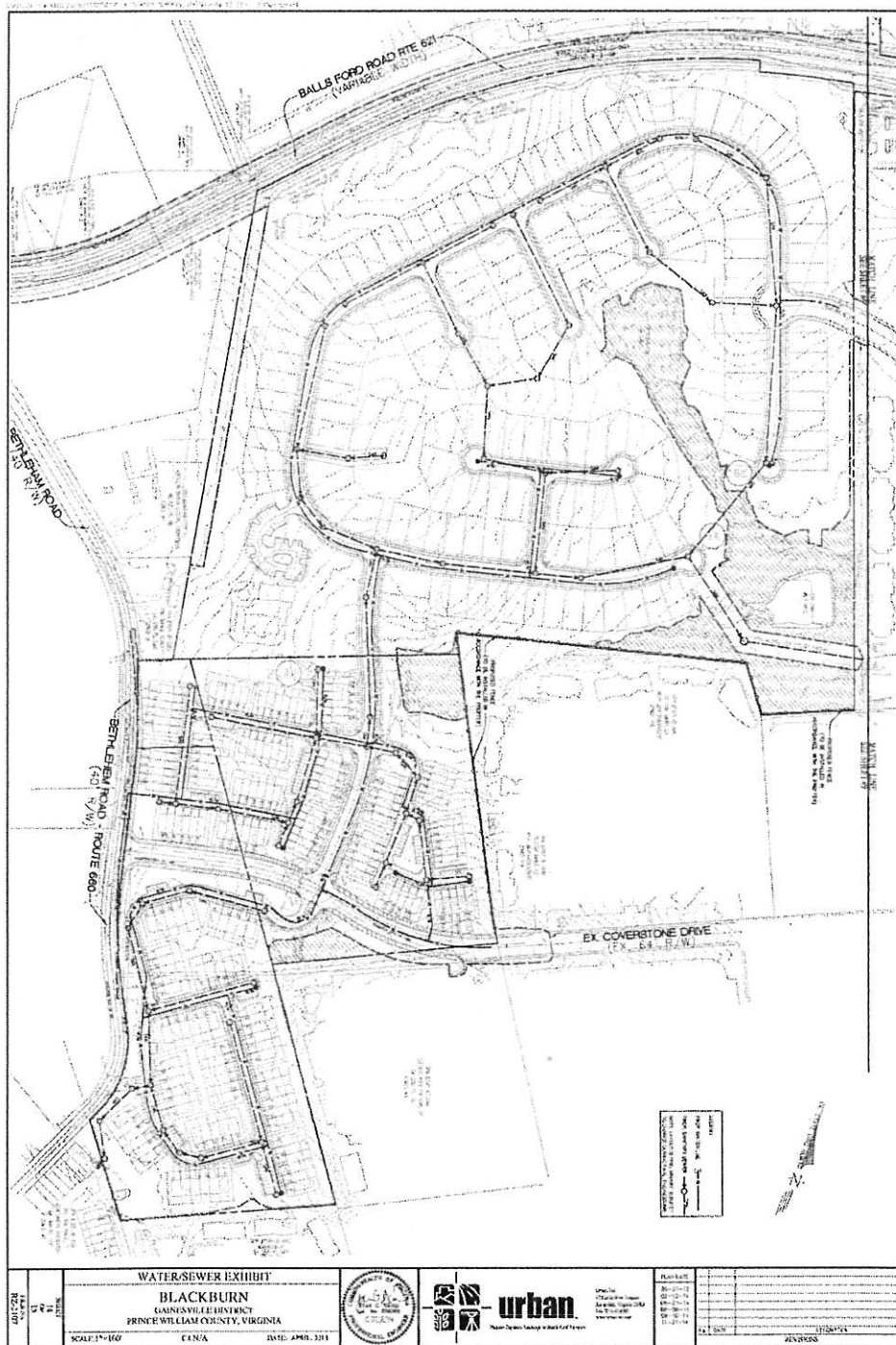
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Attachment C – Master Zoning Plan



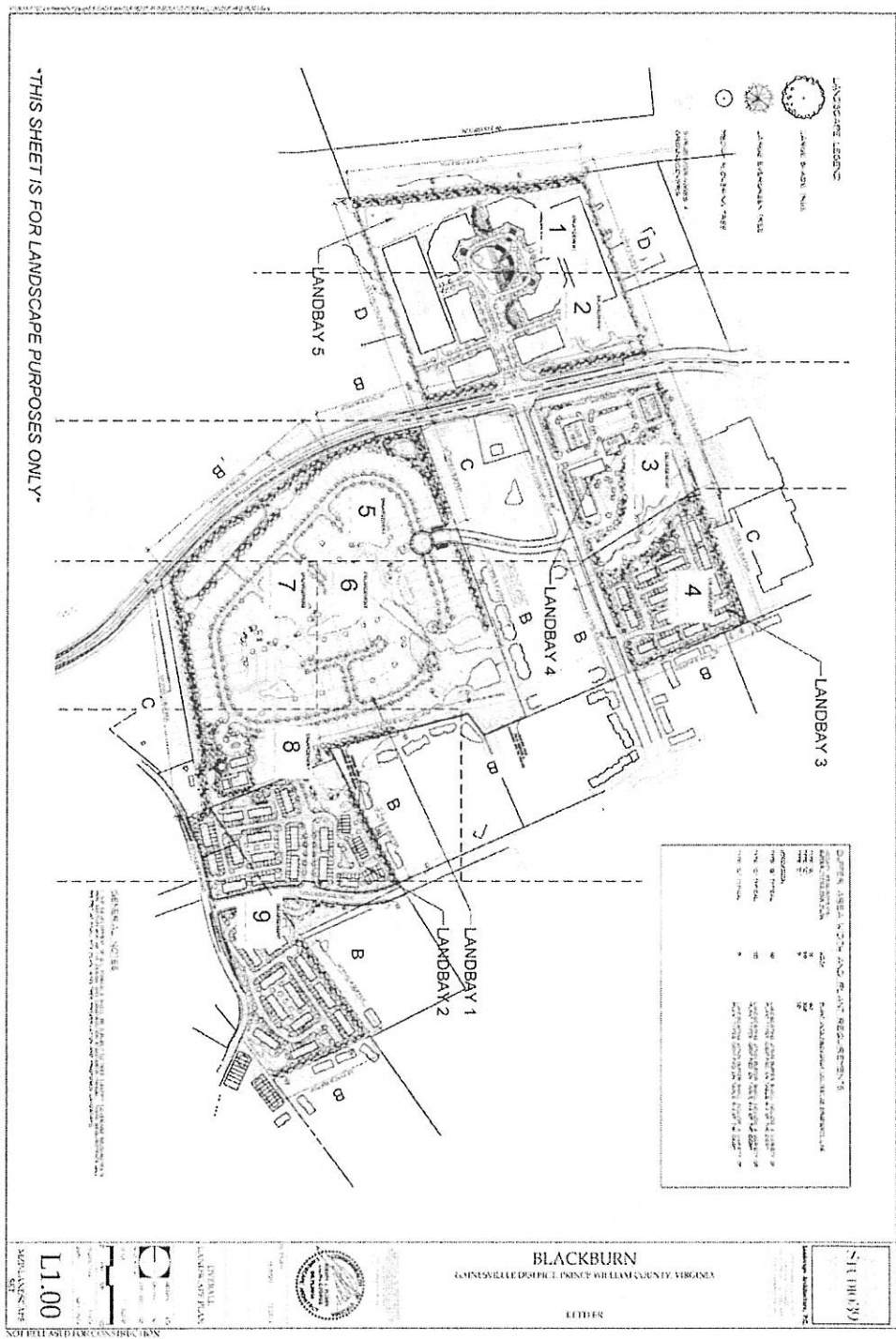
Attachment C – Master Zoning Plan



Attachment C – Master Zoning Plan



Attachment D – Landscape Plan



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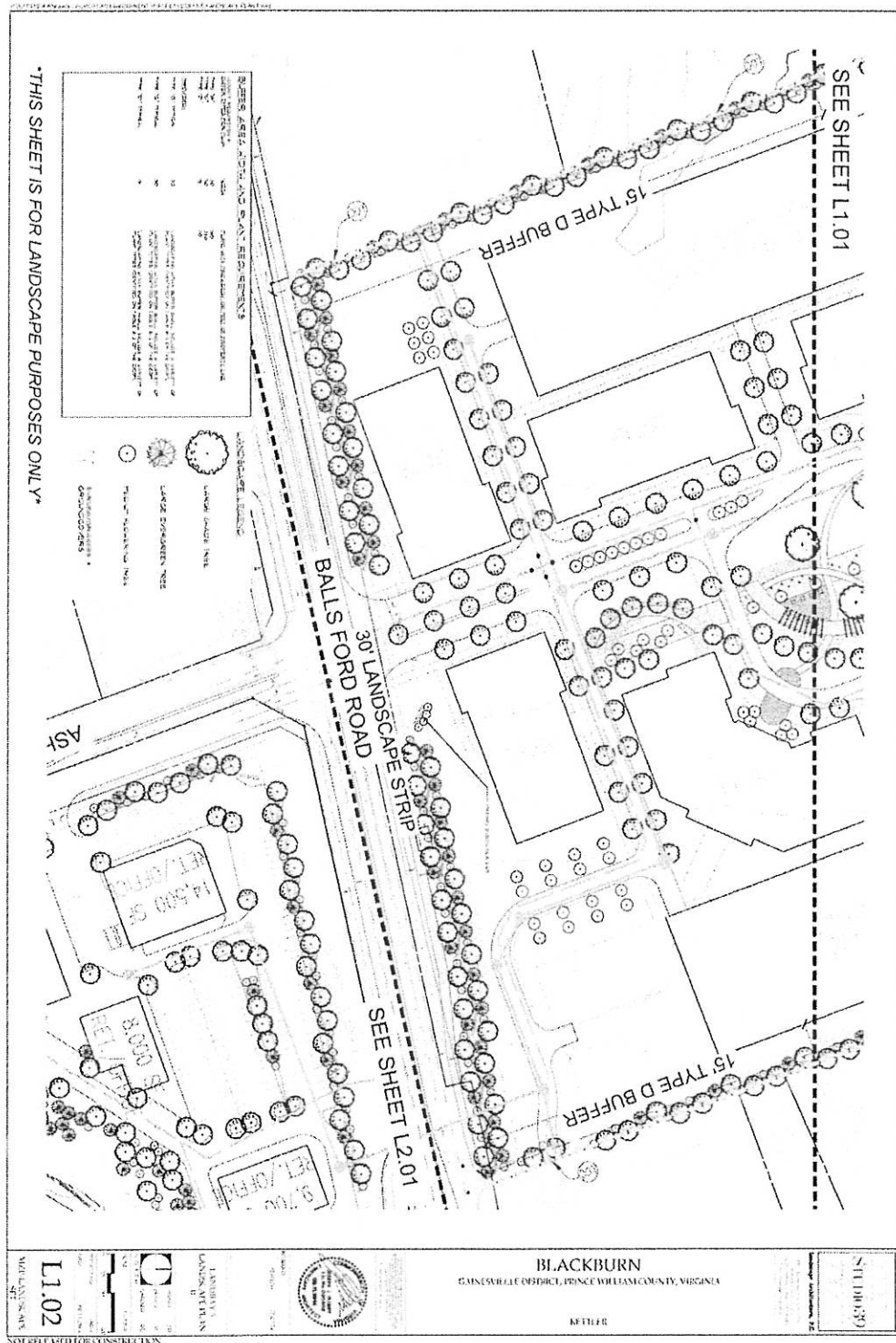
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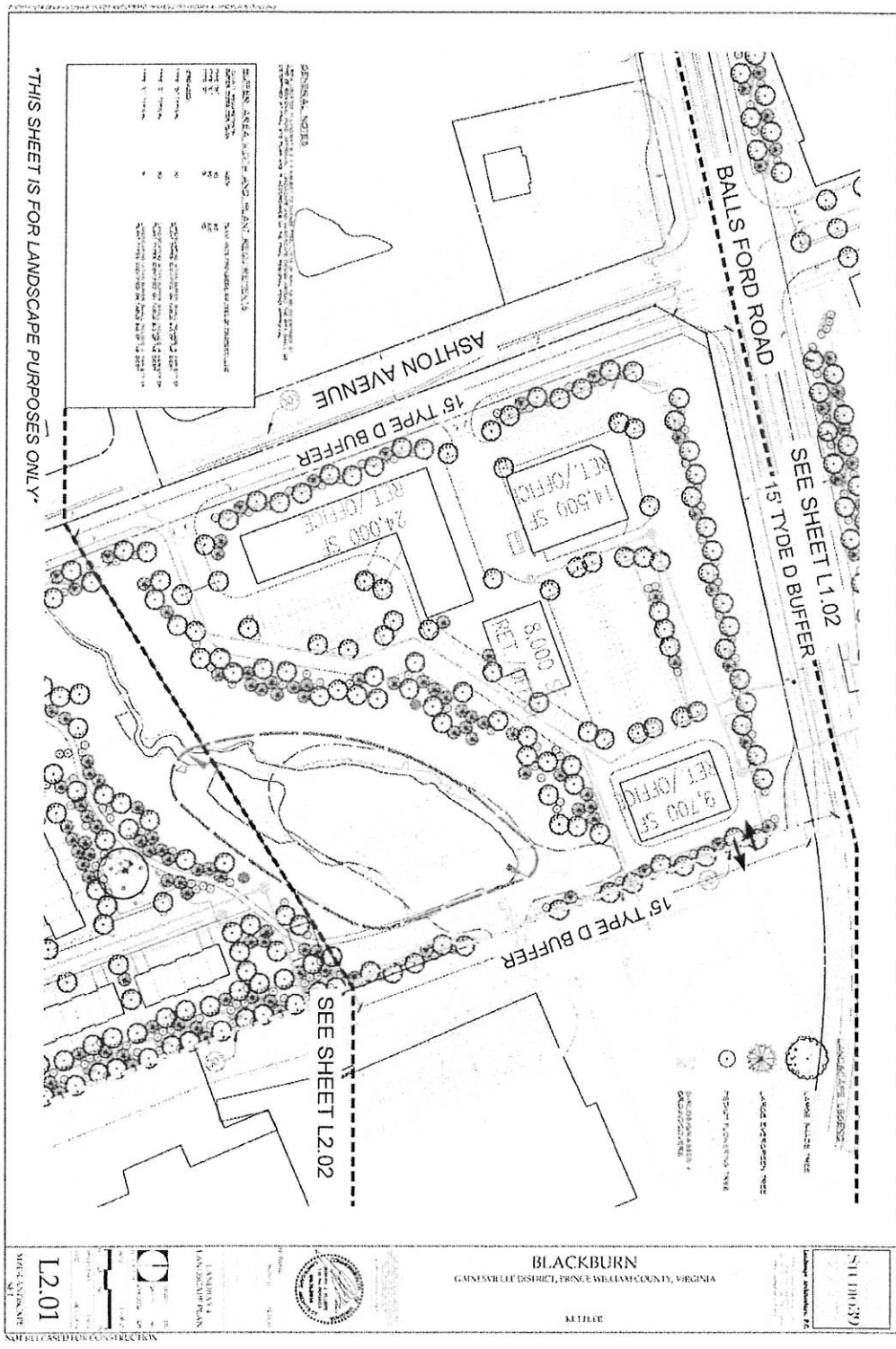
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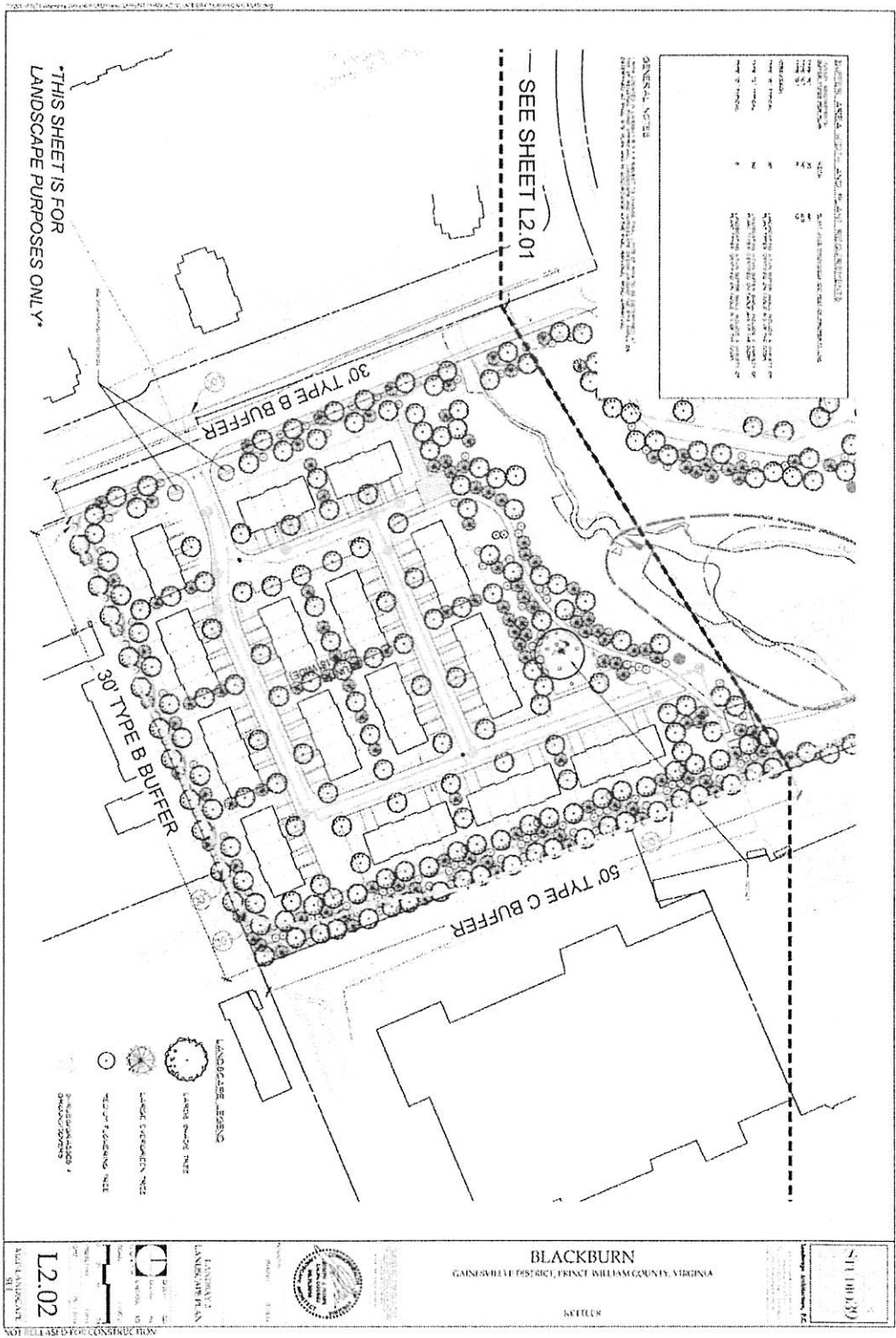
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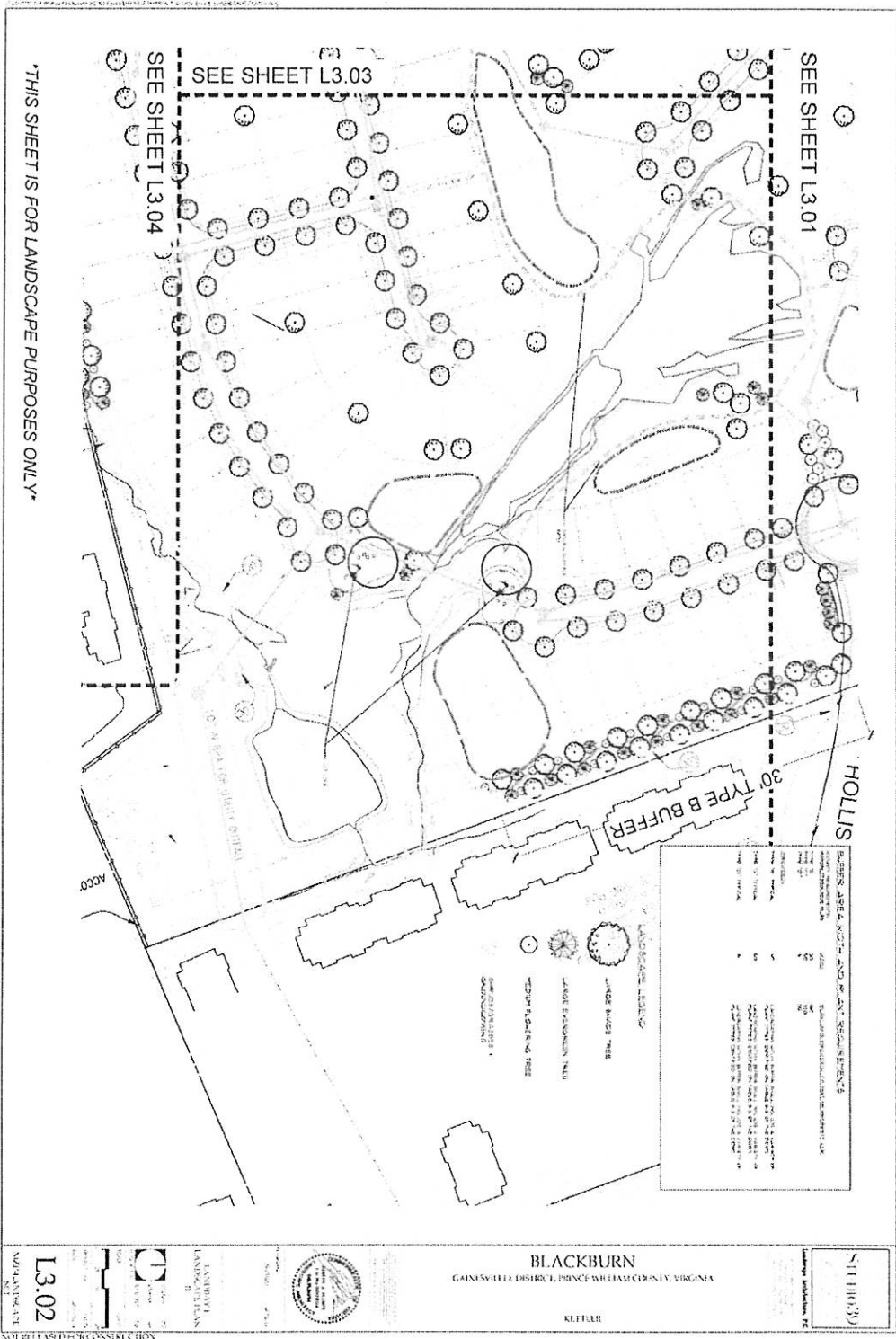
Attachment D – Landscape Plan

