

**MOTION:**

**September 21, 2021**

**SECOND:**

**Regular Meeting**

**Ord. No. 21-**

**RE:**

**REZONING AND RELEASE DECLARATION #REZ2016-00030, INNOVATION TOWN CENTER – BRENTSVILLE MAGISTERIAL DISTRICT**

**ACTION:**

**WHEREAS**, this is a request to rezone ±107.4 acres from A-1, Agricultural, and PBD, Planning Business District, to PMD, Planned Mixed Use District, to allow a mixed-use development that will include up to 1,032 residential units and ±447,100 square feet of nonresidential uses and associated waivers and modifications, including building height floor area ratio increases, and a modification to the Technology Overlay District regulations to align with the Innovation Park Small Area Plan; and

**WHEREAS**, the property is located at the southeast quadrant of the intersection of Prince William Parkway/Route 234 By-Pass and Wellington Road; and

**WHEREAS**, the site is currently zoned A-1, Agricultural, and PBD, Planned Business District. The site is also located within the Technology Overlay District (TeOD), and the Airport Safety Overlay District; and

**WHEREAS**, the site is designated OMU, Office Mixed Use, CMU, Commercial Mixed-Use, and TC, Town Center, in the Comprehensive Plan and is located within the Innovation Park Small Area Plan, which was adopted by the Prince William Board of County Supervisors (Board) on December 15, 2020; and

**WHEREAS**, staff has reviewed the subject application and recommends approval, as stated in the staff report; and

**WHEREAS**, the Planning Commission, at its public hearing on September 8, 2021, recommended approval, as stated in Resolution Number (Res. No.) 21-088, on the Regular Agenda; and

**WHEREAS**, a Board public hearing, duly advertised in a local newspaper for a period of two (2) weeks, was held on September 21, 2021, and interested citizens were heard; and

**WHEREAS**, the Board finds that public necessity, convenience, general welfare, and good zoning practice are served by the approval of the request;

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**NOW, THEREFORE, BE IT ORDAINED** that the Prince William Board of County Supervisors hereby approves Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center subject to the proffers dated August 26, 2021;

**BE IT FURTHER ORDAINED** that the Prince William Board of County Supervisors approves the attached Amendment to Declaration for Property of the Prince William Board of County Supervisors of Prince William County, Virginia, and authorizes the County Executive to sign that document on behalf of the Prince William Board of County Supervisors and approved as to form by the County Attorney's Office for recordation in the County land records;

**BE IT FURTHER ORDAINED** that the Prince William Board of County Supervisors' approval and adoption of any proffered conditions does not relieve the applicant and / or subsequent owners from compliance with the provisions of any applicable ordinances, regulations, or adopted standards.

ATTACHMENTS: Proffers, Dated September 9, 2021, Including Exhibits  
Amendment to Declaration

**Votes:**

**Ayes:**

**Nays:**

**Absent from Vote:**

**Absent from Meeting:**

**For Information:**

Planning Director

Ms. Jonelle Cameron  
Walsh, Colucci, Lubeley, & Walsh, P.C.  
4310 Prince William Parkway, Suite 300  
Woodbridge, VA 22192

**ATTEST:** \_\_\_\_\_

**Clerk to the Board**



## PROFFER STATEMENT

**RE:** REZ #PLN2016-00030, Innovation Town Center

Record Owners: MJV Associates LLC, PWC – Parcel A, LLC and Prince William Board of County Supervisors

Contract Purchaser/Agent: Stanley Martin Homes, LLC

Property: 7596-92-6825 and 7696-00-5505 (part) (hereinafter the "Property")  
Brentsville Magisterial District  
107.40 total acres  
106.08 acres A-1, Agricultural, to PMD, Planned Mixed Use District  
1.320 acres PBD, Planned Business District, to PMD, Planned Mixed Use District

Date: September 9, 2021

The undersigned hereby proffers that the use and development of the subject Property shall be in strict conformance with the following conditions and shall supersede all other proffers made prior hereto, including proffers associated with REZ #PLN2007-00535. In the event the referenced rezoning is not granted as applied for by the Applicant, the below-described proffers shall be withdrawn and shall become null and void and proffers associated with REZ #PLN2007-00535 shall remain in full force and effect on the applicable portion of the Property.

The headings of the proffers set forth below have been prepared for convenience or reference only and shall not control or affect the meaning or be taken as an interpretation of any provision of the proffers. Any improvements proffered herein below shall be provided at the time of development of the portion of the site served by the improvement, unless otherwise specified. The terms "Applicant" and "Developer" shall include all future owners and successors in interest.

"Final Rezoning", as the term is used herein, shall be defined as that zoning which is in effect on the day following the last day upon which the Prince William Board of County Supervisors' decision granting the rezoning may be contested in the appropriate court or, if contested, the day following entry of a final court order affirming the decision of the Board of County Supervisors which has not been appealed, or if appealed, the day following which the decision has been affirmed on appeal.

References in this Proffer Statement to plans and exhibits shall include the following:

- A. Master Zoning Plan for Innovation Town Center, prepared by Land Design Consultants, Inc., dated October, 2020, last revised September 9, 2021, consisting of the following sheets ("MZP"):
- Cover Sheet;
  - Land Use Plan ("Sheet 2");
  - Landscape Buffer Plan ("Sheet 4");
  - Landscape Buffer Details ("Sheet 4A");
  - Onsite Multi-Modal Transportation Exhibit ("Sheet 5");
  - Street Sections ("Sheet 5A");
  - Street Sections ("Sheet 5B");
  - University Boulevard Improvements ("Sheet 5C");
  - Wellington Road Improvements ("Sheet 5D");
  - Future Katherine Johnson Avenue ("Sheet 5E");
  - Bethlehem Road & Wellington Road Cross Sections ("Sheet 5F");
  - Infrastructure Phasing Plan ("Sheet 6");
  - Town Center Phasing Plan ("Sheet 6A");
  - Interim Plaza ("Sheet 6B"); and
  - Utility Plan ("Sheet 7").
- B. Design Guidelines prepared by Land Design Consultants Inc., dated September 9, 2021 (the "Design Guidelines").
- C. Exhibit entitled "Exhibit A – Residential and Non-Residential Performance Standards," dated September 9, 2021 ("Exhibit A").

### **USES AND SITE DEVELOPMENT**

1. Development of the Property shall be in substantial conformance with the Land Use Plan. The exact boundaries and acreage of the land bays may be increased or decreased at the time of final site plan/subdivision approval, not to exceed ten percent (10%) of the gross area of the larger land bay impacted by each such change.
2. Pursuant to 32-506.09.1 and 32-700.25 residential uses shall be permitted on the

Property. The maximum number of residential units constructed on the Property shall not exceed 1,032 units.

3. The Applicant may use the Property in accordance with the Planned Mixed-Use District (PMD), as waived/modified in accordance with these Proffers. Pursuant to Section 32-400.07 of the Zoning Ordinance, subject to a special use permit, the Applicant shall be permitted to have drive-through uses on the Property.
4. Land Bay A Uses
  - a. Pursuant to Section 32-400.07 of the Zoning Ordinance, the Applicant shall be permitted to have a maximum of one motor vehicle fuel station use (with associated convenience and/or quick service food store/restaurant) on the Property subject to a special use permit. In the event a motor vehicle fuel station is constructed on Property, the following performance standards shall apply:
    - i. The use shall be located adjacent to Wellington Road and the canopy shall not be oriented parallel to Wellington Road;
    - ii. The building design shall be compatible with the Town Center and urban in nature, and shall include outdoor seating;
    - iii. The roof shall not be flat and the canopy shall have similar design as the principal building;
    - iv. Exterior lighting shall incorporate low impact lighting design, which shall include dimming of exterior lights after dusk but not to the detriment of public safety; and
    - v. Outdoor storage shall be prohibited and refuse containers shall be fully enclosed.
  - b. Retail uses within Land Bay A shall be limited to a motor vehicle fuel station, subject to Proffer 4.a. above, commercial recreation, indoor and commercial recreation, outdoor.

- c. The following M-2 uses shall be prohibited in Land Bay A:
  - i. Data center.
  - ii. Motor vehicle service.
  - iii. Recycling collection point.
  - iv. Self-storage center.
  - v. Tool and equipment rental.
5. Density may be allocated between land bays at final site plan approvals, provided the maximum density as noted in 2 above shall not be exceeded.
6. For purposes of calculating open space and tree canopy coverage, the entire Property shall be utilized, and such calculations shall not apply to individual Land Bays or parcels within a land bay. At the time of each final site plan review, the Applicant shall provide a cumulative tabulation of the open space and tree canopy coverage calculations for the entire Property. Tree canopy coverage for the Property shall be a minimum of ten percent (10%) and lot coverage shall not exceed ninety percent (90%).
7. Town Center Phasing – The Town Center will be developed in Phases that are coordinated with transportation and other infrastructure improvements as further set out in these Proffers. Notwithstanding the ordering of these phases, the units, improvements, and infrastructure identified herein may be constructed in any sequence consistent with the requirements of the Property and related improvements and infrastructure.
  - a. Phase 1 – Prior to the issuance of the 150<sup>th</sup> residential building permit on the Property, the Applicant shall make pad ready a portion of Block 1 as identified on Sheet 6A of the MZP. For purposes of this Proffer 7 “pad-ready” shall mean clearing, grading, stabilization, and extension and availability of water and sewer to better prepare the Pad-Ready Area for future commercial build-to-suit opportunities as permitted by the zoning.

- b. Phase 2 – Prior to the issuance of the 300<sup>th</sup> residential building permit on the Property, the Applicant shall construct Buildings C, M, N, E, Gateway Plaza and Interim Plaza, as generally shown in Blocks 1 and 2.
- c. Phase 3 – Prior to the issuance of the 300<sup>th</sup> residential building permit on the Property, the remainder of Block 1 shall be improved to a pad-ready condition.
- d. Phase 4 – Prior to the issuance of the 600<sup>th</sup> residential building permit on the Property, the remainder of Block 2 shall be improved to a pad-ready condition.
- e. Phase 5 – Prior to the issuance of the 670<sup>th</sup> residential building permit on the Property, the Applicant shall complete construction of Block 1 and it shall include a minimum of 50,000 square feet of non-residential space.
- f. Phase 6 – Prior to the issuance of the 900<sup>th</sup> residential building permit on the Property, Block 4 shall be improved to a pad-ready condition.
- g. Development within the Town Center may be phased such that any building may be built first, and the Applicant may use surface parking in Town Center Blocks 1-4 until structured parking is required in order to meet the required parking standards set forth in the Design Construction and Standards Manual (DCSM) and/or the Prince William County Zoning Ordinance.

### **AFFORDABLE HOUSING**

- 8. Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$250.00 per residential unit constructed on the Property to be used for the Housing Preservation and Development Fund. Said contribution shall be paid prior to and as a condition of occupancy permit issuance for each residential unit constructed on the Property. The Applicant shall receive a total credit against these Housing Preservation and Development monetary contributions in the amount of \$2,500 (which represents the credit for each single-family residential unit that could be constructed on the Property by-right).

## COMMUNITY DESIGN

9. The Applicant shall create covenants, conditions, and restrictions to coordinate development within the Property, which include such items as architectural controls, signage, building materials, lighting, and landscaping. Further, the Applicant shall establish an association or multiple associations (residential and/or nonresidential) for the Property to own, operate, and maintain open space, common areas, private roads, trails, sidewalks, signage, other recreation or common facilities (as applicable), street trees and, if appropriate, stormwater management/BMP Facilities installed by the Applicant for the Property, if not otherwise maintained by the County, in accordance with adopted County policies.
  
10. Design Guidelines – Development on the Property shall be in conformance with the design concepts and details set forth in the Design Guidelines. A Design Review Committee ("DRC") shall be established to implement the Design Guidelines and said committee shall consist of a minimum of three members, which shall include one (1) representative of the owner and one (1) architect. The DRC shall be responsible for the review and approval of all plans for development on the Property to ensure that such plans are in conformance with the design concepts and details set forth in the Design Guidelines. Minor modifications to the Design Guidelines may be made at the time of final site/subdivision plan approval. More substantial modifications to the Design Guidelines may be approved by the DRC provided the overall concept and intent of the Design Guidelines is met and subject to approval of the Planning Office as follows: prior to final approval by the DRC, each such modification to the Design Guidelines shall be forwarded to the Prince William County Planning Director, or his designee, who shall have forty-five (45) business days from receipt of such modification to notify the DRC in writing that he has determined said modification to be substantially inconsistent with the Design Guidelines or zoning. The Planning Director's written determination shall include specific references to those portions of the Design Guidelines or conditions of the zoning which are the basis for such determination. The DRC shall not approve any such substantive amendment found to be inconsistent by the Planning Director. Failure of the Planning Director to provide such written notice shall automatically mean that he has determined such amendment to be substantially consistent with the Design Guidelines and the conditions of the zoning and the DRC shall be entitled to either approve or not approve said amendment in its sole discretion.

11. Buffers, Landscaped Areas, and Open Space & Amenities

- a. Landscaping – The Applicant shall provide landscaping on the Property as generally shown on Sheets 4 and 4A of the MZP. The buffers shall be planted in accordance with the DCSM planting standards with indigenous, native species. For those buffers shown as “Natural Landscape Area” on Sheets 4 and 4A, the Applicant shall implement preservation measures to ensure the natural landscape area remains outside the limits of disturbance, and no construction activities or storage of materials shall be allowed within the natural landscape area. The aforementioned does not preclude the removal of diseased, noxious, and/or invasive vegetation outside of these limits of disturbance. Buffers shall be provided at the time the adjacent portions of the Property are developed and shall be shown on each respective final site plan.
- b. Open Space & Amenities – The Applicant shall provide open space and amenities on the portions of the Property as described in the Design Guidelines, and shall include: community gathering area, innovation playground, neighborhood promenade, three pocket parks, clubhouse and community pool, nature playground, nature park, courtyard green, linear courtyard, pavilion green & urban park, urban promenade, and gateway plaza. The open space and amenities elements shall be developed in accordance with the Design Guidelines. Buffers shall be provided at the time the adjacent portions of the Property are developed and shall be shown on each respective final site plan.
- c. Street Trees – Street trees shall be planted along roadways within the Property in substantial conformance with the details/parameters set forth in the Design Guidelines. Street trees may be clustered at various locations along the streets for purposes of providing adequate sight distance and to avoid utilities. Plantings shall be shown on all final site/subdivision plans. Street trees located within any VDOT right-of-way shall be subject to VDOT approval, and shall be maintained by the HOA or POA subject to the execution of a tree maintenance agreement, if required. The Applicant reserves the right to locate street trees within 15’ of the face of curb where deemed necessary as a result of final engineering in accordance with the DCSM.

- d. Tree Preservation – A tree preservation plan for trees identified on Sheet 4 of the MZP as “Surveyed Specimen Tree To Be Preserved,” shall be prepared by a certified arborist and shall be submitted to the Prince William County Public Works’ Watershed Management Branch’s County Arborist for review and approval at the time of final site plan review for that portion of the Property. The tree preservation plan shall be in accordance with the Prince William County DCSM standards and shall be submitted at the time the adjacent portions of the Property are developed.
12. Community Entryways – The Applicant shall provide entryway monument signage and associated landscaping as identified in the Design Guidelines. The entryway signs may be constructed at any time and shall be subject to obtaining sign permit approval.
13. Interim Plaza – Subject to County approval, the Applicant shall construct, pursuant to Phase 2 identified in Proffer 7 above, an Interim Plaza as identified in the Design Guidelines and as generally shown on Sheet 6B of the MZP. Final design of the Interim Plaza shall be determined at final site plan approval and in accordance with applicable waivers and modifications from the Zoning Ordinance or DCSM.
14. Public Art – In consultation with George Mason University, the Applicant shall provide public art in the Town Center as outlined in the Design Guidelines.
15. Town Center Building Height – All buildings within the Town Center, except for the retail kiosk buildings (Buildings M, N, O and P) and structures within the Interim Plaza, shall be a minimum of three stories in height.

### **CULTURAL RESOURCES**

16. Curation – As a condition of first final subdivision/site plan approval, the Applicant shall curate with the County, artifacts, field records, laboratory records, photographic records, computerized data and other historical records the Applicant possesses as recovered as a result of its Phase I cultural resources investigations prepared by Dutton and Associates, LLC, entitled Phase I Cultural Resources Survey of the ±42.8 Hectare (106-Acre) Wellington Road (Innovation Town Center) Property, dated May 2016. All artifacts and records submitted for curation shall



meet current professional standards and The Secretary of the Interior's Standards and Guidelines for Archeology and Historic Preservation. A curation fee equal to VDHR's curation fee (not to exceed \$350.00 per box) will be paid by the Applicant at the time of delivery to the County. Ownership of all records submitted for curation shall be transferred to the County with a letter of gift.

17. The Applicant shall prepare an interpretive history plan for the Nature Trail and Town Center. The interpretive history plan shall consist of interpretive signs (multiple) and/or historical markers which describe twentieth century farming, dairy farming in Prince William County, or sharecropping and pre-contact (Native American) history in the area. The location and content on the interpretive signs shall be prepared in consultation with the County Archaeologist and/or Historical Commission and shall follow the general guidance identified in the Design Guidelines. The interpretive history information shall be installed/constructed in conjunction with the development immediately adjacent thereto, and shall be shown on the respective final site plan for those portions of the Property. The signage associated with the interpretive history plan, as outlined above, shall not count toward the number of signs permitted by right on the Property.

### **ENVIRONMENTAL**

18. Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$75.00 per acre to conduct water quality monitoring, stream restoration projects and/or drainage improvements. Said contribution shall be paid prior to and as a condition of the approval of each final site plan.

### **LIBRARIES**

19. Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$705.24 per single family attached residential unit and \$517.77 per multi-family residential unit constructed on the Property to be used for library purposes. Said contribution shall be paid prior to and as a condition of occupancy permit issuance for each said unit constructed on the Property.

## **PARKS AND RECREATION**

20. Amenities – The Applicant shall provide an amenity package for the residents that shall include, at a minimum, a clubhouse, 5-lane, 25 meter swimming pool, and two (2) playgrounds. The Applicant shall complete construction of the clubhouse and related recreational facilities prior to and as a condition of issuance of occupancy permit for the 485<sup>th</sup> residential unit on the Property. Other amenities identified in the Design Guidelines, which include the community gathering area, innovation playground, neighborhood promenade, three pocket parks, nature playground, nature park, courtyard green, linear courtyard, pavilion green and urban park, urban promenade, and gateway plaza shall be constructed prior to the final building permit for the section in which they are located and shall be shown on each respective final site plan.
21. Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$770.48 per single family attached residential unit and \$565.67 per multi-family unit constructed on the Property to be used for parks and recreation purposes. Said contribution shall be paid prior to and as a condition of the occupancy permit issuance for each said unit constructed on the Property.

## **PUBLIC SAFETY**

22. Commercial Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$0.61 per square foot of nonresidential gross floor area (“gfa”), excluding any parking structures, to be used for fire and rescue purposes. Said contribution shall be paid prior to and as a condition of the issuance of an occupancy permit for each nonresidential building constructed on the Property and the amount paid shall be based on the gfa in each nonresidential building.
23. Residential Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$1,280.04 per single family attached residential unit and \$939.78 per multi-family residential unit constructed on the Property to be used for public safety purposes. Said contribution shall be paid prior to and as a condition of occupancy permit issuance for each said residential unit constructed on the Property.

## SCHOOLS

24. Monetary Contribution – The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$8,549.47 per single family attached residential unit and \$4,499.72 per multi-family unit constructed on the Property. Said contribution shall be paid prior to and as a condition of the occupancy permit issuance for each said unit constructed on the Property. No Schools monetary contribution shall be paid for any age-restricted units constructed on the Property.

## TRANSPORTATION

25. Access – Subject to Prince William County Department of Transportation (“PWCDOT”) and the Virginia Department of Transportation (“VDOT”) approval, access to the Property shall be as shown on Sheet 5C and Sheet 5D.
26. Bicycle Parking – The Applicant shall provide bicycle parking in accordance with the DCSM. Bicycle parking shall be shown on each final site plan submitted for the Property, as warranted.
27. Katherine Johnson Avenue South – Pursuant to the terms set forth in the Inter-Parcel Land Transfer Agreement dated March 23, 2021, (as may be amended), by and among Stanley Martin Homes, LLC, Innovation Development LLC, MJV-Parcel A, LLC, PW-Parcel A, LLC, and the Board of County Supervisors of Prince William County, Virginia (the “Agreement”), the Applicant shall construct Katherine Johnson Avenue South as shown on Sheet 5, Sheet 5E, and Sheet 6 prior to the issuance of the first occupancy permit on the Property, subject to PWCDOT and VDOT approval.
28. Katherine Johnson Avenue North – The Applicant shall construct Katherine Johnson Avenue North as shown on Sheet 5, Sheet 5E, and Sheet 6 prior to the issuance of a building permit for a use that generates more than 4,731 VPD, as shown in the TIA (dated June 15, 2021) utilizing the 10<sup>th</sup> edition of the ITE Trip Generation Manual, on the Property or submit a revised TIA and provide mitigation measures, if any, in accordance with the revised TIA, subject to County and VDOT approval. In addition, the Applicant shall provide a tabulation on each final site/subdivision plan submitted to the County that identifies (1) the total VPD and

square footage to be generated on the Property for each final site/subdivision plan submitted; and (2) the cumulative VPD and square footage on the Property.