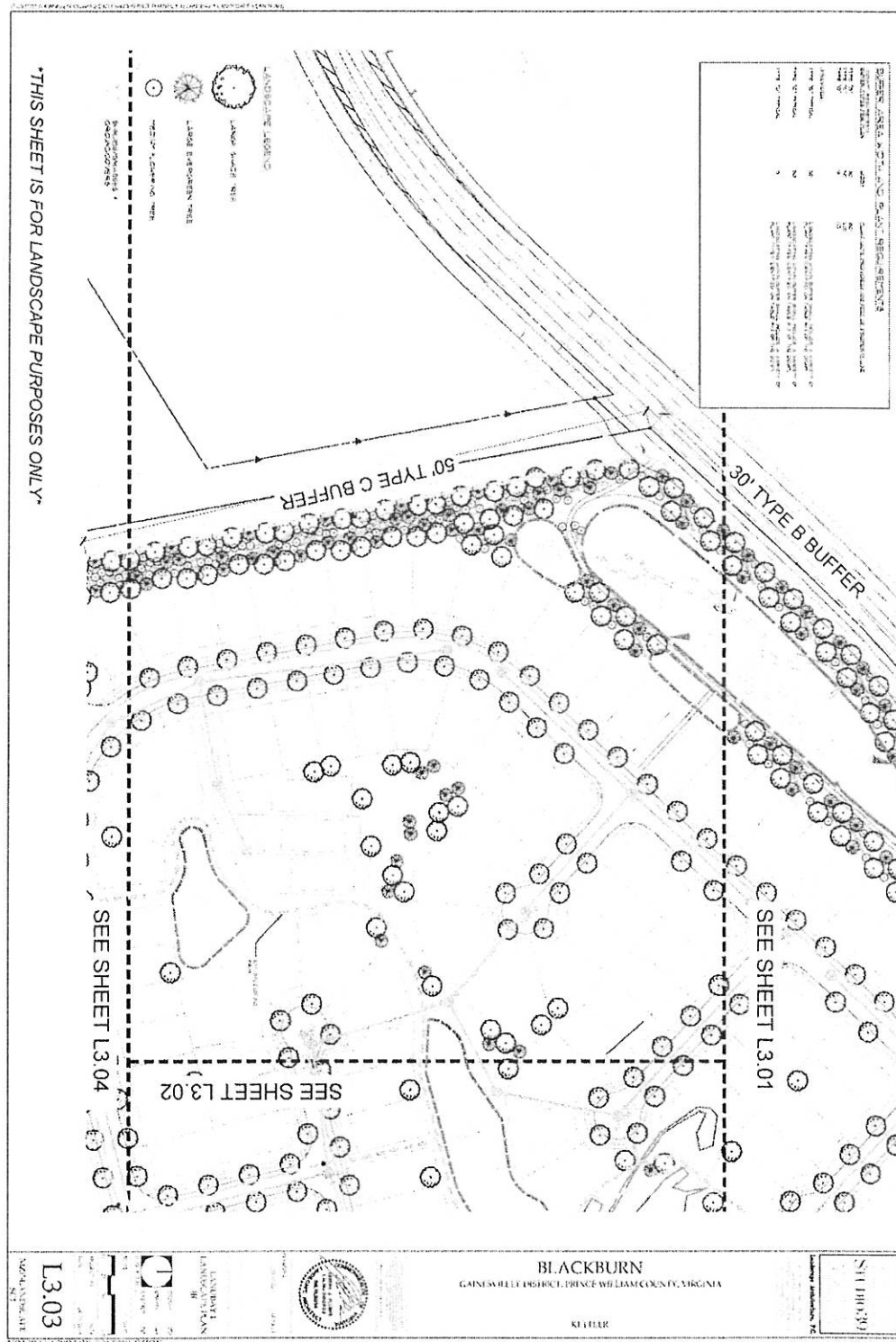


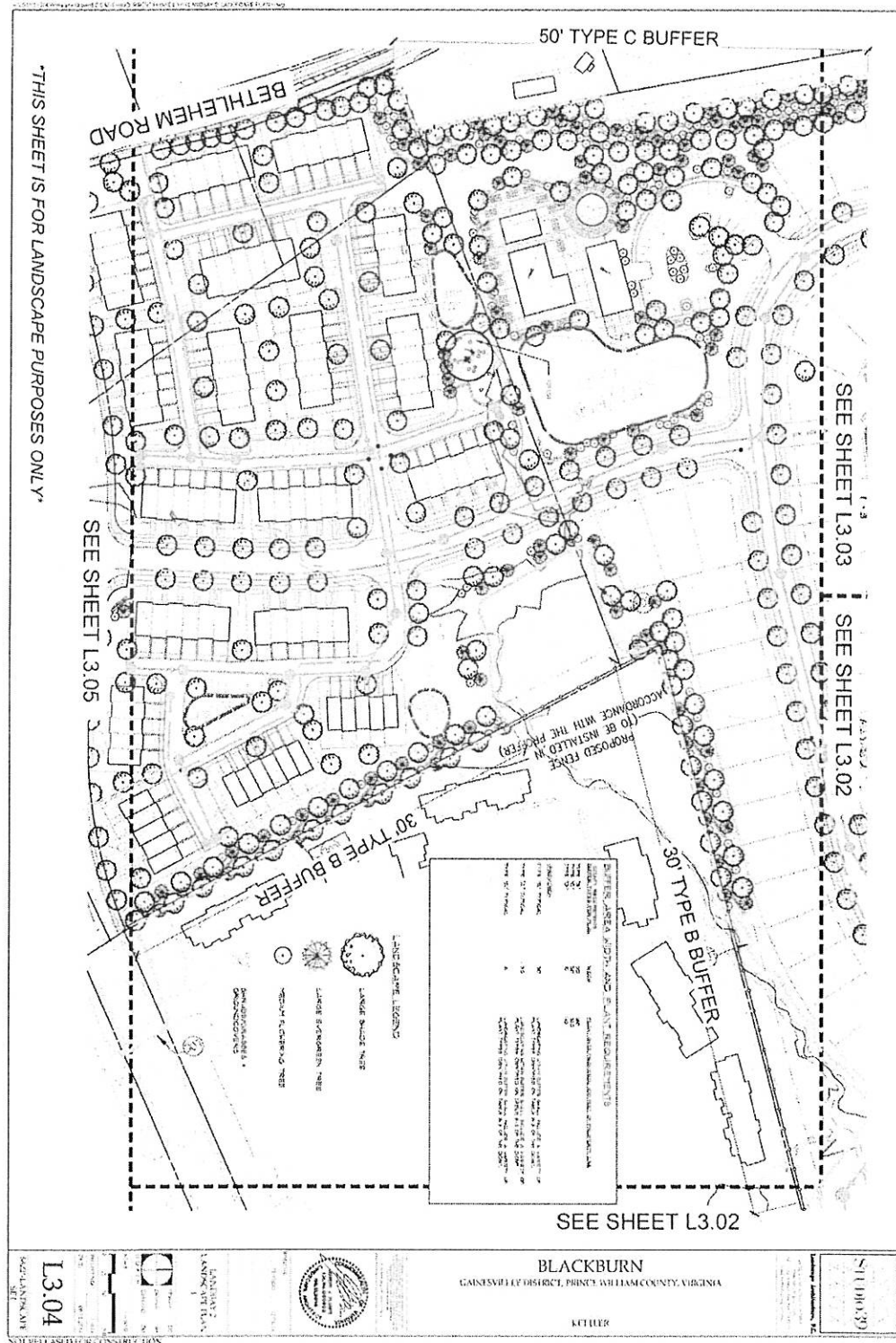




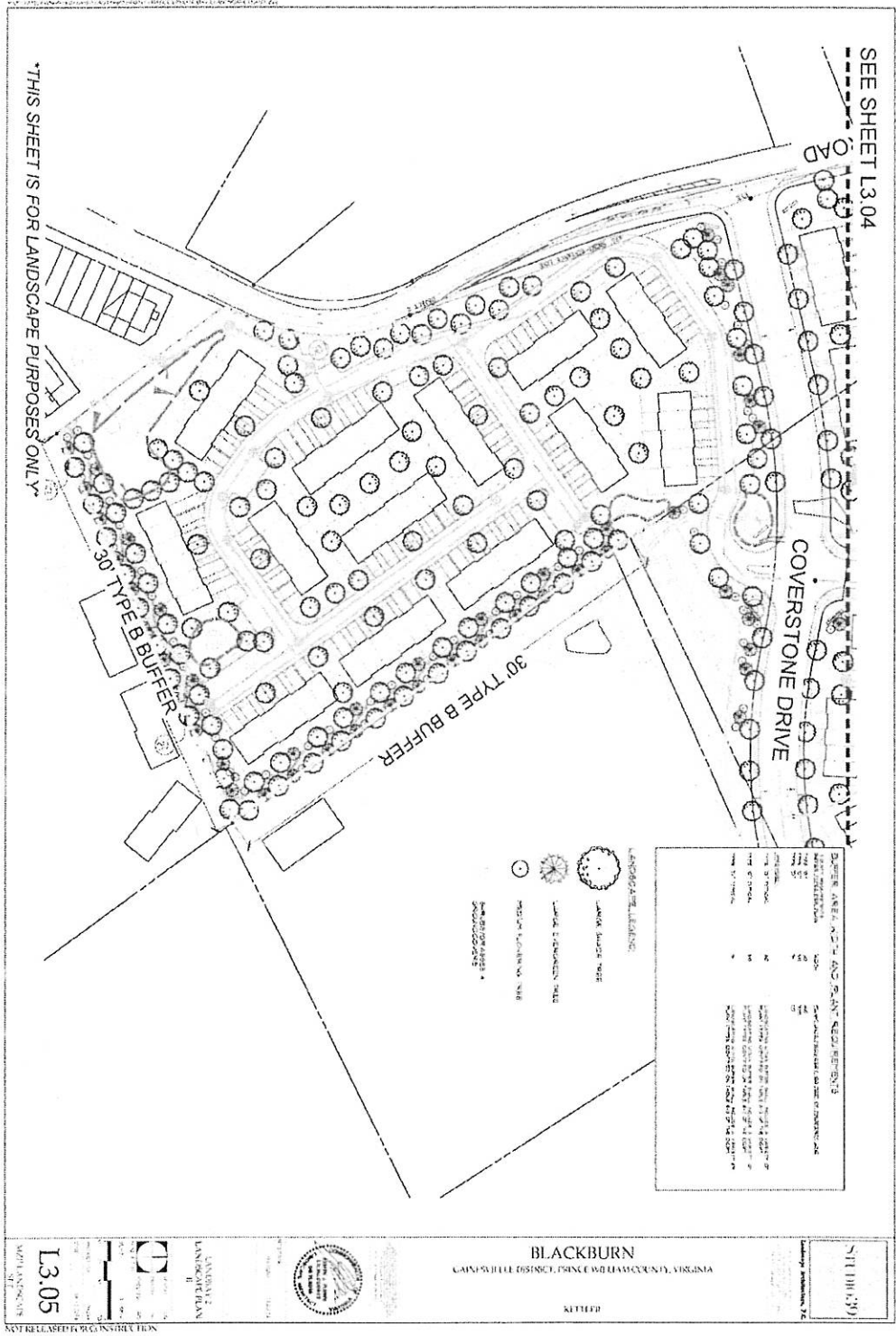
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Attachment D – Landscape Plan





**DESIGN GUIDELINES AND
DEVELOPMENT STANDARDS - 5/27/14**



BLACKBURN

ILLUSTRATIVE MASTER PLAN

All pictures contained herein are for illustrative purposes only and not to be construed as final design intent.

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INTRODUCTION

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22102 . 703.641.9000

Blackburn, Prince William, is a major employment center with office and professional services, supported by retail shops and restaurants. Residences complement these uses. Together they create a place where businesses and residents come together in a community with modern amenities and conveniences. Blackburn has the opportunity to re-establish and define the qualities of community and sense of place. In order to achieve this goal, development guidelines are provided to ensure that basic urban design principles are followed as the master plan is implemented.

The guidelines begin with street standards and urban design standards, moving into streetscape and site improvements, such as screening and other cosmetic recommendations. Design guidelines are typically limited to cosmetic issues such as landscape improvements and architectural treatments. These elements become more effective when implemented with appropriate site development constraints. The following guidelines will therefore address urban design and planning concerns that are prime factors in creating community and sustainable development.

PROJECT OVERVIEW

Location: Blackburn is a 130 acre property (Prince William County GPIN 7597-92-1061, 7597-82-4068, 7597-82-7607) located at the intersection of Balls Ford Road and Ashton Avenue in Prince William County, Virginia. The property is bounded by Interstate 66 on the north and is surrounded by mixed uses including retail, multifamily housing, hotel and offices. The land is primarily flat and open with some small drainage areas.

Relationship to Surrounding Uses: Blackburn is envisioned as a mixed-use center of retail, office and residential spaces, creating a neighborhood with extended hours of activity. Strong residential neighborhoods are located to the south and east of the site. Blackburn will be an anchor for the community, creating an important link between these neighborhoods and the mixed use portions of the project. Furthermore, the joining of commercial, retail and residential uses will make this area a significant activity node within the county.

Project Description: Blackburn is envisioned above all else as a community: a place where people invest economically and, more importantly, emotionally. This involves creating a sustainable community that remains vital long after the original newness fades. Fifty years from now, the community will be a place where people want to live, work and invest. The community will serve as a core amenity and destination for residents throughout Prince William County.

As a mixed-use development, Blackburn will offer residents the advantages of an urban environment, a place where people can live, work, shop and play without having to get into their car. The proposed community offers an alternative to the typical suburban residential development, creating a diverse community where the local residents, workforce and general public may gather in a welcoming environment.

Creating a Pedestrian-Friendly Community: Blackburn is to be a pedestrian-friendly environment. This can be achieved in several ways. The community streets incorporate a continuous network of sidewalks and walkways, providing linkages to the community's different activity centers. The location and scale of streets, open space and buildings create a space that welcomes and engages the pedestrian.

Streetscape: Careful design treatment of streets and sidewalks is critical to creating a pedestrian-friendly community. A variety of streetscape details at Blackburn will cater to pedestrians. Canopy street trees will be

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regimented in plant strips separating the road from the sidewalk. Pedestrian lights will be an attractive daytime element while providing nighttime illumination. Furniture, such as benches, trash receptacles, and other elements will be placed throughout the community, inviting people to participate in the public spaces.

Creating and Preserving Open Spaces: Open space and park areas will help define Blackburn. Open space, both natural and hardscaped will run through Blackburn's core areas, and will be the focus and organizing feature of the community.

Natural green space will not only serve as a cornerstone for the community's design, but also as an activity center for residents and local businesses alike. The community amenities area will be a place to relax on a Sunday afternoon or to eat lunch on sunny days, and a location for community-wide special events such as a Fourth of July parade or and August picnic.

The stream valley in Land Bay 1 will be preserved as an existing natural space, and integrated into the more urban spaces. The juxtaposition of natural space and the architecture will help to create a variety of vistas and environments throughout the community.

Architecture: Blackburn's architectural design will be visually interesting. A mix of residential and commercial architectural typologies based on traditional architectural styles will be used throughout the community. In addition, a variety of architectural massing, interruptions to elevations and roof forms, and the introduction of bays, balconies, and porches will help individualize residential units and commercial buildings. A sense of architectural diversity will be achieved through the non-symmetrical placement of design elements and the use of a broad pallet of building materials and colors.

Buildings will be placed close to sidewalks, with small front and side yard setbacks, to create a continuous urban edge along streets. The smaller setbacks provide views of the neighborhood and define the public realm of streets with the result of engendering a sense of community at Blackburn.

The Design Guidelines are intended to describe a general level of conformance and will be enforced by the Design Review Committee (DRC). The Design Guidelines may be modified or waived by the DRC to the extent necessary to administer the guidelines in a manner consistent with good engineering practices and the design objectives of traditional neighborhood design themes outlined here.



Section One: Land Bays and Use Group Designations:

Blackburn is organized into five development parcels (land bays). Each land bay has its own character and mix of uses based on location, parcel size, and proximity to Interstate 66, Balls Ford Road, Ashton Avenue and open space. Within many of the land bays, multiple uses are permitted to provide Traditional Neighborhood Development (TND) principles.



LAND BAY 1 contains 54 acres of single family homes, open space and community amenities. There shall be no direct access to Balls Ford Rd. Land Bay 1 is accessed from Hollis Drive, Ashton Avenue and Coverstone Rd. Due to its prominence along Balls Ford Rd, Land Bay 1 will incorporate streetscape and landscape treatments that will include a varied sequence of masonry walls and piers, iron fencing, berms and landscaping.

PWC Zoning Code Classification

- PMR, LDR

Planned Uses

- Detached Residences (SFD)
- Community Open Space
- Community Amenities

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-

Guiding Principles for SFD Architecture – Land Bay 1

- Building architecture should be of traditional styling and proportion.
- Building façade(front) should be primarily masonry or stone materials and limited to two materials.
- Building façade(sides) should be limited to two materials.
- Building massing and landscaping should relate strongly to adjoining land bays.



- Building facades to be varied and articulated for pedestrian visual interest.
- Universal Design should be considered.
- Roofscape should be delineated by gables or other architectural features.

- Roof pitches should be a minimum of 8/12.
- Single story porches, bay and bow windows are encouraged. Two story porches, porticos or entryways are prohibited. Turrets are prohibited.
- Round top, palladium and arch windows are prohibited on any building front.
- Exposed sides of units and foundation walls should be clad in brick or stone.
- Roof penetrations such as vents and stacks should be placed on rear roof slopes.
- Oversize or specialty windows are prohibited on any building front.
- If shutters are used on a façade, the size of the shutters shall be in proportion to the window.
- Two car garages shall be provided on all homes.
- Attic windows are prohibited.



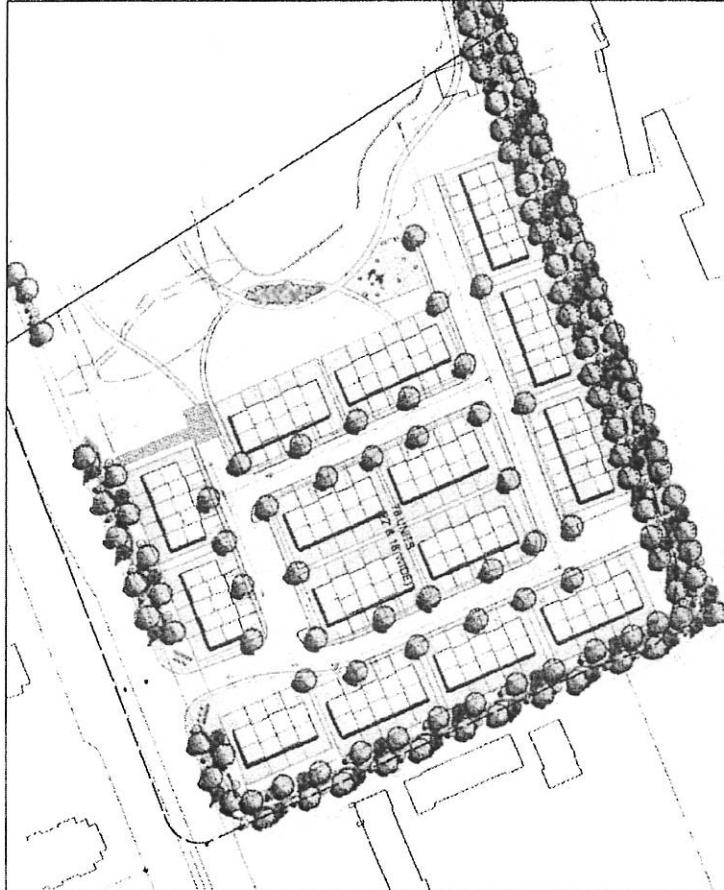
LAND BAY 2 contains 23 acres of townhomes, open space and community amenities. It is accessed primarily from Bethlehem Rd and Coverstone Drive. The design honors urban design principles by facing many units close to main streets with rear entry garages.

PWC Zoning Code Classification

- PMR, HDR

Planned Uses

- Attached Residences (SFA)
- Community Open Space
- Community Amenities



LAND BAY 3 is 10.5 acres of townhomes, open space and community amenities. It is accessed from Ashton Avenue. A great open space, ample visitor parking and tot lot are incorporated into the land bay.

PWC Zoning Code Classification

- PMR, HDR

Planned Uses

- Attached Residences (SFA)
- Community Open Space
- Community Amenities

Guiding Principles for SFA Architecture – Land Bay 2 and 3

- Building architecture should be of traditional styling and proportion
 - Building façade(front) should be primarily brick or stone and limited to two materials.
 - Building façade(sides) should be limited to two materials.
 - Building massing should relate strongly to adjoining land bays
 - Building facades shall be varied and articulated.
 - Universal Design should be considered.
 - Roofscape should be delineated by gables or other architectural features.
 - Roof pitches should be a minimum of 8/12.
-



- Single story porches, bay and bow windows are encouraged. Two story porches, porticos or entryways are prohibited. Turrets are prohibited.
- Round top, palladium and arch windows are prohibited on any building front.
- Exposed sides of units and foundation walls should be brick or stone.
- Roof penetrations such as vents and stacks should be placed on rear roof slopes.
- Oversize or specialty windows are prohibited on any building front.
- If shutters are used on a façade, the size of the shutters shall be in proportion to the window.
- One or two car garages shall be provided on all homes.
- Attic windows are prohibited.

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LAND BAY 4 is 12 acres of approximately 130,000 square feet of commercial space. The corner location at Balls Ford Road/Ashton Avenue affords excellent visibility for many community serving commercial uses. Access to this land bay is from Ashton Avenue.

PWC Zoning Code Classification

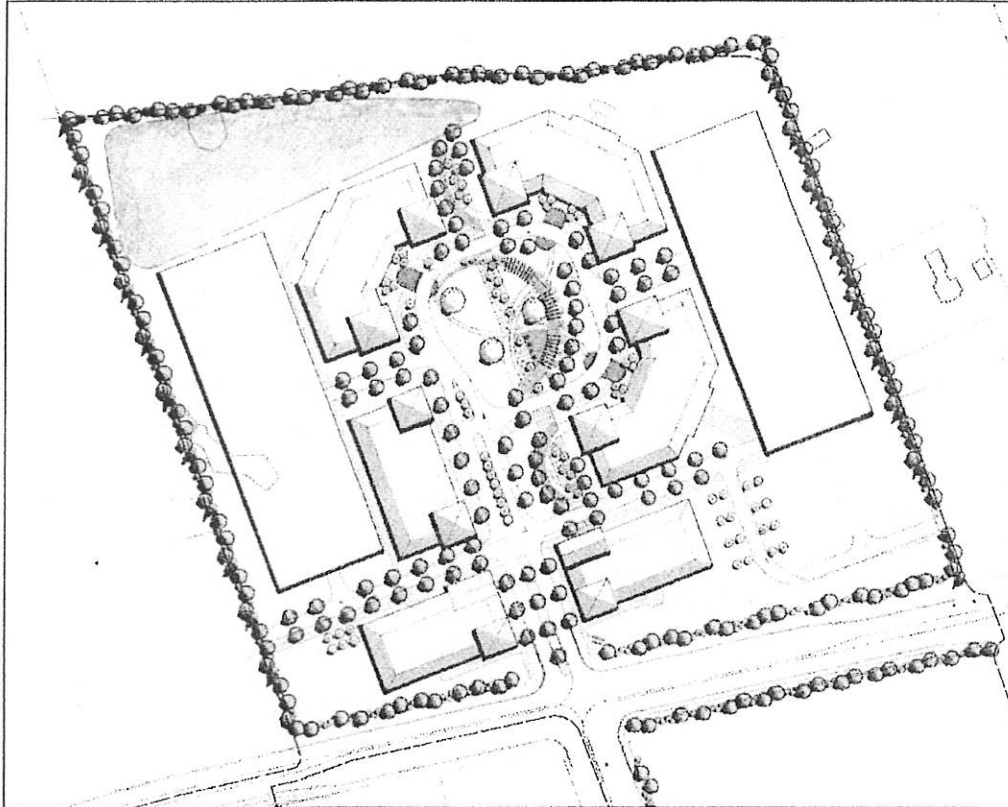
- PMD-B-1

Planned Uses

- Commercial enterprises.
- Open Space.

Guiding Principles for Commercial Architecture – Land Bay 4

- Building architecture should be respectful/complementary of traditional styling and proportion in other land bays, while allowing individuality for corporate identities/architectural themes.
- Building façade (front) should be primarily masonry or glass and limited to three materials.
- Building façade (sides) should contain masonry and limited to two materials.
- All rooftop mechanical equipment will be screened from view
- Loading and trash receptacle areas shall be screened from view.



LAND BAY 5 is 29 acres with up to one million square feet of office, ground floor retail and parking structures. It is accessed from Balls Ford Road. The land bay is designed around a major central green which contains both hard scape and landscape features to activate the green. There are six buildings ranging from three to six stories. Lower buildings are planned along Balls Ford Road to transition from the land bays across the street then increasing in height toward Interstate 66 to take advantage of the highway exposure.

PWC Zoning Code Classification

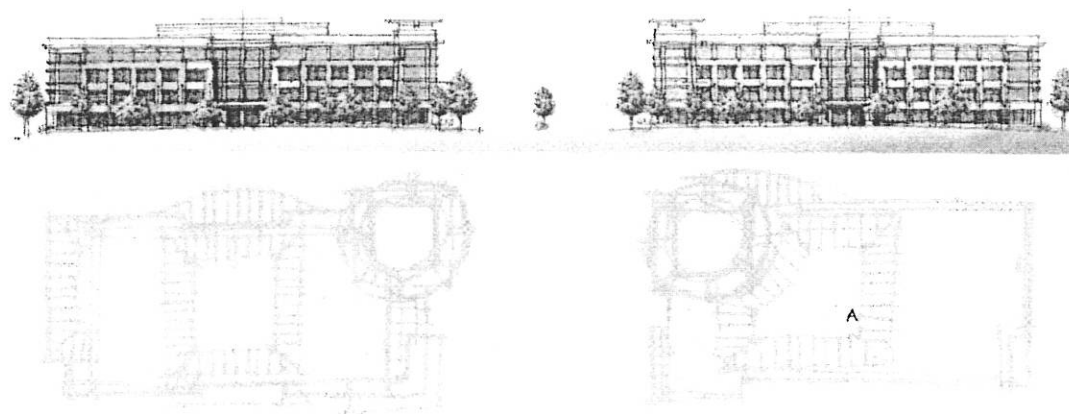
- PMD, O(H)

Planned Uses

- Professional offices.
- Ground floor retail uses.
- Central Green

Guiding Principles for Commercial Architecture – Land Bay 5

- Building architecture should be of quality design by an architect who regularly designs office buildings.
- All building façades shall be brick, precast or masonry and glass.
- Building skin should avoid exterior reflective materials and mirrored glass.
- Building facades should be varied and articulated, but complementary.
- Exterior Insulation Finish Systems (EIFS) are not permitted.
- All rooftop mechanical equipment will be screened from view.
- Loading and trash receptacle areas shall be screened from view.



SECTION TWO DRC Approval Process

Anyone wishing to build in Blackburn shall submit required materials (see below) to the DRC to initiate administrative review. Once materials for review are accepted by the DRC it will take between ten (10) and fifteen (15) business days to process an application for administrative approval. If DRC determines that changes need to be made a comment letter will be sent to the applicant notifying them of the elements of the project that do not meet the intent of the Design Guidelines and Development Standards. Once the project has received final administrative approval an approval letter will be mailed informing the applicant that the project has been approved.

Materials Required for Administrative Review - New Construction:

- Scaled site plan depicting the location and footprint of the proposed structure.
- Scaled elevations depicting all façades of the proposed structure. Elevations must indicate the building's dimensions as well as the dimensions of all known windows and doors, building materials and other information deemed necessary by the DRC.
- Color samples for paint or any pre-finished materials such as fiber cement.
- Material samples for the walls and roof.
- Any additional information deemed necessary by the DRC

Materials Required for Administrative Review - Signage:

- Site plan that depicts the location of the sign if it is a freestanding sign.
- Elevation drawing depicting the portion of the building where the sign will be mounted, with the sign shown on the elevation.
- Scale drawing that includes the method of support, sign dimensions, and design. The sign design must show the text style and size, logo, ornamentation, and border.
- Scale drawing that demonstrates the method of mounting for all signs that will be attached to Buildings.
- Color rendering or chips to indicate the sign color scheme.
- Illustration showing the method and details of illumination, if applicable.

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Amendments to Previously Approved Project

An applicant may need to make changes to a project following administrative approval. All amendments shall undergo the administrative approval process, unless DRC deems that the change is non-substantive. Non-substantive amendments will generally take between five (5) and ten (10) business days to process. Following non-substantive administrative approvals the applicant will be mailed an approval letter.

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Section Three: Street Standards

Blackburn is envisioned as a pedestrian-oriented community, with a street framework will support a wide range of land uses, and create a public infrastructure that encourage pedestrian activity, street life, and a sense of community and place.

Overview:

Streets will be connected to distribute traffic evenly throughout the community. They will be organized into a private street system, which will be built and maintained by Blackburn. Blackburn's private roadways interconnect into a "loop" through the community. All alleys and parking lots are private.

Street Standards Include:

Alleys: Alleys occur primarily within development blocks where townhouses are located. The use of alley fosters an urban landscape by providing an alternative to the driveway interruptions and garage doors that often characterize residential architecture along the street edge. Furthermore, alleys minimize the hazards of vehicles moving across sidewalks.

Curb Cuts: Curb cuts are the entrances and driveways that interrupt the street curb line. Blackburn's master plan seeks to minimize curb cuts through the use of shared entrances and alleys that separate driveway traffic from normal street traffic. Minimizing curb cuts also improves traffic safety.

On-Street Parking: Located primarily in Land Bay 1, on-street parking will help create a traffic calming effect by minimizing speed. On-street parking enhances the sidewalk environment by providing a buffer between pedestrians and moving cars.

Turn Radii: Intersection and entrance drive radii dimensions will be kept to minimum sizes to reduce traffic speed and make pedestrian crossings less daunting.

Two-way Traffic: All streets will accommodate two-way traffic.

Utilities: Where possible, and all utility lines shall be underground. Additional information on individual utilities and their location within Blackburn will be developed during final engineering.

Section Four: Urban Design and Architectural Guidelines

Building locations close to streets defines the public realm, strengthens the pedestrian environment and creates a sense of space.

The following principles address critical urban design issues, which will guide the design of Blackburn:

Lot Size and Configuration: Lot sizes and shapes are designed to encourage an efficient and intense use of land.

Setbacks: Building front yard setbacks dimensions will be minimized to increase the sense of enclosure and urban community. Setback dimensions will provide adequate room for sidewalks, streetscape improvements and, where appropriate, private landscape improvements.

Buildings: Buildings should exhibit a commitment to quality of design and materials. Each building should be designed to complement the character of its neighbors and be sensitive to their materials, color and scale. Careful design and material considerations will help develop a distinctive overall community. The purpose is to develop standards which recognize the importance of the collective impression.

Architectural Massing: Buildings will exhibit variety in their massing, height, projections and recessions. Where appropriate, asymmetry in design is encouraged, to provide visual vitality to the community setting. Blackburn seeks to achieve an urban design that suggests multiple settings but also reflects participation in a single over-arching theme.

Visual interest and architectural diversity in residential land bays should be considered through the use of building massing elements such as balconies, porches, dormers and bay windows areaways.

Roof Massing: An important part of the impression architecture can provide is roof massing. Roof forms enhance the sense of scale and interest, as well as providing a screen to mechanical equipment. Interruptions and changes to roof forms will provide visual interest and can create a sense of opening towards the sky.

Facades: Long building facades shall provide incremental setback variations with a variety of facade designs, such as balconies, porches and bay windows, to create shadows and visual interest.

Side and Rear Elevations: Side and rear elevations should have the same design elements and materials as the street facade.

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Corner Elevations: Elevations of buildings facing corner streets should receive the same architectural treatments, design, materials, and colors compatible to the front facade.

Building Orientation: Building orientation should generally be perpendicular and face directly onto the street.

Building Materials: The use of a variety of architectural materials and colors is encouraged, but should reflect a single, over-arching design concept for the community. In general, no more than two colors or materials should be used on each building.

Color: Black or appliance white shall not be used as a predominant exterior color. Garish or fluorescent colors shall not be used. Accent colors may be used to complement the building color and may be applied to window mullions, cornices and other architectural elements.

Building Entrances: Primary building entrances should face the street, with necessary secondary entrances serving the side and rear elevations, parking and rear yards.

Window Openings: Window openings help to create a friendly environment and are critical in establishing a building's architectural character and proportions. Windows should be encouraged on all elevations, including those facing parking and service areas, when feasible.

Glazing: All street level exterior windows will use clear glass, and highly reflective glass will not be used.

Mechanical/Electrical Equipment: Rooftop and building mounted equipment should be screened from view in a manner that is architecturally compatible with the building design. Antennas, towers or satellite dishes of any kind should not be erected without prior approval from the Design Review Committee.

Service Areas: Dumpsters and loading areas shall be located away from roadways towards a rear parking storage area. Service bays and dumpsters shall be screened from view of adjacent roadways, sidewalks and trails either with dense evergreen landscape materials or with masonry walls, 6' to 8' high, matching the adjacent architecture.

Section Five: Streetscape Standards

The implementation of a continuous streetscape will visually organize Blackburn, and link areas and architectural elements.

Streetscape guidelines will address improvements between the street and right-of-way line, and elements include sidewalk, landscape, lighting, utilities, signs and other items. Through simple improvements and organization, this relatively narrow area can improve the quality of the community.

Sidewalks and Walkways

Crosswalk: Demarcation of crosswalks should be provided at major street intersections.

Sidewalks: Sidewalks will be provided on both sides of all internal streets. The walk should be minimum of 5' wide, concrete and separated from the curb by a green area where appropriate. Sidewalks should be positioned straight and parallel to the roadway curb.

Walkways: Access to building entrances should be provided by walkways adjacent to or around buildings, from parking areas and/or the sidewalk network. Walkways are designed as part of the individual buildings and should support pedestrian accessibility among the buildings.

Street Furniture, Materials and Trees

Street Trees: Both sides of street should contain street trees occurring in a continuous and regimented fashion. Canopy trees should be placed between the sidewalk and curb in a (planting zone), lawn area. Only high canopy trees should be used to improve visibility, security and ease of maintenance. Once established, trees should be limbed from 12-15' above grade. Avoid using flowering trees with low branching habits and evergreens within the right-of-way.

Streetscape Furniture: The whole of Blackburn should have consistent streetscape furniture, including, but not limited to, benches, trash baskets, and kiosks. Elements should be strong and durable, as quality will provide savings over the life expectancy of cheaper fixtures. A single color theme and type will be adopted for all streetscape fixtures and furniture. Specific styles of streetscape furniture shall be submitted for approval by the DCR prior to site plan application.

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Special paving materials, sculptures, water features, banners and flags can be used for visual interest and to create memorable images.

Walls, Fencing and Screening: Where parking and rear yards occur along major roadways, walls, fencing and screening shall be encouraged. Permanent chain link fencing is prohibited.

Streetscape Lighting

A single design-family of light fixtures and poles will be provided within Blackburn. Uniformity of fixture and pole design, color and compatibility with other site furniture elements will help organize the landscape setting. This contributes to the sense of a quality environment in addition to providing a sense of security.

Street Lights: Street lights refer to light fixtures located along Balls Ford Road, Ashton Ave, Hollis Drive, Coverstone Drive, and street intersections. Public street lighting shall be provided as directed by the Virginia Department of Transportation. Other street lighting shall be provided on poles with a maximum height of 35' and stationed at intervals of 100' to 150'.

Pedestrian Lights: Pedestrian lighting refers to light fixtures located along sidewalks and pedestrian areas. Poles shall be 12' to 15' high and stationed at intervals of 40' to 60'.

Ambient Lighting: Additional nighttime interest and illumination may be provided by ambient lighting, such as up-lighting into trees and onto building elevations at key locations.

Parking Lots

Blackburn will provide adequate parking without sacrificing the ambiance of a pedestrian-scaled setting. The location of parking and its design treatment are planned to reduce the visual impact of parking for both the community and visitors passing through.

Parking Cells: Large surface parking lots shall be divided into smaller, visually contained areas through the use of perimeter landscaping. This will provide views and pedestrian ambiance. The typical size of a parking cell should range from 80 to 120 cars (for example, two double loaded isles, with 25 cars per side, equal 100 cars.)

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Landscaping: Parking lot landscaping should be simple in design and easy to maintain. Hearty canopy trees low evergreens and ground cover will achieve the required results. Planting areas should be a minimum of 6' to 8' wide. Canopy trees should be of a variety that allows basal pruning from 12' to 15'. Selected trees should be able to withstand summer heat rising from paving. Planting areas will be concentrated where they will have the greatest impact.

Security is an important consideration in landscaping a parking lot. A "view horizon" located 3' to 12' above grade should be kept open to allow surveillance. The location of low flowering trees and evergreens with low branching habits should be considered with regard to important surveillance corridors.

Lighting: Parking lot lighting should provide adequate illumination for security. Generally, a few taller fixtures are preferred to many smaller poles. The pole and fixture shall be coordinated with the design and color chosen for pedestrian fixtures.

Paving: Either bituminous or concrete parking is required for all drives and parking lots. Masonry pavers are an optional, decorative opportunity. Dirt, gravel and other unpaved surfaces are inappropriate.

Concrete Curbs: Concrete curbs, 6" to 8" high, will be required for all drives and parking lots. The use of timber wheel stops and asphalt for curbing will not be allowed because of appearance, short life span and vulnerability to trucks.

Striping: Striping will be required to identify all parking spaces.

Curb Cuts: Curb cuts shall be minimized by consolidating access points and providing mutual access easements where appropriate. Curb cut widths shall be a maximum of 24' wide. Curb cut entrances to parking lots shall be treated with landscaping and lighting, where appropriate, to enhance traffic safety and visitor orientation.

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Section Six: Open Space

Blackburn will include a green infrastructure of managed and natural areas, and provide an appropriate and continuous landscape setting accessible to the whole community.



Open space park Land Bay 1 southeast corner

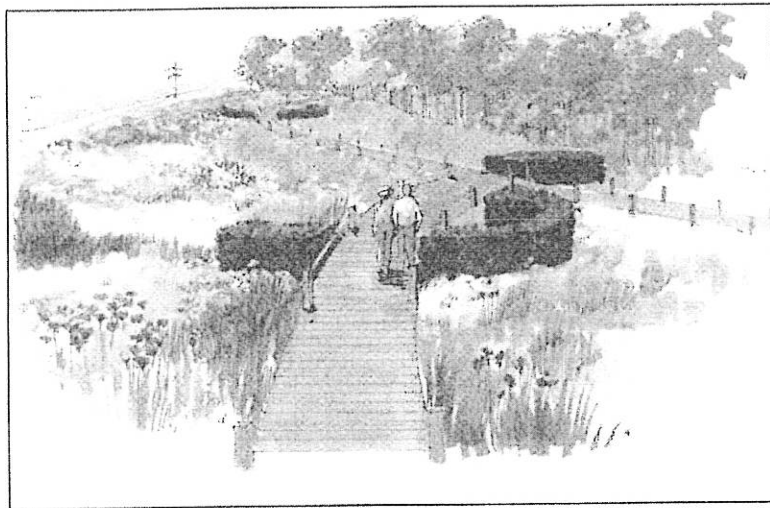
- Tot lots
- Paths
- Natural areas

Green Infrastructure: Managed and natural areas of green infrastructure that provide an appropriate and continuous landscape setting shall be integrated into Blackburn.

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Landscaped Streets, Sidewalks and Trails: Various parks and open spaces will be linked with the community through landscaped streets, sidewalks and trails. Streetscape improvements are envisioned as a critical part of the community's open space.

Nature Areas: A focus of Blackburn will be nature areas. Natural vegetation and rolling topography are important assets that provide relief to the continuum of development. Where feasible, areas of existing vegetation will be preserved, along with existing wetlands. The palette of landscape materials throughout Blackburn will include indigenous plants.



Section Seven: Sign Standards

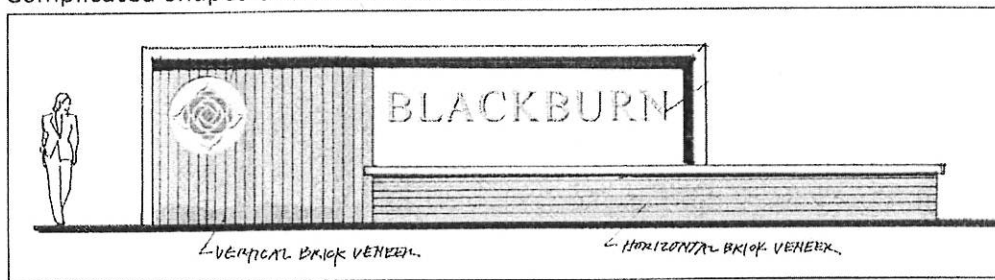
A sign has two primary functions: 1) to identify a business or place; and/or 2) to provide direction. Sensitive design and appropriate placement of signs contributes to a community's image and facilitates easy travel across streets and sidewalks. To successfully achieve this purpose, a sign must be eye-catching without being offensive.

All signs at Blackburn, Prince William should adhere to the following principles:

Materials: Materials will be architectural in character and compatible with the advertised business, and should be durable, weather resistant and maintained in good repair. Fabrication and installation of all signs should be by qualified and experienced mechanics. All electrical elements, such as wires, conduit, junction boxes, transformers, ballasts, switches and panel boxes shall be concealed from view. Lettering should be printed in known or recognizable styles and correctly executed. Moving signs or devices of any types should not be permitted.

Freestanding Monument Signs

Design of freestanding signs should be simple, architectural in appearance, and continued the design theme set into the adjoining architecture. There should be ample dimension, length and thickness to give the appearance of solidity. Signs should sit on grade or be attached to a singular base. Complicated shapes and silhouettes should be avoided.



Sign Location: Signs should be in proximity to the main vehicular entrance but outside the public right of way.

Number: Freestanding signs should be limited to one for each roadway entrance.

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Illumination: Signs may be both internally or externally illuminated, as appropriate to the setting and as permitted by the zoning ordinance.

Height and Size: Signs may vary in height and size, reflective of traffic speed and volume, but lower profile or ground level signs are preferred and should be used whenever possible. Height and size of signs should be limited.

Sign Copy: Information should be limited to the name of the business or place and other pertinent information related to the bona-fide business conducted on the premises. Advertising by material or product manufacturers and suppliers should not be permitted on any exterior sign except as the primary identification for the place of business.

Building Signs – A sign should not be lower than top of show window or 8' feet from sidewalk.

Building-Mounted Signs: If improperly located or out of scale with a building, building-mounted signs can cheapen the appearance of the establishment and lower the standards for surrounding developments. Signs and graphics should not dominate a building's architectural appearance. Signs should not be higher than 12" below the second story windowsill or top of one story building.

Location: Storefront sign locations should be monitored. Retail storefront signs should be prohibited on the fronts of buildings above the ground floor level, or above a height of one inch below the second floor window sill line, whichever is lower; on the roofs of buildings; on the side walls of buildings; or directly on the insides of show windows. Building signs should not be permitted on the roof or project above the vertical wall area of the façade.

Projecting Signs: Projecting signs can add vitality to the visual environment if they are in scale with the pedestrian. There should be only one projecting sign per shop and it should not exceed 12 square feet in area. They should not be located any higher than 12' or lower than 8' above grade, and should not project more than 4' from the building face. A sign attached to a building façade should not project above the wall area of the façade.

Flat Signs: Flat signs should be placed parallel to the face of the building and not project more than 12" beyond the primary surface of the building.

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Size: Signs should be in proportion to the storefront and the building façade as a whole. Signs should never be allowed to obscure or overwhelm the basic architectural character of the building. A ratio of two-square-feet of sign area for each linear foot of building frontage is a good rule of thumb in determining total sign areas.

Color: Sign should generally be limited to no more than three colors. Bright colors are entirely appropriate for signs, but the use of too many colors should be avoided.

Message: Words used on a sign should be limited to the name of the business and other pertinent information related to its operation; the sign should not be used for franchised product advertising. Simplicity is the key to legibility and elegance. Bold, simple lettering styles and the use of recognized symbols are most effective. Symbols and graphic shorthand can express more concisely the ideas which sign seek to communicate.

Illumination: Signs may be internally or externally illuminated, as appropriate to the specific setting. Signs with flashing or moving lights shall not be permitted.

Awning Signs: Lettering and symbols, or design painted or sewn onto awnings will be permitted.

Number: There should only be one sign for the front façade and one smaller sign for commercial uses with entrances from a side or rear elevation.

Public Signs

Because of their piecemeal implementation, proliferation and varied fabrication, public signs can clutter a community's appearance. Historically, signs have been placed without regard for their surroundings, freestanding or haphazardly attached to lamps and poles. A sign with excessive wording and too many letters also reduces effectiveness. The following rules will help organize public signs and enhance the visual environments.

Placement: Signs should be standardized by location and be placed far enough from the curb to prevent damage from cars and trucks. Signs should be placed so that they are not visual or maintenance obstacles.

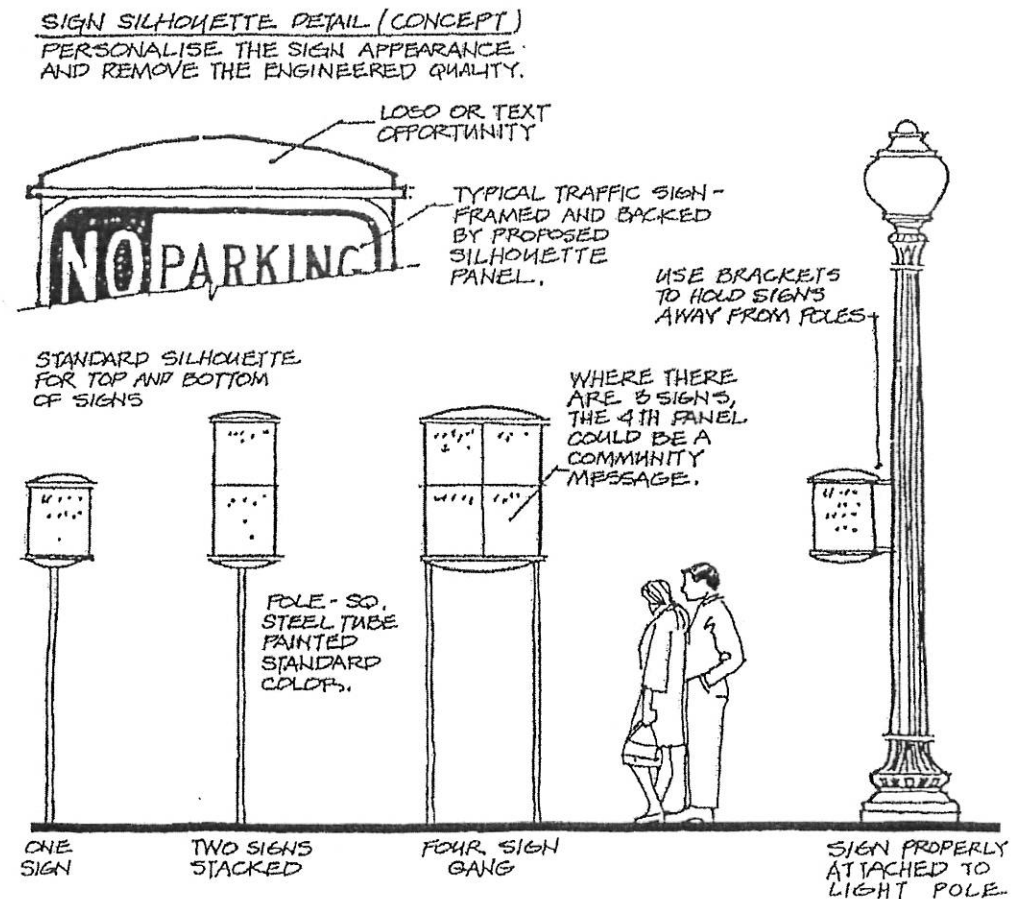
Number: Where possible, the number of signs and sign posts should be reduced and consolidated by pairing and grouping of signs.

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Poles: All signage shall have simple square or round straight steel poles (2" to 4" width, depending on height). The poles should be painted to match the color adopted for Blackburn's street furniture.

Signs must be attached to sign posts. If signs are to be attached to lamp poles, a bracket shall be used, which projects the sign away from the post. Do not bolt through the sign center into the post.