29. Hylton Boulevard East – Pursuant to the Agreement, the Applicant shall construct the portion of Hylton Boulevard East to the intersection of Katherine Johnson Avenue South as shown on Sheet 6, prior to the issuance of the first occupancy permit on the Property, subject to PWCDOT and VDOT approval.
30. Hylton Boulevard West – Pursuant to the Agreement, the Applicant shall construct the portion of Hylton Boulevard West from Prince William Parkway to the intersection of Katherine Johnson Avenue South as shown on Sheet 5, subject to PWCDOT and VDOT approval. Said portion of Hylton Boulevard West shall be open to traffic prior to the later to occur: (1) issuance of the first occupancy permit on the Property; or (2) 36 months after a final Virginia Commonwealth Transportation Board (“CTB”) decision on the access from Prince William Parkway to Hylton Boulevard.
31. Prince William Parkway Road Improvements – In accordance with the Agreement and subject to PWCDOT and VDOT approval the Applicant shall make the following transportation improvements on Prince William Parkway as shown on Sheet 5C and 5D (collectively referred to as the “Parkway Improvements”).
  - a. Construction
    - i. Subject to CTB approval, the Applicant shall construct a right turn lane and acceleration lane at Hylton Boulevard West. These lanes shall be open to traffic prior to the later to occur: (1) issuance of the first occupancy on the Property; or (2) 36 months after CTB approval of the access from Prince William Parkway to Hylton Boulevard.
    - ii. Prior to the issuance of a building permit for a use that generates more than 4,731 VPD, as shown in the TIA (dated June 15, 2021) utilizing the 10<sup>th</sup> edition of the ITE Trip Generation Manual, on the Property the Applicant shall either: (1) extend the northbound and southbound turn lanes on Prince William Parkway 200 feet at the intersection of Prince William Parkway and Wellington Road in the event the proposed Prince William Parkway & Sudley Manor Drive/Wellington Road intersection improvements at the intersection

of Prince William Parkway and Wellington Road are not fully funded; or (2) contribute \$363,400 if the aforementioned intersection improvement is funded, which may be applied to capital improvement projects in the area that are identified in the Capital Improvement Program, 6-year road plan, or other capital improvement projects adopted by the Board.

- b. Signalization – If required in connection with the Parkway Improvements the Applicant shall modify the traffic signal at the Prince William Parkway and Quadrant West intersection. Said modification shall be completed prior to the Hylton Boulevard connection to Prince William Parkway being open to the public.
  - c. In the event the approval of the connection from Prince William Parkway to Hylton Boulevard West is denied by the CTB, the Applicant shall be relieved of the obligation to construct the Parkway Improvements and shall submit a revised TIA excluding the Parkway Improvements. The Applicant shall provide mitigation measures, if any, in accordance with the revised TIA, subject to County and VDOT approval. The revised TIA shall be submitted within 60 days from CTB denial of the connection.
32. University Boulevard Road Improvements – In accordance with the Agreement and subject to PWCDOT and VDOT approval, the Applicant shall make the following improvements on University Boulevard as shown on Sheet 5C (collectively referred to as the “University Boulevard Improvements”) in accordance with the Agreement.
- a. Construction
    - i. Construct a westbound right turn lane on to Katherine Johnson Avenue Off-Site. Said turn lane shall be made prior to the issuance of the first occupancy permit on the Property.
    - ii. Construct a second westbound through lane along University Boulevard from Cannon Creek Lane to the “Katherine Johnson Avenue Off-Site. Said through lane shall be made prior to the issuance of the first occupancy permit on the Property.
    - iii. Extend the westbound right turn lane onto Prince William Parkway.

Said turn lane shall be made prior to the issuance of the first occupancy permit on the Property.

- iv. Modify the existing median and construct a third westbound through lane along University Boulevard from Katherine Johnson Avenue Off-Site to Prince William Parkway. Said improvement shall be made prior to the issuance of the first occupancy permit on the Property.
  - v. Reconstruct the raised median along University Boulevard from Prince William Parkway to Katherine Johnson Avenue Off-Site to allow for an eastbound left turn lane at Katherine Johnson Avenue Off-Site. Said improvement shall be made prior to the issuance of the first occupancy permit on the Property.
  - vi. Restripe the eastbound approach of University Boulevard to align with the adjusted receiving lanes. Said improvement shall be made prior to the issuance of the first occupancy permit on the Property.
  - vii. Construct an eastbound through lane along University Boulevard from Prince William Parkway to the right-in/right out entrance of GPIN 7695-09-7902. Said improvement shall be made prior to the issuance of the first occupancy permit on the Property.
  - viii. Remove the existing median in front of GPIN 7695-09-7902 to allow for through movement beyond the entrance of GPIN 7695-09-7902. Said improvement shall be made prior to the issuance of the first occupancy permit on the Property.
- b. Signalization – The Applicant shall be responsible for the design and installation of a traffic signal at the intersection of the site entrance for GPIN 7696-00-5505 and University Boulevard, at such time as a warrant study establishes the need and the signal is approved by the County and VDOT. A Traffic Signal Justification Report (“SJR”) shall be submitted for review at the intervals noted below, unless waived by County Transportation and VDOT or unless a prior warrant study justifies the signal and the signal is installed. Said signal shall include preemptive and pedestrian equipment as required by the County. The Applicant shall submit an SJR to VDOT for said signal prior to plan approval for the University Boulevard Transportation Improvements (unless constructed by others) and again prior to issuance of the last residential unit for the Property. If said signal has not been

deemed warranted by VDOT and/or approved for installation prior to the issuance of the last residential occupancy permit for the Property, no additional warrant studies will be required and the Applicant shall have no further obligation to provide signalization at the aforementioned intersection.

33. Wellington Road Improvements – Subject to PWCDOT and VDOT approval the Applicant shall make, at no cost to the County, the following transportation improvements on Wellington Road as shown on Sheet 5D (collectively referred to as the “Wellington Road Improvements”). The Applicant shall be responsible for the acquisition, or cost thereof, of off-site right-of-way, including any easements required for utilities, utility relocation, and stormwater management, in accordance with Proffer #35 below, as required for the Wellington Road Improvements.
- a. Construction – Prior to the issuance of a building permit for a use that generates more than 4,731 VPD, as shown in the TIA (dated June 15, 2021) utilizing the 10<sup>th</sup> edition of the ITE Trip Generation Manual, on the Property the Applicant shall construct the following improvements or submit a revised TIA excluding the Wellington Road Improvements and provide mitigation measures, if any, in accordance with the revised TIA, subject to County and VDOT approval. In addition, the Applicant shall provide a tabulation on each final site/subdivision plan submitted to the County that identifies (1) the total VPD and square footage to be generated on the Property for each final site/subdivision plan submitted; and (2) the cumulative VPD and square footage on the Property.
    - i. Construct/restripe an eastbound right turn lane at the intersection of Wellington Road and JD Redding Drive (in front of the Property entrance). Construct an eastbound right turn lane, a westbound right turn lane and a westbound left turn lane at the intersection of Wellington Road, Bethlehem Road and Katherine Johnson Avenue
    - ii. Reconstruct the raised median along Wellington Road from Prince William Parkway to JD Reading Drive to accommodate improvements at Bethlehem Road and Katherine Johnson Avenue North.

b. Signalization

- i. Subject to PWCDOT and VDOT approval, the Applicant shall provide timing modification to the traffic signal at the intersection of Wellington Road and Prince William Parkway. Said timing modifications shall be completed in connection with the transportation improvements being constructed at the intersection of Wellington Road, Bethlehem Road, and Katherine Johnson Avenue North.
- ii. The Applicant shall be responsible for the design and installation of a traffic signal at the intersection of Katherine Johnson Avenue, Wellington Road, and Bethlehem Road, at such time as a warrant study establishes the need and the signal is approved by PWCDOT and VDOT. An SJR shall be submitted for review at the intervals noted below, unless waived by PWCDOT and VDOT or unless a prior warrant study justifies the signal and the signal is installed. Said signal shall include preemptive and pedestrian equipment as required by the County. The Applicant shall submit an SJR to PWCDOT and VDOT for said signal prior to plan approval for the Wellington Road Transportation Improvements (unless constructed by others) and again prior to issuance of the last residential occupancy permit for the Property or when requested by VDOT and/or PWCDOT. If said signal has not been deemed warranted by PWCDOT and VDOT and/or approved for installation prior to the issuance of the last residential occupancy permit for the Property, no additional warrant studies will be required and the Applicant shall have no further obligation to provide signalization at the aforementioned intersection.
- iii. The Applicant shall be responsible for the design and installation of a traffic signal or a roundabout within right-of way to be provided by the County at the intersection of Wellington Road and Hornbaker Road at such time as a warrant study establishes the need and the signal or roundabout is approved by PWCDOT and VDOT. An SJR shall be submitted for review at the intervals noted below, unless waived by PWCDOT and VDOT or unless a prior warrant study justifies the signal or roundabout and the signal or roundabout is



installed. Said signal shall include preemptive and pedestrian equipment as required by the County. The Applicant shall submit an SJR to PWCDOT and VDOT for said signal or roundabout prior to plan approval for the Wellington Road Transportation Improvements (unless constructed by others) and again prior to issuance of the last residential occupancy permit for the Property or when requested by VDOT and/or PWCDOT. If said signal or roundabout has not been deemed warranted by PWCDOT and VDOT and/or approved for installation prior to the issuance of the last residential occupancy permit for the Property, no additional warrant studies will be required and the Applicant shall have no further obligation to provide signalization or the roundabout at the aforementioned intersection.

34. Bethlehem Road Improvements – Subject to PWCDOT and VDOT approval, the Applicant shall, at no cost to the County or VDOT, realign Bethlehem Road from Wellington Road to Thong Pan Road as shown on Sheet 5D. The Applicant shall be responsible for the acquisition, or cost thereof, of off-site right-of-way, including any easements required for utilities, utility relocation, and stormwater management, in accordance with Proffer #38 below, as required for the Bethlehem Road Improvements. Said realignment shall occur prior to the issuance of a building permit for a use that generates more than 4,731 VPD, as shown in the TIA (dated June 15, 2021) utilizing the 10<sup>th</sup> edition of the ITE Trip Generation Manual, on the Property or submit a revised TIA and provide mitigation measures, if any, in accordance with the revised TIA, subject to County and VDOT approval. In addition, the Applicant shall provide a tabulation on each final site/subdivision plan submitted to the County that identifies (1) the total VPD and square footage to be generated on the Property for each final site/subdivision plan submitted; and (2) the cumulative VPD and square footage on the Property.
35. Turn Lanes at Internal Intersections – At time of final site plan approval, the Applicant shall prepare and submit turn lane warrant analyses on internal roads within the Property that intersect with Katherine Johnson Avenue. If warranted and not waived by PWCDOT and/or VDOT, the Applicant shall construct said turn lanes.
36. Interparcel Access – Subject to County and VDOT approval, the Applicant shall reserve right-of-way for three (3) vehicular interparcel connections in locations as

generally shown on Sheet 5 of the MZP. The exact location of the interparcel connections shall be determined at final site plan approval.

37. Shared Use Path and Multi-Use Trail – Subject to County and VDOT approval, the Applicant shall construct a ten foot (10') multi-use trail (“Trail”) along Prince William Parkway and a Shared Use Path (“Path”) along Wellington Road as generally shown on Sheet 5. Said Trail or Path shall be shown on the final site plan for development of the adjacent portion of the Property, and said Trail or Path shall be constructed at the time the adjacent portion of the Property is developed, subject to the consent of the easement holder, if applicable. The Path along Wellington Road shall be located within the Wellington Road right-of-way, however, in the event the construction of the Path within the right-of-way requires the relocation of power lines, the Applicant shall be allowed to place the Path outside of the right-of-way within a public pedestrian access easement.
  
38. Eminent Domain – In the event the Applicant is not able to acquire off-site right-of-way required in order to provide the Wellington Road Improvements and/or the Bethlehem Road Improvements identified above, for roads and associated pedestrian facilities identified in the Comprehensive Plan , including, but not limited to, any easements required for utilities, utility relocation, grading, and stormwater management for the road improvements, the Applicant shall request the County to acquire the right-of-way and easements by means of its condemnation powers at Applicant's expense. Applicant's request shall be in writing and shall comply in all respects with the County's Eminent Domain Policy. Said request shall be made to the appropriate County agency and shall be accompanied by the following:
  - a. The names of the record owners, the property addresses, tax map parcel numbers, and GPIN numbers for each landowner from whom such right-of-way and/or easements are sought.
  - b. Plats, plans, and profiles showing the necessary right-of-way and/or easements to be acquired and showing the details of the proposed transportation improvements to be located on each such property.
  - c. Pursuant to Virginia Code § 25.1-417, a determination of the value of the Property shall be based on the following:

- i. If the assessed value is less than \$25,000, then the value shall be determined by assessment records or other objective evidence; or
  - ii. If the assessed value is greater than \$25,000 an independent appraisal of the value of the right-of-way and easements to be acquired, and any and all damages to the residue of the involved property, said appraisal to be performed by an appraiser licensed in Virginia and approved by the County.
- d. A 60 year title search of each involved property.
- e. Documentation demonstrating to the County's satisfaction Applicant's good faith, best efforts to acquire the right-of-way and/or easements, at a cost of at least the appraised value of the involved property interests.
- f. A letter of credit acceptable to the County, cash or equivalent (from a financial institution acceptable to the County) in an amount equal to the appraised value of the property to be acquired, and all damages to the residue, together with an amount representing the County's estimate of its cost of condemnation proceedings, in a form permitting the County to draw upon the same as necessary to effectuate the purposes hereof.
- g. An agreement signed by Applicant's representative and approved by the County Attorney's Office whereby Applicant agrees to pay all costs of the condemnation, including expert witness fees, court costs, exhibit costs, court reporter fees, attorney fees for the Office of the County Attorney, and all other costs associated with the litigation, including appeals. The agreement shall specifically provide that in the event the property owner is awarded in the condemnation suit more than the appraised value estimated by Applicant's appraiser, Applicant shall pay to the County the amount of the award in excess of the amount represented by the letter of credit or cash deposit within 15 days of the award.

39. Transportation Demand Management Plan

- a. The Applicant shall implement a Transportation Demand Management Program (the "Program") in connection with the development of the

Property to be managed and funded by an on-site management company and/or a transportation management association (“TMA”). This Program shall be developed to include strategies to educate about and promote within the Property, such programs as telework and similar alternative work arrangements, bike/walk programs, on-site amenities to reduce vehicle trips, such as shuttle stops, transit benefits program, local public transportation options, flexible work schedules to reduce peak hour vehicle trips, and rideshare opportunities. The Program is meant to serve the entirety of the Innovation Park Small Area Plan, and therefore will be funded through a joint commitment of those within the TMA. The Applicant shall provide a monetary contribution to the TDM fund on a building by building basis at the rate of \$0.01 per gross SF of new commercial uses to be constructed on the Property and shall be paid at issuance of occupancy permit. Such contributions may be used to implement elements of the TDM Program, as determined by the TMA in coordination with Prince William County.

- b. It is the intent of this Proffer 39 that the Program be flexible over time to respond to the evolving transportation-related circumstances of the Property, the community, and the region as well as to technologies and/or other improvements that may occur, all which may impact travel behavior and the transportation network. Accordingly, the Program may be modified and amended from time-to-time, subject to consultation with and approval of local transportation service providers and PWCDOT.
40. Pedestrian Bridge – As a condition of final site plan approval for the parking garage located in Land Bay E, the Applicant shall design the parking garage in a manner that will facilitate a connection to the future pedestrian bridge, as identified in the Innovation Park Small Area Plan, dated December 15, 2020. Construction of the pedestrian bridge shall be by others and the Applicant shall grant, at no cost to the County and/or VDOT, those on-site easements necessary to construct the pedestrian bridge.

#### **WATER AND SEWER**

41. The Property shall be served by public sanitary sewer and water and the Applicant shall be responsible for the costs and construction of those on and off-site

improvements required in order to provide such service for the demand generated by the development on the Property.

### **MISCELLANEOUS**

42. In the event the monetary contributions set forth in the Proffer Statement are paid to the Prince William County Board of County Supervisors ("Board") within eighteen (18) months of the approval of this rezoning, as applied for by the Applicant, said contributions shall be in the amounts as stated herein. Any monetary contributions set forth in this Proffer Statement which are paid to the Board after eighteen (18) months following the approval of this rezoning shall be adjusted in accordance with the Urban Consumer Price Index ("CPI-U") published by the United States Department of Labor, such that at the time contributions are paid they shall be adjusted by the percentage change in the CPI-U from that date eighteen (18) months after the approval of this rezoning to the most recently available CPI-U to the date the contributions are paid, subject to a cap of 6 percent (6%) per year, noncompounded.

### **WAIVERS & MODIFICATIONS**

43. Pursuant to Sections 32-506.09.1 and 32-700.25 of the Zoning Ordinance, approval of the subject rezoning shall constitute a waiver/modification in accordance with the following:
  - a. Waiver of Sections 32-201.18 of the Zoning Ordinance and 802.47 of the DCSM requirement for a 15 foot wide landscape area around public use and community recreation sites.
  - b. Waiver of Sections 32-210.11 and 32-210.12 of the Zoning Ordinance to allow mobile or modular offices for construction or sales offices to be located on a parcel for which there may not be an approved site plan or building permit.
  - c. Waiver of Sections 32-250.31 and 32-250.32 of the Zoning Ordinance and 802.11 and 802.12 of the DCSM requiring internal buffers between different uses to be replaced and implemented by the MZP and the Design Guidelines.

- d. Modification of Sections 32-250.40 et seq. of the Zoning Ordinance and 802.20, 802.21, 802.30, and 802.31 of the DCSM requirements for tree canopy and open space to allow calculations based upon the entire Property instead of each individual site plan.
- e. Modification of Sections 32-280.41.1, 32-405.03.2 ,and 32-506.04.1 of the Zoning Ordinance to permit the following uses on the Property: commercial recreation (outdoor) by-right and drive-through uses and a motor vehicle fuel station use subject to Board approval a special use permit.
- f. Modification of Sections 32-306.12.6.F, 32-306.12.6.G, and 32-306.12.6.H to allow for performance standards as outlined in Exhibit A.
- g. Waiver of Section 32-405.04(7) of the Zoning Ordinance to allow the residential use to exceed 35% of the gross land area of the PMD as shown on the Land Use Plan.
- h. Modification of Sections 32-401.14, 32-401.15, 32-403.24, 32-403.25, 32-402.44, 32-402.45, 32-402.14, 32-402.15, 32-402.34, 32-402.35, 32-506.05, 32-506.06, and 32-506.07 of the Zoning Ordinance to allow for the performance standards as outlined in Exhibit A.
- i. Modification of Sections 32-405.04(4) of the Zoning Ordinance and 802.47.B of the DCSM to reduce the perimeter buffer to be as shown on Sheets 4 and 4A of the MZP.
- j. Modification of Section 32-506.06 of the Zoning Ordinance requiring a 100 foot setback from all public street rights-of-way classified as interstate/freeway, parkway, and principal arterial, and to the requirement for a buffer type C with such setback, as shown on Exhibit A.
- k. Modification of Sections 32-506.06(1)(a) of the Zoning Ordinance and 802.48.B of the DCSM to reduce the required 50 foot buffer for sides and rears of single family attached units against Parkway and Principal Arterial to allow for the landscaping as shown on Sheet 4 and Sheet 4A.

## SIGNATURE PAGE

**MJV PARCEL A, LLC**, a Virginia limited liability  
company

By: MJV Associates, LLC, Sole ember

By: \_\_\_\_\_

Name: \_\_\_\_\_

Title: \_\_\_\_\_

\_\_\_\_\_

## SIGNATURE PAGE

**PWC-PARCEL A, LLC**, a Virginia limited liability company

By: PWC Associates, Limited  
Partnership, Sole Member

By: \_\_\_\_\_

Name: \_\_\_\_\_

Title: \_\_\_\_\_



## SIGNATURE PAGE

### PRINCE WILLIAM COUNTY BOARD OF SUPERVISORS

By: \_\_\_\_\_

Name: \_\_\_\_\_

Title: \_\_\_\_\_

**AMENDMENT TO DECLARATION FOR  
PROPERTY OF THE BOARD OF COUNTY SUPERVISORS OF  
PRINCE WILLIAM COUNTY, VIRGINIA**

This AMENDMENT TO DECLARATION (“**Amendment**”) dated as of the \_\_\_\_\_ day of \_\_\_\_\_, 2021, by THE BOARD OF COUNTY SUPERVISORS OF PRINCE WILLIAM COUNTY, VIRGINIA, a political subdivision (the “**Board**”), as GRANTOR and GRANTEE for indexing purposes, and STANLEY MARTIN HOMES, LLC, a Virginia limited liability company (“**SMC**”), as GRANTOR and GRANTEE for indexing purposes, recites and provides.

**RECITALS:**

WHEREAS, the Board is the governing body of Prince William County, Virginia. The Board acquired approximately 525 acres of land (the “**County Property**”) for economic development purposes located in the high technology office and business park known as INNOVATION@ Prince William. The County Property is more particularly described in the deed recorded in Deed Book 2216, at page 39, as corrected and recorded in Deed Book 2246, at page 198 and in Deed Book 2428, at page 219, and as modified by the instrument recorded as Instrument No. 200106260063346, all among the land records of Prince William County, Virginia (the “**Land Records**”).