Signage Standards: Signage standards should be developed to distinguish corridor signs from the ordinary utilitarian appearance of public signs. Developing a pleasing silhouette for standard sign faces will soften the sign profile and provide a distinctive quality.



Section Eight: Landscape Standards

Landscaping is one of the rare site investments that improves with age and is therefore an important component in ensuring Blackburn's long-term viability as a community>

Three basic rules apply to all landscaping:

- Keep the design simple – landscaping should emphasize simplicity of design. Quality and durability are much easier to achieve with simple, straightforward designs.
- Use quality materials – quality materials age well, stand up to abuse and have a comparatively long life expectancy.
- Provide easily maintained installations – even the best design, implemented with quality materials, and is ineffective if it cannot be maintained.

The following considerations will be integrated in the design of Blackburn.

Color and Specimen Material: To be effective, color and specimen material must be used generously in a few key locations. Selecting a couple highly visible locations will be more effective than sprinkling color in small patches over the total site.

Massing of Plant Material: Large groupings of a single species are preferred to planting a large area with numerous species. Trees and shrubbery should be in scale with street dimensions.

Maintenance Edges: Maintenance edges provide an important sense of orderliness. Lawn areas should be separated from landscaped beds with edging such as concrete walks, steel edging and curbing.

Security and Surveillance: Plant material should be confined to 36" and lower, and tree canopy (tree limbs and leaves) should begin at 12" and above to allow for visual surveillance and a sense of security.

Landscape Materials

The following guidelines shall apply to the major categories of landscape materials in order to create the desired visual harmony among the individual development sites. All plant material shall be selected from varieties compatible with, or directly from, the native vegetation, and all shall be odorless and free of messy fruits and seeds. Species with chronic diseases, pest problems and/or structural weaknesses shall be avoided. The following categories are addressed:

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Deciduous Shrubs: Deciduous shrubs should be used as accents to create seasonal color interest. Due to their informal appearance, they should not be used in high image areas where a manicured formal presentation is desired. Deciduous shrubs should be maintained with a "natural" growth habitat and shall not be used as formal hedges. The minimum preferred size is 3'-4' in height.

Edging Material: Plant bed edging, mowing strips or other edging materials are encouraged. Acceptable materials include steel (black), concrete, stone and brick.

Evergreen Shrubs: Evergreen shrubs shall be selected from varieties of winter-hardy shrubs commonly found in the mid-Atlantic region. They may be used where a low-level screen or hedge is desired, such as parking lots, sidewalks or as mass plantings in the landscape; they may also be used as effective ground covers on slopes. The minimum preferred size is 18" to 24" spread.

Evergreen Trees: Evergreen trees shall be used at strategic locations, and designed into group plantings to enhance "winter" seasonal interest, screen objectionable views of service areas and parking, and act as a backdrop for ornamental (flowering) trees. Evergreen trees shall be selected from species native to the area or similar in appearance. The minimum preferred size is 8'-10' in height.

Indigenous Plant Material: Indigenous plant material shall comprise a significant portion of all new landscape improvements.

Ornamental Trees: Normally 12' to 25' tall at maturity, ornamental trees should be planted for accents and visual emphasis. The flowering habits of ornamental trees are very effective.

Streetscape & Canopy Trees: Used between the building zone and the street edge, streetscape and canopy trees shall be major deciduous trees. They may be located either within sidewalk planters or in planting zones between the sidewalk and curb. Canopy trees shall be used in parking islands and lawn areas, where they do not interfere with the design intent of streetscape plantings. The minimum preferred size is 3"-3 1/2 caliper.

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Turf: Lawns and other turf zones within the street right-of-ways and other development areas may be seeded, or planted with sod as an optional substitute. A standard turf shall be established for the entire project, which may be modified by the Design Review Committee. Where sod is employed, it shall be of a quality meeting the certification requirements of the Virginia Association of Turf Growers.

Landscaping Practices

Planting Practices: Planting practices should be standardized. All plants should be nursery grown in accordance with the highest standards of horticultural practices, and grown under climactic conditions similar to those of Blackburn. Plants shall be typical of their species or variety with normal growth habits. Plants shall be disease-free and have healthy, well-developed root systems. All plants shall conform to the American Standard for Nursery Stock as published by the American Association of Nurserymen.

Landscape Maintenance: Green space landscape maintenance in Blackburn will be prescribed for both individual landowners and the Owner's Association in the Declaration of Protective Covenants.

Individual owners shall be responsible for executing a landscape maintenance program for landscape areas within their personal site. The program shall include:

- Pruning trees and shrubs to maintain an attractive shape, removing dead branches and providing clearance for vehicles and pedestrians.
- Replacing dead and disfigured plant material immediately with equivalent plants.
- Fertilizing, mulching, watering and weeding plant beds. Applying insecticides and fungicides as necessary to maintain plant vigor and appearance.
- Lawns shall be watered, mowed and maintained in a dense, weed-free condition.

Screening

The goal of screening is to visually integrate and, where necessary, screen those elements that, though essential, can become unsightly.

There are components to any development that should be screened from public view. A majority of these elements, such as services and utility functions, can be hidden through a selective location out of easy view by the visitor. The objective is to locate an element where the fewest number of people see it for the least amount of time. Relocation of the object to an area out of the public's view should be the first preference, especially for service and utility elements that are visible from the standard streets. Elements such as dumpsters, services and storage units should not be seen from the standard streets.

There are, however, elements and activities, which are necessarily located in the public area. Visually integrating and meshing the service or utility component into the public environment is the goal of screening, not hiding or camouflaging the element. For example, the additional attention created by trying to conceal an electrical transformer may be worse than the unscreened appearance. Screening is a last resort, but a very functional resolution when appropriate.

Full Screening: Areas where, if exposed, would require constant maintenance should have full screening. Screening design and durability are important. Solutions such as chain link or wooden fencing may be inappropriate if they will fall into despair. Dumpsters require full screening when the refuse is visible and soils the dumpster itself (i.e. restaurants). Full screening, however, should be designed to require little maintenance, and be compatible with architectural and site development themes.

Partial Screening: The introduction of landscape and design elements as partial screening provides a setting and sense of upkeep without the necessity of fully hiding the element or activity. Partial screening serves the same purpose as foundation planting around a building—it provides a setting and visually anchors the ground plant.

Low Screens: The most effective treatment of large commercial parking lots is with low screens (max 42" height). This is preferred to full screening (72" to 96" height) for both aesthetic and security reasons.

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No Screening: Service elements which are only marginally obtrusive require no screening. Some utilities and surface elements may be less of a blight if they are not decorated and thus attention is drawn to them. A meter box, which is not close to a sidewalk, may only require a coat of color to blend with its background, whereas a fussy landscape would draw unnecessary attention and memorialize it.

Dumpsters: Full screening is required for dumpsters, providing a sense of maintenance to the property. Loading and other service areas should be located where least visible, but not require full screening unless the dock area is excessively soiled, thus requiring constant maintenance. Generally, partial screening providing boundaries between public and service areas should be sufficient. This contributes a sense of maintenance, as well as screening foreground pavings, wheels and bumpers.

Transformers and Utilities: Transformers and utilities, which are not close to walkways should be painted a color to blend with the background instead of being accentuated with a landscape screen.

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Recommended Plant Material

Canopy Trees

Acer rubrum
Acer saccharum
Chionanthus virginicus
Cladrastis lutea
Fagus grandifolia
Ginkgo biloba · Ginkgo (male)
Gleditsia triacanthos var. inermis
Liquidambar styraciflua
Nyssa sylvatica
Oxydendrum arborium
Platanus occidentalis
Quercus alba
Quercus coccinea
Quercus palustris
Quercus phellos
Quercus rubra
Quercus shumardii
Taxodium disticum
Tilia tomentosa
Zelkova serrata

Ornamental Trees

Amelanchier canadensis
Cercis canadensis
Chionanthus virginicus
Cornus florida
Cornus kousa
Crataegus sp.
Halesia carolina
Lagerstroemia indica
Magnolia soulangeana
Magnolia stellata
Magnolia virginiana
Malus sp.
Prunus serrulata
Stryax japonicum
Ilex opaca
Pinus virginiana
Pinus bungeana
Pinus echinata
Tsuga canadensis

Red Maple
Sugar Maple
White Fringetree
American Yellowwood American
Beech

Thornless Honeylocust
Sweet Gum
Sourgum
Sourwood
Sycamore
White Oak
Scarlet Oak
Pin Oak
Willow Oak
Red Oak
Shumard Oak
Common Baldcypress
Silver Linden
Japanese Zelkova

Shadblow Serviceberry
Eastern Redbud
White Fringetree
Flowering Dogwood
Kousa Dogwood
Hawthorn
Carolina Silverbell
Crapemyrtle
Saucer Magnolia
Star Magnolia
Sweetbay Magnolia
Flowering Crabapple
Mountain Stewartia
Japanese Snowbell
American Holly
Virginia Pine
Lacebark Pine
Shortleaf Pine
Canadian Hemlock

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Evergreen Trees

Ilex opaca
Pinus virginiana
Pinus bungeana
Pinus echinata
Tsuga canadensis

American Holly
Virginia Pine
Lacebark Pine
Shortleaf Pine
Canadian Hemlock

Deciduous Shrubs

Berberis ap.
Forsythia x intermedia
Fothergilla gardenia
Hamamelis virginiana
Rhododendron Sp.
Rhododendron -sp.
Viburnum -s.p.

Barberry
Forsythia
Dwarf Fothergilla
Witchhazel
Azalea
Rhododendron
Viburnum

Evergreen Shrubs

Ilex sp.
Juniperus sp.
Kalmia latifolia
Mahonia aquifolium
Pieris japonica
Taxus sp.

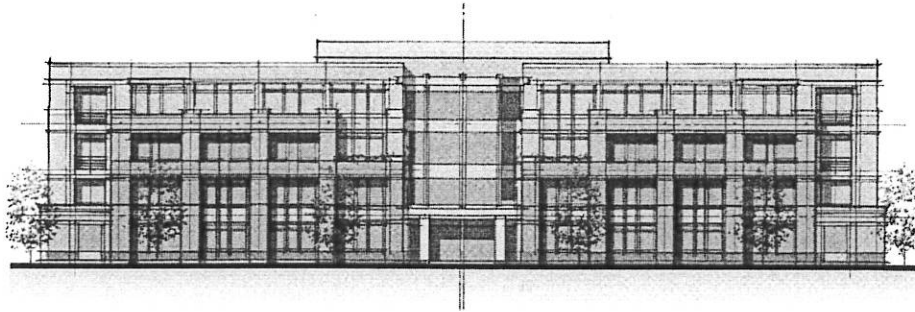
Holly
Juniper
Mountain Laurel
Oregon Grapeholly
Japanese Pieris
Yew

Ground Covers

Ajuga reptans
Hedera helix
Hosta sp.
Hypericum sp.
Liriope muscari
Liriope spicata
Pachysandra terminalis
Vinca minor

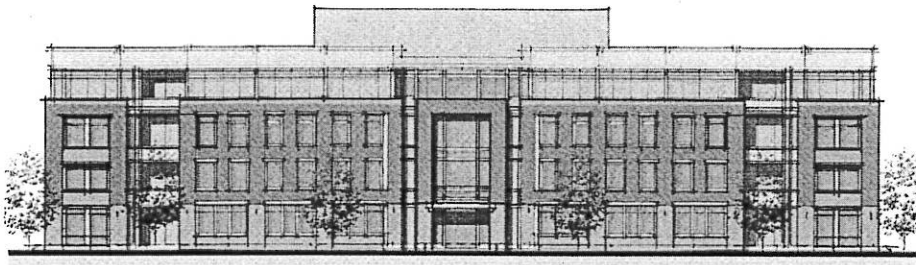
Buglewort
English Ivy
Hosta
Hypericum
Liriope
Spreading Liriope
Japanese Spurge
Periwinkle

Attachment F – Illustrative Plans



2 | Blackburn Office Park
Sheet 2 of 4

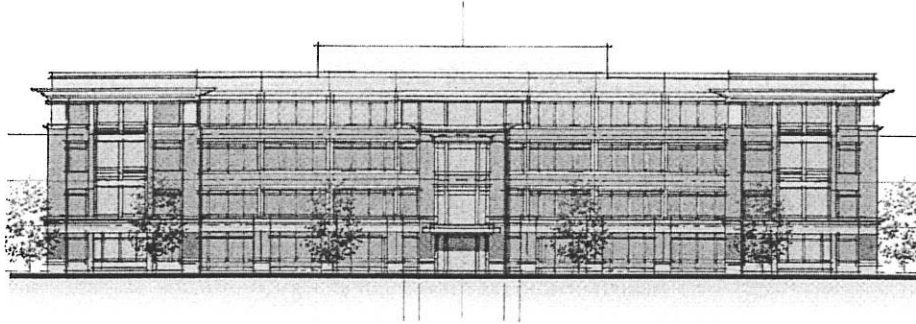
DESIGN CONCEPTS 12/17/2013



3 | Blackburn Office Park
Sheet 3 of 4

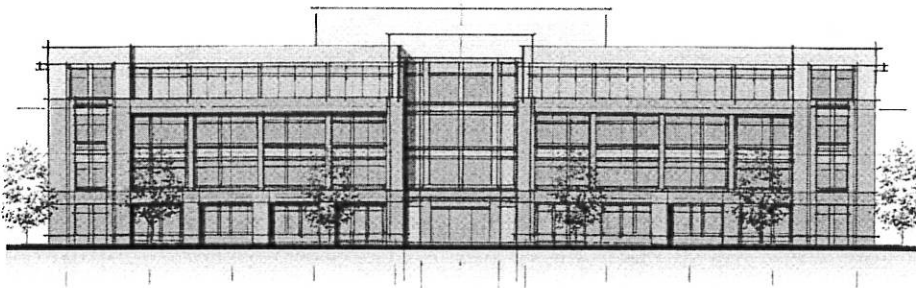
DESIGN CONCEPTS 12/17/2013

Attachment F – Illustrative Plans



4 | Blackburn Office Park
Sheet 4 of 4

DESIGN CONCEPTS 12/17/2013

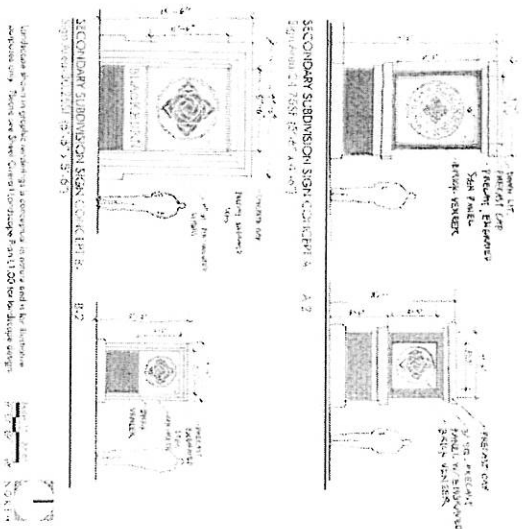


Attachment B



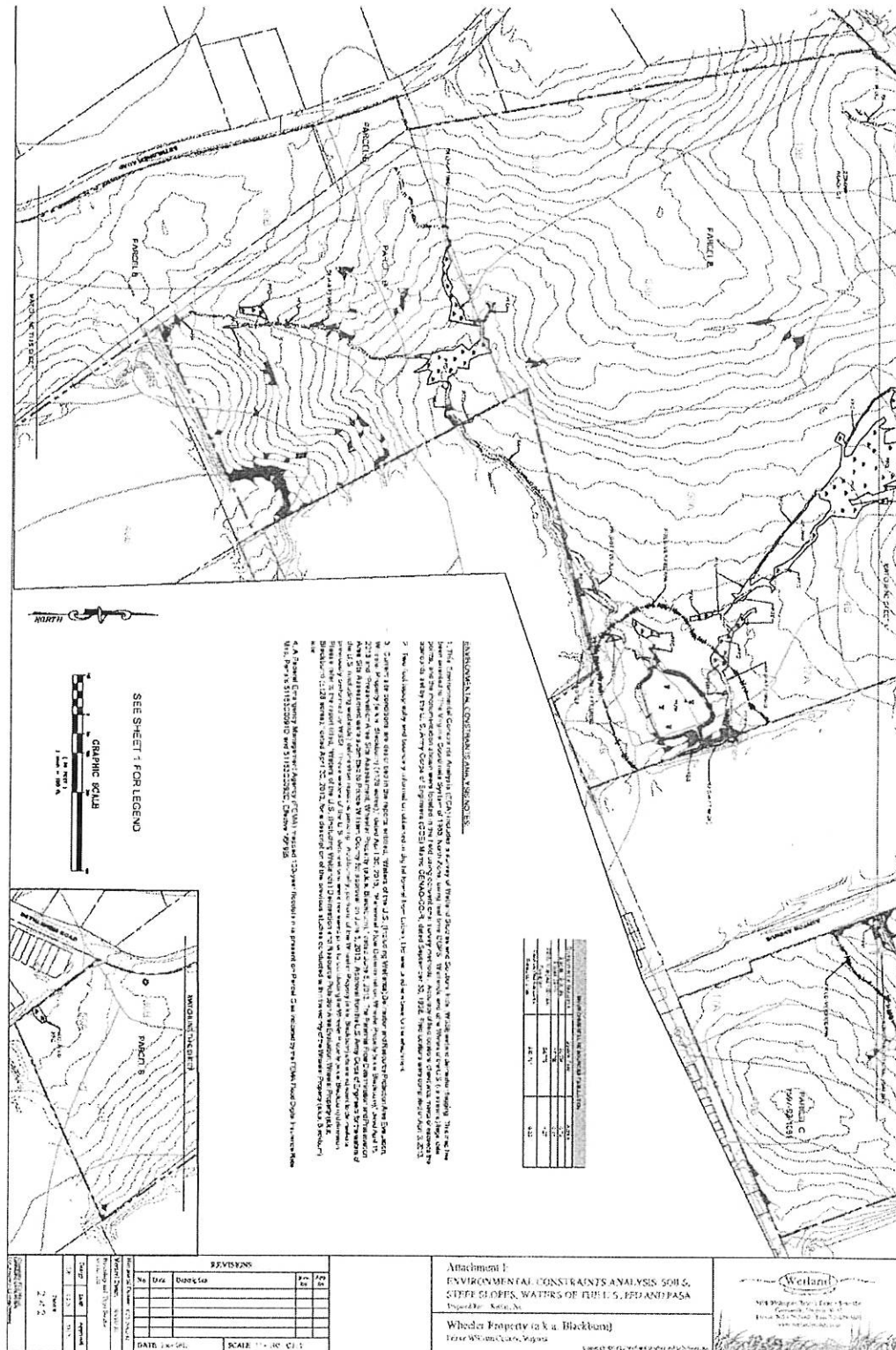
1 | Blackburn Office Park
Sheet 1 of 4

DESIGN CONCEPTS 12/17/2013

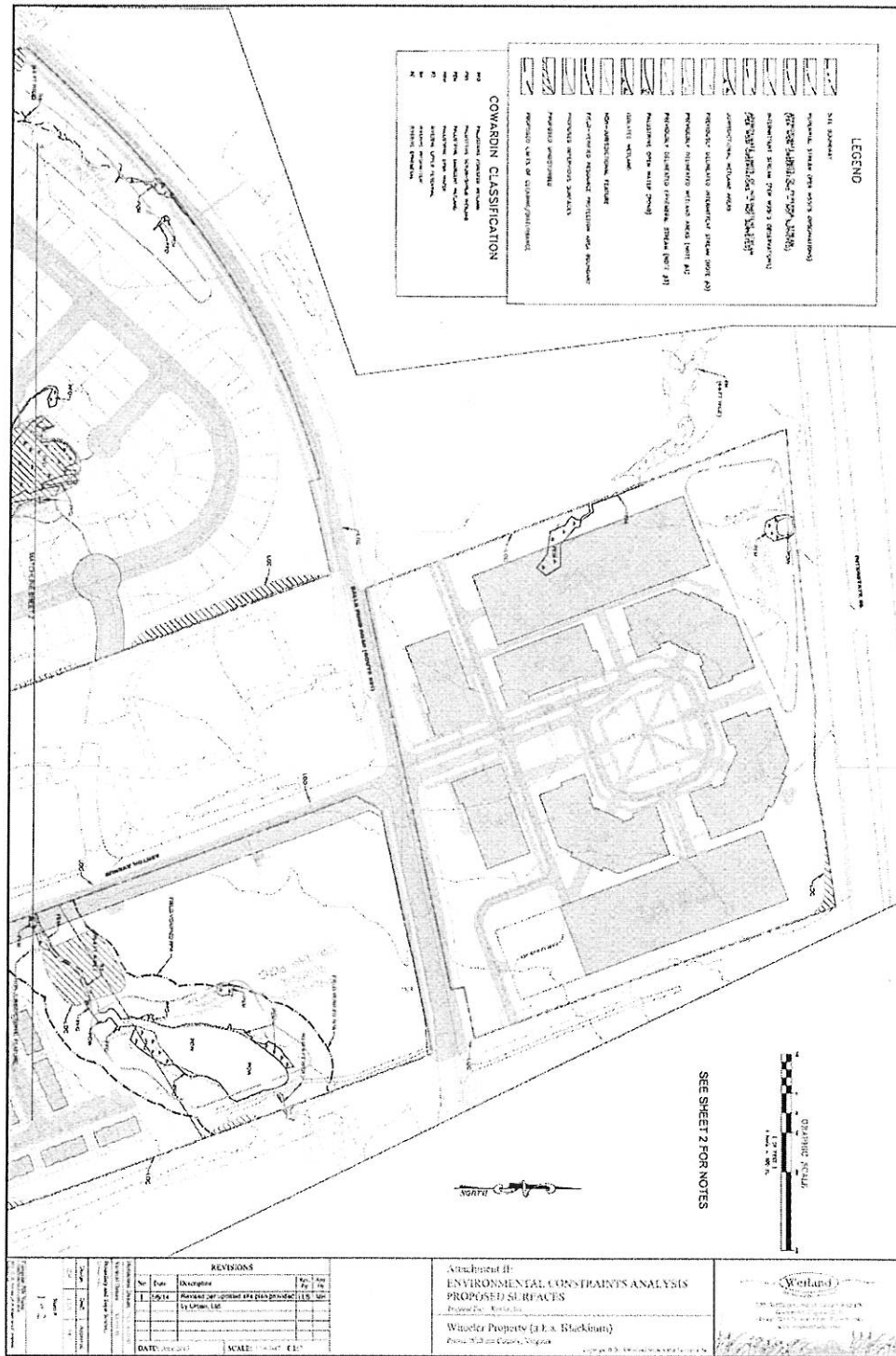
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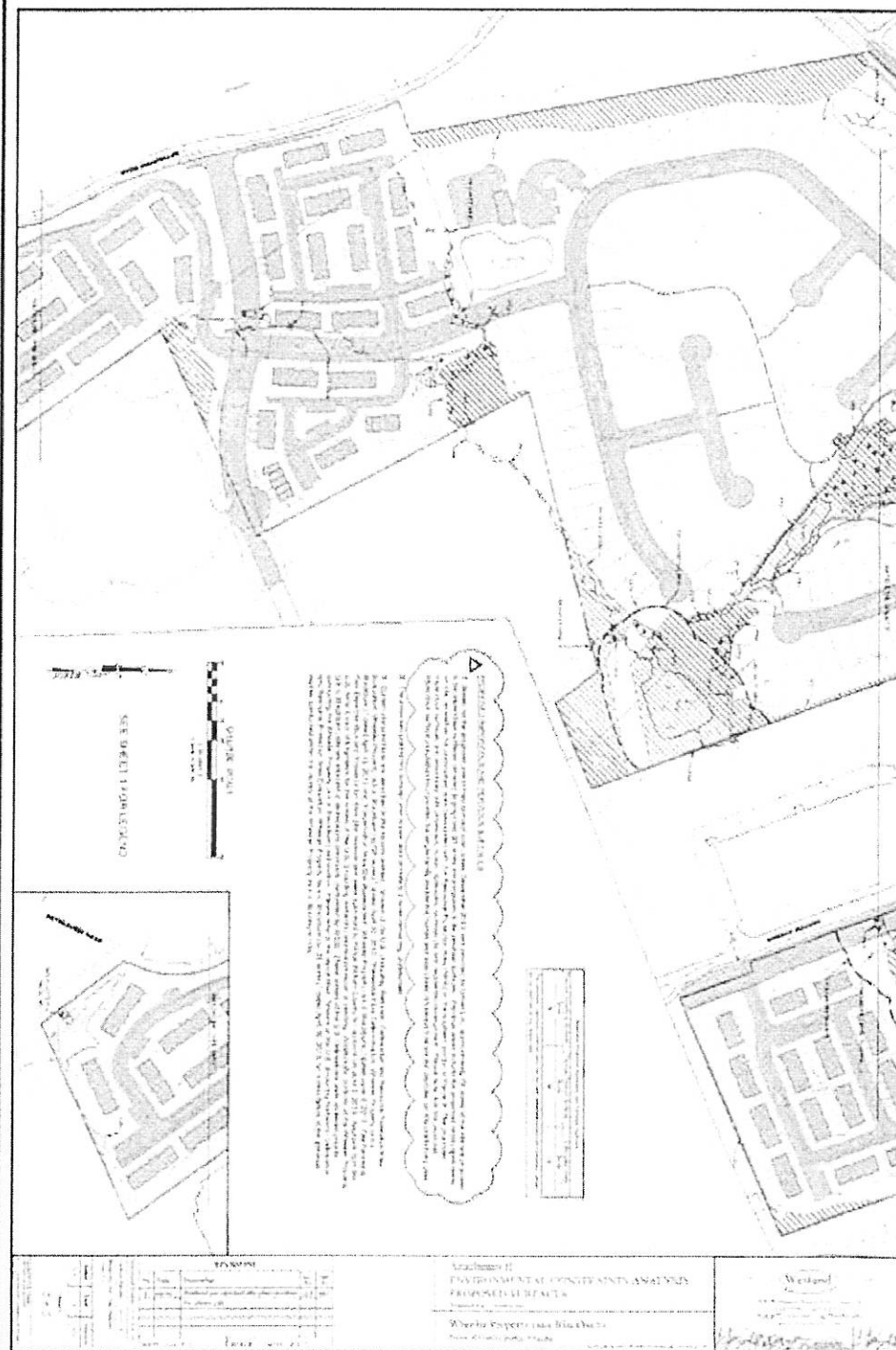
Attachment G – Environmental Constraints Analysis