WHEREAS, the County Property is subject to that Declaration for Property of the Board of County Supervisors of Prince William County, Virginia, Frederick M. Mako and Connie E. Mako at Innovation @ Prince William dated January 7, 2000 and recorded in Deed Book 2864, at page 390, and re-recorded as Instrument No. 200104050032230, all among the Land Records (the “**Declaration**”). The primary purpose of the Declaration was to subject the County Property to the restrictions contained in the proffered conditions accepted by the Board with the approval of REZ 00-0001. Defined terms used in this Amendment and not specifically defined herein shall have the same meaning given to such terms in the Declaration.

WHEREAS, Article VIII of the Declaration provides that the Board may amend the Declaration at any time by following the same procedure for amendment of a proffer under Virginia law and the Board's procedures. Article VIII specifically states: “No other approval shall be required to effectuate the amendment. The amendment shall take effect upon recordation in [the Land Records].” The Board and SMC are parties to an Interparcel Land Transfer Agreement effective March 23, 2021, in which the Board agrees to transfer to SMC two portions of the County Property, such portions being located on University Boulevard and further identified as a 0.8761 acre parcel and 0.5353 acre parcel of Prince William County Geographic Parcel Identification No. 7696-00-5505 (together, the “**Innovation Town Center Property**”), both as more particularly described in Exhibit A attached to this Amendment and incorporated herein by this reference.

WHEREAS, SMC is pursuing a rezoning with the Board for purposes of developing the Innovation Town Center Property and its intended use upon full and final settlement between

SMC and the Board. To accomplish the rezoning, the Board and SMC desire and intend to amend the Declaration to remove the Innovation Town Center Property from the Declaration, so that the rezoning proffer statement will thereafter apply to the Innovation Town Center Property without conflict from the provisions set forth in the Declaration. The Board and SMC are executing and recording this Amendment pursuant to Article VIII of the Declaration to accomplish this purpose.

### **PROVISIONS**

NOW, THEREFORE, pursuant to Article VIII of the Declaration, the Board hereby removes the Innovation Town Center Property, including any public right-of-way that subsequently becomes part of the Innovation Town Center Property, from the provisions of the Declaration. By this Amendment, the provisions of the Declaration shall no longer apply to the Innovation Town Center Property.

Consistent with and in furtherance of the removal of the Innovation Property from the Declaration, the chart set forth in Article II, Section 1 of the Declaration, which is entitled "Permitted Development," is amended and restated as set forth in Exhibit B attached to this Amendment to Declaration and incorporated herein by this reference.

Except as expressly amended by this Amendment, the provisions of the Declaration shall remain in full force and effect.

[SIGNATURES ON ATTACHED PAGES]

WITNESS the following signatures and seals:

Attest:

BOARD OF COUNTY SUPERVISORS OF  
PRINCE WILLIAM COUNTY

\_\_\_\_\_  
Clerk to the Board

By: \_\_\_\_\_  
CHRISTOPHER E. MARTINO, COUNTY  
EXECUTIVE, pursuant to Ordinance No. 22-

COMMONWEALTH OF VIRGINIA,  
County of Prince William, to-wit:

I, the undersigned Notary Public of and for the jurisdiction aforesaid, do hereby certify that Corey Stewart, Chairman of the Board of County Supervisors of Prince William County, whose name is signed to the foregoing Amendment dated \_\_\_\_\_, 2021, has this date appeared before me, and acknowledged the same.

Given under my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 2021

\_\_\_\_\_  
Notary Public

My commission expires: \_\_\_\_\_

Registration No.: \_\_\_\_\_

COMMONWEALTH OF VIRGINIA,  
County of Prince William, to-wit:

I, the undersigned Notary Public of and for the jurisdiction aforesaid, do hereby certify that \_\_\_\_\_, Clerk to the Board of County Supervisors of Prince William County, whose name is signed to the foregoing Amendment dated \_\_\_\_\_, 2021, has this date appeared before me, and acknowledged the same.

Given under my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

\_\_\_\_\_  
Notary Public

My commission expires: \_\_\_\_\_

Registration No.: \_\_\_\_\_

FORM APPROVED

\_\_\_\_\_  
Date

\_\_\_\_\_  
Assistant County Attorney

Exhibit A: Legal Description of Innovation Town Center Property

Metes and Bounds Description  
for the northwesterly portion of the property of  
**Board of Supervisors of Prince William County, Virginia**  
Deed Book 2428, Page 219  
Brentsville Magisterial District  
Prince William County, Virginia

Beginning at the northwesterly corner of the property herein described, said corner being on the easterly public right-of-way line of Prince William Parkway,

Thence running with said Prince William Parkway S83°47'26"W 10.50 feet to a point, said point being the southwesterly corner of PWC-Parcel A, LLC, Instrument #200601300015999 and MJV Parcel A, LLC, Instrument #201606170045675,

Thence departing said Prince William Parkway and running with said PWC-Parcel A, LLC and MJV Parcel A, LLC N83°46'09"E 506.74 feet to a point,

Thence departing said PWC-Parcel A, LLC and MJV Parcel A, LLC and running through the property of the Board of Supervisors of Prince William County, Virginia S66°20'59"W 492.90 feet to a point on the aforementioned easterly public right-of-way line of Prince William Parkway,

Thence running with said Prince William Parkway N23°52'46"W 154.85 feet to the point of beginning and containing 38,161 square feet or 0.8761 acre of land more or less.

Subject, however, to all easements, rights-of-way and restrictions of record.

Metes and Bounds Description  
for the northeasterly portion of the property of  
**Board of Supervisors of Prince William County, Virginia**  
Deed Book 2428, Page 219  
Brentsville Magisterial District  
Prince William County, Virginia

Beginning at the northeasterly corner of the property herein described, said corner being on the line of The Rector and Visitors of George Mason University, deed book 2919, page 682, and the southeast corner of PWC-Parcel A, LLC, Instrument #200601300015999 and MJV Parcel A, LLC, Instrument #201606170045675,

Thence departing said property of PWC-Parcel A, LLC and MJV Parcel A, LLC and running with The Rector and Visitors of George Mason University S25°25'41"E 122.96 feet to a point,

Thence departing said The Rector and Visitors of George Mason University and running through the property of the Board of Supervisors of Prince William County, Virginia S66°20'50"W 379.42 feet to a point on the southerly line of aforementioned property of PWC-Parcel A, LLC and MJV Parcel A, LLC,

Thence departing said Board of Supervisors of Prince William County, Virginia and running with said PWC-Parcel A, LLC and MJV Parcel A, LLC N48°13'50"E 395.20 feet to the point of beginning and containing 23,316 square feet or 0.5353 acre of land more or less.

Subject, however, to all easements, rights-of-way and restrictions of record.

Exhibit B: Amended and Restated Density Chart

**Broadview/Innovation  
County Owned Properties and FM Technologies (Mako) Properties  
Maximum Development Area**

Land Bay	PBD Land Bay Use Designation	Gross Land Bay Area (Acres)	Net Land Bay Area (Acres)	Max. Height (including Mech. equipment)	Maximum Building Area (Square Feet)	Hotel Max. Rooms	Maximum retail Building Area	Residential
8 thru 15	OC3/IC1	310.88	285	70 feet	3,394,267			
16	OC3/IC1	34.48	24.8	70 feet	290,597			
17	<del>OC3/IC1</del>	<del>4.3</del>	<del>20.3</del>	<del>70 feet</del>	<del>237,868</del>	300		
17	OC3/IC1	22.45	20.3	70 feet	237,868	300		
18, 19 and 21	OC3/IC1	61.98	56.6	70 feet	662,448			
20	OC3/IC1	13.78	7.6	70 feet	72,501	150	50,000	
31	OC3/IC1	26.40	19.81	70 feet	309,346			
Totals		Gross County and FM Technologies (Mako) acreage  471.27	Net County and FM Technologies (Mako) Development Area  414.11		Total Building Area (not including hotel and hotel/conference center)  4,967,027	Total Hotel Rooms  450	Total Retail  50,000	NONE

\*FM Technologies (Mako) owns 2.8 acres located in Land Bay 21



# STAFF REPORT

**The Board of County  
Supervisors**

Ann B. Wheeler, Chair  
Andrea O. Bailey, Vice Chair  
Victor S. Angry  
Kenny A. Boddye  
Pete Candland  
Margaret Angela Franklin  
Jeanine M. Lawson  
Yesli Vega

<b>BOCS Hearing Date:</b>	September 21, 2021
<b>Agenda Title:</b>	Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center and Special Use Permit #SUP2016-00031, Innovation Town Center
<b>District Impact:</b>	Brentsville Magisterial District
<b>Requested Action:</b>	Approve Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center, subject to proffers dated September 9, 2021, and Special Use Permit #SUP2016-00031, Innovation Town Center, subject to conditions dated September 9, 2021.
<b>Department:</b>	Planning Office
<b>Staff Lead:</b>	Parag Agrawal, AICP, Planning Director

## **EXECUTIVE SUMMARY**

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This is a request to rezone ±107.4 acres from A-1, Agricultural, and PBD, Planning Business District, to PMD, Planned Mixed Use District, to allow a mixed-use development that will include up to 1,032 residential units and ±447,100 square feet of nonresidential uses and associated waivers and modifications, including building height floor area ratio increases, and a modification to the Technology Overlay District regulations to align with the Innovation Small Area Plan. The companion Special Use Permit is for a modified “Town Center” for a ±25.1-acre area to include up to 364 residential units and up to 331,100 sq. ft. of nonresidential uses and associated waivers and modifications. The application also includes a request to amend / release the subject site from the Declaration for the property of the Prince William Board of County Supervisors (Board) at Innovation at Prince William County. The property is located at the southeast quadrant of the intersection of Prince William Parkway / Route 234 By-Pass and Wellington Road.

The Planning Commission and Staff recommends that the Board approve Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center, subject to proffers dated September 9, 2021, and Special Use Permit #SUP2016-00031, Innovation Town Center, subject to conditions dated September 9, 2021.



**BACKGROUND**

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- A. Request: This is a request to rezone ±107.4 acres from A-1, Agricultural, and PBD, Planning Business District, to PMD, Planned Mixed Use District, to allow a mixed-use development that will include up to 1,032 residential units and ±447,100 square feet of nonresidential uses and associated waivers and modifications, including building height floor area ratio increases, and a modification to the Technology Overlay District regulations to align with the Innovation Small Area Plan. The companion Special Use Permit (SUP) is for a modified “Town Center” for a ±25.1-acre area in the PMD, Planned Mixed Use District to include up to 364 residential units and up to 331,100 sq. ft. of nonresidential uses and associated waivers and modifications. The SUP is limited to the area of Land Bay E. The application also includes a request to amend / release the subject site from the Declaration for the property of the Board at Innovation at Prince William County.

<b>Uses/Features</b>	<b>Existing</b>	<b>Proposed</b>
<b>Zoning</b>	A-1, Agricultural (106.1 acres) PBD, Planned Business District (1.3 acres)	PMD, Planned Mixed Use District
<b>Use</b>	Vacant	370 single-family attached 298 multifamily (two-over-two) 364 multifamily (1,032 residential units total) ±447,100 sq. ft. of nonresidential uses
<b>Uses/Features</b>	<b>Required (PMD District)</b>	<b>Proposed</b>
<b>Minimum District Size</b>	25 acres	±107.4 acres
<b>Open Space</b>	20% minimum nonresidential (varies for different land use classifications)	30% overall for project
<b>Buffering</b>	50 feet perimeter buffering*	Variable buffering (waivers requested)

*\*Greater buffers are required by the TeOD regulations (outside of the Town Center).*

The project includes the following land bays and uses, as stated on the Master Zoning Plan:

Land Bay	Land Use Classification	Land Uses	Land Bay Acreage	Maximum Dwelling Units	Maximum Non-Residential Square Footage
A	Office Mixed Use (OMU)	Tech Commercial	12.4	N/A	116,000
B	Office Mixed Use (OMU)	Single-Family Attached	28.3	271	N/A
C	Community Mixed Use (CMU)	Single-Family Attached	23.6	99	N/A
D	Community Mixed Use (CMU)	Multifamily Stacked	18	298	N/A
E	Town Center (TC)	Commercial, Office, Retail, Multifamily, and Multifamily Mid-Rise	25.1	364	331,100
<b>Totals</b>			107.1	1,032	447,100

- B. Site Location: The property is located at the southeast quadrant of the intersection of Prince William Parkway / Route 234 By-Pass and Wellington Road. The property consists of two (2) parcels for of a total +/- 107.4 acres and is zoned A-1, Agricultural, and PBD, Planned Business District.
- C. Comprehensive Plan: The site is designated OMU, Office Mixed Use, CMU, Commercial Mixed-Use, and TC, Town Center, in the Comprehensive Plan and is located within the Innovation Park Small Area Plan, which was adopted by the Board on December 15, 2020.
- D. Zoning: The site is currently zoned A-1, Agricultural, and PBD, Planned Business District. The site is also located within the Technology Overlay District (TeOD), and the Airport Safety Overlay District.
- E. Surrounding Land Uses: The site is located on the east side of Prince William Parkway / Route 234 By-Pass, north of University Boulevard and south of Wellington Road. The abutting properties to the east consist of the George Mason University – Prince William campus on the southern portion of the site and industrial uses on the northern portion. To the south, across University Boulevard, consists of office and employment uses. To the north of the property, across Wellington Road, the uses are primarily industrial in nature.
- F. Background and Context:
- Application History: The subject applications were initially submitted for review on June 24, 2016. At that time the total project acreage was ±129.79 acres and included ±23.74 acres of County-owned property. The request was companion to a then pending Comprehensive Plan Amendment to redesignate the entire project acreage Town Center Mixed Use within the Innovation Sector Plan. The initially submitted development program included 343,000 square feet of nonresidential uses, a hotel, and a maximum of 1,416 residential units, including 200 age-restricted units.

After an extended period of inactivity, the proposal was substantially amended on November 23, 2020, and resubmitted for review. The new Innovation Park Small Area Plan was substantially completed and contained recommendations for a Town Center in this location. The acreage of the rezoning and special use permit (SUP) was reduced, and a separate rezoning application was submitted for the majority of the County-owned property (University Village at Innovation).

The acreage of the Special Use Permit (SUP) application was reduced to align with the Town Center designation in the Comprehensive Plan, as amended with the adoption of the Innovation Park Small Area Plan. The current special use permit application is 25.1 acres and does not meet the acreage minimums to require that a special use permit be filed for a "Town Center". While the site is considered a town center in general terms of implementing the Town Center designation in the Innovation Small Area Plan, it does not meet the definition of a "Town Center" in the Zoning Ordinance. This applicant has nevertheless retained the SUP and asked for waivers of the minimum acreage requirements. Staff has explained that the SUP is not required for the proposed uses.

- County-Owned Property: The 1.3-acre County property is subject to #PLN2007-00535, Innovation at Prince William. The southern portion of the property is within the "Tech Flex" area, is zoned PBD, and can be developed with uses permitted in the O(F), Office Flex, O(H), Office High-Rise, or M-2, Light Industrial, zoning districts.

This County-owned property is in the Innovation Business Park and is subject to the County Declaration, dated January 7, 2000 (Declaration for Property of the Board of County Supervisors of Prince William County, Virginia, Frederick M. Mako and Connie E. Mako at Innovation @ Prince William dated January 7, 2000, recorded in Deed Book 2864, at Page 390, and subsequently amended). The procedure for the amendment of the County Declaration is the same procedure as a proffer amendment. This Rezoning includes a companion request to amend the Declaration. The proffers proposed by the Applicant are intended to fully replace the role of the County Declaration in the development of the property. The Applicant requests that the County record an amendment to the County Declaration (draft provided), which will remove the subject site from the County Declaration.

The 1.3-acre County property is also subject to the overall Declarations for Innovation @ Prince William, originally dated January 21, 2000, recorded at Deed Book 2864, Page 307 among the County land records, and is subject to the Innovation Design Guidelines and Innovation Architectural Review Board (ARB) review.

- Innovation Park Small Area Plan: On December 15, 2020, the Board adopted the Innovation Park Small Area Plan (Res No. 20-852). The Small Area Plan is intended to encourage development to realize the desired University Center, Technology Center, and Employment Center with a common goal of creating a multimodal, interconnected community with a strong sense of place.

- Technology Overlay District (TeOD) and Innovation @ Prince William Science and Technology Sector Plan: The subject site is located within the TeOD, which exists to promote harmonious development around George Mason University. The TeOD exists as an implementation tool for the Innovation @ Prince William Science and Technology Sector Plan. As stated in Res No. 20-852, the new Innovation Park Small Area Plan is intended to replace the existing Innovation @ Prince William Science and Technology Sector Plan. With the adoption of the Small Area Plan the Board also initiated amendments to the Zoning Ordinance, including the Technology Overlay District, to address the adoption of, and support the implementation of, the Innovation Park Small Area Plan. At this time amendments to the TeOD have not been drafted or approved by the Board. In order to implement the orderly development of the subject project, the Applicant requests to apply the guidance of the Innovation Park Small Area Plan, and not the Innovation @ Prince William Science and Technology Sector Plan (as implemented through the TeOD). Pursuant to Section 32-506.09, waivers, and modifications within a TeOD, the Board may waive or modify any provision, including use restrictions, pursuant to an approved rezoning. With this request the application proposes to waive TeOD guidance, as they may be inconsistent with the recently adopted Innovation Park Small Area Plan.
  - George Mason University: The subject site is abutting the George Mason University's Science and Technology Campus (SciTech Campus). The campus opened in 1997, and today, the 134-acre campus serves more than 2,000 students with approximately 150 graduate students living on campus. The George Mason University is currently undergoing a master plan process to guide future decisions on the university's three primary campuses in Arlington, Fairfax, and Prince William; and will prepare student projections. At this time student projections are not available; however, the amenities associated with a town center in this location are planned to support the growing SciTech Campus and its students, staff, and faculty.
- G. Planning Commission Public Hearing: On September 8, 2021, the Planning Commission held a public hearing and recommended approval of Rezoning and Release of Declaration #REZ2016-00030 subject to proffers dated August 26, 2021, and Special Use Permit Innovation Town Center #SUP2016-00031, with conditions dated June 15, 2021.

## **STAFF RECOMMENDATION**

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Staff recommends approval of Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center, subject to revised proffers dated September 9, 2021, and Special Use Permit #SUP2016-00031, Innovation Town Center, subject to revised conditions dated September 9, 2021, which include minor technical revisions, for the following reasons:

- The proposal will reasonably implement the Innovation Park Small Area Plan.
- The Applicant has committed to multistory construction within the Town Center (TC) designation.

- One-story standalone retail or retail service uses have been restricted.
- Triggers for the construction of nonresidential uses have been provided to ensure that a mix of uses will be provided within the Town Center (TC) designation.
- Design guidelines are proposed to guide future development of the site. The design guidelines further the recommendations in the Innovation Park Small Area Plan.
- As proffered, the level of service impacts will be mitigated.

**Comprehensive Plan Consistency Analysis**

Long-Range Land Use:

This site is located within the Development Area of the County, and is designated OMU, Office Mixed Use, CMU, Commercial Mixed-Use, and TC, Town Center, on the Long-Range Land Use Map, and is located within the Innovation Park Small Area Plan. The Innovation Park Small Area Plan establishes a regional employment center featuring advanced technology and research and development industries surrounding a core consisting of a vibrant, pedestrian-oriented, mixed-use development in a Town Center. The Town Center is intended to enhance and support the George Mason University SciTech Campus. The subject rezoning builds upon the Town Center concept, although the overall mix of uses is less dense than is recommended in the Small Area Plan and is substantially more residential. Throughout the review process staff has encouraged the Applicant to emphasize nonresidential uses within the project area.

Level of Service (LOS):

The application includes a mix of residential and nonresidential uses. The residential development includes a maximum of 1,032 units. The rezoning application was filed prior to July 1, 2016. Subsequent to the filing of the rezoning application, the County eliminated its established level of service monetary policy and initiated the Innovation Park Small Area Plan. This plan was approved by the Board in December 2020. As such, the Applicant is submitting this application pursuant to Section 15.2-2303.4(D)(1) of Virginia Code and deems these proffers reasonable and appropriate, as evidenced by the signed proffers. Additionally, the Applicant has submitted a full proffer justification narrative to support the proposed proffers.

The monetary proffers are summarized, as follows:

<b>Schools</b>	\$8,549.47 per single-family attached unit (SFA)	\$8,549.47 x 370 SFA	\$6,142,118.54
	\$4,499.72 per multifamily unit (MF)	\$4,499.72 x 662 MF	
<b>Fire &amp; Rescue</b>	\$1,280.04 per single-family attached unit (SFA)	\$1,280.04 x 370 SFA	\$1,368,480.16
	\$939.78 per multifamily unit (MF)	\$939.78 x 662 MF	

	\$0.61 per square foot	\$0.61 x 447,100 SF	
<b>Libraries</b>	\$705.24 per single-family attached unit (SFA)	\$705.24 x 370 SFA	\$603,702.54
	\$517.77 per multifamily unit (MF)	\$517.77 x 662 MF	
<b>Affordable Housing</b>	\$250 per residential unit	\$250 x 1,032 units (minus \$2,500 credit)	\$255,500.00
<b>Parks and Recreation</b>	\$770.48 per single-family attached unit (SFA)	\$770.48 x 370 SFA	\$659,551.14
	\$565.67 per multifamily unit (MF)	\$565.67 x 662 MF	
<b>Environment</b>	\$75 per acre	\$75 per acre x 107.4 acres	\$8,055.00
<b>TOTAL LOS \$ CONTRIBUTION</b>			<b>\$9,037,407.38</b>

**Strategic Plan**

**Goal 3: Resilient Economy:** The proposal is for a town center development connected to George Mason University, which as designed with emphasis on walkable environments and attractive public spaces, is in alignment with the recently adopted Strategic Plan for live / work / play environments. The project is well-situated for partnerships with the SciTech Campus in the future. Additionally, the rezoning from A-1 and PBD to PMD, which includes the potential for 447,100 square feet of nonresidential uses will expand the County’s commercial tax revenue base.

**Goal 5: Environmental Conservation:** The proposal meets the environmental conservation policies provided in the Strategic Plan. Thirty percent open space is provided on the site, which exceeds minimum requirements. The limits of clearing and grading preserve 6 of the 13 specimen trees located onsite. Additionally, the layout of the residential sections of the development have been designed to allow for stream preservation.

**Goal 6: Sustainable Growth:** The Innovation Park area is one of six designated regional activity centers in the County and is an area targeted to accommodate desired economic development. Growth in this area is recommended as a sustainable growth policy.

**Goal 7: Transportation and Mobility:** The application includes sidewalks, trails, and a well-developed system of pedestrian circulation. The Applicant has included numerous connections between the proposed student housing, the town center, and the GMU campus in order to reduce the number of vehicular trips and parking spaces required within the development and to create a walkable community. A new roadway network is also proposed to serve the town center and planned growth.

### **Community Input**

Notice of this application has been transmitted to property owners within 1,320 feet of the site. As of the date of this staff report, the Planning Office is not aware of any opposition. In the weeks leading up to a work session with the Planning Commission on the project, staff received two emails with questions and comments related to the request and the successful implementation of the Innovation Park Small Area Plan. These comments were forwarded to the Planning Commissioners.

### **Other Jurisdiction Comments**

The applications were forwarded to the City of Manassas for review and comment. The City of Manassas did not provide comment on the applications.

### **Legal Issues**

Legal issues resulting from the Board action are appropriately addressed by the County Attorney's Office.

### **Timing**

The Board generally has one year from the date of acceptance to take action on a rezoning request. The Planning Office also generally recommends that action on SUP applications occur within one year. The subject applications were initially submitted for review on June 24, 2016 and have had extended periods of inactivity prior to the adoption of the Innovation Park Small Area Plan. The one-year timeframe for the Boards action on a rezoning has been deemed extended. The Applicant submitted a substantially modified rezoning proposal on November 23, 2020, just prior to the adoption of the Innovation Park Small Area Plan in December 2020.

## **STAFF CONTACT INFORMATION**

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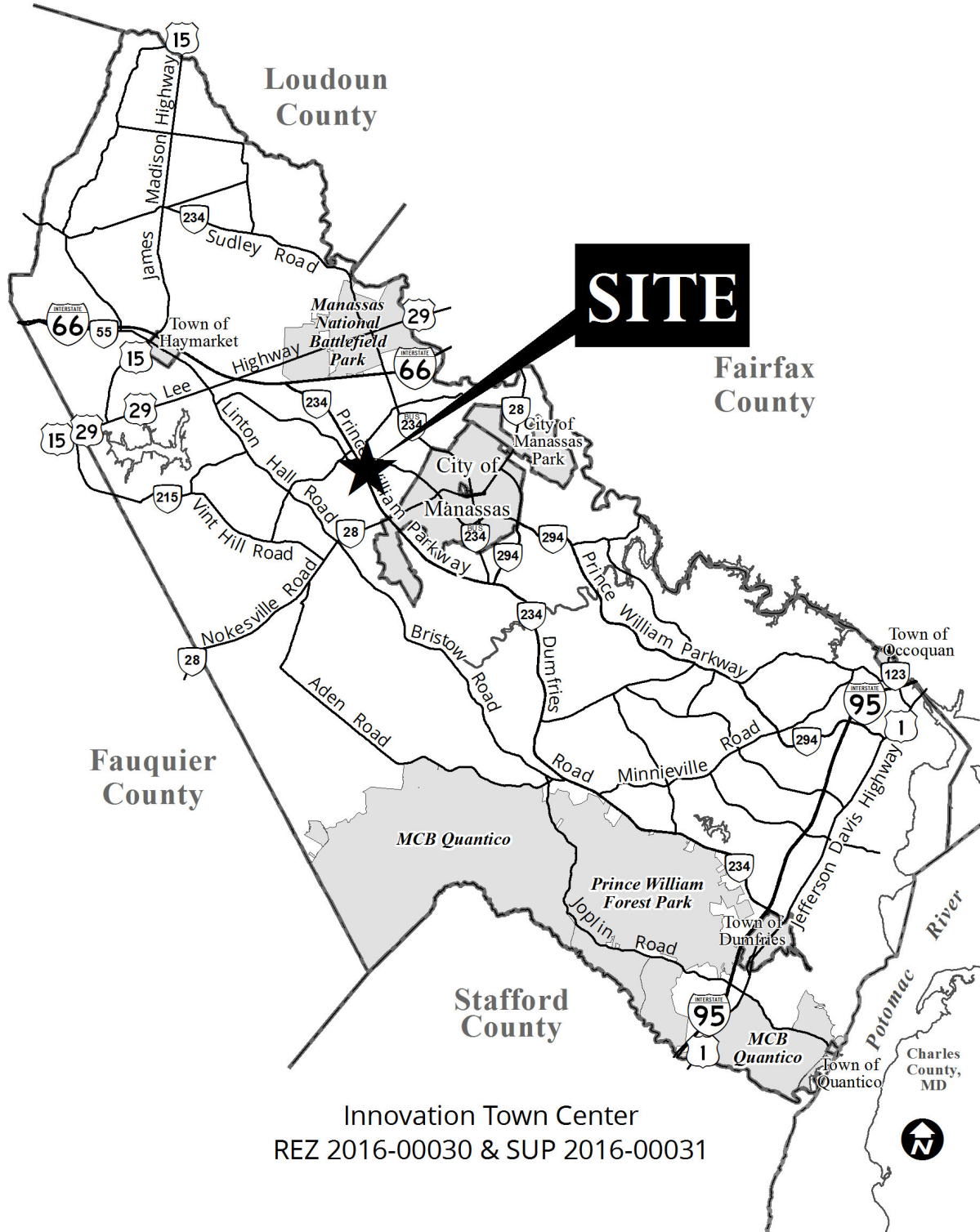
Meika Daus | (703) 792-7901  
Mdaus@pwcgov.org

## **ATTACHMENTS**

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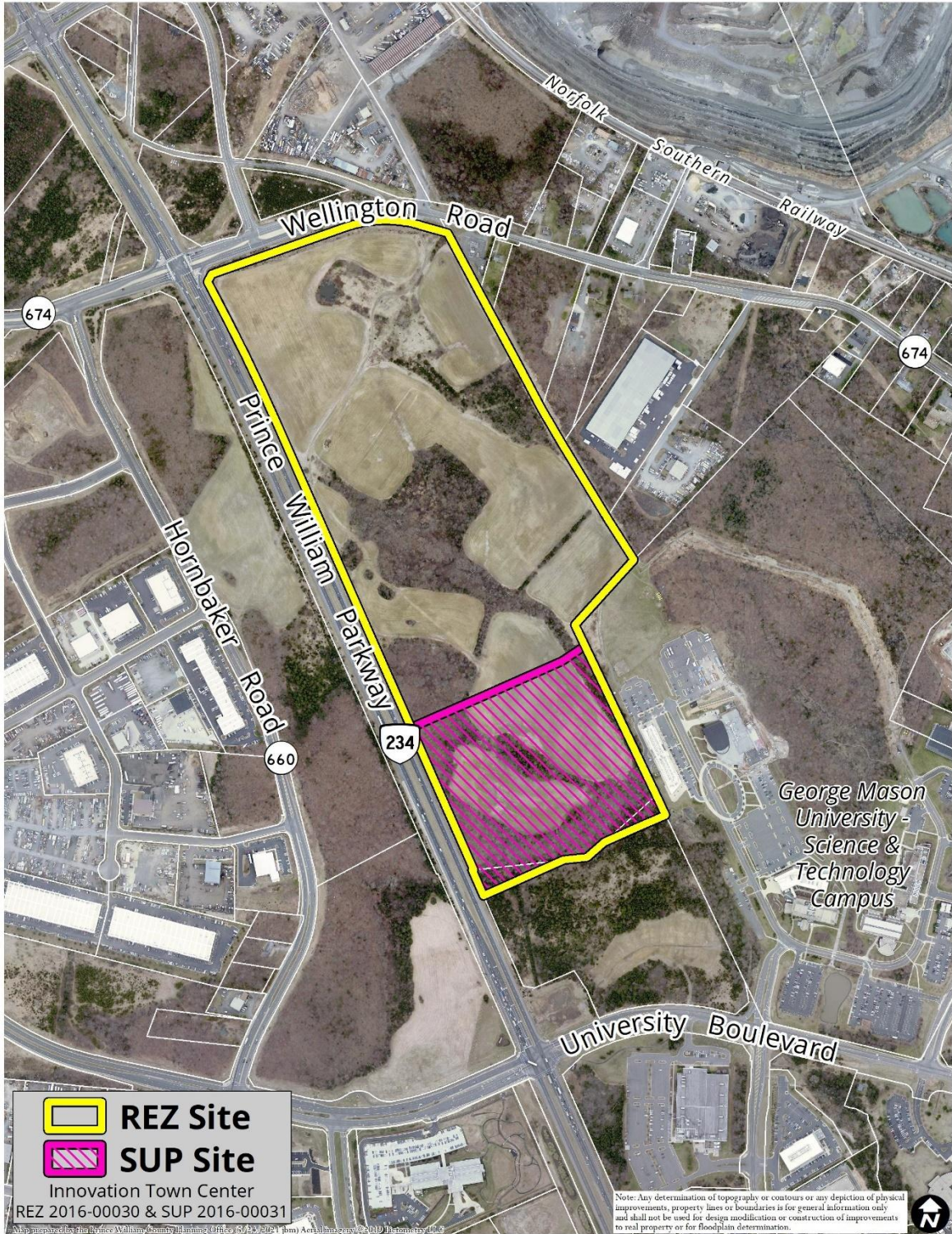
Area Maps  
Staff Analysis  
Master Zoning Plan  
Special Use Permit Plan  
Waivers and Modifications  
Design Guidelines  
Performance Standards  
Environmental Constraints Analysis (ECA)  
Transportation Demand Management (TDM) Guidelines  
Proffer Justification Narrative  
Agency Comments  
Planning Commission Resolution

Vicinity Map



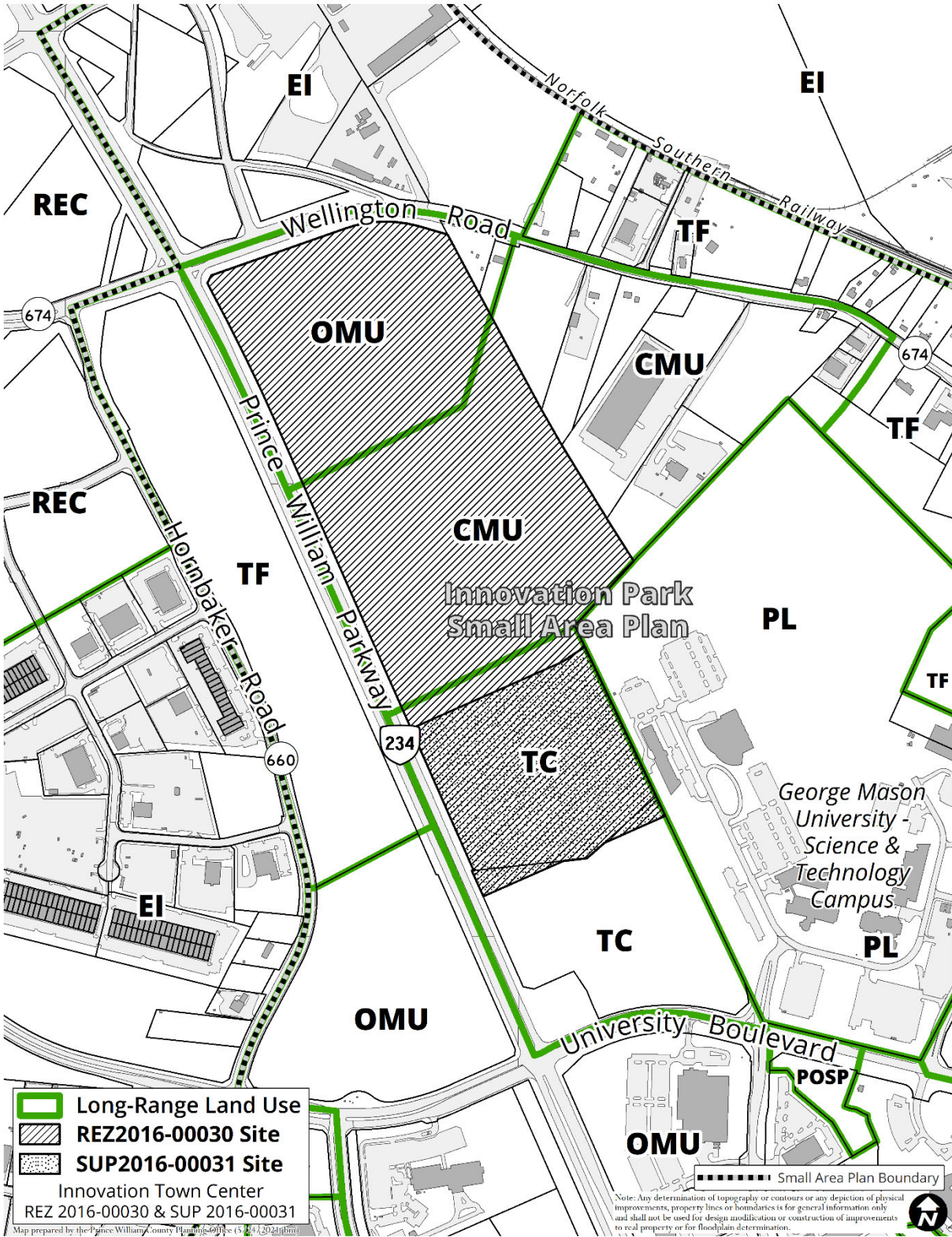
Innovation Town Center  
REZ 2016-00030 & SUP 2016-00031