Attachment G – Environmental Constraints Analysis



REZ #PLN2014-00040, Blackburn
Page G-4



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REZ #PLN2014-00040, Blackburn
Page G-6

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Attachment H – Military Site Scope of Work

Attachment C

PROPOSED SCOPE OF WORK FOR CONDUCTING MILITARY SITES (INTENSIVE METAL DETECTOR) SURVEY OF THE WHEELER (A.K.A. BLACKBURN) PROPERTY PRINCE WILLIAM COUNTY, VIRGINIA

April 22, 2013

Herein is presented a Scope of Work (SOW) for conducting an intensive metal detector investigation on the +/- 105 acre Wheeler/Blackburn property, consisting of three parcels located to the north and south of Balls Ford Road (Rt. 621) near Manassas, Virginia (Exhibit A). The property occupies a portion of the land associated with 'Willow Green,' the farm owned by William L. B. Wheeler during the Civil War. The project area lies within the American Battlefield Protection Program (ABPP)-defined Study Area for the First Battle of Bull Run (Manassas I) and within the Core Area for the Second Battle of Bull Run (Manassas II).

The investigation is being undertaken at the behest of Justin Patton, Prince William County Archeologist. Phase I archeological surveys were conducted on the property in 2003 and 2004 by Archeological Testing and Consulting, Inc. of Silver Spring, Maryland (Hill and Pfanstiehl 2004, 2005). The investigations identified within the project area one historic architectural resource, a 20th century dwelling and associated farm complex recorded as DHR 076-5137, and four archeological resources recorded as sites 44PW1557, 44PW1604, 44PW1605, and 44PW1606. Sites 44PW1557, 44PW1604 and 44PW1606 were historic resources associated with 20th-century occupation and agricultural use of the property. Site 44PW1605 was classified as an ephemeral prehistoric camp of unknown temporal affiliation. None of the resources were recommended eligible for listing on the National Register of Historic Places (NRIIP) or the Virginia Landmarks Register (VLR) and no further archeological work was recommended. No military artifacts were recovered during the investigation.

Because the property is in close proximity to the Manassas I and II battlefield, and associated troop movements and other military activities are believed to have taken place within the Wheeler farm, the Prince William County Archeologist requested that an intensive metal detector survey of the entire property be performed. Metal detector survey has been shown to be notably more capable of locating historic military sites compared to shovel testing, which comprised the survey method utilized during the ATC Inc. Phase I survey.

Due to the time and expense required for intensive metal detection for the entire property, Thunderbird Archeology, a division of Wetland Studies and Solutions, Inc. of Gainesville, Virginia conducted a documentary and background study to establish an historical context for Wheeler Farm during the Civil War and to ascertain whether portions of the site should be excluded from the metal detector investigation. As part of

Blackburn (A.K.A. Wheeler) Property - Intensive Documentary Research

WSSI #22245.01 – April 2013



Page 65

Attachment H – Military Site Scope of Work

the documentary and background study, staff at the ABPP and Manassas National Battlefield Park as well as local relic hunters were interviewed.

Metal Detection Survey: Based on the results of the documentary and background study, Thunderbird Archeology recommends that the portion of the project area lying north of and within 250 feet to the south of Balls Ford Road (Rt. 621) be subjected to intensive metal detector investigation (Exhibits B and C). These portions of the property were shown during the documentary research to have been the location of troop movements during the attack of Longstreet's Right Wing of the Confederate army in the afternoon and evening of August 30th, 1862 during Manassas II. Short-term artillery emplacements may have been located within the northern parcel near the Wheeler house which was located immediately to the east; this area may also have been subjected to artillery bombardment. Other information indicates that a Union commissary or storehouse may have stood east of the project area near the Wheeler house at some time during the war. Relic hunting on the property has resulted in reported recovery of buttons, dropped bullets, and other artifacts that suggest the use of the northern portion of the property may have been occupied as a short-term camp site by Confederate troops; no Civil War-related finds have been reported from the southern two portions of the project area.

The metal detection survey will be conducted in accordance with 2011 Virginia DIIR guidelines. A 25-foot transect grid will be established across 100% of the indicated portions of the project area (less any areas of confirmed disturbance), and the survey will be conducted in a zig-zag pattern within each transect with six-foot instrument sweeps. The locations subject to metal detector survey are mostly in open field (see Exhibit C); if deemed necessary, the grass in these areas will be cut to prevent interference with the proper conduct of the metal detector survey. The methodology within the small portions of the recommended survey area that are wooded may be varied depending on density of vegetation. Positive contacts will be marked with pin flags, and additional intensive metal detection will be performed near contacts to ascertain the presence of other materials in the immediate vicinity. The positive contacts will be excavated in order to identify the historic ferrous or non-ferrous artifact and their location will be mapped. If an excessive number of positive contacts are identified, a suitable sample may be excavated in lieu of 100% excavation of contacts. A distribution map will be made to show the locations of the positive contacts and recovered artifacts. Any archeological sites located will be delineated in accordance with the find locations. Such sites will be accurately mapped.

If the metal detector survey results in the recovery of Civil war-era military finds in the portion of the survey area to the south of Ball's Ford Road (rt. 621), the survey area will be extended to include areas on the property within a 250 feet radius of the southernmost finds.

Laboratory: All historic (i.e. artifacts dating to circa 1865 or earlier and any incidentally discovered prehistoric) artifacts will be cleaned, inventoried, and curated according to

Blackburn (A.K.A. Wheeler) Property - Intensive Documentary Research

WSSI #22245.01 - April 2013



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Attachment H – Military Site Scope of Work

DHR standards. Modern refuse will be noted and discarded in the field. Historic artifacts will be separated into four basic categories: glass, metal, ceramics, and miscellaneous. The ceramics will be identified as to ware type, method of decoration, and separated into established types, following South (1977), Miller (1992) and Magid (1990). All glass will be examined for color, method of manufacture, function, etc., and dated primarily on the basis of method of manufacture when the method could be determined (Hurst 1990). Metal and miscellaneous artifacts will be generally described; the determination of a beginning date is sometimes possible, as in the case of nails.

Any prehistoric artifacts recovered will be classified by cultural historical and functional types and lithic material. In addition, the debitage will be studied for the presence of striking platforms and cortex, wholeness, quantity of flaking scars, signs of thermal alteration, size, and presence or absence of use.

Reporting: The results will be described in a manner that follows all applicable local, state, and federal guidelines, and will involve the preparation of a report which will integrate the results of the background research and present the results of the field investigations. The report will include the preparation of graphics which will illustrate the results of the metal detector investigations. In addition, the Virginia DHR site forms will be created or updated as necessary with any additional information gained from the metal detector survey.

The report is subject to review by Prince William County. Three (3) copies of the report will be submitted to the County archaeologist prior to the preliminary subdivision plan review. The final report will be submitted on acid-free paper, along with an electronic copy in Adobe PDF and MS-Word formats, and a copy of the artifact database in a form readable by MS-ACCESS. The report may also be subject to review by the Virginia Department of Historic Resources (DHR), if required during the Clean Water permitting process or any other federal undertaking. DHR requires two bound and one electronic copy of the final report.

Attachment H – Military Site Scope of Work



**Exhibit A
Vicinity Map**

Blackburn (A.K.A. Wheeler) Property - Recommendation for Metal Detector Survey

WSSI #22245.01 - April 2013

Thunderbird
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Attachment H – Military Site Scope of Work

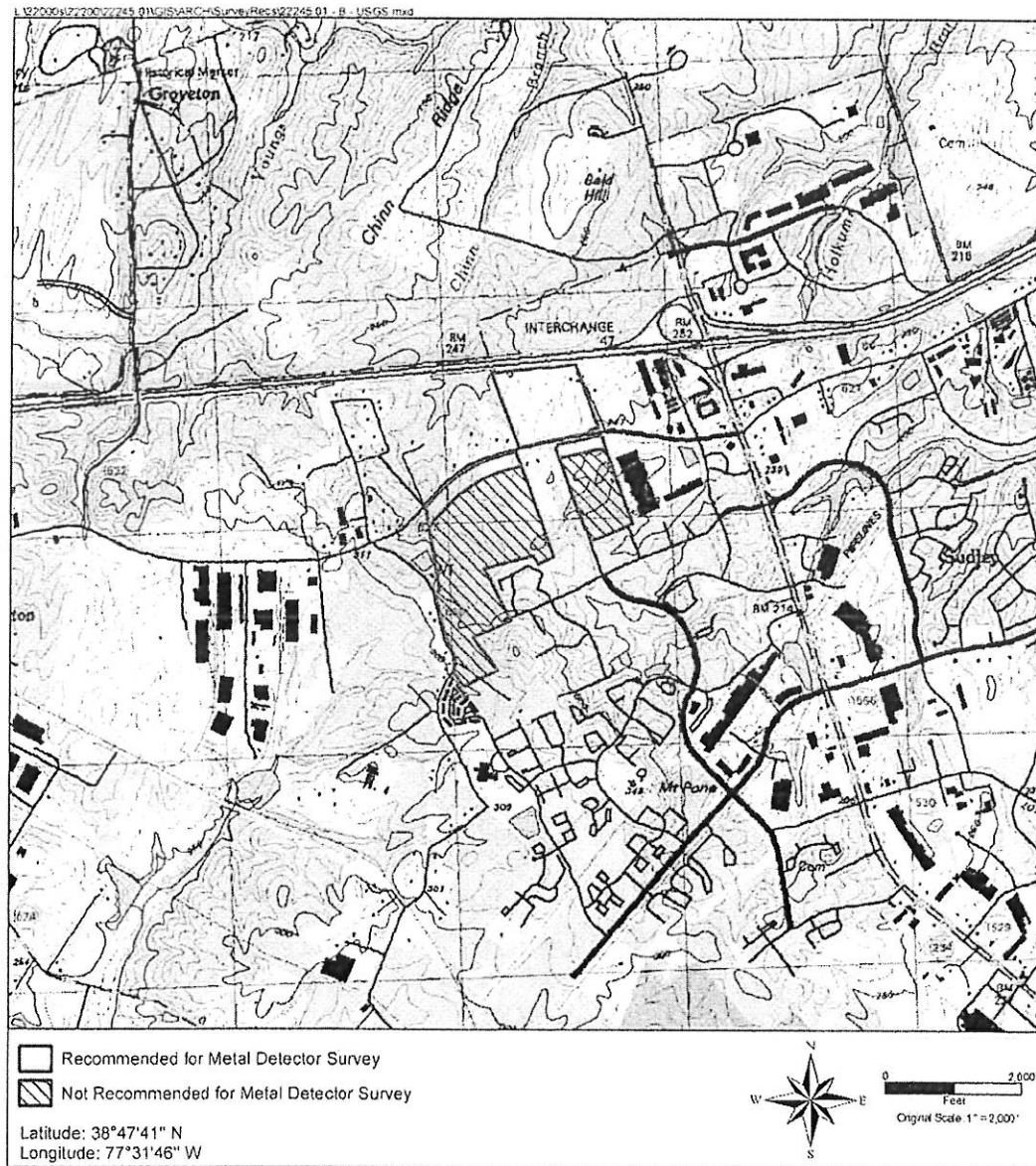
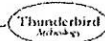


Exhibit B
USGS Quad Map - Gainesville, VA 1994

Blackburn (A.K.A. Wheeler) Property - Recommendation for Metal Detector Survey

WSSI #22245.01 - April 2013



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Attachment H – Military Site Scope of Work

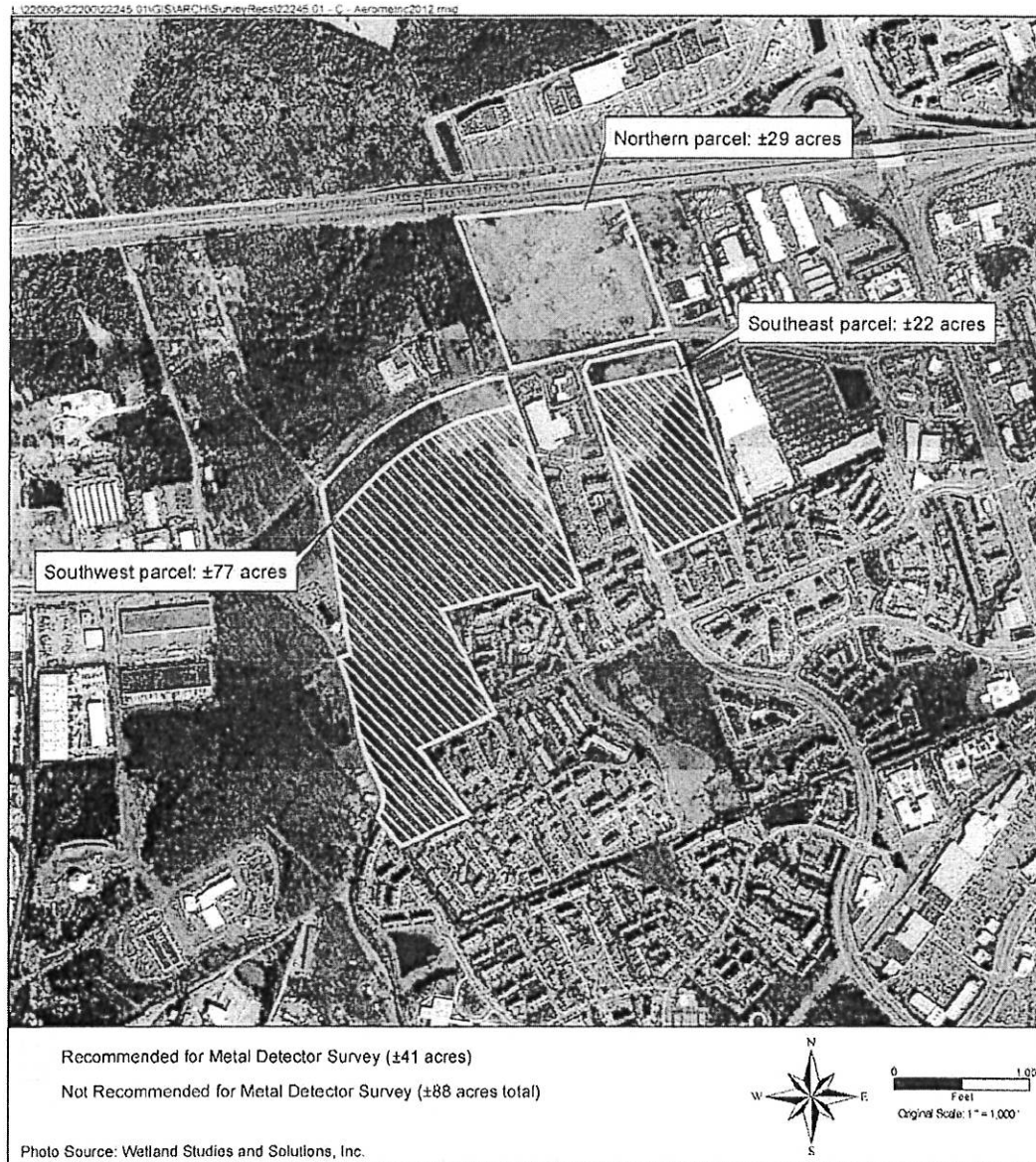


Exhibit C
February 2012 Natural Color Imagery

Blackburn (A.K.A. Wheeler) Property - Recommendation for Metal Detector Survey

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Thunderbird
Aerials

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The drawing is a technical plan view of a road improvement project for Balls Ford Road. It shows the road layout, including a bridge crossing a waterway, and various road features like medians, shoulders, and intersections. Match lines are indicated at the top and bottom of the drawing, with text 'MATCH LINE SEE THIS SHEET'. A title block in the bottom right corner contains the following information:

OFFSITE ROAD IMPROVEMENT EXHIBIT
BALLS FORD ROAD
BLACKBURN
CHATEAUVILLE DISTRICT
PRINCE WILLIAM COUNTY, VIRGINIA

Scale: 1" = 100'
 Date: APRIL, 2015

The drawing also includes a north arrow and a graphic scale bar. The road is labeled 'BALLS FORD ROAD ROUTE 621' and 'ROUTE 621'. A note indicates 'R.O.W. VARIES DESIGN SPEED 30 MPH'. The drawing shows the road layout, including a bridge crossing a waterway, and various road features like medians, shoulders, and intersections.

Attachment J – Housing Unit Modifications

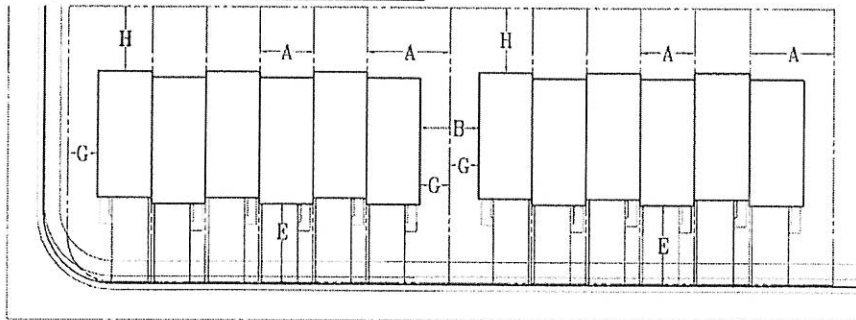
Attachment E

MODIFIED TOWN HOUSE (PMR)

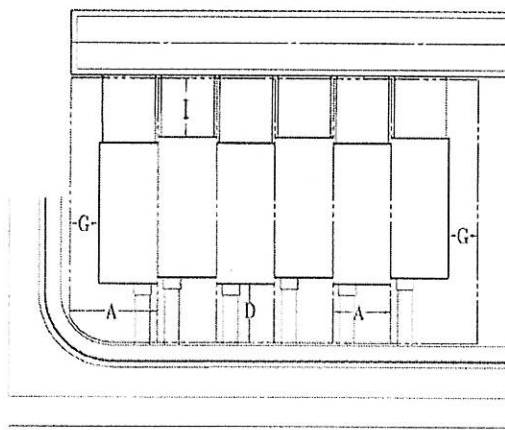
This dwelling type consists of a single-family attached unit, with individual outside access. Rows of townhouses shall contain no more than six dwelling units in a group, except as may be allowed in town centers. Back-to-back townhouses shall contain no more than 12 dwelling units in a group. Live/work units may be either townhouse or back-to-back townhouse developments, subject to residential and non-residential parking requirements in the design and construction standards manual. Residential uses shall not be located on the first story of live/work units. The following table specifies the minimum standards.

A= MINIMUM LOT WIDTH.....	20 FT.	<u>18 FT.</u>
B= GROUP SETBACK.....	20 FT.	
C= MAXIMUM BUILDING HEIGHT.....	35 FT.	
MINIMUM SETBACKS:		
D= FRONT (WITH REAR LOAD GARAGES).....	10 FT.	
E= FRONT (WITH FRONT LOAD GARAGES).....	20 FT.	
F= SETBACK FROM ANY PUBLIC RIGHT-OF-WAY.....	20 FT.	
G= SIDE (END UNIT).....	10 FT.	
H= REAR (CORRESPONDING TO FRONT LOAD GARAGE)	20 FT.	
I= REAR (CORRESPONDING TO REAR LOAD GARAGE).....	20 FT.	
J= MINIMUM BUILDING FOOTPRINT.....	720 SQ. FT.	

LOTS WITH FRONT LOAD GARAGES:



LOTS WITH REAR LOAD GARAGES:



Revised: August 2014
Sheet 1 of 5

Attachment J – Housing Unit Modifications

NOTES:

- 1.) Setbacks shall be varied at least two feet for all townhouse units within a group, except that two abutting units may have the same setback, provided no more than four units in the group have the same setback.
- 2.) Architectural treatment shall vary so that no more than two abutting units are substantially the same, and so that no more than four units in any group are substantially the same.
- 3.) For an individual section or phase of a multiphase project, one-half of the required open space, or 15 percent, shall be provided within that phase or section. The remaining 15 percent may be provided on a project-wide basis.
- 4.) Unroofed decks, stoops, landings, and similar feature minimum setbacks are as follows:
 - a. Front (with rear load garage): 5 FT.
 - b. Front (with front load garage): 15 FT.
 - c. Side (end unit): 10 FT.
 - d. Rear (with rear load or front load garage): 8 FT.

Revised: August 2014
Sheet 2 of 5

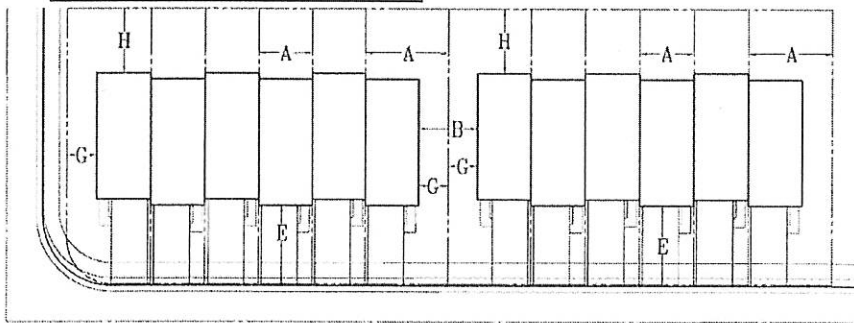
Attachment J – Housing Unit Modifications

MODIFIED TOWN HOUSE (PMD)

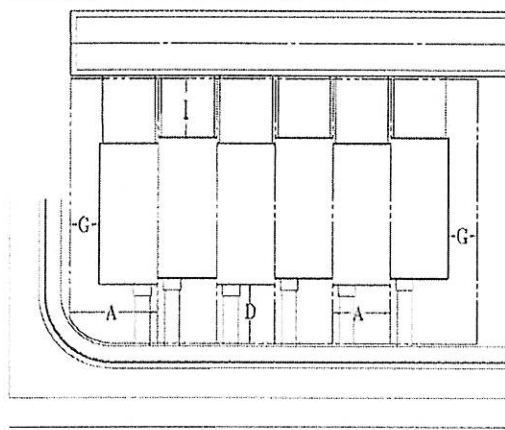
This dwelling type consists of a single-family attached unit, with individual outside access. Rows of townhouses shall contain no more than six dwelling units in a group, except as may be allowed in town centers. Back-to-back townhouses shall contain no more than 12 dwelling units in a group. Live/work units may be either townhouse or back-to-back townhouse developments, subject to residential and non-residential parking requirements in the design and construction standards manual. Residential uses shall not be located on the first story of live/work units. The following table specifies the minimum standards

A= MINIMUM LOT WIDTH.....	20 FT.	<u>18 FT.</u>
B= GROUP SETBACK.....	20 FT.	
C= MAXIMUM BUILDING HEIGHT.....	35 FT.	
MINIMUM SETBACKS:		
D= FRONT (WITH REAR LOAD GARAGES).....	10 FT.	
E= FRONT (WITH FRONT LOAD GARAGES).....	20 FT.	
F= SETBACK FROM ANY PUBLIC RIGHT-OF-WAY.....	20 FT.	
G= SIDE (END UNIT).....	10 FT.	
H= REAR (CORRESPONDING TO FRONT LOAD GARAGE)	20 FT.	
I= REAR (CORRESPONDING TO REAR LOAD GARAGE).....	20 FT.	
J= MINIMUM BUILDING FOOTPRINT.....	720 SQ. FT.	

LOTS WITH FRONT LOAD GARAGES:



LOTS WITH REAR LOAD GARAGES:



Revised: August 2014
Sheet 3 of 5

Attachment J – Housing Unit Modifications

NOTES:

- 1.) Setbacks shall be varied at least two feet for all townhouse units within a group, except that two abutting units may have the same setback, provided no more than four units in the group have the same setback.
- 2.) Architectural treatment shall vary so that no more than two abutting units are substantially the same, and so that no more than four units in any group are substantially the same.
- 3.) For an individual section or phase of a multiphase project, one-half of the required open space, or 15 percent, shall be provided within that phase or section. The remaining 15 percent may be provided on a project-wide basis.
- 4.) Unroofed decks, stoops, landings, and similar feature minimum setbacks are as follows:
 - a. Front (with rear load garage): 5 FT.
 - b. Front (with front load garage): 15 FT.
 - c. Side (end unit): 10 FT.
 - d. Rear (with rear load or front load garage): 8 FT.

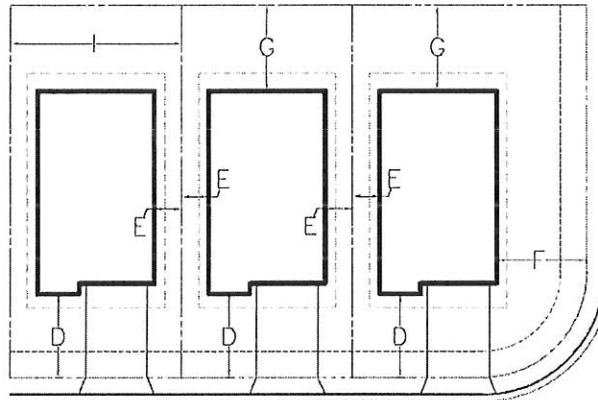
Revised: August 2014
Sheet 4 of 5

Attachment J – Housing Unit Modifications

MODIFIED VILLAGE HOUSE

THIS DWELLING TYPE IS A SINGLE-FAMILY RESIDENCE WHICH IS FULLY DETACHED FROM NEIGHBORING STRUCTURES. THE VILLAGE HOUSE IS DISTINGUISHED BY SMALL FRONT AND SIDE YARDS. THE STREETScape OF VILLAGE HOUSES IS A CRITICAL ELEMENT GIVEN THE SMALL SETBACKS FROM THE STREET, THUS EACH PLAN USING THIS HOUSING TYPE SHALL DEMONSTRATE GOOD DESIGN PRACTICES. THE FOLLOWING TABLE AND TEXT SPECIFY THE MINIMUM STANDARDS FOR A VILLAGE HOUSE.

A= MINIMUM LOT AREA.....	6000 SQ. FT.	<u>5000 SQ. FT.</u>
B= MAXIMUM LOT COVERAGE.....	.60	
C= MAXIMUM BUILDING HEIGHT.....	35 FT.	
MINIMUM SETBACKS:		
D= FRONT SETBACK TO HOUSE.....	20 FT.	
E= SIDE.....	10 FT.	<u>5 FT.</u>
F= CORNER LOT (SIDE).....	20 FT.	<u>16 FT.</u>
G= REAR LOT LINE TO HOUSE.....	25 FT.	<u>20 FT.</u>
H= REAR LOT LINE TO GARAGE.....	5 FT.	
I= MINIMUM LOT WIDTH.....	60 FT.	<u>50 FT.</u>



NOTES:

- 1.) UNROOFED STRUCTURES SUCH AS, BUT NOT LIMITED TO, LANDINGS, PORCHES, DECKS, STEPS, AND STOOPS IN ANY COMBINATION GREATER THAN EIGHT (8) INCHES ABOVE EXISTING GRADE MAY BE LOCATED IN THE FRONT OR REAR YARD ONLY, SUBJECT TO THE MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND 5 FEET FROM THE REAR PROPERTY LINES.
- 2.) ROOFED STRUCTURES SUCH AS, BUT NOT LIMITED TO, LANDINGS, PORCHES, DECKS, STEPS, AND STOOPS IN ANY COMBINATION MAY BE LOCATED IN THE FRONT OR REAR YARD ONLY, SUBJECT TO THE SETBACK REQUIREMENTS FOR THE PRIMARY STRUCTURE.
- 3.) PARKING REQUIREMENTS SHALL MEET STANDARDS FOR SINGLE FAMILY DETACHED UNITS.
- 4.) ARCHITECTURAL FEATURES SUCH AS, BUT NOT LIMITED TO, WINDOWS, SILLS, CORNICES, EAVES, AND GUTTERS, BUT EXCLUDING FLOOR AREA SUPPORTED BY CANTILEVERED CONSTRUCTION MAY PROJECT UP TO 3 FEET INTO ANY REQUIRED YARD SETBACK.
- 5.) PIPESTEM LOTS SHALL MEET THE STANDARDS IN SECTION 32-300.61.10 OF THE ZONING ORDINANCE.

Revised: August 2014
Sheet 5 of 5



PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC DATE: 08/29/2014
ADDRESS: c/o Kettler, Inc. WAIVER NUMBER: WAI 2015-00013
1751 Pinnacle Drive, Suite 700
McLean, VA 22102
SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040
PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST


SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.05
REQUESTED TO BE WAIVED:

REQUIREMENT(S): For residential minor collector street standard RM-2 category IV typical section is required.

APPLICANT'S JUSTIFICATION FOR WAIVER: To avoid additional offsite right-of-way dedication and to achieve the intersection turning movements and accommodate sight distance, a reduction in lane width from 12 feet to 11 feet lanes is proposed. The turn lanes and curb and gutter will be provided on east side of Bethlehem Road as well. Further justification of the waiver is that the existing lane widths on Bethlehem Road are only 11 feet.

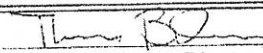
SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

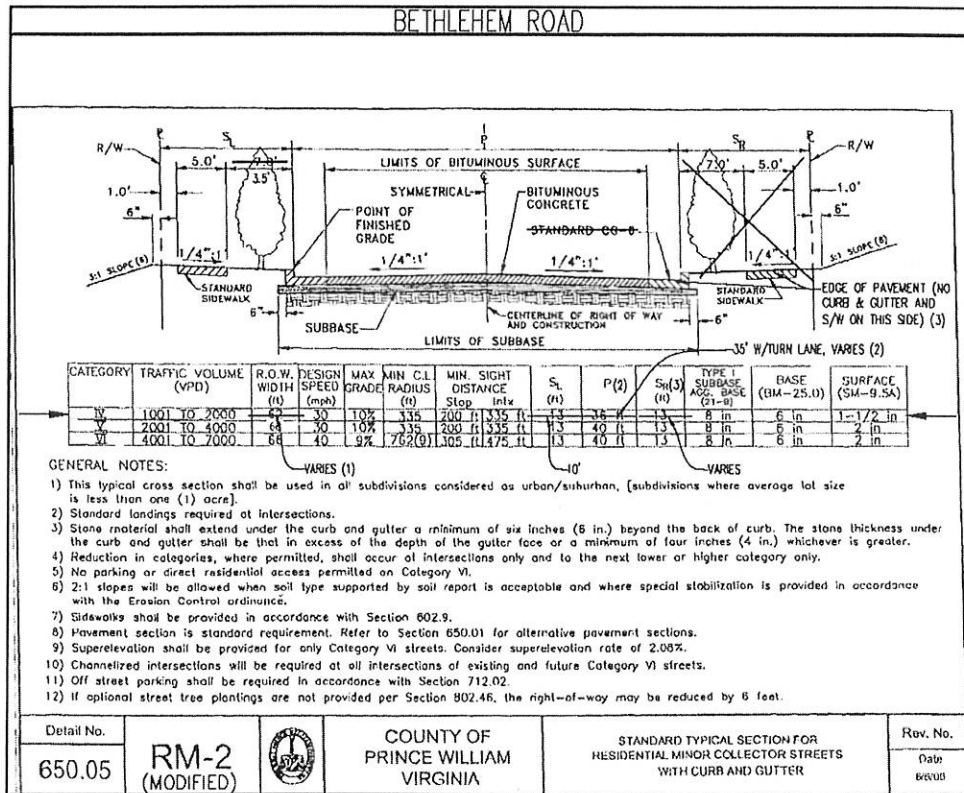
PLAN REVIEWER: Sarbjit Sidhu ^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales 

REASON(S) FOR APPROVAL/DENIAL: The justification provided is acceptable. Due to existing conditions the 11 feet lane width will be acceptable along Bethlehem Road.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE  DATE 3/29/17
Thomas Blaser, Director of Transportation

cc: Development Services

**NOTES:**

- (1) R.O.W. VARIES WITHIN PROPOSED LIMITS OF WORK. FRONTAGE IMPROVEMENTS WILL OCCUR ON THE EAST (LEFT) SIDE OF BETHLEHEM ROAD ONLY, THEREFORE R.O.W. WILL BE DEDICATED ONLY ON THE EAST SIDE OF THE ROAD WITHIN THE LIMITS OF WORK. R.O.W. WIDTH WILL VARY DUE TO TURN LANE TAPERS AND EXISTING VARIABLE WIDTH R.O.W.
- (2) SINCE CURB & GUTTER IS ONLY PROPOSED ON EAST (LEFT) SIDE OF THE ROAD, THE "P" MEASUREMENT IS TAKEN FROM EDGE OF PAVEMENT TO FACE OF CURB. "P" = 35' FROM EDGE OF PAVEMENT TO FACE OF CURB AT FULL TURN LANE WIDTH. "P" VARIES DUE TO TURN LANE TAPERS. TYPICAL "P" = 22' OUTSIDE LIMITS OF WORK (2-11' LANES, E.O.P. TO E.O.P.)
- (3) THE EAST (LEFT) SIDE OF BETHLEHEM ROAD WILL HAVE CURB & GUTTER, THE WEST (RIGHT) SIDE WILL TIE INTO EXISTING EDGE OF PAVEMENT SINCE IMPROVEMENTS WILL NOT BE MADE ON THAT SIDE OF THE ROAD.



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DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

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(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: 08/29/2014

ADDRESS: c/o Kettler, Inc.
1751 Pinnacle Drive, Suite 700
McLean, VA 22102

WAIVER NUMBER: WAI 2015 - 00014

SITE/SUBDIVISION PLAN NUMBER: PLN14-00040

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 602.07K
REQUESTED TO BE WAIVED:

REQUIREMENT(S): For all roadways classified as category VI and above, a distance of at least 280 feet shall be maintained between curb return of commercial entrance and intersection.

APPLICANT'S JUSTIFICATION FOR WAIVER: This site proposes a commercial entrance to be built 255 feet away from southeast curb return at the intersection of Balls Ford Road and Ashton Avenue. The commercial landbay for southeast of the intersection must have two entrances, both of which are proposed on Ashton Avenue, frontage, one of the entrances is right in and right out only. The entrances cannot shift south because it will conflict with the RPA boundary. The applicant prefers to have both entrances on Ashton Avenue so that an entrance won't have to be placed on Balls Ford Road.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

PLAN REVIEWER: Sarbjit Sidhu^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales

REASON(S) FOR APPROVAL/DENIAL: Because of environmental constraints the site entrance spacing of 255 feet from Balls Ford Road intersection will be acceptable.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE Thomas Blaser DATE 8/29/14
Thomas Blaser, Director of Transportation

cc: Development Services



PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC DATE: September 8, 2014
ADDRESS: c/o Kettler Inc. WAIVER NUMBER: WAI 2015-00015
1751 Pinnacle Drive, Suite 700
McLean, VA 22102
SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040
PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 601.04 C
REQUESTED TO BE WAIVED:

REQUIREMENT(S): All single-family detached dwelling lots, unless otherwise specified in the zoning ordinance, shall have frontage on and access to existing state-maintained public streets or streets approved and bonded to be constructed to a standard acceptable for addition to the state street system.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant requests that single-family detached lots have frontage on private streets that will be owned and maintained by the homeowners association (HOA). The private streets will be covered by an access easement that will connect to existing public right-of-way. All lots will have frontage on the private streets or pipe stem access to the private streets. The private streets will be designed and constructed to the RL-2 standard as modified under separate waiver application. The private street concept will allow for flexibility in the typical sections (through modification with County only) that will create a more environmentally sensitive project.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

PLAN REVIEWER: Sarbjit Sidhu^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales

REASON(S) FOR APPROVAL/DENIAL: The justification provided is acceptable. The private streets will be allowed for proposed single-family detached lots in this development.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE Thomas Blaser DATE 9/16/17
Thomas Blaser, Director of Transportation

cc: Development Services



PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: September 8, 2014

ADDRESS: c/o Kettler Inc.

WAIVER NUMBER: WAI 2015-00016

1751 Pinnacle Drive, Suite 700

McLean, VA 22102

SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.11

REQUESTED TO BE WAIVED:

REQUIREMENT(S): MA-1 standard typical section for urban minor arterial (six lane divided with curb and gutter) street.

APPLICANT'S JUSTIFICATION FOR WAIVER: To avoid conflicts with existing large power transmission poles along Balls Ford Road, the applicant proposes modifying the typical section to remove future 12-foot lanes, making the section a four-lane divided road. These changes result in an overall decrease in the right of way from 128 feet to 103.5 feet (nonsymmetrical due to trail). See the attached modified MA-1 typical section.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

PLAN REVIEWER: Sarbjit Sidhu ^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales ^{RC}

REASON(S) FOR APPROVAL/DENIAL: Because of the location of existing large power transmission line poles along site frontage, the modified MA-1 typical section for Balls Ford Road will be acceptable as shown on the attached exhibit.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE

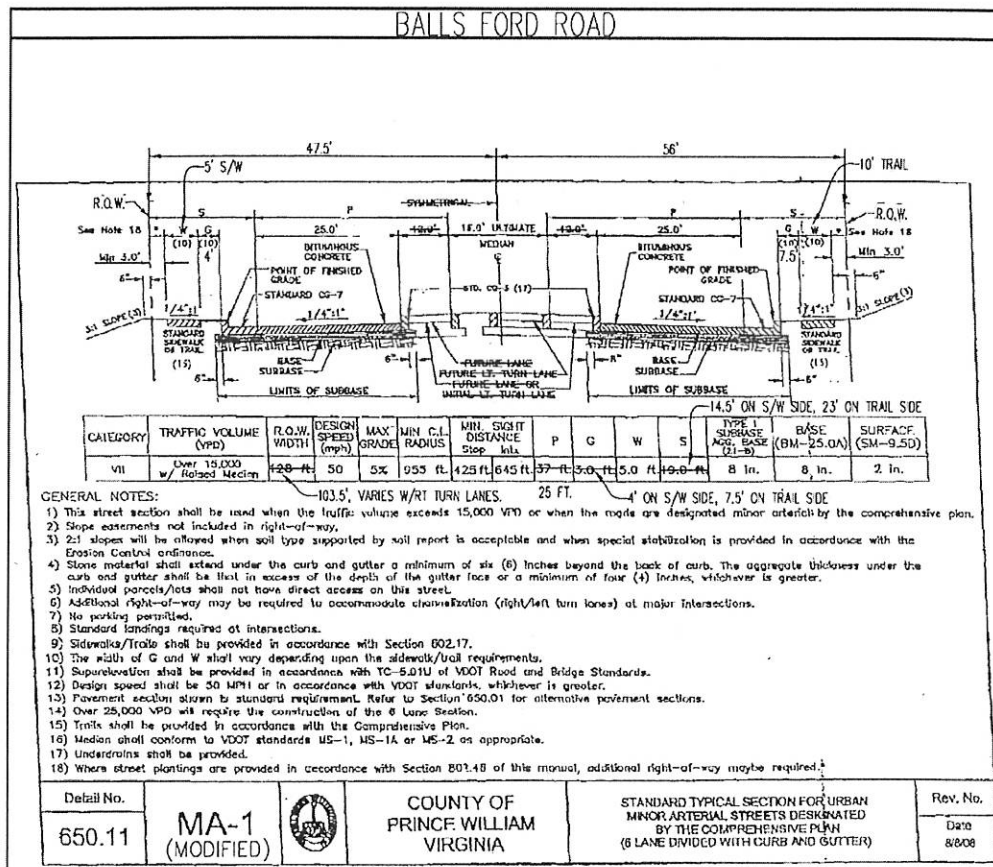
Thomas Blaser

DATE

9/16/11

Thomas Blaser, Director of Transportation

cc: Development Services





PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: September 8, 2014

ADDRESS: c/o Kettler Inc.
1751 Pinnacle Drive, Suite 700
McLean, VA 22102

WAIVER NUMBER: WAI 2015-00017

SITE/SUBDIVISION PLAN NUMBER: PLN2014-00140

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.06
REQUESTED TO BE WAIVED:

REQUIREMENT(S): Standard TS-1 Category III typical section requires pavement width of 24 feet from EP to EP.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant would like to allow for deviations from TS-1 road-width requirements in landbays containing townhomes from 24 feet to 22 feet measured EP to EP. A reduction of TS-1 standard road width is necessary in order to avoid impacting adjacent RPA boundaries, to allow for adequate stormwater management facilities, to keep proposed roadway from encroaching on existing Bethlehem Road right of way, and to allow for grading to remain on site. Additionally, this waiver will allow for greater open space and additional wetlands preservation.

See modified road section on Sheet 7 of 15 REZ PLN# 2014-00040.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

PLAN REVIEWER: Sarbjit Sidhu ^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales ^{RC}

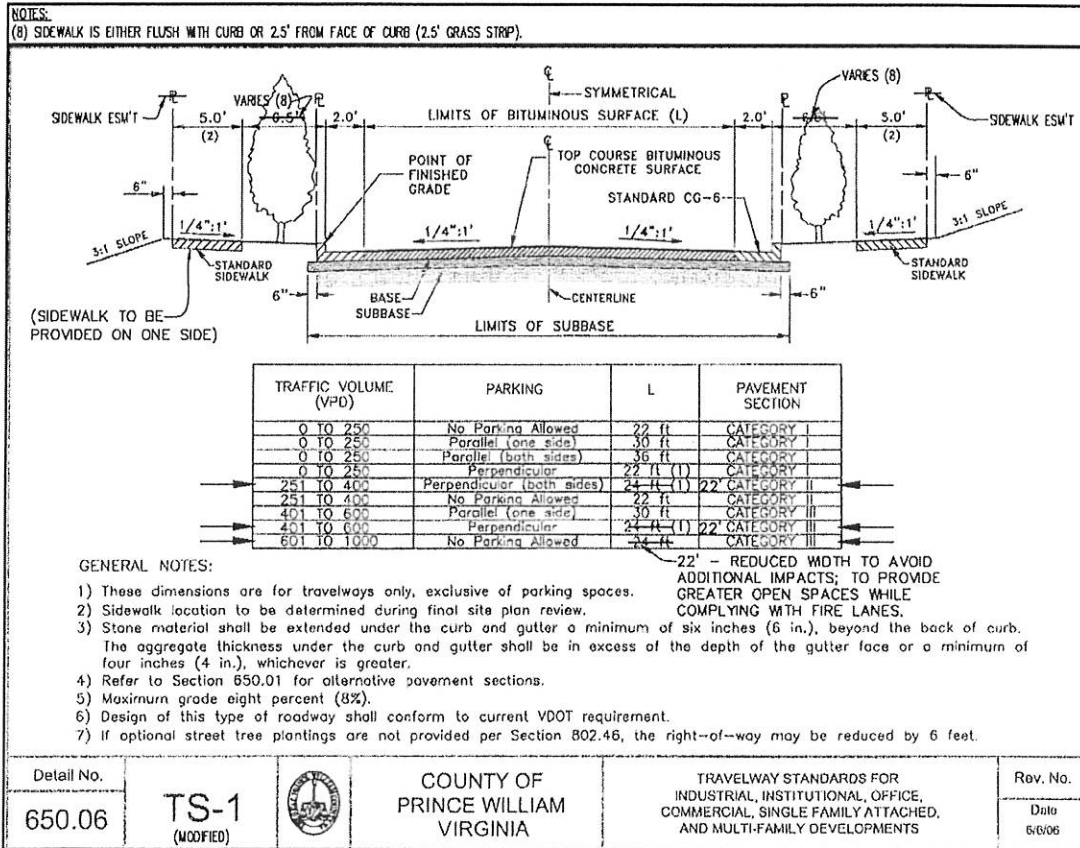
REASON(S) FOR APPROVAL/DENIAL: Because of site constraints, 22 feet in pavement width will be acceptable for TS-1 Category III typical section for the proposed streets in landbay 2 containing the townhouse units.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE Thomas Blaser
Thomas Blaser, Director of Transportation

DATE 7/16/14

cc: Development Services





PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: 08/29/2014

ADDRESS: c/o Kettler, Inc.
1751 Pinnacle Drive, Suite 700
McLean, VA 22102

WAIVER NUMBER: WAI 2015-00018

SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.08
REQUESTED TO BE WAIVED:

REQUIREMENT(S): CI-1 standard typical section in commercial developments requires a 74 feet right-of-way accounting for 5 feet sidewalks on each side of the road and 7 feet buffer between face of curb and sidewalk.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant would like to allow that the portion of Hollis Drive ending at the property border shortly after the entrance for the Salvation Army to be extended into the single family residential development as the same modified CI-1 road section that was approved for the Salvation Army plan. This will provide a better streetscape for the community and will allow the existing Hollis Drive to be constructed completely in uniformity. This portion of Hollis Drive to be constructed into the community would be a modest 200 feet of roadway. We believe that this is the best way to provide an extension to Hollis Drive without having to provide a roadway extension that wouldn't match what is exiting or leaves the sidewalk south of Hollis Drive unconstructed.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION:

☒ APPROVAL

☐ DENIAL

PLAN REVIEWER: Sarbjit Sidhu ^{SS}

TRANSPORTATION PLANNING MANAGER: Ricardo Canizales ^{RC}

REASON(S) FOR APPROVAL/DENIAL: Since this is an extension of the existing Hollis Drive, the modified CI-1 typical section for the proposed Hollis Drive to match the existing section to the east will be acceptable.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE