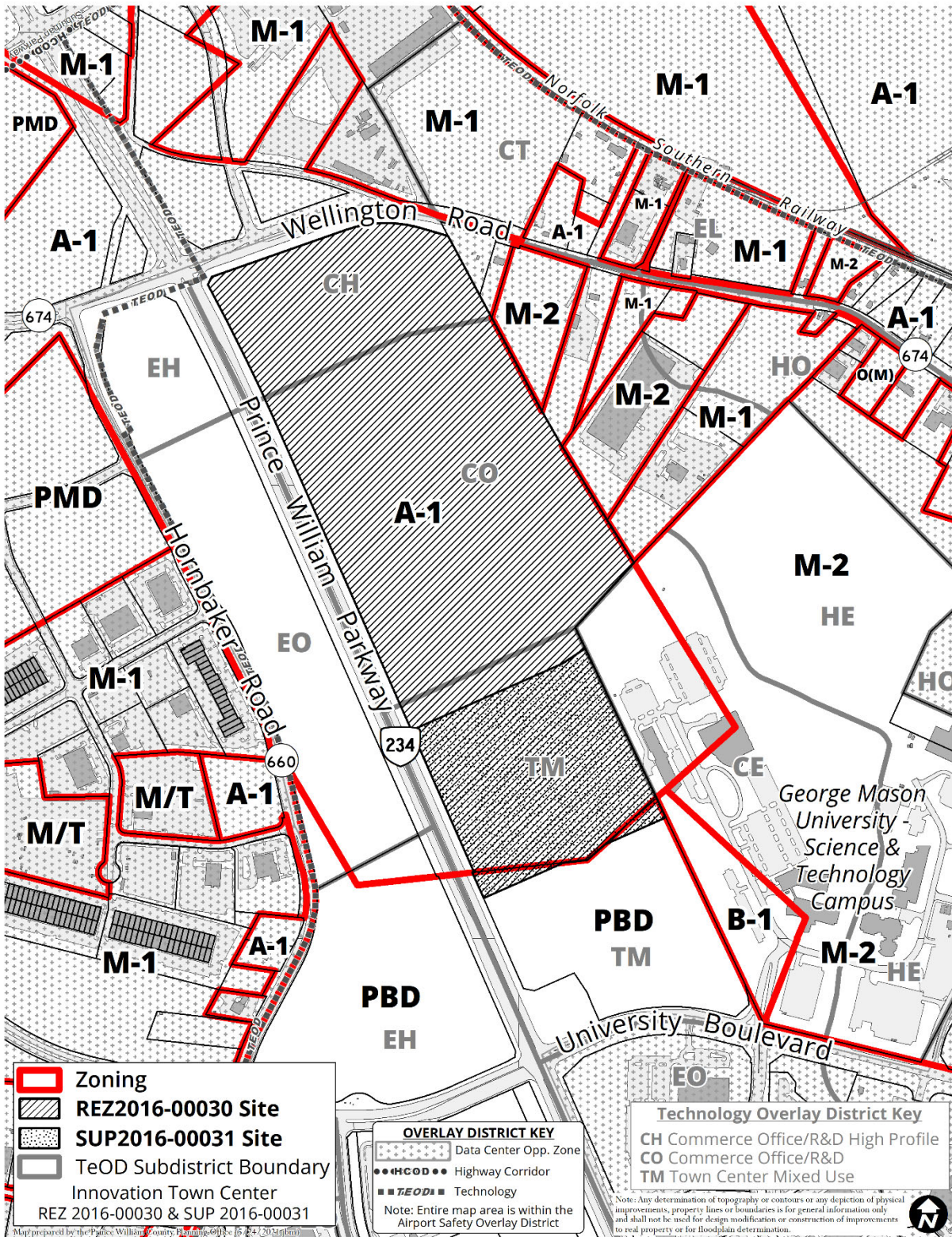




# Long-Range Land Use Map

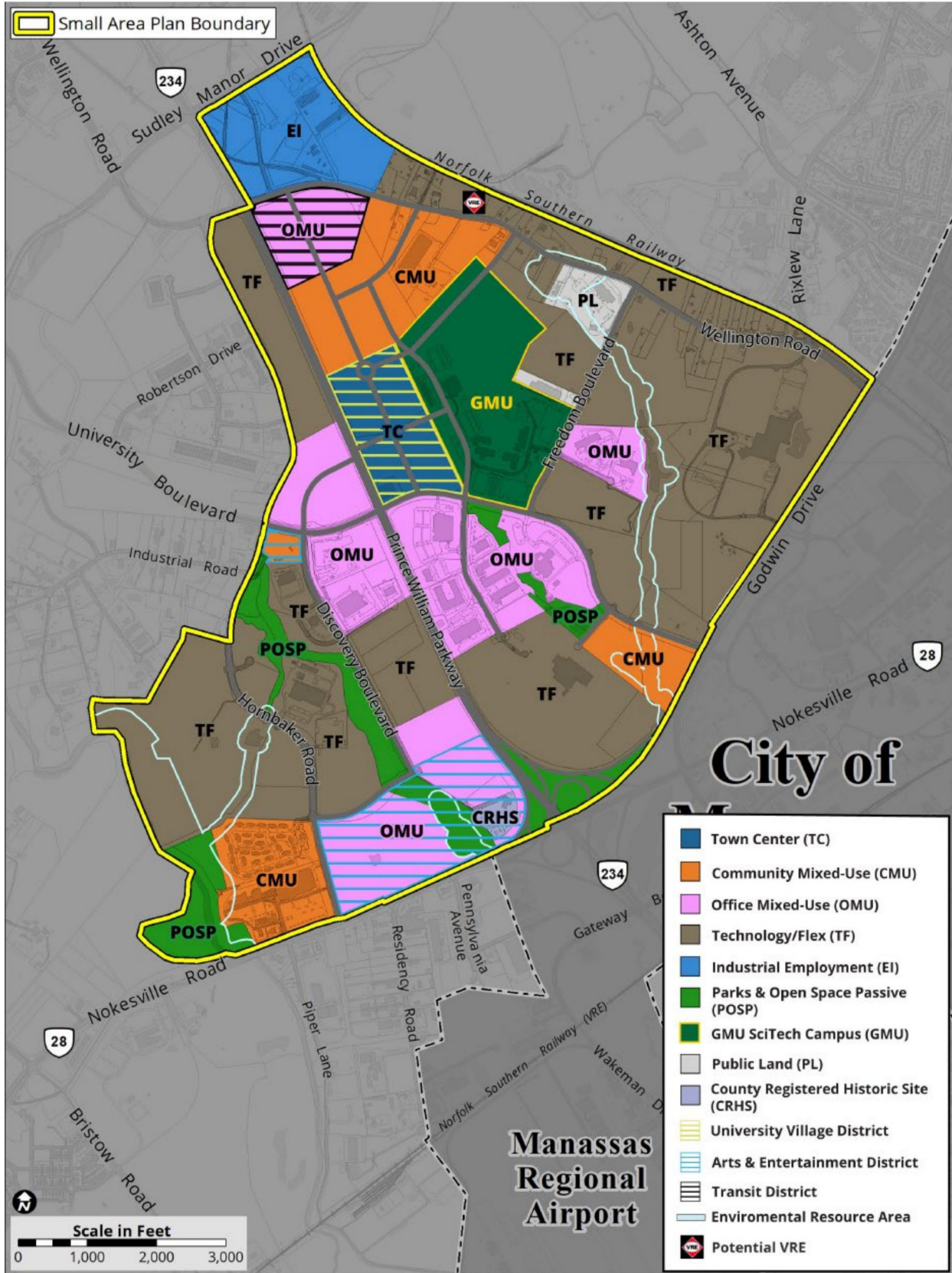








# Innovation Park Small Area Plan: Land Use Map



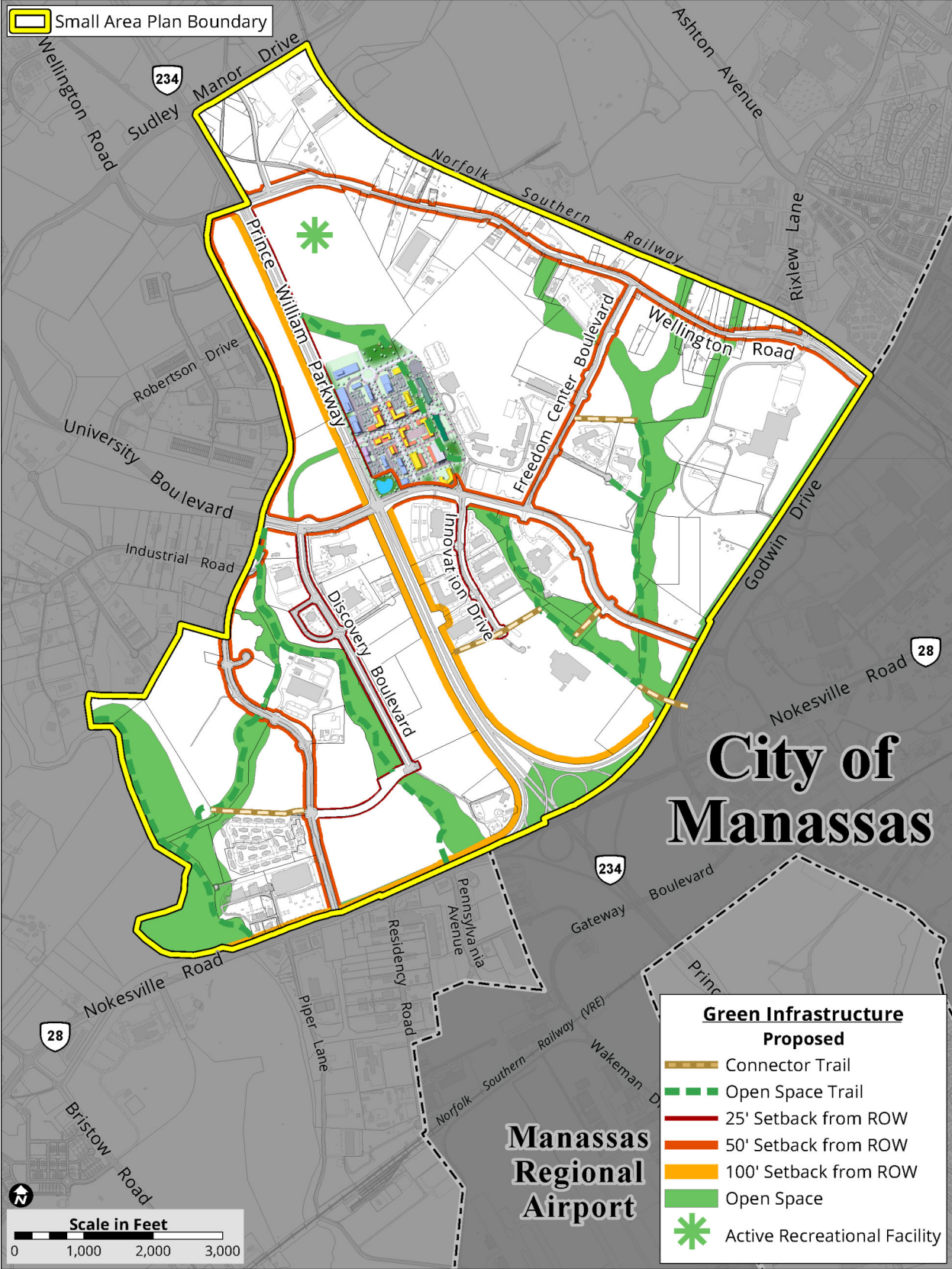


# Innovation Park Small Area Plan: Town Center Illustrative Plan





# Innovation Park Small Area Plan: Public Space and Green Infrastructure



**Part I. Summary of Comprehensive Plan Consistency**

**Staff Recommendation: APPROVAL**

This summary analysis is based on the relevant Comprehensive Plan action strategies, goals, and policies. A complete analysis is provided in Part II of this report.

<b>Comprehensive Plan Sections</b>	<b>Plan Consistency</b>
Long-Range Land Use	Yes
Community Design	Yes
Cultural Resources	Yes
Environment	Yes
Fire and Rescue	Yes
Housing	Yes
Library	Yes
Parks, Open Space and Trails	Yes
Police	Yes
Potable Water	Yes
Sanitary Sewer	Yes
Schools	Yes
Transportation	Yes

**Part II. Comprehensive Plan Consistency Analysis**

The following table summarizes the area characteristics:

<b>Direction</b>	<b>Land Use</b>	<b>Long Range Future Land Use Map Designation</b>	<b>Zoning</b>
North	Storage Yard	EI	A-1, M-1
South	Vacant Land	TC	PBD
East	George Mason University	CMU, PL	M-1, M-2
West	Prince William Parkway and Vacant Land beyond	TF, OMU	PMD, M-1, M/T

**Long-Range Land Use Plan Analysis**

Through wise land use planning, the County ensures that landowners are provided a reasonable use of their land while the County is able to judiciously use its resources to provide the services for residents and employers’ needs. The Long-Range Land Use Plan sets out policies and action strategies that further the County’s goal of concentrating on population, jobs, and infrastructure within vibrant, walkable, mixed-use centers serviced by transit. In addition to delineating land uses on the Long-Range Land Use Map, the Plan includes smart growth principles that promote a countywide pattern of land use that encourages fiscally sound development and achieves a high-quality living environment; promotes distinct centers of commerce and centers of community; complements and respects our cultural and natural resources, and preserves historic landscapes and site-specific cultural resources; provides adequate recreational, park, open space and trail amenities that contribute to a high quality of life for county residents; and revitalizes, protects, and preserves existing neighborhoods.

This site is located within the Development Area of the County, and is designated OMU, Office Mixed Use, CMU, Commercial Mixed-Use, and TC, Town Center, on the Long Range Land Use Map. The Innovation Park Small Area Plan establishes a regional employment center featuring advanced technology and research and development industries surrounding a core consisting of a vibrant, pedestrian-oriented, mixed-use development in a Town Center. The Town Center is intended to enhance and support the George Mason University SciTech Campus.

The following table summarizes the uses and densities that are intended, based upon these designations:

<b>Long Range Land Use Map Designation</b>	<b>Intended Uses and Densities</b>
<b>Town Center Mixed Use (TM) in the University District</b>	Town Centers provide a mix of uses arranged in a pedestrian-friendly urban form. These centers are locations for regional commercial and entertainment destinations as well as access to amenities for adjacent residential and employment centers. Streets are interconnected and multimodal with parking located behind buildings. Short blocks with shallow setbacks and onstreet parking are appropriate.
<b>Commercial Mixed Use (CMU)</b>	Mixed-use live work centers include both residential and commercial uses arranged in a pedestrian-friendly form. Locations for community, commercial, entertainment destinations, and public facilities directly accessible to surrounding neighborhoods. Streets are interconnected and serve cars, cyclists, and pedestrians. These mixed-use centers should be connected by transit to nearby destinations and to nearest rail transit.



<b>Long Range Land Use Map Designation</b>	<b>Intended Uses and Densities</b>
<b>Office Mixed Use (OMU) in the Transit District</b>	<p>The purpose of this designation is for areas of low to high-rise offices or research and development activities. Projects developed in this classification shall be for office use, with retail and retail service uses discouraged as a stand-alone structure. Office development areas are encouraged to be in accordance with the Illustrative Guidelines for Office Development, provided as a supplement to the Community Design Plan chapter of the Comprehensive Plan.</p> <p>Transit District Overlay also falls over the OMU property. This overlay provides that primary uses are defined by the underlying district and in addition the Transit district allows the following secondary uses, multifamily residential and attached residential.</p>

<b>Form and Character Standards TC</b>	<b>Town Center In the University Village District</b>	<b>Proposed</b>
<b>Implementing Zoning District</b>	PMD PMR PBD R-U B-2 O(L)	PMD with B-1 O(L) O(M) and UDR/UHDR land bay designations
<b>Target Residential Density</b>	T-6: 50-100 du/acre T-5: 20-50 du/acre	±14.5 du/acre
<b>Target Non-Residential FAR</b>	T-6: 2.30-3.00 FAR T-5: 1.38-2.30 FAR	±0.30 FAR
<b>Target Building Height</b>	T-6: 8-20 stories T-5: 6-12 stories	3-5 stories (as proffered, buildings within the Town Center shall be a minimum of 3 stories in height)

<b>Form and Character Standards CMU</b>	<b>Community Mixed-Use Center</b>	<b>Proposed</b>
<b>Implementing Zoning District</b>	PMD PMR PBD R-16 B-2 O(L)	PMD with MDR and HDR land bay designations
<b>Target Residential Density</b>	T-4- 8-24 du/acre	±9.64 du/acre
<b>Target Non-Residential FAR</b>	0.57 – 1.38 FAR	None
<b>Target Building Height</b>	T-4- 4-8 stories	3-4 stories

Form and Character Standards OMU	Office Mixed Use in the Transit District	Proposed
Implementing Zoning District	O(H) O(M) O(F) PBD PMR	PMD with B-1 M-2 O(F) and HDR land bay designations
Target Residential Density	T-3- 4-12 du/ac	±6.67 du/acre
Target Non-Residential FAR	0.23 to 0.57 FAR	±0.07 to ±0.21 FAR
Target Building Height	T-3- 3-5 stories	1-3 stories

**Proposal's Strengths**

- Zoning Compatibility: The proposed zoning to PMD, Planned Mixed Use District, is recommended in the Innovation Park Small Area Plan as an implementation tool for the Town Center and Community Mixed Use designation; however, there are land bay designations that do not fully implement the Small Area Plan guidance.
- Vertical Mix of Uses: While the residential and nonresidential densities within the Town Center designation are less intense than recommended within the Small Area Plan, the Applicant has committed to multistory buildings and a vertical mix of uses within the Town Center designation. Multistory mixed use buildings help create a sense of enclosure for the streetscape. Ground floor retail uses within storefronts help enhance the pedestrian experience. A vertical mix of uses and public spaces are proposed along Hylton Center Boulevard. This is the front door of the Town Center.
- Phasing Tied to the Core of the Town Center: As proffered, the Applicant agrees to construct Block 1 within the Town Center prior to the completion of two-thirds of the residential units, and this block shall include a minimum of 50,000 square feet of nonresidential uses.

**Proposal's Weaknesses**

- Inconsistency with Community Mixed Use (CMU) Form and Character Recommendations: The application includes 100 percent residential uses (with supportive recreational space) within the Community Mixed Use designation. The Small Area Plan recommends up to 60 percent residential uses. Staff notes that the residential use is within the recommended density range; however this area lacks the complementary mix of uses recommended in the Small Area Plan. The Applicant does not propose commercial uses in this area in order to focus commercial activities and intensity within the Town Center designation.
- Inconsistency with Office Mixed Use (OMU) Form and Character Recommendations: Within this designation, the FAR and building heights are lower than recommended for nonresidential uses.

**On balance**, while there are inconsistencies with the form and character standards in the Small Area Plan, the application has been modified to strengthen aspects of the design within the Town Center designation and Office Mixed Use designation, and staff believes the proposal is consistent with the overall land use guidance in the Small Area Plan.

### **Community Design Plan Analysis**

An attractive, well-designed County will attract quality development, instill civic pride, improve the visual character of the community and create a strong, positive image of Prince William County. The Community Design Plan sets out policies and action strategies that further the County's goals of providing quality development and a quality living environment for residents, businesses, and visitors, and creating livable and attractive communities. The Plan includes recommendations relating to building design, site layout, circulation, signage, access to transit, landscaping and streetscaping, community open spaces, natural and cultural amenities, stormwater management, and the preservation of environmental features.

#### **Proposal's Strengths**

- **Design Guidelines**: The Applicant proffers that development on the Property shall be in conformance with the design concepts and details set forth in the proposed Design Guidelines. The Design Guidelines contain standards for buildings and architecture, streetscape, storefronts, parks and open space, loading, fences, walls, and parking design that will guide future development on the site.
- **Public Spaces**: The Small Area Plan encourages well-developed public spaces, including pedestrian plazas. These concepts are integrated into the Design Guidelines and the Amenity Plan. Specifically, the Applicant proposes a 25,600 square-foot Gateway Plaza at the project's entrance from Hylton Center Boulevard extended. This is a key feature of the plan as this includes lawn areas, specialty paving, retail kiosks, and opportunities for outdoor seating. Additionally, a Pavilion Green and Urban Park is proposed. This space includes opportunities for special events and flex community space. The Applicant has also added an Interim Plaza within Block 2 of the development. This is a more informal space that could include a gravel parking area, outdoor seating, and have opportunities for food trucks, festival, markets and other activities that would draw interest into the Town Center.
- **Public Art**: In consultation with George Mason University, the Applicant proposes public art the Town Center as outlined in the Design Guidelines.

#### **Proposal's Weaknesses**

- **Inconsistency with Some Design Form & Character Recommendations**: The building heights and floor area ratios are within the lower range of the recommendations within the Small Area Plan.

**On balance**, this application is found to be consistent with the relevant components of the Community Design Plan.

### **Cultural Resources Plan Analysis**

Prince William County promotes the identification, evaluation, and protection of cultural resource sites throughout the County, as well as the tourism opportunities these sites present. The Cultural Resources Plan recommends identifying, preserving, and protecting Prince William County's significant historical, archaeological, architectural, and other cultural resources – including those significant to the County's minority communities – for the benefit of all of the County's citizens and visitors. To facilitate the identification and protection of known significant properties that have cultural resource values worthy of preservation, the land use classification County Registered Historic Site (CRHS) is used in the Comprehensive Plan. The Plan includes areas of potentially significant known but ill-defined or suspected pre-historic sites, Civil War sites, historic viewsheds, landscapes or areas of potential impact to important historic sites, and encourages the identification, preservation, protection, and maintenance of all cemeteries and/or gravesites located within the County.

The Historical Commission reviewed the proposal on February 9, 2021, and recommended that the Applicant conduct additional research of past ownership and history of the property in order to provide interpretive signage and/or other media regarding the property's history with content to be determined by the Historical Commission and the Planning Office.

#### **Proposal's Strengths**

- **Curation of Artifacts**: The Applicant proffers to curate with the County, artifacts, field records, laboratory records, photographic records, computerized data and other historical records the Applicant possesses as recovered as a result of its Phase I cultural resources investigations.
- **Interpretive History Plan for Nature Trail**: The Applicant proffers to prepare an interpretive history plan for the Nature Trail and Town Center. The interpretive history plan shall consist of interpretive signs (multiple) and/or historical markers which describe twentieth century farming, dairy farming in Prince William County, or sharecropping and pre-contact (Native American) history in the area. The location and content of the interpretive signs shall be prepared in consultation with the County Archaeologist and/or Historical Commission and shall follow the general guidance identified in the Design Guidelines. The interpretive history information shall be installed/constructed in conjunction with the development immediately adjacent thereto, and shall be shown on the respective final site plan for those portions of the Property. The signage associated with the interpretive history plan, as outlined above, shall not count toward the number of signs permitted on the Property.

#### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Cultural Resources Plan.

### **Environment Plan Analysis**

Prince William County has a diverse natural environment, extending from sea level to mountain crest. Sound environmental protection strategies will allow the natural environment to co-exist with a vibrant, growing economy. The Environment Plan sets out policies and action strategies that further the County's goal of preserving, protecting and enhancing significant environmental resources and features. The Plan includes recommendations relating to the incorporation of environmentally sensitive development techniques, improvement of air quality, identification of problematic soil issues, preservation of native vegetation, enhancement of surface and groundwater quality, limitations on impervious surfaces, and the protection of significant viewsheds.

Site is predominantly agricultural fields, mixed hardwood forests and old field successional forest. There is no RPA onsite. There are jurisdictional wetlands and two forested intermittent stream channels. There are 13 specimen trees.

### **Innovation Park Small Area Plan Green Infrastructure Recommendations**

The Innovation Park Small Area Plan contains a Green Infrastructure Plan. Green infrastructure planning provides an opportunity to evaluate, prioritize, and manage the landscape as a connected and interdependent system. Green infrastructure is important for the protection of species and increasing biodiversity. Innovation Park's green infrastructure is made up of the area's existing and future trails, public and private open space, stream corridors, and trees. The purpose of this section is to protect the environmental features of this area and develop policies to address how these will be protected, enhanced, and integrated into the study area once it is fully implemented. This Green Infrastructure Plan is general and will be subject to final site engineering, but provides pedestrian, open space, and habitat linkages. The establishment of a green infrastructure network of interconnected areas, parks, and green streets recognizes the value of nature for a healthy, resilient, and prosperous community.

All future development within Innovation Park will be encouraged to designate a percentage of open space within the project consistent with the Description & Uses of Proposed Long-Range Land Use Classifications table. All developments throughout the Small Area Plan area will be expected to commit to incorporate greenways, trails, and natural open space areas into an overall trail plan.

The following specific recommendations on the subject site are included in the Green Infrastructure Plan:

- 25-foot-wide setback from the right-of-way along Prince William Parkway
- 50-foot-wide setback from the right-of-way along Wellington Road
- An active recreational facility
- An open space and trail network

### **Proposal's Strengths**

- Meets Open Space Standards: The Small Area Plan established open space requirements for the Town Center area of 10 percent. This open space for the project will be 30 percent overall, which exceeds the minimum requirements.
- Green Infrastructure: The application commits to greenways, trails, and natural open space areas. This is consistent with the Green Infrastructure recommendations in the Small Area Plan.
- Specimen Tree Preservation: The limits of clearing and grading have been revised to preserve 6 of the 13 specimen trees located onsite. While this is a strength, additional effort should be made in order to preserve the critical root zone (CRZ) of Specimen Trees T-2, T-3 and T-193. The CRZ is the area of root zone that it is imperative to preserve in order to preserve these massive trees.

### **Proposal's Weaknesses**

- Setback and Buffer Reductions: Staff provides the following comments regarding the setback/ buffer recommendations in the Small Area Plan and the subject request:
  - Along Wellington Road the Applicant proposes a 30' landscape strip (50' recommended), which is almost entirely occupied by existing utilities. This does not meet the Small Area Plan recommendations.
  - While the Small Area Plan anticipated reduced setbacks along Prince William Parkway to 25-feet, this was associated with the concept for a vertically integrated mix of uses in TC, CMU, and OMU designations. Special consideration has been given to adequately buffering residential units within Land Bays B and D from Prince William Parkway. The application includes a variable width buffer in this area that approaches 50 feet in some locations with a trail. This buffer includes a grade change. This has been heavily discussed during the review and the proposed residential buffering along the Parkway represents a compromise that does not have agency consensus. Wider buffering is requested by the Watershed Management Division.
  - Based on the mix of uses proposed in Land Bay A, Tech Commercial (B-1, M-2, and O-F), wider buffering in this area along Prince William Parkway could be appropriate. Uses of this type include 100-foot-wide buffers on the opposite side of Prince William Parkway from the subject site. While the Small Area Plan anticipated a 25-foot-wide setback along Prince William Parkway, it also recommended low to midrise offices in this location. Wider buffering could be appropriate based on the potential mix of uses.

**On balance**, this application is found to be consistent with the relevant components of the Environment Plan.

## **Fire and Rescue Plan Analysis**

Quality fire and rescue services provide a measure of security and safety that both residents and businesses have come to expect from the County. The Fire and Rescue Plan sets out policies and action strategies that further the County's goal of protecting lives, property, and the environment through timely, professional, humanitarian services essential to the health, safety, and well-being of the community. The Plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility for fire and rescue facilities. The Plan also includes recommendations to supplement response time and reduce risk of injury or death to County residents, establishment of educational programs, such as cardio-pulmonary resuscitation (CPR) training, automatic external defibrillators (AED), and encourage installation of additional fire protection systems – such as sprinklers, smoke detectors, and other architectural modifications.

The site is within the service area of Groveton Fire and Rescue Station #22. Groveton went in service on January, 2021 and is now the first due Fire/Rescue to this property. Due to the timeframe involved, the Fire Marshal's Office does not have work-load information available.

### **Proposal's Strengths**

- Travel Time Advanced Life Support: The site is located inside the recommended 8.0-minute travel time for advanced life support services.
- Level of Service Mitigation: The Applicant proffers monetary contributions in the amount of \$1,280.04 per single family attached residential unit and \$939.78 per multi-family residential unit constructed on the property to be used for public safety purposes. The Applicant also proposes a monetary contribution to the Prince William Board of County Supervisors in the amount of \$0.61 per square foot of nonresidential gross floor area ("gfa"), excluding any parking structures, to be used for fire and rescue purposes.

### **Proposal's Weaknesses**

- Travel Time Basic Life Support: The site is located outside the recommended 4.0-minute travel time for Basic Life Support and Fire.

**On balance**, this application is found to be consistent with the relevant components of the Fire and Rescue Plan.

## **Housing Plan Analysis**

Prince William County is committed to clean, safe and attractive neighborhoods for all its residents, and the elimination of neighborhood blight and substandard housing. The Housing Plan sets out policies and action strategies that further the County's goal of identifying locations and criteria for the provision of diverse housing opportunities for all segments of our population and to promote economic development. The Plan includes recommendations relating to neighborhood preservation

and improvement, affordable housing, special needs housing, and public/private partnerships to address housing needs.

**Proposal's Strengths**

- Housing Variety: The subject rezoning proposes housing variety within Innovation Park.
- Affordable Housing Contribution: As proffers, the Applicant shall make a monetary contribution to the Board in the amount of \$250.00 per residential unit constructed on the Property to be used for the Housing Preservation and Development Fund. This amounts to a significant contribution to Affordable Housing of up to \$255,500 total.

**Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Housing Plan.

**Library Plan Analysis**

Access to a variety of information is a valuable service provided by the County. The Library Plan sets out policies and action strategies that further the County's goal of providing adequate library facilities and information resources to our residents. The Plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility.

**Proposal's Strengths**

- Monetary Contribution: As proffered, the Applicant will make a monetary contribution to the Board of County Supervisors in the amount of \$705.24 per single family attached residential unit and \$517.77 per multi-family residential unit to be used for library purposes.

**Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Libraries component of the Comprehensive Plan.

**Parks, Open Space and Trails Plan Analysis**

The quality of life for residents of Prince William County is linked closely to the development and management of a well-maintained system of parks, trails, and open space. Prince William County



contains a diversity of park, open space, and trail resources. These parklands, open spaces, and recreational facilities play a key role in shaping both the landscape and the quality of life of Prince William County residents through the conservation of natural and cultural resources, protection of environmental quality, and provision of recreational facilities. The Parks, Open Space and Trails Plan sets out policies and action strategies that further the County’s goal of providing park lands and recreational facilities of a quantity, variety, and quality appropriate to meet the needs of the current and future residents of Prince William County. The Plan includes recommendations to preserve existing protected open space, maintain high quality open space, expand the amount of protected open space within the County, and to plan and implement a comprehensive countywide network of trails.

**PARKS AND RECREATION FACILITIES NEAR THE PROJECT AREA**

Per the Parks, Recreation & Tourism Chapter of the County’s Comprehensive Plan, the subject property is located within Park Planning District 4. Parks within the service area of this development include the following:

<b>Park Type</b>	<b>Park Name</b>
Neighborhood	None
Community	Rollins Ford Park (undeveloped; in design)
Regional	Valley View Park Ben Lomond Regional Park/Splashdown Waterpark
Linear/Resource	Broad Run Linear Park
Natural/Cultural Res.	Bristoe Station Battlefield Heritage Park Ben Lomond Historic Site
School/Community Use	None
Trails	Trails in Broad Run Linear Park and Ben Lomond Regional Park
Other	Freedom Aquatics & Fitness Center (via partnership with GMU)

**LEVEL OF SERVICE ANALYSIS**

The Prince William County Comprehensive Plan contains levels of service (LOS) standards for parks and recreation areas. The Department of Parks, Recreation & Tourism is responsible for parks and recreation services and facilities and has determined LOS standards from government requirements, professional and industrial standards, citizen surveys, and citizen expectations.

The Department no longer utilizes per capita standards to calculate level of service for parks or park facilities. The current LOS standards for parks and recreation facilities are now based on metrics that consider park type, park service areas, park quality, and percentage of park acres county-wide. Per the Comprehensive Plan, the Department has identified a need for: an additional 5,000 acres of County-owned parkland; additional neighborhood parks throughout the county, which can be offset by HOA neighborhood park type features or pocket parks; and, upgrades to parks so that all parks obtain a grade “B” or higher based on the Department quality rankings. In addition, the Department’s Needs Assessment, and subsequent Master Plan, have identified priorities for amenities in each magisterial district.

Applying the LOS standards to this development and its vicinity, the Department concludes:

- The parks within the service area of the subject development have an average score of 0.64,

or a letter grade of "B-", and that the lowest scoring park in the vicinity of the development is Ben Lomond Regional Park/Splashdown Waterpark, a regional park, with a score of 0.57 (or 'C').

- There are no neighborhood parks within the service area of the proposed development.

In looking at the Department's Needs Assessment and Master Plan, the following facilities and amenities have been identified as the top priorities for residents who reside within the Brentsville Magisterial District:

- walking and biking trails
- natural wildlife habitats
- small neighborhood parks
- picnic areas & shelters
- large regional parks

### **Proposal Strengths**

- **Monetary Contribution:** As proffered, the Applicant will make a monetary contribution to the Board of County Supervisors in the amount of \$770.48 per single family attached residential unit and \$565.67 per multi-family unit to be used for parks and recreation purposes.
- **Onsite Recreational Facilities:** The Applicant shall provide an amenity package for the residents that shall include, at a minimum, a clubhouse, 5-lane, 25 meter swimming pool, and two (2) playgrounds. The Applicant shall complete construction of the clubhouse and related recreational facilities by the time a building permit is issued for the 485th residential unit on the Property. Other amenities identified in the Design Guidelines include the community gathering area, innovation playground, neighborhood promenade, three pocket parks, nature playground, nature park, courtyard green, linear courtyard, pavilion green and urban park, urban promenade, and gateway plaza. The amenities shall be constructed prior to the final building permit for the section in which they are located and shall be shown on each respective final site plan.

### **Proposal Weaknesses**

- None identified.

**On balance,** this application is found to be consistent with the relevant components of the Parks component of the Comprehensive Plan.

## **Police Plan Analysis**

Residents and businesses expect a high level of police service for their community. This service increases the sense of safety and protects community investments. The Police Plan is designed to promote Prince William County's public safety strategic goal to continue to be a safe community, reduce criminal activity, and prevent personal injury and loss of life and property, as well as to ensure effective and timely responses throughout the County. This Plan encourages funding and

locating future police facilities to maximize public accessibility and police visibility as well as to permit effective, timely response to citizen needs and concerns. The Plan recommends educational initiatives, such as Neighborhood and Business Watch, and Crime Prevention through Environmental Design (CPTED), which encourages new development to be designed in a way that enhances crime prevention. The Plan also encourages effective and reliable public safety communications linking emergency responders in the field with the Public Safety Communications Center.

The Police Department recommends that the Applicant apply the various Crime Prevention Through Environmental Design (CPTED) strategies during site development, which can be found at <http://www.pwcgov.org/government/dept/police/Pages/CPTED.aspx>.

A full copy of the Police Memo is attached to this report.

### **Proposal's Strengths**

- No Significant Impact: The Police Department does not believe that the proposal will significantly impact Police calls for service.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Police Plan.

## **Potable Water Plan Analysis**

A safe, dependable drinking water source is a reasonable expectation of County residents and businesses. The Potable Water Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound drinking water system. The Plan includes recommendations relating to system expansion, required connections to public water in the Development Area, and the use of private wells or public water in the Rural Area.

The property is within the Prince William County Service Authority service area. Public water is available from an existing 16-inch stub-out located on parcel 7596-92-6825, an existing 8-inch water main on the adjacent property east of parcel 7596-92-6825, and an existing 12-inch stub-out located on the southeast corner of parcel 7696-00-5505, with availability of capacity determined in conjunction with plan submission. The developer shall be required to install a minimum 12-inch diameter main through the site connecting the two specified stub-outs to provide a system loop closure for increased redundancy and water quality. In addition, connections to the existing 16-inch dry main located on the southwest corner of parcel 7596-92-6825 and the existing 12-inch water main along George Mason Circle shall be required. All connections to the public water system shall be in accordance with the Service Authority's USM requirements and restrictions.

**Proposal's Strengths**

- Water Connection: The Property will be served by public water and the Applicant will be responsible for the costs and construction of those on and off-site improvements required in order to provide such service for the demand generated by the development on the Property.

**Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Potable Water Plan.

**Sanitary Sewer Plan Analysis**

Appropriate wastewater and sanitary facilities provide needed public health and environmental protections. The Sanitary Sewer Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound sanitary and stormwater sewer system. The Plan includes recommendations relating to system expansion, required connections to public sewer in the development area, and the use of either private or public sewer systems in locations classified as Semi-Rural Residential (SRR), as well as the Rural Area.

Public sewer is available offsite from an existing 30-inch gravity sewer main located along the northern boundary of parcel 7596-92-6825 and an existing 16-inch gravity sewer main located east of the intersection of University Boulevard and Cannon Creek Lane, with availability of capacity determined in conjunction with plan submission. All connections to the public sewer system shall be in accordance with the Service Authority's USM requirements and restrictions

**Proposal's Strengths**

- Sewer Connection: The Property will be served by public sewer and the Applicant will be responsible for the costs and construction of those on and off-site improvements required in order to provide such service for the demand generated by the development on the Property.

**Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Sanitary Sewer Plan.

### Schools Plan Analysis

A high-quality education system serves not only the students and their families, but the entire community by attracting employers who value educational opportunities for their employees. The Schools Plan sets out policies and action strategies that further the County's goal of providing quality public education to our school-aged population. The Plan includes recommendations relating to facility size and location, sitting criteria, compatible uses, and community use of school facilities. The Innovation Park Small Area Plan identifies the potential for a portion of the proposed residential housing to be student housing for the GMU campus which may cause a significant reduction in demand for school facilities proposed in the plan.

In a memorandum dated September 8, 2021, the School Division provided the following analysis:

Proposed Residential Rezoning (number of units)		Student Generation for Proposed Rezoning																					
<table border="1"> <thead> <tr> <th colspan="2">Housing Units Proposed</th> </tr> </thead> <tbody> <tr> <td>Single-Family</td> <td>0</td> </tr> <tr> <td>Townhouse</td> <td>370</td> </tr> <tr> <td>Multi-family</td> <td>662</td> </tr> <tr> <td><b>Total</b></td> <td><b>1032</b></td> </tr> </tbody> </table>		Housing Units Proposed		Single-Family	0	Townhouse	370	Multi-family	662	<b>Total</b>	<b>1032</b>	<table border="1"> <thead> <tr> <th colspan="2">Students Generated</th> </tr> </thead> <tbody> <tr> <td>Elementary</td> <td>206</td> </tr> <tr> <td>Middle</td> <td>99</td> </tr> <tr> <td>High</td> <td>125</td> </tr> <tr> <td><b>Total</b></td> <td><b>430</b></td> </tr> </tbody> </table>		Students Generated		Elementary	206	Middle	99	High	125	<b>Total</b>	<b>430</b>
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Under the School Division's 2021-22 school attendance area assignments, students generated from the Proposed Rezoning will attend the following schools:

School Level	Available Space			2020-21			2025-26			2030-31		
	Planning Capacity <sup>1</sup>	Program Capacity <sup>2</sup>	Portable Classrooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Ellis ES	492	--	3	436	56	88.6%	436	56	88.6%	517	-25	105.1%
Marsteller MS	1,233	1,217	5	1,246	-13	101.1%	1,141	76	93.8%	1,140	77	93.7%
Unity Reed HS	2,409	---	7	2,662	-253	110.5%	2,143	266	89.0%	2,504	-95	103.9%

<sup>1</sup> (MS) Planning Capacity is used for the 2020-21 school year.

<sup>2</sup> (MS) Program Capacity will be replacing Planning Capacity in the 2021-22 school year. The numbers in the table reflect the change starting in 2021-22.

There are CIP improvements that could provide some positive effect in the attendance areas, as follows:

Schools Capital Improvements Program (CIP) Projects that may impact schools in attendance areas of the Proposed Rezoning (with year anticipated)	
Elementary School	Rosemount Lewis Elementary School (2023)
Middle School	Reagan Middle School 6-room addition (2022); Gainesville Middle School 11-room addition (2022)
High School	Gainesville High School (2021)

Note: The capacity utilization of an individual school due to the impact of future Schools CIP projects will vary based upon the attendance area modifications approved by the School Board.

**School Board Comments dated September 8, 2021**

- Current enrollment exceeds capacity at the assigned middle school (Marsteller) and assigned high school (Unity Reed). Projections for the assigned high school has capacity with the opening of Gainesville High School.
- As indicated above, the assigned elementary school and middle school are expected to exceed capacity within five years with the additional students under this application. However, Ellis Elementary School should see overcrowding relief with the opening of the “Rosemount Lewis” Elementary School in 2023 and the additions at Reagan Middle School and Gainesville Middle School in 2022 will provide overcrowding relief to Marsteller Middle School. The applicant is proffering to contribute \$6,142,118.30 to Schools as mitigation strategy.
- For these reasons, the School Board is not opposed to the subject application.

**Proposal's Strengths**

- Mitigation of Impacts: The Community Education Chapter of the Comprehensive Plan recommends the mitigation of the impact of proposed new residential development on the level of service (LOS) standards of current school enrollment for which a rezoning and/or special use permit is requested when appropriate and allowed by applicable law. As proffered, the Applicant will make a monetary contribution to the Board of County Supervisors in the amount of \$8,549.47 per single family attached residential unit and \$4,499.72 per multi-family unit to be used for school sites/facilities in order to meet future projected needs. Current proffer law allows the Applicant to proffer contributions that the Applicant believes are appropriate. These amounts are also consistent with the Applicant's submitted proffer justification narrative.

**Proposal's Weaknesses**

- School Capacity: The subject proposal is estimated to generate approximately 206 new elementary school students. The elementary school for this area, Ellis Elementary School, does not have sufficient excess capacity (set by the School Board) for the additional students. There is a planned CIP project that could provide a positive effect on elementary school capacities in the future, but the potential impacts are not known.

The subject proposal is estimated to generate approximately 99 middle school students. The middle school for this area, Marsteller Middle School, is currently over the capacity set by the School Board. There are some planned school additions in the CIP.

The subject proposal is estimated to generate approximately 125 high school students. The high school for this area, Unity Reed High School, is currently over the capacity set by the School Board. There is a planned CIP improvement that could provide positive effect on high school capacities in the future.

**On balance**, this application is found to be consistent with the relevant components of the Schools Plan; based on the proposed mitigation measures.

### **Transportation Plan Analysis**

By providing a multi-modal approach to traffic circulation Prince William County promotes the safe and efficient movement of goods and people throughout the County and surrounding jurisdictions. The Transportation Plan sets out policies and action strategies that further the County's goal of creating and sustaining an environmentally friendly, multi-modal transportation system that meets the demands for intra- and inter-county trips, is integrated with existing and planned development, and provides a network of safe, efficient, and accessible modes of travel. The Plan includes recommendations addressing safety, minimizing conflicts with environmental and cultural resources, maximizing cost effectiveness, increasing accessibility of all travel modes, minimizing projected trip demand, and providing sufficient network capacity. Projects should include strategies that result in a level of service (LOS) of "D" or better on all roadway corridors and intersections, reduce traffic demand through transportation demand management strategies, dedicate planned rights-of-way, provide and/or fund transit infrastructure, pedestrian and bicycle pathways, and improved and coordinated access to transit facilities.

The roads which directly serve the site are described below:

Prince William Parkway (Route 234) is a 4-lane divided Principal Arterial limited access road that is planned for six lanes within a 160' right of way. This roadway provides intra-county connections to employment areas and provides a major connection between I-95 and I-66.

Wellington Road (Route 674) is a Minor Arterial with two lanes east of Route 234 and four lanes west of Route 234. It is planned as a 6-lane divided facility within 128' right of way from Prince William Parkway to Linton Hall Road and a 4-lane divided facility east of Prince William Parkway to Godwin Drive. Wellington Road provides important intra-county access to Innovation, Virginia Gateway, the concert pavilion, and industrial areas along the roadway. The proposed ultimate interchange with Sudley Manor Drive and Prince William Parkway includes bridging Wellington Road over Prince William Parkway. The interim interchange/innovative intersection includes eliminating left turn lanes at the intersection of Wellington Road and Prince William Parkway, quadrant loops utilizing existing roads with Sudley Manor Drive and Prince William Parkway grade separated.

Bethlehem Road (Route 821) is a local road that serves the industrial area along the corridor. It will be used as a quadrant loop for the proposed Sudley Manor Drive/Prince William Parkway/Wellington Rd. interchange/innovative intersection.

The following tables provide the most current VDOT traffic volumes, road and signalized intersection levels of service:

Roadway Name	Number of Lanes	2019 VDOT Annual Average Daily Traffic	2015 Daily LOS
Prince William Parkway (Route 234) South of Wellington Road	4	43,000	C
Wellington Road (Route 674) East of PW Pkwy	2/4	12,000	B
Bethlehem Road (Route 821) Between Sudley Manor Drive and Wellington Road.	2	3,600	C

**Proposal's Strengths**

- Transportation Improvements: The rezoning includes appropriate transportation commitments that will support the development of the Town Center and the residential area.
  - The Applicant has proffered to construct Katherine Johnson Avenue through the property to provide connectivity from University Blvd. to Wellington Road and serve as a spine road for the Town Center and residential area.
  - The Applicant has proffered to construct Hylton Center Blvd. that will connect Prince William Parkway to the GMU campus, subject to the Commonwealth Transportation Board approval. GMU has expressed interest in making Hylton Center Blvd. as the primary access to the campus and Hylton Center.
  - The Applicant has proffered to construct a right-in/right out access from Hylton Blvd. to Prince William Pkwy. provided that the Commonwealth Transportation Board (CTB) approves the limited access break.
  - The Applicant has proffered to relocate Bethlehem Road off-site to connect to Katherine Johnson Avenue at Wellington Road and to implement the design of the Sudley Manor/PW Pkwy/Wellington intersection improvements.
  - The Applicant has proffered to improve Wellington Road at Bethlehem Road and JD Reading Drive.
  - The Applicant has proffered to prepare a traffic signal warrant analysis at the intersections of Wellington Road and Hornbaker Road and Wellington Road/Bethlehem Road/Katherine Johnson Avenue and install/construct a traffic signal or roundabout as warranted.
  
- Pedestrian Facilities: Sidewalks, trails, shared use paths, and pedestrian circulation are critical to this project. The Applicant has included numerous connections between the residential area, the Town Center and the GMU campus in order to reduce the number of vehicular trips and parking spaces required within the development and to create a walkable community. The Applicant has proffered to design the future parking garage on Land Bay E to facilitate a connection to the future pedestrian bridge over Prince William Parkway (by others).



- Transportation Demand Management (TDM): The Applicant has proffered to work with the County and VDOT to submit and implement a Transportation Demand Management (TDM) plan. A TDM establishes measures to influence travel behavior by mode, frequency, time, route or trip length in order to achieve an efficient use of the transportation facilities and to create a viable Town Center without an excessive amount of vehicular traffic and required parking. The Applicant must provide a method of controlling long term student parking on Town Center streets. Major components of a TDM could include participating in a Transportation Management Association (TMA), a partnership between businesses, Homeowners Associations, GMU and the County to provide possible shuttle service to the Broad Run VRE Station and within Innovation Park to provide opportunities for dining, recreation and shopping. The Applicant has proffered a monetary contribution to the TDM fund on a building by building basis at the rate of \$0.01 per gross square footage of new commercial uses to be constructed on the property to be paid at issuance of occupancy permit.
- MuniCap Proffer Justification Narrative Analysis: The Public Transportation Facility Improvement Impacts section shows that the traffic generated by the proposed development is approximately 5.6% of the traffic at adjacent intersections. This percentage of the estimated costs of the total of the Capital Improvements and planned Infrastructure Improvements is \$11,061,948. The Applicant's cost for Infrastructure Improvements is \$16,990,612, which is \$5,928,663 more than its share of the cost, per the MuniCap study,
- Phasing Plan: The phasing plan has been tied to the Traffic Impact Analysis (TIA) to show the capacity of the roadway network to handle the traffic associated with each of the planned phases. The Applicant has proffered to submit a revised TIA if the connection from Hylton Blvd. to Prince William County Parkway is not approved by the Commonwealth Transportation Board (CTB).

**Proposal's Weaknesses**

- None Identified.

**On balance**, this application is found to be consistent with the relevant components of the Transportation Plan.

**Strategic Plan**

This section of the report is intended to address the project's alignment with the outcomes provided within the County's Strategic Plan. The [2021-2024 Strategic Plan](#) was adopted by the Board of County Supervisors on July 20, 2021. It was developed through extensive community input, research of county policies and services and interviews with the Board to collect input on strategic priorities for the County over the next 4 years. The Plan envisions Prince William County as a diverse community striving to be healthy, safe, and caring with a thriving economy and a protected natural

environment. In an effort to implement this vision, the Strategic Plan Team developed seven strategic focus areas to guide Board actions: "Health, Wellbeing & Human Services," "Safe and Secure Community," "Resilient Economy," "Quality Education & Workforce Development," "Environmental Conservation," "Sustainable Growth," and "Transportation and Mobility." It is important to note that no single area is viewed as more critical than another. Rather, each are interrelated and have direct impact on each other. Collectively, these goal areas impact the quality of life in all facets of the community issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The aspects of the proposal relative to the Strategic Plan are as follows:

### **Goal 3: Resilient Economy**

- Develop opportunities for self-contained lifestyle, recreation, residential, entertainment and town center developments with an emphasis on creating accessible, walkable live/work/play destinations that include parks, trails, and green spaces.
- Develop regional partnerships, internships, mentorships, and workforce development programs with schools and institutions of higher learning in targeted industries.
- Continue efforts to preserve and expand the commercial tax revenue base.