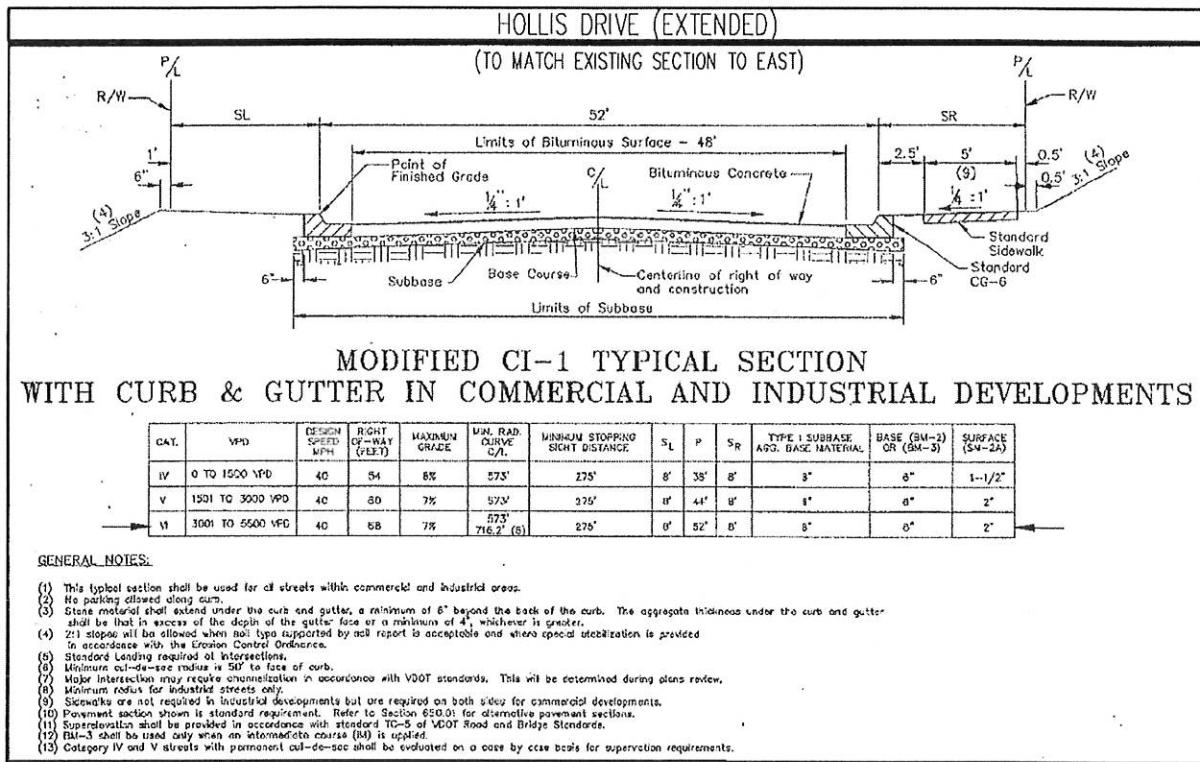
Thomas Blaser

Thomas Blaser, Director of Transportation

DATE

8/29/14

cc: Development Services





PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC DATE: September 8, 2014
ADDRESS: c/o Kettler Inc. WAIVER NUMBER: WAI 2015-00019
1751 Pinnacle Drive, Suite 700
McLean, VA 22102
SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040
PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.08
REQUESTED TO BE WAIVED:

REQUIREMENT(S): The C1-1 standard typical section for commercial areas with curb and gutter requires a landscape buffer of 7 feet between face of curb and sidewalk.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant would like to allow the landscape buffer on Coverstone Drive to be reduced from 7 feet to 4.5 feet measured from the face of curb to sidewalk to conform with the VDOT buffer strip requirements (VDOT Road Design Manual pg. A-118). It should also be noted that the existing portion of Coverstone to the east does not have the 7-foot buffer strip. See modified road section on Sheet 7 of 15 REZ PLN# 2014-00040.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

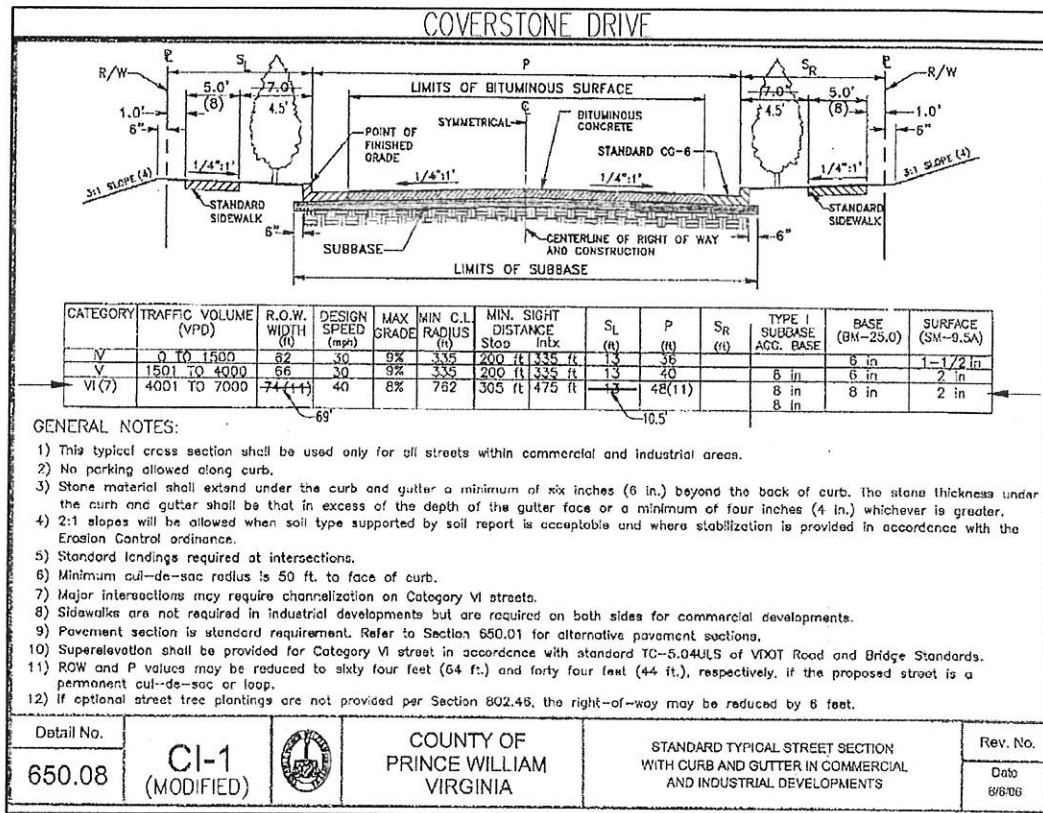
PLAN REVIEWER: Sarbjit Sidhu^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales

REASON(S) FOR APPROVAL/DENIAL: The justification provided is acceptable. The modified C1-1 typical section will be acceptable for Coverstone Drive as shown on the attached exhibit.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE Thomas Blaser DATE 9/16/14
Thomas Blaser, Director of Transportation

cc: Development Services





PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: September 8, 2014

ADDRESS: c/o Kettler Inc.
1751 Pinnacle Drive, Suite 700
McLean, VA 22102

WAIVER NUMBER: WAI 2015-00020

SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.09
REQUESTED TO BE WAIVED:

REQUIREMENT(S): The MC-1 typical section for major collector roads requires a symmetrical layout and 19 feet between the edge of the pavement and right of way.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant would like to add a 12-foot lane for right turns and also decrease the 19-foot requirement between the edge of pavement and the right of way to 12.5 feet on the proposed east side of Ashton Avenue as shown in the detail. This will allow for a 4.0-foot buffer strip (measured from back of curb to sidewalk) and a 1-foot strip between the right of way and sidewalk to match the existing half section to comply with VDOT. See modified road section on Sheet 7 of 15 REZ.
PLN# 2014-00040.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION: ☒ APPROVAL ☐ DENIAL

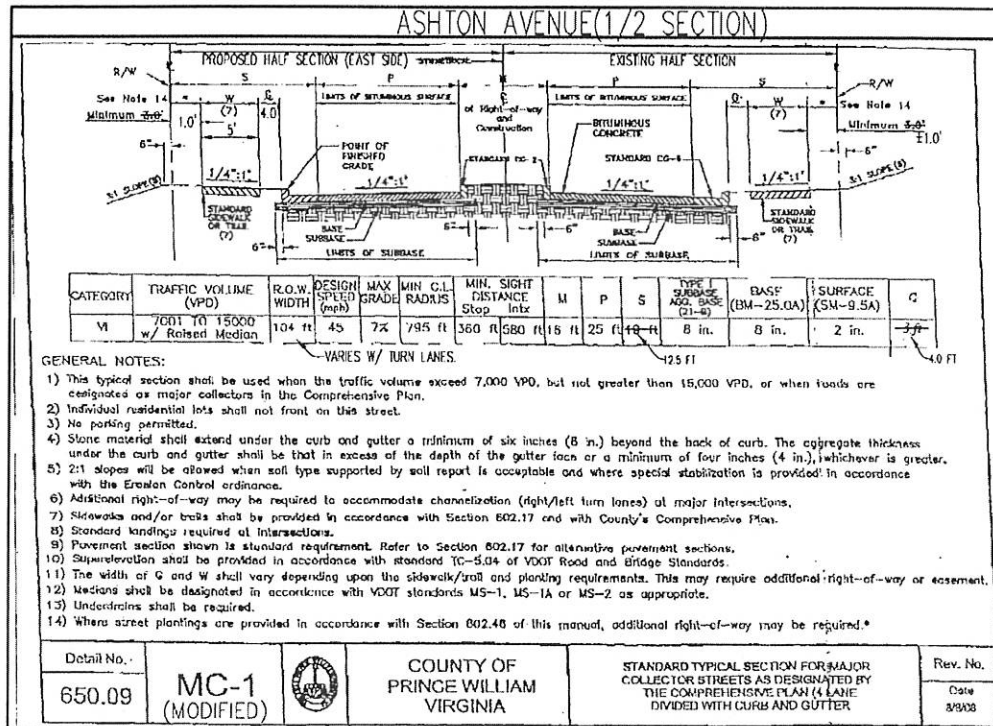
PLAN REVIEWER: Sarbjit Sidhu ^{SS} TRANSPORTATION PLANNING MANAGER: Ricardo Canizales ^{RC}

REASON(S) FOR APPROVAL/DENIAL: The justification provided is acceptable. The modified MC-1 typical section will be acceptable for Ashton Avenue as shown on the attached exhibit.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE Thomas Blaser DATE 9/16/17
Thomas Blaser, Director of Transportation

cc: Development Services





PRINCE WILLIAM COUNTY
DEPARTMENT OF TRANSPORTATION
DCSM WAIVER

5 County Complex Court, Ste. 290, Woodbridge, Virginia 22192-5308
(703) 792-6825 Metro 631-1703, Ext. 6825 Fax (703) 792-7159

SECTION I – GENERAL INFORMATION

APPLICANT: RCKF Bull Run LLC

DATE: 08/29/2014

ADDRESS: c/o Kettler, Inc.

WAIVER NUMBER: WAI 2015-00021

1751 Pinnacle Drive, Suite 700

McLean, VA 22102

SITE/SUBDIVISION PLAN NUMBER: PLN2014-00040

PROJECT NAME: Blackburn

SECTION II – SPECIFICS OF WAIVER REQUEST

SECTION OF DESIGN AND CONSTRUCTION STANDARDS MANUAL: 650.04

REQUESTED TO BE WAIVED:

REQUIREMENT(S): The RL-2 standard typical section for residential streets with curb and gutter require a 5 foot sidewalk within the dedicated right-of-way.

APPLICANT'S JUSTIFICATION FOR WAIVER: The applicant would like to allow that the sidewalk in the single family detached residential section be constructed outside the street right-of-way. In addition a request is made to reduce the street right-of-way per attached exhibit. This will provide a better street scape for the community. The Homeowner's Association will maintain the private residential streets as well as the sidewalks.

SECTION III – RECOMMENDATION OF TRANSPORTATION

RECOMMENDATION:

☒ APPROVAL

☐ DENIAL

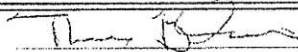
PLAN REVIEWER: ^{SS}
Sarbjit Sidhu

TRANSPORTATION PLANNING MANAGER: Ricardo Canizales

REASON(S) FOR APPROVAL/DENIAL: The justification provided is acceptable. The modified RL-2 typical section and location of sidewalk outside the street right-of-way as shown on the attached exhibit will be acceptable with the following condition: The proposed streets will not be maintained by VDOT.

SECTION IV – ADDITIONAL COMMENTS

SIGNATURE

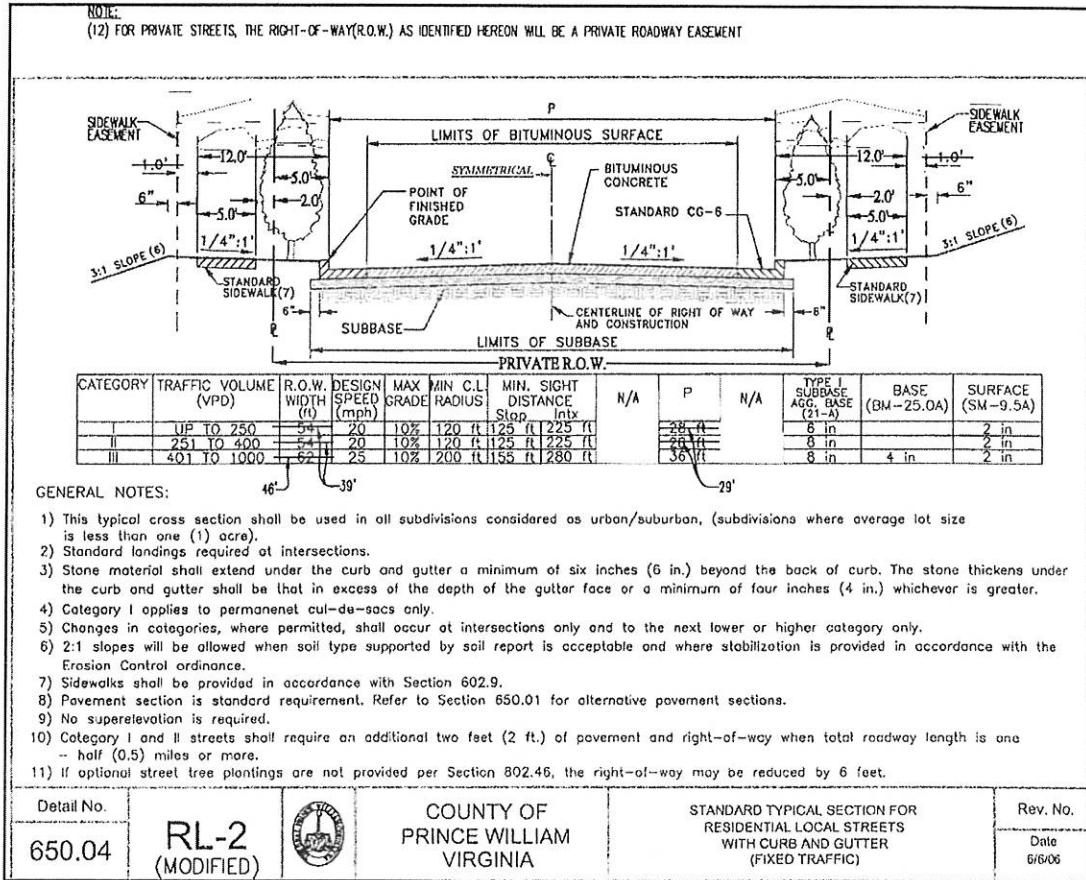


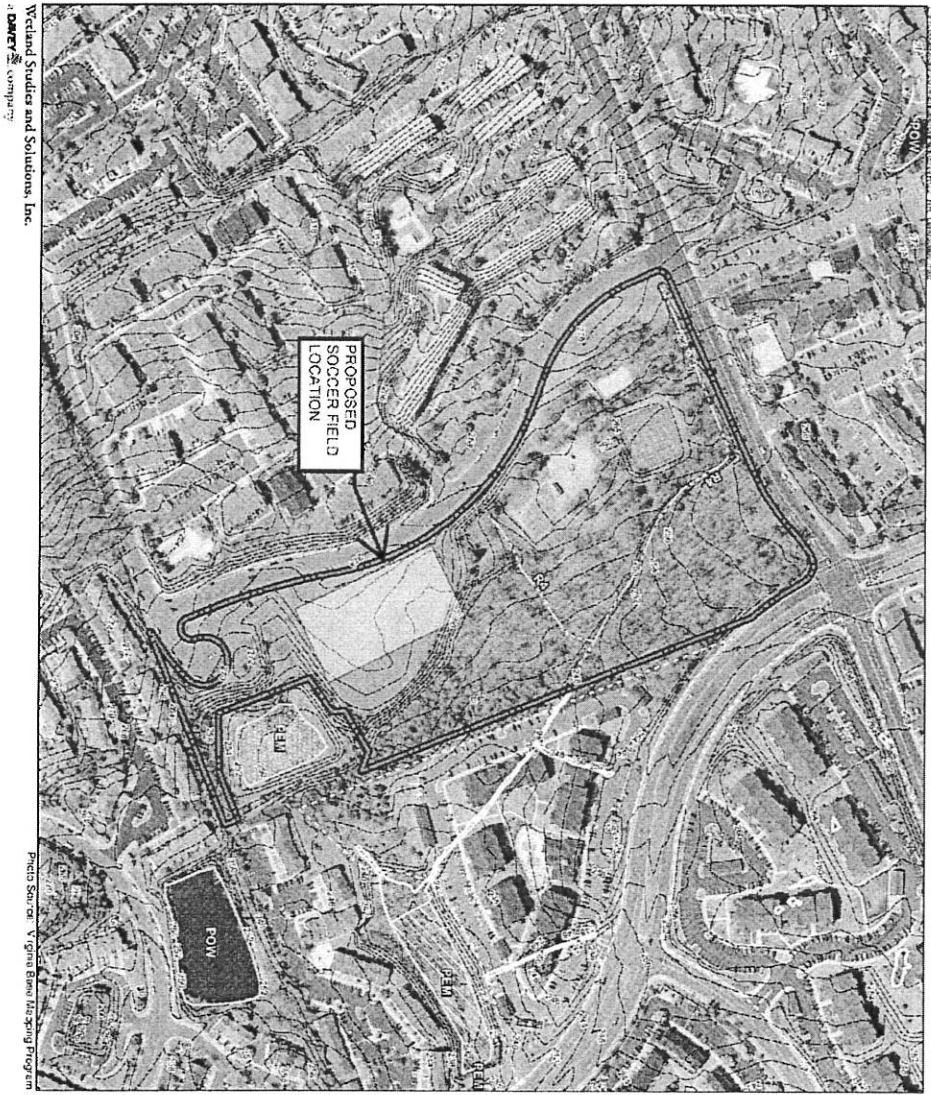
Thomas Blaser, Director of Transportation

DATE

8/29/14

cc: Development Services





Wetland Studies and Solutions, Inc.
a DWT company

Legend

Site

- High Potential for Jurisdictional Wetlands
- Medium Potential for Jurisdictional Wetlands
- High Potential for Perennial Stream
- High Potential for Intermittent Stream
- High Potential for Ephemeral Stream
- and Lesser Wetlands of the U.S. (WOLUS)
- Previously Delineated Jurisdictional Wetlands/ WOLUS with Survey
- Previously Delineated Jurisdictional Wetlands/ Delineation without Survey
- Existing Field Pedestrian Wetlands Reconnaissance
- Existing Field Pedestrian Wetlands Reconnaissance

Cowardin Classification

- PFG - Palustrine Forested Wetland
- PSS - Palustrine Scrub Shrub Wetland
- PEM - Palustrine Emergent Wetland
- POW - Palustrine Open Water
- R3 - Riverine Palustrine Stream
- R4 - Riverine Intermittent Stream
- RE - Riverine Ephemeral Stream

Other

- POW - Palustrine Open Water
- PEM - Palustrine Emergent Wetland
- PFG - Palustrine Forested Wetland
- PSS - Palustrine Scrub Shrub Wetland
- RE - Riverine Ephemeral Stream
- R4 - Riverine Intermittent Stream
- R3 - Riverine Palustrine Stream

Scale

0 200
Feet
Cartus Terra 2
Vector Suite NAVD 88

North Arrow

Metadata

Northern Virginia Desktop
Wetlands Reconnaissance
Spring 2013 Color Infrared Imagery
Ellis Baron Park
Original Scale: 1" = 300'

ATTACHMENT O
FEBRUARY 2, 2015

Attachment M
Historical Commission Resolution

HISTORICAL COMMISSION RESOLUTION

MOTION: YANKEY August 13, 2013
Regular Meeting
Res. No. 13-027
SECOND: SMITH
RE: LAND DEVELOPMENT RECOMMENDATIONS
ACTION: APPROVED

WHEREAS, the Prince William County Historical Commission seeks to identify, preserve and protect historic sites and structures in Prince William County; and

WHEREAS, the Prince William County Historical Commission's review of pending land development applications assists in determining the necessity for cultural resource surveys and other research and evaluations; and

WHEREAS, the Prince William County Historical Commission believes that the identification, preservation and protection of historic sites and structures throughout Prince William County is well served by this action;

NOW, THEREFORE, BE IT RESOLVED, that the Prince William County Historical Commission does hereby recommend to the Prince William County Planning Commission the action(s) noted for the following properties:

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
REZ PLN2013-00372	Reserve at Cannon Branch	No Further Work
REZ PLN2013-00373	First Mount Zion Baptist Church	No Further Work
PFR PLN2013-00342, Revised	NOVEC Battlefield Monopole	No Further Work
REZ PLN2009-00514	Neabsco Overlook	No Further Work
REZ PLN2013-00384	Pioneer Assemblage	Table
PRA PLN2014-00013	Tribute at The Glen	No Further Work
CPA PLN2013-00064	Heritage Crossing	Table
REZ PLN2014-00020	Heritage Crossing	Table
REZ PLN2014-00040	Blackburn	Request a metal detector study at the Phase I level and, if warranted, Phase II and Phase III Data Recovery studies. Artifacts to be donated to and curated with the County.