The proposal is for a town center development connected to George Mason University, which as designed with emphasis on walkable environments and attractive public spaces, is in alignment with the recently adopted Strategic Plan for live/work/play environments. The project is well-situated for partnerships with the SciTech Campus in the future. Additionally, the rezoning from A-1 and PBD to PMD, which includes the potential for 447,100 square feet of nonresidential uses will expand the County's commercial tax revenue base.

### **Goal 5: Environmental Conservation**

- Prioritize minimizing land disturbance during construction and leaving natural terrain in a natural state. When not feasible, evaluate alternatives such as tree-banking programs.
- Promote reforestation and meadow development with native plants on county land and on private land.
- Preserve and replace vegetated stream buffers on perennial and intermittent streams.

The proposal meets the environmental conservation policies provided in the Strategic Plan. Thirty percent open space is provided on the site, which exceeds minimum requirements. The limits of clearing and grading preserve 6 of the 13 specimen trees located onsite. Additionally, the layout of the residential sections of the development have been designed to allow for stream preservation.

### **Goal 6: Sustainable Growth**

- Focus future growth into activity centers with a variety of housing types and access to transit.

The proposal is consistent with the above Sustainable Growth policy, because the Innovation Park area is one of six designated regional activity centers in the County and is an area targeted to accommodate desired economic development.

**Goal 7: Transportation & Mobility**

- Improve connectivity of sidewalks and trails (paved and unpaved) for pedestrians and cyclists.
- Prioritize critical infrastructure projects that expand roadway capacity through the construction of new roadways or widenings, and new interchanges that support both local and regional mobility and sustainable growth.

The application includes sidewalks, trails and a well-developed system of pedestrian circulation. The Applicant has included numerous connections between the proposed student housing, the town center and the GMU campus in order to reduce the number of vehicular trips and parking spaces required within the development and to create a walkable community. A new roadway network is also proposed to serve the town center and planned growth.

**Materially Relevant**

This section of the report is intended to identify issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The materially relevant issues in this case are as follows:

- Transportation/Land Use Connections (TLC) Grant: The County recently received funding from Metro Washington Council of Governments (MWCOG) for a TLC grant for a Feasibility Study for an Innovation Park Town Center shuttle. The Study provides a direct convenient transit option between points within the Innovation Regional Activity Center and the Broad Run VRE Station. The shuttle would satisfy a first/last mile connection need to transit to reduce reliance on vehicle trips. The study will also provide connections for residents, students, and employees to access work, school and play opportunities within the area during off peak hours and on weekends. The study will include ridership projections, recommended routes/stops, operational needs, capital infrastructure (signing, passenger amenities, lighting, shelters), planning level cost estimates and a provision for autonomous shuttle vehicles. Coordination and collaboration with GMU will be a component of the study. Based on recommendations of the study, the County may pursue a pilot Autonomous Vehicle project for future service after a sufficient portion of the Town Center is completed to provide the necessary ridership. The study will begin in September 2021 and the results will be available in Summer 2022.
- Inter-Parcel Land Transfer Agreement: The Inter-Parcel Land Transfer Agreement provides for the sale and dedication of County-owned and privately owned property to create a straight property line between the County-owned land, 11100 University Boulevard, GPIN 7695-00-5505, and land owned by MJV and under contract by Stanley Martin Homes, LLC. Stanley Martin Homes, LLC intends to purchase the land from MJV to develop "Innovation Town Center" on 107.4 acres adjacent to the Innovation Development, LLC 23.7 acre "University Village" project. The straightened property line allows Innovation Development, LLC to show a straight road from Prince William County Parkway to the George Mason University Sci Tech Campus; proposed, future Hylton Center Boulevard. The straight road

was requested by County Planning, County DOT and VDOT and will be on public right of way dedicated through the Inter Parcel Land Transfer Agreement. The right/in right/out to the Prince William Parkway is subject to CTB approval.

This Agreement also provides for County reimbursement of road construction costs for Hylton Center Boulevard and the portion of Catherine Johnson Boulevard in the “University Village” project. County reimbursement of these costs requires the developers to have built parts of their development.

To qualify for reimbursement, Innovation Development LLC must accomplish the following:

- a. Complete approximately 325 residential units and 11,250 square feet of amenity retail.
- b. Construct a pedestrian connection from University Village to George Mason University's campus.
- c. Construct a temporary open space amenity in the area planned for Phase 3.
- d. Provide pad-ready sites for other mixed-use buildings in University Village.

To qualify for reimbursement, Stanley Martin Homes LLC must accomplish the following:

- a. Construct the pedestrian plaza area portion of their development.
- b. Substantially complete a commercial or mixed-use building adjacent to the pedestrian plaza area.
- c. Provide pad-ready sites for mixed-use buildings in the town center’s commercial area.

**Minimum Requirements**

The following Design and Construction Standards Manual (DCSM) standards apply to the proposal, and the Applicant is not requesting a modification.

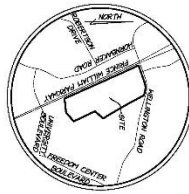
<b>TABLE 6-8 REQUIRED LOADING AND OFF-STREET PARKING SPA</b>	
<b>USE</b>	<b>PARKING</b>
<b>Single-family attached*</b>	2.75 (2 spaces per unit, plus 0.75 for every unit toward visitor parking; garage spaces may be counted*)
<b>Single-family attached with two or more car garage</b>	2.40 (2 spaces per unit, plus 0.4 for every unit toward visitor parking spaces; garage spaces may be counted*)

**Modifications / Waivers**

As allowed by Section 32-700.25, the Applicant requests approval of waivers and/or modifications to the requirements of the Zoning Ordinance and the Design and Construction Standards Manual (DCSM) as identified in a Waivers and Modification Exhibit provided as an attachment to this report.

# INNOVATION TOWN CENTER

MASTER ZONING PLAN  
 REZ #2016-00030  
 BRENTSVILLE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA



VICINITY MAP  
 SCALE: 1" = 200'

**PROJECT TEAM**

**DEVELOPER/CLIENT:**  
 STRAET PARTNERS, LLC  
 4000 PARK TRACON DRIVE, SUITE 100  
 FARMERS BRANCH, VA 22033  
 CONTACT: MR. BRETT TONG

**CIVIL ENGINEER:**  
 LAND DESIGN CONSULTANTS, INC.  
 400 BENTON ROAD, SUITE 200  
 PRINCE WILLIAM, VA 22031  
 CONTACT: MRS. JESSICA BROGEMAN

**LAND USE ATTORNEY:**  
 ANDREW COZZO, LIBERTY & PALMER  
 1000 COMMONWEALTH CENTER DRIVE  
 PRINCE WILLIAM, VA 22031  
 CONTACT: MRS. JANELLE CATERSON  
 MRS. MARGAN JACOBSON

**TOWNSHIP ENGINEER:**  
 TOWNSHIP ENGINEERS, INC.  
 8525 WASHINGTON STREET, SUITE 202  
 FARMERS BRANCH, VA 22033  
 CONTACT: MR. CHAD BARD

**ENVIRONMENTAL CONSULTANT:**  
 TAP ENVIRONMENTAL, INC.  
 6700 MARKET CENTER DRIVE, SUITE 103  
 FARMERS BRANCH, VA 22033  
 CONTACT: MR. ARI SWENSEN

**SHEET INDEX**

- 1 COVER SHEET
- 2 LAND USE PLAN
- 3 LAYOUT PLAN
- 4 LANDSCAPE BUFFER PLAN
- 5 SITE NEIGHBORHOOD TRANSPORTATION PLAN
- 5A STREET SECTIONS
- 5B UNIVERSITY BOULEVARD IMPROVEMENTS
- 5C WELLINGTON ROAD IMPROVEMENTS
- 5D WELLINGTON ROAD IMPROVEMENTS
- 5E BELLEVUE KATHARINE JOHNSON AVENUE IMPROVEMENTS
- 5F UNIVERSITY BOULEVARD IMPROVEMENTS
- 6A TOWN CENTER PARKING PLAN
- 6B INTERIM PARKING PLAN
- 7 UTILITY PLAN



COVER SHEET

INNOVATION TOWN CENTER

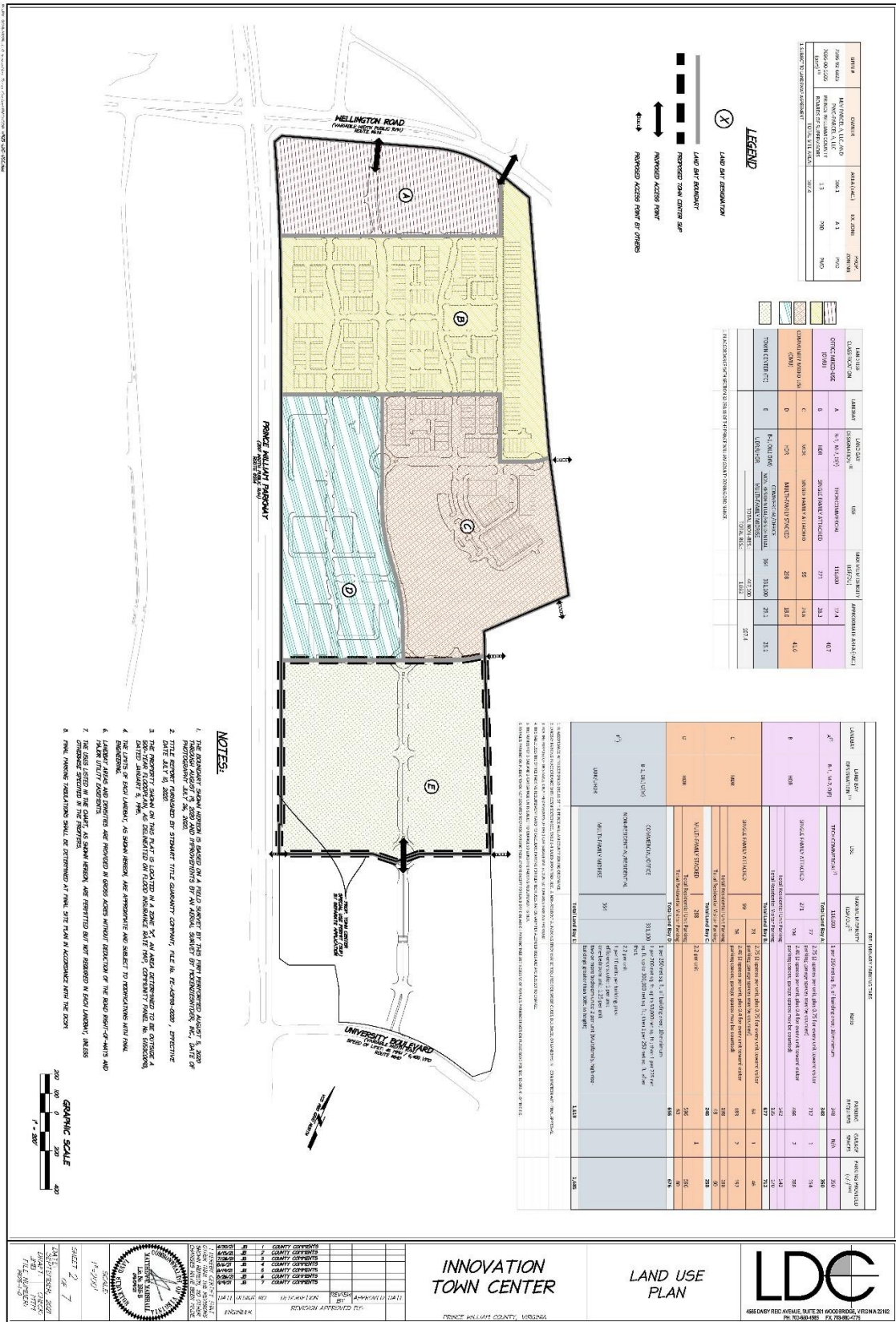
PRINCE WILLIAM COUNTY, VIRGINIA

NO.	DATE	DESCRIPTION	DESIGNED BY	CHECKED BY	APPROVED DATE
1	08/17/2016	DESIGN	J.S. SWENSEN		
2					
3					
4					
5					
6					
7					



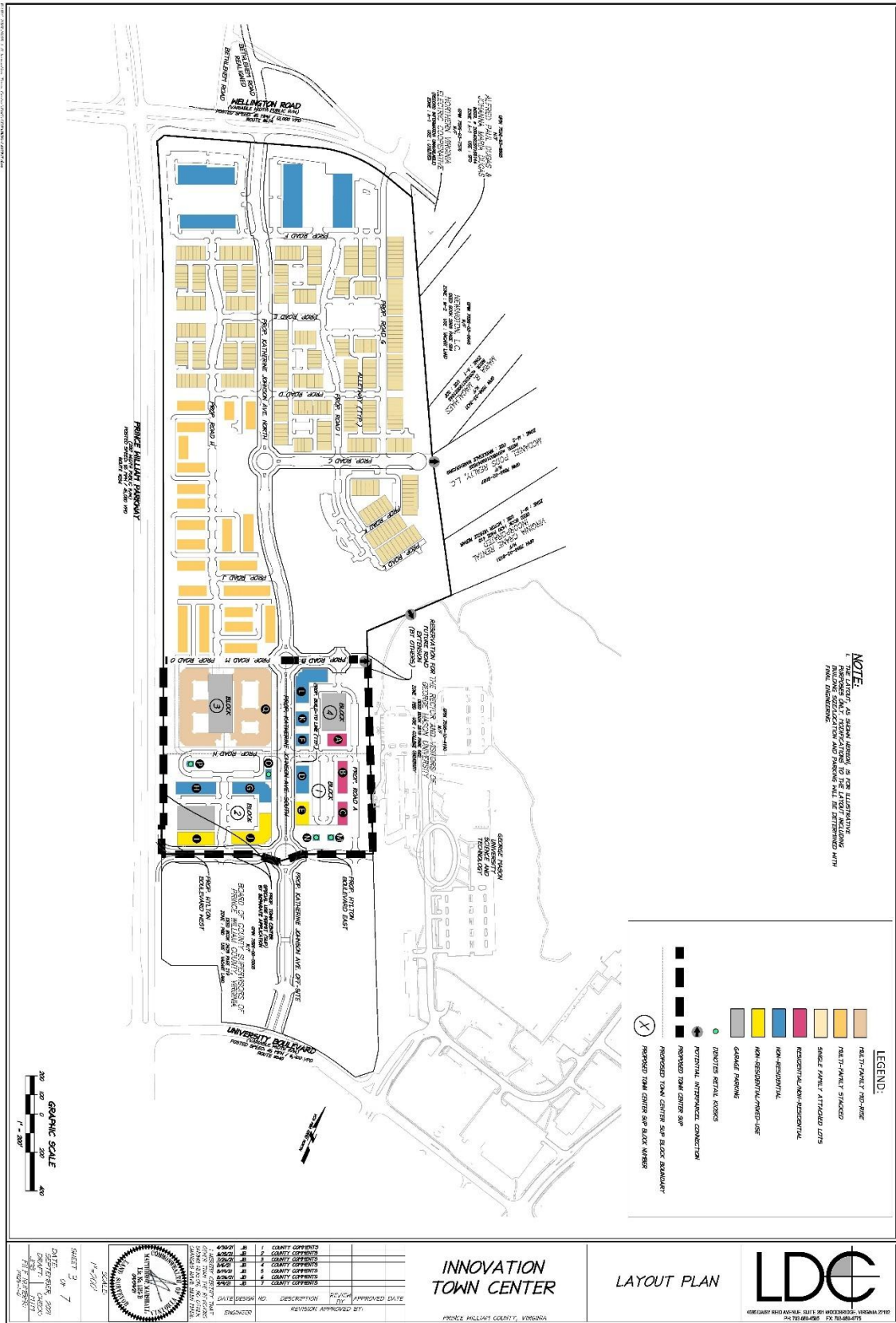
DATE DESIGNED	08/17/2016
DATE CHECKED	
DATE APPROVED	
SCALE	AS SHOWN
SHEET	1 OF 7
PROJECT NO.	REZ2016-00030
CLIENT	STRAET PARTNERS, LLC
LOCATION	BRENTSVILLE, VA

# Master Zoning Plan





# Master Zoning Plan















OPTION WITHOUT CUBIC EXTENSION (A)

OPTION WITH CUBIC EXTENSION (B)

OPTION WITHOUT CUBIC EXTENSION (C)

OPTION WITH CUBIC EXTENSION (D)

OPTION WITHOUT CUBIC EXTENSION (E)

OPTION WITH CUBIC EXTENSION (F)

OPTION WITHOUT CUBIC EXTENSION (G)

OPTION WITH CUBIC EXTENSION (H)

**NOTES:**

1. ALL CONCEPT PROPOSALS TO BE REVIEWED BY THE BOARD OF ZONING APPEALS AND THE ZONING COMMISSION IN CONJUNCTION WITH THE ZONING COMMISSION AND RESOLUTION FOR LANSING AND THE ZONING COMMISSION FOR THE ZONING COMMISSION.
2. THE BOARD OF ZONING APPEALS AND THE ZONING COMMISSION SHALL BE PROVIDED FOR BY THE ZONING COMMISSION.
3. THE BOARD OF ZONING APPEALS SHALL BE PROVIDED THROUGH THE ZONING COMMISSION IN CONJUNCTION WITH THE ZONING COMMISSION.
4. THE BOARD OF ZONING APPEALS SHALL BE PROVIDED THROUGH THE ZONING COMMISSION IN CONJUNCTION WITH THE ZONING COMMISSION.
5. THE BOARD OF ZONING APPEALS SHALL BE PROVIDED THROUGH THE ZONING COMMISSION IN CONJUNCTION WITH THE ZONING COMMISSION.

INNOVATION TOWN CENTER

PRINCE WILLIAM COUNTY, VIRGINIA

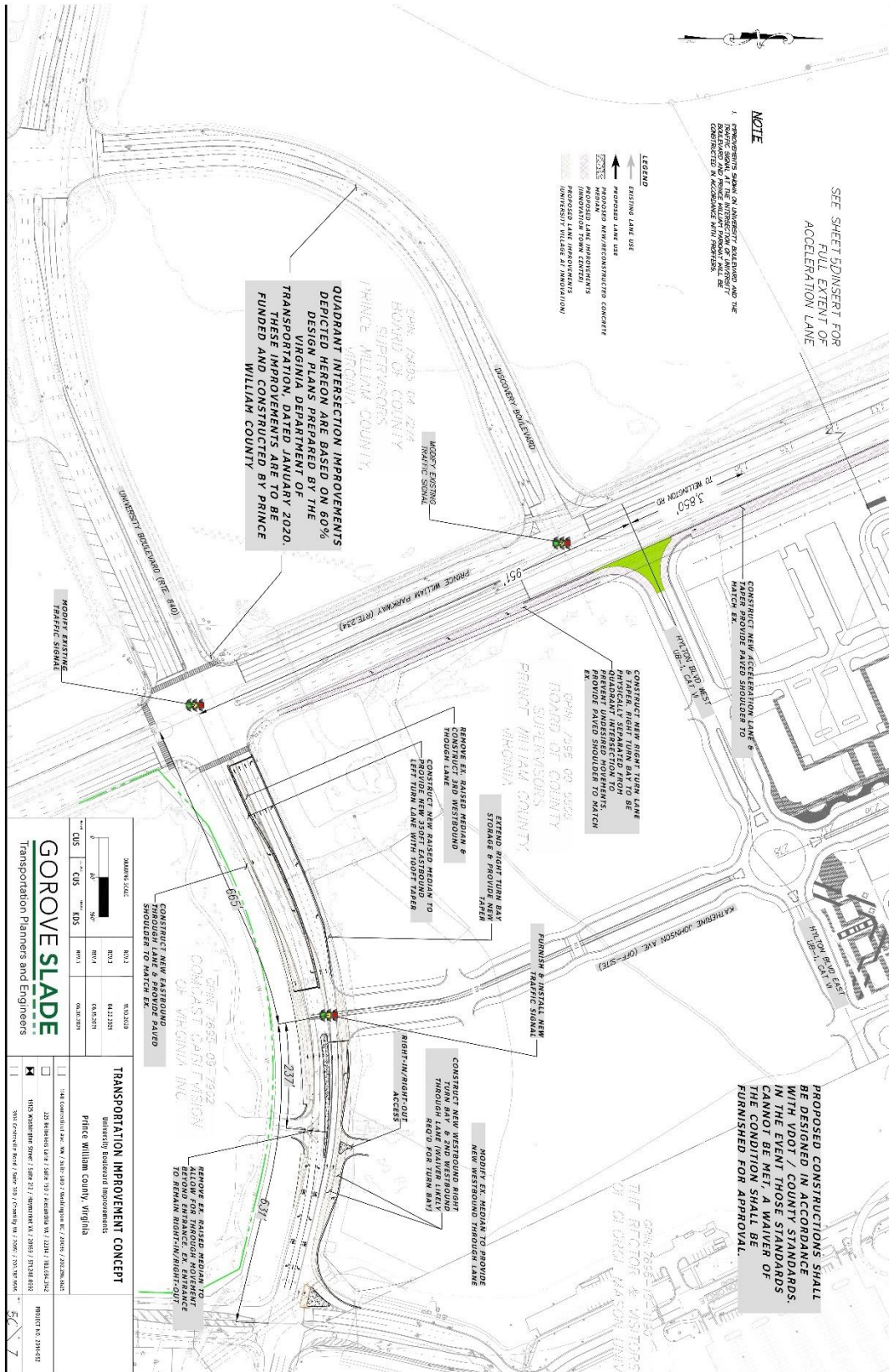
STREET SECTIONS

9000 WOODBURN BLVD, SUITE 200, WOODBURN, VA 22191  
PH: 703.961.4600 FAX: 703.961.4775

NO.	DATE	DESCRIPTION	APPROVED BY
1	07/15/2016	CONCEPT PROPOSAL	[Signature]
2	07/15/2016	CONCEPT PROPOSAL	[Signature]
3	07/15/2016	CONCEPT PROPOSAL	[Signature]
4	07/15/2016	CONCEPT PROPOSAL	[Signature]
5	07/15/2016	CONCEPT PROPOSAL	[Signature]
6	07/15/2016	CONCEPT PROPOSAL	[Signature]
7	07/15/2016	CONCEPT PROPOSAL	[Signature]
8	07/15/2016	CONCEPT PROPOSAL	[Signature]
9	07/15/2016	CONCEPT PROPOSAL	[Signature]
10	07/15/2016	CONCEPT PROPOSAL	[Signature]