Attachment M
Historical Commission Resolution

August 13, 2013
Regular Meeting
Res. No. 13-027
Page 2

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
REZ PLN2014-00041	The Reserve at Long Forest	Request a Phase I study and, if warranted a Phase II and Phase III study. Artifacts to be donated to and curated with the County.
SUP PLN2014-00044	DMV Auto Sales and Service	No Further Work

Votes:

Ayes: by acclamation

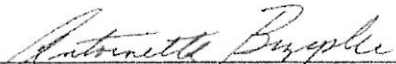
Nays: None

Absent from Vote: None

Absent from Meeting: Duley, MacDonald, Turner, Wright

MOTION CARRIED

CERTIFIED COPY


Secretary to the Commission

Attachment M
Historical Commission Resolution

HISTORICAL COMMISSION RESOLUTION

MOTION: YANKEY

April 8, 2014
Regular Meeting
Res. No. 14-019

SECOND: BURGESS

RE: LAND DEVELOPMENT RECOMMENDATIONS

ACTION: APPROVED

WHEREAS, the Prince William County Historical Commission seeks to identify, preserve and protect historic sites and structures in Prince William County; and

WHEREAS, the Prince William County Historical Commission's review of pending land development applications assists in determining the necessity for cultural resource surveys and other research and evaluations; and

WHEREAS, the Prince William County Historical Commission believes that the identification, preservation and protection of historic sites and structures throughout Prince William County is well served by this action;

NOW, THEREFORE, BE IT RESOLVED, that the Prince William County Historical Commission does hereby recommend to the Prince William County Planning Commission the action(s) noted for the following properties:

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
REZ PLN2014-00190	Midwood	Tabled
REZ PLN2014-00040	Blackburn (second submission)	Request a metal detector study at the Phase I level and, if warranted, Phase II and Phase III Data Recovery studies. Artifacts to be donated to and curated with the County. Due to viewshed concerns, request Historical Commission review of balloon study data. Request special attention regarding the likelihood that there are Civil War era troop campsite locations on the property.
SUP PLN2014-00275	Cardinal Montessori School	No Further Work
REZ PLN2014-00276	Apostolic Church International	No Further Work

Attachment M
Historical Commission Resolution

April 8, 2014
Regular Meeting
Res. No. 14-019
Page 2

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
SUP PLN2014-00288	Auto Trademark – Renewal SUP	No Further Work

Votes:

Ayes: by acclamation

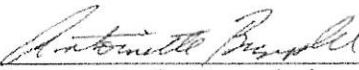
Nays: None

Absent from Vote: None

Absent from Meeting: Cunard, Smith, Van Derlaske, Wright

MOTION CARRIED

CERTIFIED COPY


Secretary to the Commission

Attachment N
Planning Commission Resolutions

PLANNING COMMISSION RESOLUTION

MOTION: VANEGAS

April 1, 2015
Regular Meeting
Res. No. 15-028

SECOND: ARNOLD

RE: REZONING #PLN2014-00040, BLACKBURN, GAINESVILLE
MAGISTERIAL DISTRICT

ACTION: CONTINUE TO DATE CERTAIN APRIL 15, 2015

WHEREAS, this is a request to rezone +/- 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and +/-51.7 acres from A-1 to PMD, Planned Mixed Use District, to allow for the development of a maximum of 416 residential units (161 single family detached units & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development; and

WHEREAS, the site is located north and south sides of Balls Ford Rd. approx. 1/2 mile west of the intersection of Balls Ford Rd. and Sudley Rd and

WHEREAS, the site is identified as GPINs 7597-92-1061, 7597-82-7607 & 7597-82-4068, and consists of +/- 128.4 acres. The property is designated REC, Regional Employment Center, and SRH, Suburban Residential High in the Comprehensive Plan; and

WHEREAS, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on April 1, 2015, at which the public testimony was received and the merits of the above-referenced request were considered; and

WHEREAS, the Prince William County Planning Commission believes that public general welfare as well as good planning practices are served by the continuance of this request;

NOW, THEREFORE, BE IT RESOLVED, that the Prince William County Planning Commission does hereby continue Rezoning #PLN2014-00040, Blackburn, to date certain April 15, 2015 with the public hearing open.

Votes:

Ayes: Arnold, Bryant, Fry, Hosen, McKay Vanegas

Nays: Holley

Absent from Meeting: None

Abstained from Vote: Haynes

MOTION CARRIED

Attest



Antoinette G. Brzyski
Acting Clerk to the Planning Commission

Attachment N
Planning Commission Resolutions

PLANNING COMMISSION RESOLUTION

MOTION: BRYANT

April 15, 2015
Regular Meeting
Res. No. 15-034

SECOND: FRY

**RE: REZONING #PLN2014-00040, BLACKBURN, GAINESVILLE
MAGISTERIAL DISTRICT**

ACTION: LAY ON THE TABLE

WHEREAS, this is a request to rezone +/- 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and +/-51.7 acres from A-1 to PMD, Planned Mixed Use District, to allow for the development of a maximum of 416 residential units (161 single family detached units & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development; and

WHEREAS, the site is located north and south sides of Balls Ford Rd. approx. 1/2 mile west of the intersection of Balls Ford Rd. and Sudley Rd.; and

WHEREAS, the site is identified as GPINs 7597-92-1061, 7597-82-7607 & 7597-82-4068, and consists of +/- 128.4 acres; and

WHEREAS, the site is designated REC, Regional Employment Center, and SRH, Suburban Residential High in the Comprehensive Plan; and

WHEREAS, upon conclusion of the public hearing, Chairman Vanegas recommended the Planning Commission table the item until Commissioner Arnold's arrival;

NOW, THEREFORE, BE IT RESOLVED, the Prince William County Planning Commission lay Rezoning #PLN2014-00040, Blackburn, on the table until such time Commissioner Arnold arrives.

Votes:

Ayes: Bryant, Fry, Holley, Hosen, McKay, Vanegas

Nays: None

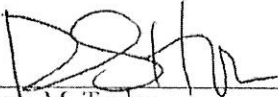
Abstained from Vote: Haynes

Absent from Vote: None

Absent from Meeting: Arnold

MOTION CARRIED

Attest: _____


Teresa M. Taylor
Clerk to the Planning Commission

Attachment N
Planning Commission Resolutions

PLANNING COMMISSION RESOLUTION

MOTION: BRYANT

April 15, 2015
Regular Meeting
Res. No. 15-037

SECOND: FRY

RE: REZONING #PLN2014-00040, BLACKBURN, GAINESVILLE
MAGISTERIAL DISTRICT

ACTION: REMOVE FROM THE TABLE

WHEREAS, this is a request to rezone +/- 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and +/- 51.7 acres from A-1 to PMD, Planned Mixed Use District, to allow for the development of a maximum of 416 residential units (161 single family detached units & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development; and

WHEREAS, the site is located north and south sides of Balls Ford Rd. approx. 1/2 mile west of the intersection of Balls Ford Rd. and Sudley Rd.; and

WHEREAS, the site is identified as GPINs 7597-92-1061, 7597-82-7607 & 7597-82-4068, and consists of +/- 128.4 acres; and

WHEREAS, the site is designated REC, Regional Employment Center, and SRH, Suburban Residential High in the Comprehensive Plan; and

WHEREAS, upon closure of the public hearing, Chairman Vanegas recommended the Planning Commission table the item until Commissioner Arnold's arrival, thus the Prince William County Planning Commission lay Rezoning #PLN2014-00040, Blackburn, on the table until such time Commissioner Arnold arrived and made her motion (RES 15-034); and

WHEREAS, Commissioner Bryant motioned for the removal of Rezoning #PLN2014-00040, Blackburn, upon her arrival;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission does hereby remove Rezoning #PLN2014-00040, Blackburn, from the table.

Votes:

Ayes: Bryant, Fry, Holley, Hosen, McKay, Vanegas

Nays: None


Abstained from Vote: Haynes

Absent from Vote: None

Absent from Meeting: Arnold

MOTION CARRIED

Attest:


Teresa M. Taylor
Clerk to the Planning Commission

Attachment N
Planning Commission Resolutions

PLANNING COMMISSION RESOLUTION

MOTION: ARNOLD

April 15, 2015
Regular Meeting
Res. No. 15-038

SECOND: FRY

**RE: REZONING #PLN2014-00040, BLACKBURN, GAINESVILLE
MAGISTERIAL DISTRICT**

ACTION: RECOMMEND APPROVAL

WHEREAS, this is a request to rezone +/- 76.7 acres from A-1, Agricultural to PMR, Planned Mixed Residential, and +/- 51.7 acres from A-1 to PMD, Planned Mixed Use District, to allow for the development of a maximum of 416 residential units (161 single family detached units & 255 townhouses), and allow up 1,000,000 square feet of office and a maximum of 115,000 square feet of retail development; and

WHEREAS, the site is located north and south sides of Balls Ford Rd. approx. 1/2 mile west of the intersection of Balls Ford Rd. and Sudley Rd.; and

WHEREAS, the site is identified as GPINs 7597-92-1061, 7597-82-7607 & 7597-82-4068, and consists of +/- 128.4 acres; and

WHEREAS, the site is designated REC, Regional Employment Center, and SRH, Suburban Residential High in the Comprehensive Plan; and

WHEREAS, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on April 15, 2015, at which the public testimony was received and the merits of the above-referenced request were considered; and

WHEREAS, the Prince William County Planning Commission believes that public general welfare as well as good planning practices are served by the approval of this request;

NOW, THEREFORE, BE IT RESOLVED, that the Prince William County Planning Commission does hereby recommend approval of Rezoning #PLN2014-00040, Blackburn, with proffers dated March 18, 2015, with amended proffers to include language for the phasing of construction for the first office building within landbay 5, and amend Proffer 24 to require fire sprinklers to be installed required for single family residential units with side yard setbacks of less than 10 feet.

Votes:

Ayes: Arnold, Bryant, Fry, Vanegas

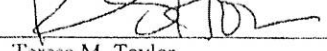
Nays: Holley, Hosen, McKay


Abstained from Vote: Haynes

Absent from Vote: None


Absent from Meeting: None

MOTION CARRIED

Attest:  FOR
Teresa M. Taylor
Clerk to the Planning Commission



Prince William County Government
Board of County Supervisors




Rezoning #PLN2014-00040 – Blackburn

Gainesville Magisterial District

Stephen Donohoe – Planning Office

REZ #PLN2014-00040 – Blackburn

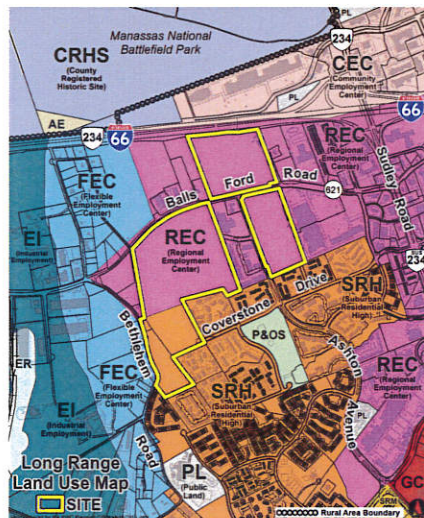
- Proposal – To rezone +/- 128.4 acres from A-1, Agricultural to PMD, Planned Mixed Use District and PMR, Planned Mixed Residential.
 - ◆ To allow for the development of a maximum of 415 residential units (160 single family detached & 255 townhouses)
 - ◆ To allow for the development of up to 1,000,000 square feet of office and a maximum of 115,000 square feet of commercial/retail development
- Location – On the north and south side of Balls Ford Rd. approx. ½ mile west of the intersection of Balls Ford Rd. and Sudley Rd.
- Recommendation – Denial



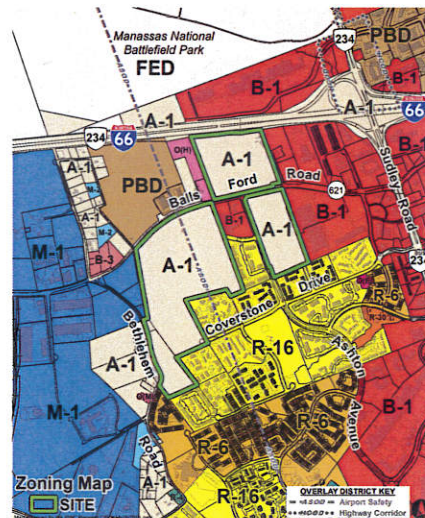
November 17, 2015

Gainesville Magisterial District

REZ #PLN2014-00040 – Blackburn



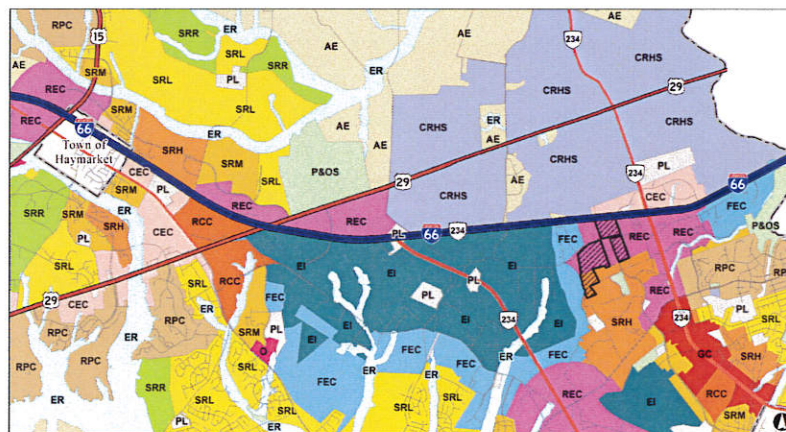
November 17, 2015



Gainesville Magisterial District

[3]

REZ #PLN2014-00040 – Blackburn

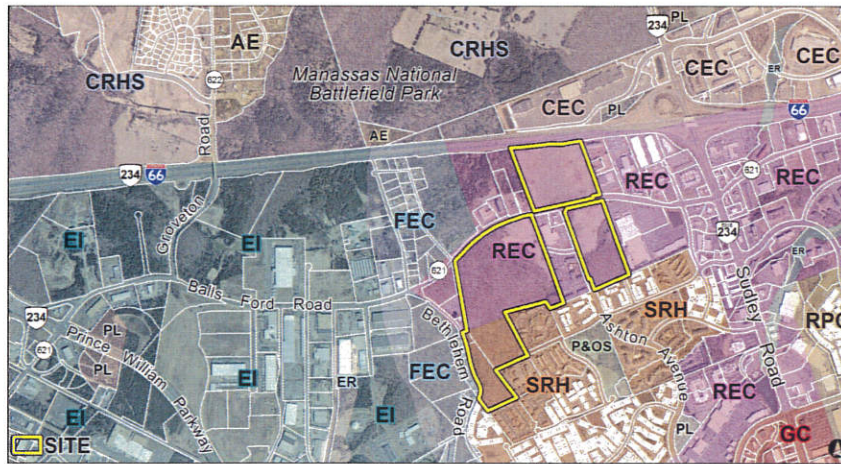


November 17, 2015

Gainesville Magisterial District

[4]

REZ #PLN2014-00040 – Blackburn



November 17, 2015

Gainesville Magisterial District

[5]

REZ #PLN2014-00040 – Blackburn



November 17, 2015

Gainesville Magisterial District

[6]

REZ #PLN2014-00040 – Blackburn

Blackburn Total Project – 128.4 acres	Acres – Total Project area designated REC - 105.5	Gross Floor Area (SF) – Total 1,673,500**
Proposed Development in Regional Employment Center	Nonresidential +/-41.1 ac. or 38% of project	Nonresidential Total = 1,115,000 SF or 67% of project
Nonresidential vs. residential comparison using <u>acreage</u> and <u>floor area</u>		
Comprehensive Plan recommends that residential development not exceed 25% of the project <u>building area</u>	Residential +/-64.4 ac. or 62% of project	Residential Total - 416 units or 558,500* SF or 33% of project

November 17, 2015

Gainesville Magisterial District

[7]

REZ #PLN2014-00040 – Blackburn

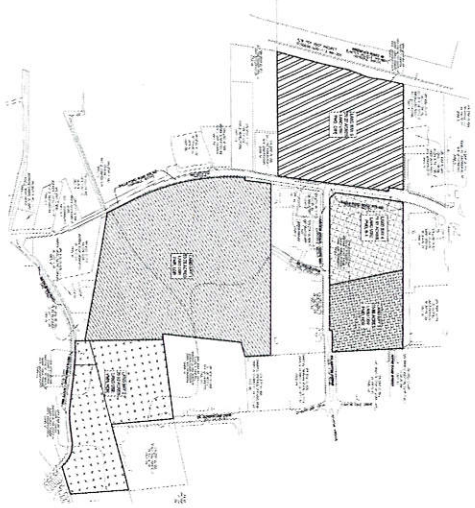


November 17, 2015

Gainesville Magisterial District

[8]

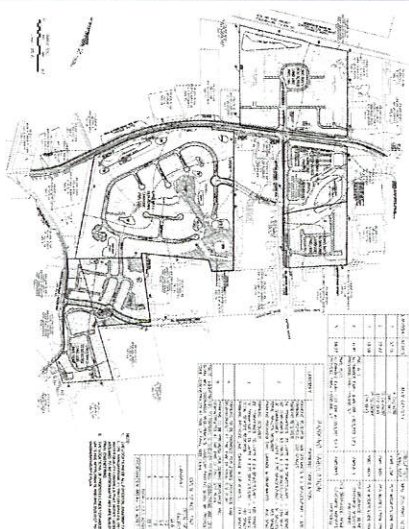
REZ #PLN2014-00040 – Blackburn



November 17, 2015 Gainesville Magisterial District

[9]

REZ #PLN2014-00040 – Blackburn



November 17, 2015 Gainesville Magisterial District

[10]

REZ #PLN2014-00040 – Blackburn



November 17, 2015

Gainesville Magisterial District

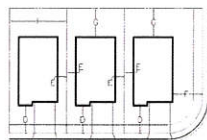
[11]

REZ #PLN2014-00040 – Blackburn

MODIFIED VILLAGE HOUSE

THIS DWELLING TYPE IS A SINGLE FAMILY RESIDENCE WHICH IS FULLY DETACHED FROM NEIGHBORING STRUCTURES. THE VILLAGE HOUSE IS DISTINGUISHED BY SMALL FRONT AND SIDE YARDS. THE STREETSCAPE OF VILLAGE HOUSES IS A CRITICAL ELEMENT GIVEN THE SMALL SETBACKS FROM THE STREET. THIS EACH PLAN USING THIS HOUSING TYPE SHALL DEMONSTRATE GOOD DESIGN PRACTICES. THE FOLLOWING TABLE AND TEXT SPECIFY THE MINIMUM STANDARDS FOR A VILLAGE HOUSE.

A. MINIMUM LOT AREA	6000 SQ. FT.	5000 SQ. FT.
B. MAXIMUM LOT COVERAGE	30	35
C. MAXIMUM BUILDING HEIGHT	35 FT.	35 FT.
D. MINIMUM SETBACKS		
D1. FRONT SETBACK TO HOUSE	20 FT.	5 FT.
D2. SIDE	10 FT.	5 FT.
D3. CORNER LOT (SIDE)	20 FT.	5 FT.
D4. REAR LOT LINE TO HOUSE	10 FT.	20 FT.
D5. REAR LOT LINE TO GARAGE	5 FT.	20 FT.
E. MINIMUM LOT WIDTH	60 FT.	50 FT.



NOTES:

- 1) UNROOFED STRUCTURES SUCH AS, BUT NOT LIMITED TO, LANDINGS, PORCHES, DECKS, STEPS, AND STOODS IN ANY COMBINATION GREATER THAN EIGHT (8) INCHES ABOVE EXISTING GRADE MAY BE LOCATED IN THE FRONT OR REAR YARD ONLY, SUBJECT TO THE MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND 5 FEET FROM THE REAR PROPERTY LINES.
- 2) ROOFED STRUCTURES SUCH AS, BUT NOT LIMITED TO, LANERIS, PORCHES, DECKS, STEPS, AND STOODS IN ANY COMBINATION MAY BE LOCATED IN THE FRONT OR REAR YARD ONLY, SUBJECT TO THE SETBACK REQUIREMENTS FOR THE PRIMARY STRUCTURE.
- 3) PARKING REQUIREMENTS SHALL MEET STANDARDS FOR SINGLE FAMILY DETACHED UNITS.
- 4) ARCHITECTURAL FEATURES SUCH AS, BUT NOT LIMITED TO, WINDOWS, SILL CORNICES, EAVES, AND GUTTERS, BUT EXCLUDING FLOOR AREA SUPPORTED BY CANTILEVERED CONSTRUCTION MAY PROJECT UP TO 3 FEET INTO ANY REQUIRED YARD SETBACK.
- 5) HOPSTEAM LOTS SHALL MEET THE STANDARDS IN SECTION 32-300.61.10 OF THE ZONING ORDINANCE.

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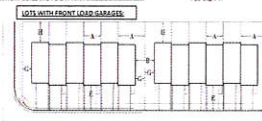
[12]

Attachment E

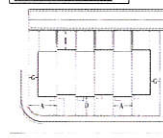
MODIFIED TOWN HOUSE (PMR)

THIS DWELLING TYPE CONSISTS OF A SINGLE FAMILY ATTACHED UNIT, WITH INDIVIDUAL OUTSIDE ACCESS. ROWS OF TOWNHOUSES THAT CONTAIN NO MORE THAN SIX DWELLING UNITS IN A GROUP, EXCEPT AS MAY BE ALLOWED IN TOWN CENTERS. BACK-TO-BACK TOWNHOUSES SHALL CONTAIN NO MORE THAN 12 DWELLING UNITS IN A GROUP. LANEWAYS UNITS MAY BE EITHER TOWNHOUSE OR BACK-TO-BACK TOWNHOUSE DEVELOPMENTS, SUBJECT TO NEIGHBORHOOD AND NON-RESIDENTIAL PARKING REQUIREMENTS IN THE DESIGN AND CONSTRUCTION STANDARDS MANUAL. RESIDENTIAL USE SHALL NOT BE LOCATED ON THE FIRST FLOOR OF TOWNHOUSE UNITS. THE FOLLOWING TABLE SPECIFY THE MINIMUM STANDARDS.

A. MINIMUM LOT WIDTH	30 FT.	15 FT.
B. MINIMUM SETBACK	20 FT.	10 FT.
C. MAXIMUM BUILDING HEIGHT	35 FT.	35 FT.
D. MINIMUM SETBACKS		
D1. FRONT (WITH REAR LOAD GARAGES)	10 FT.	10 FT.
D2. FRONT (WITH FRONT LOAD GARAGES)	20 FT.	10 FT.
D3. SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FT.	10 FT.
D4. SIDE (WITH FRONT)	10 FT.	10 FT.
D5. REAR (CORRESPONDING TO FRONT LOAD GARAGE)	10 FT.	10 FT.
D6. REAR (CORRESPONDING TO REAR LOAD GARAGE)	10 FT.	10 FT.
E. MINIMUM BUILDING FOOTPRINT	720 SQ. FT.	720 SQ. FT.



LOTS WITH FRONT LOAD GARAGES



LOTS WITH REAR LOAD GARAGES



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RECOMMENDATION

- Planning Commission recommends approval, with proffers dated March 18, 2015, with modifications to the proffers to include:
 - ◆ A proffer for the phasing of construction for the first office building within landbay 5.
 - ◆ Revision to proffer #24 to ensure that single family residential units with side yard setbacks of less than 10 feet shall have a fire sprinkler system

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RECOMMENDATION

- Staff recommends denial, for the following reasons:
 - ◆ The applicant's proposal includes construction phasing for the timing of the proposed office/employment development; however very late in the build out of the project. The majority of the project area is designated REC, and the Board's land use policy expressly states the need for nonresidential phasing in order to ensure that employment development always remains the primary use within this planned district.
 - ◆ The purpose of the Regional Employment Center classification is to provide for areas located close to and/or with good access from an interstate highway where intensive regional employment uses are to be located.
 - This property is situated with high visibility from, and excellent access to, Interstate 66. Due to the limited availability of employment planned areas along I-66, the area proposed for residential development should instead continue to provide opportunities for future intensive regional employment investment.
 - This project, without earlier construction phasing of the employment component, is not consistent with the development guidance provided in the Comprehensive Plan for the REC.

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RECOMMENDATION: continued

- The applicant's residential proposal consists of single family detached units and townhouses. Neither unit type is envisioned in the REC designation. In addition, residential uses shall represent no greater than 25 percent of the total REC gross floor area of the project.
 - ◆ Proposed residential floor area is estimated to be 33% of the overall project floor area.
 - ◆ Proposed residential acreage consists of 62% of the project/land area (designated REC).
 - ◆ Within each landbay, uses are not well integrated, each containing a single use, which does not provide walkability.
- The proposal contains no active recreation open space area within the project area. The open space shown along Balls Ford Rd. is a stormwater management dry pond. Other areas shown to contain open space are wetlands.
- The Comprehensive Plan Future Transit Alternatives Map identifies a future Metro station location at Sudley Rd. and I-66. In addition, VDOT is actively seeking to locate a major park and ride facility in this location. This further highlights the planned intent for employment development, density and a mix uses in this area.

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■ RECOMMENDATION: continued

- May 12, 2015 - Board deferred the case to a date uncertain
- Applicant requested the case be scheduled for the November 17, 2015 hearing
- No changes to the project have been made
- Staff continues to recommend denial

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