ENGINEER: [Signature]

REVISION APPROVED BY: [Signature]

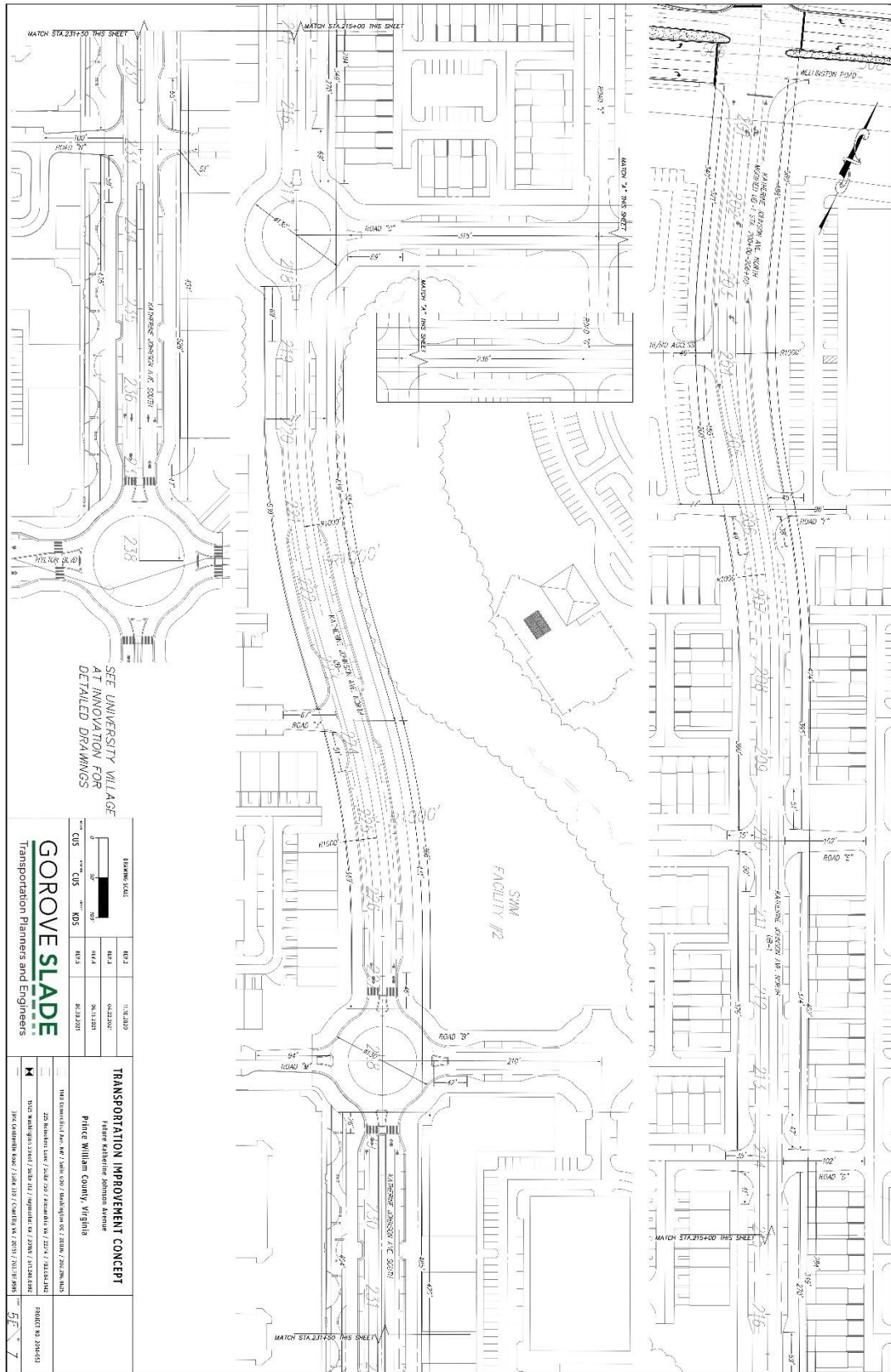


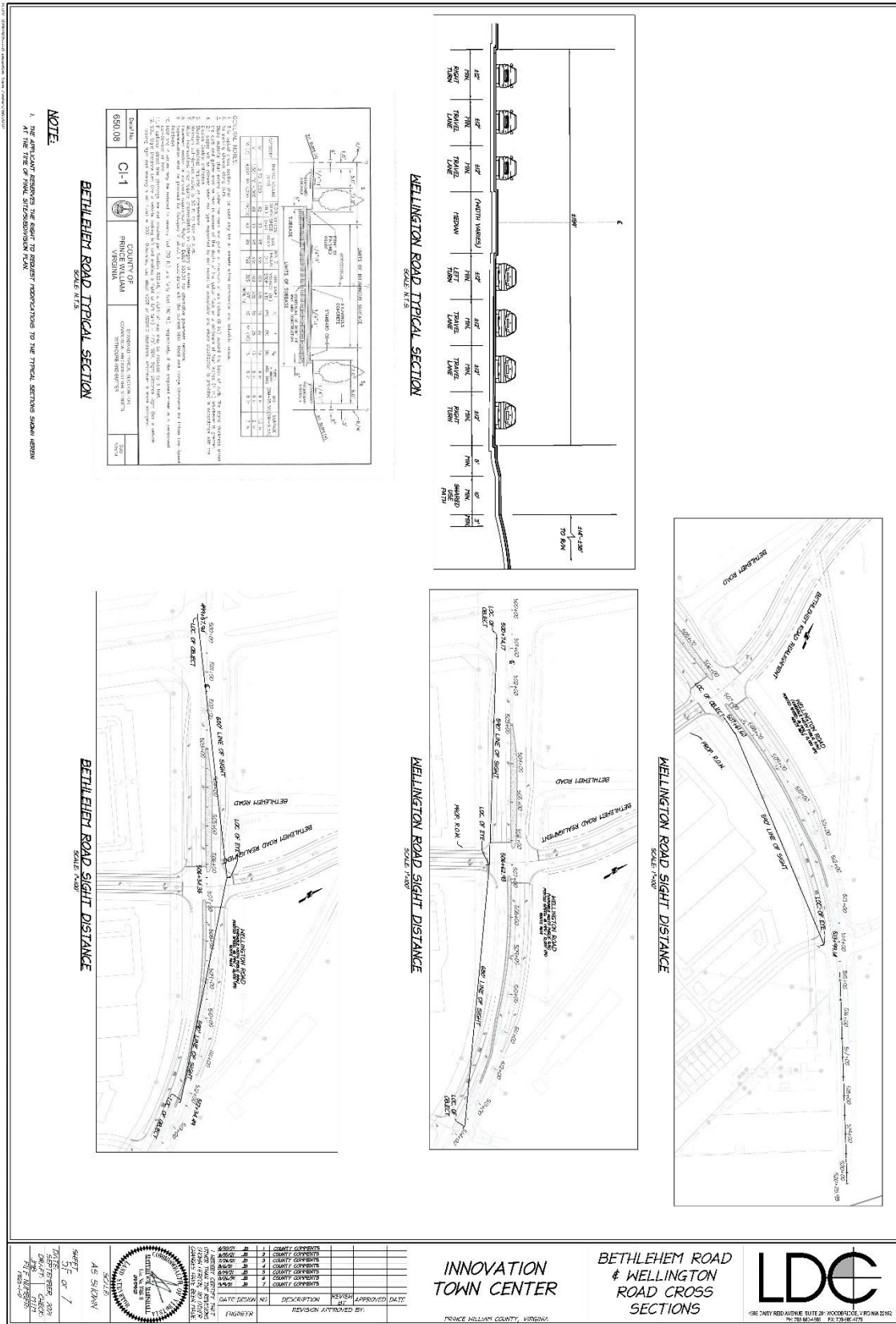




PROPOSED CONSTRUCTIONS SHALL BE DESIGNED IN ACCORDANCE WITH VDOT / COUNTY STANDARDS. IN THE EVENT THOSE STANDARDS CANNOT BE MET A WAIVER OF THE CONDITION SHALL BE FURNISHED FOR APPROVAL.

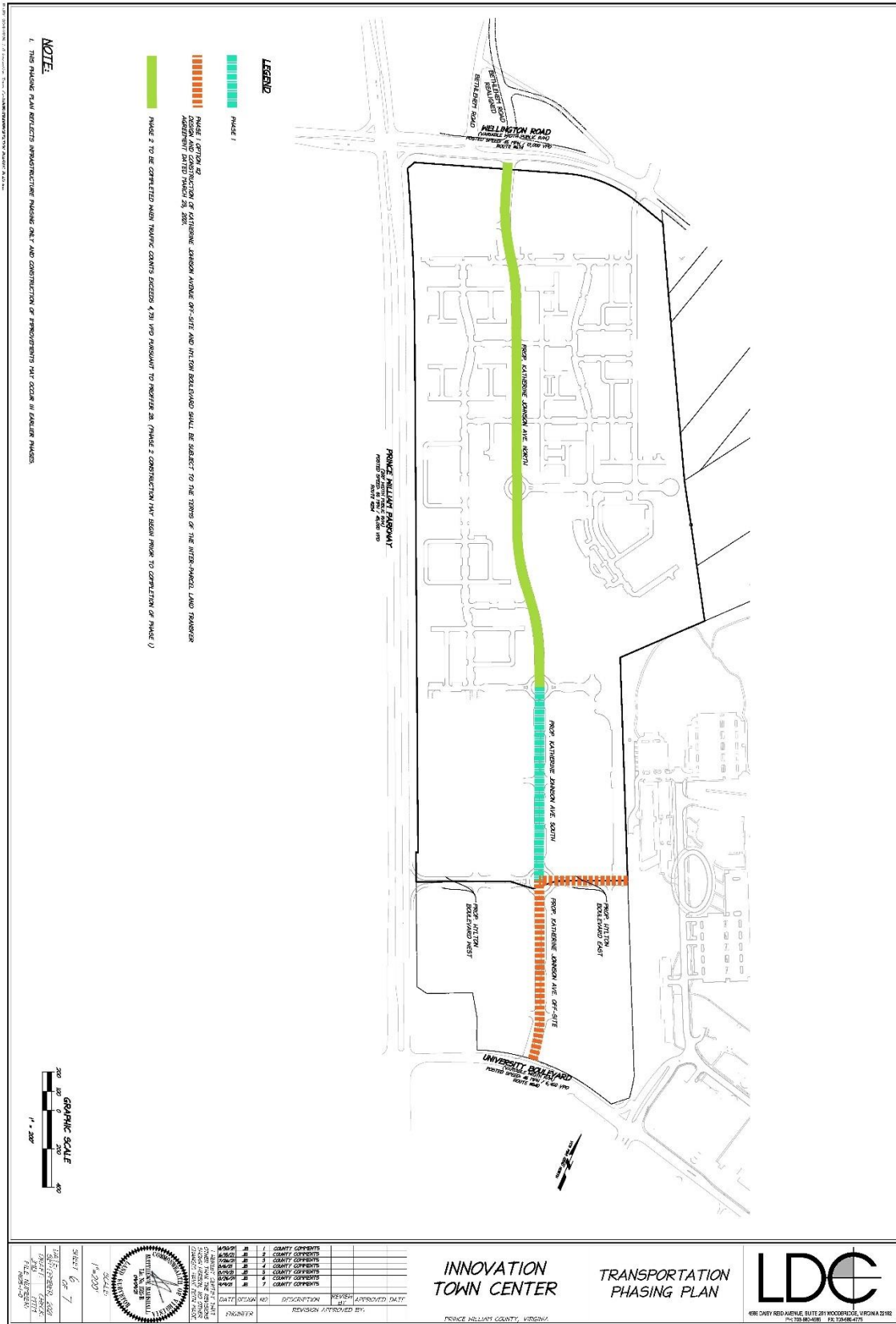
<b>GOROVE SLADE</b> Transportation Planners and Engineers	
11/13/2016 11/13/2016 11/13/2016 11/13/2016	11/13/2016 11/13/2016 11/13/2016 11/13/2016
<b>TRANSPORTATION IMPROVEMENT CONCEPT</b> Wellington Road Improvements Prince William County, Virginia	
1. 11/13/2016 2. 11/13/2016 3. 11/13/2016 4. 11/13/2016	
PROJECT NO. 2016-003	
3D 7	

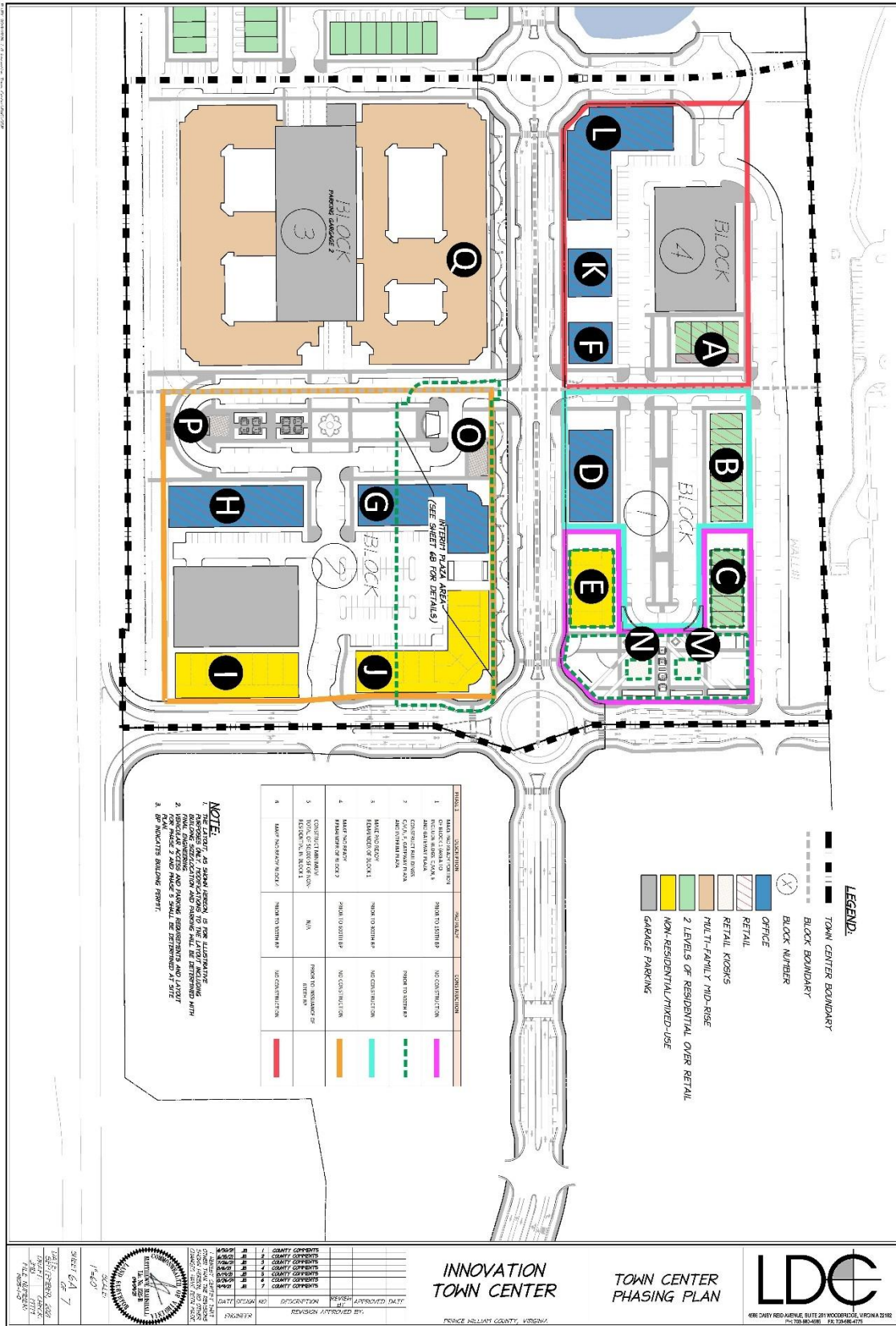


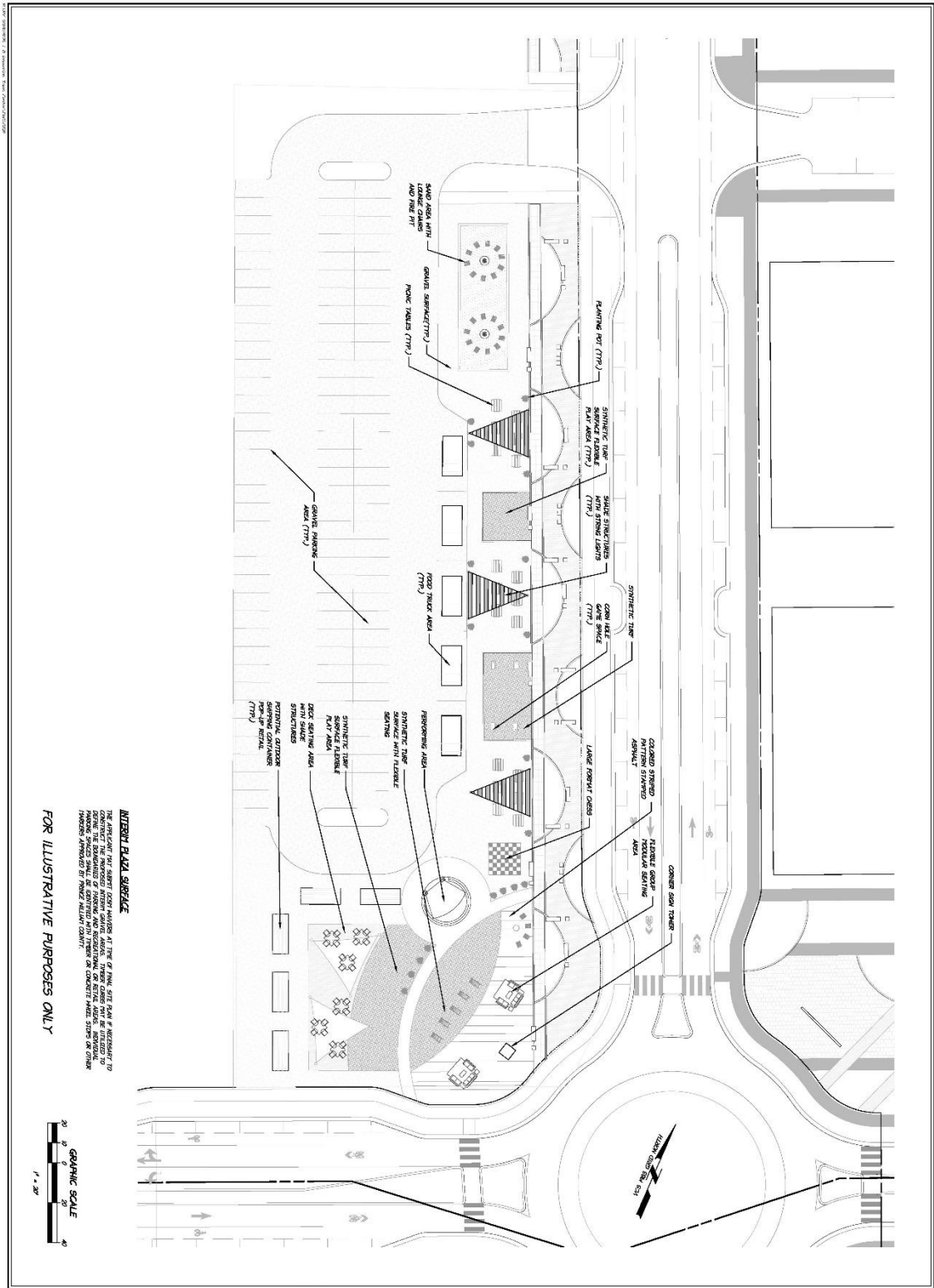




# Master Zoning Plan



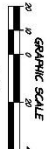




**INTERIM PLAZA SHEET**

THE ARCHITECT'S RESPONSIBILITY IS TO PREPARE ARCHITECTURAL DRAWINGS IN ACCORDANCE WITH THE CITY OF FREDERICKSBURG, VIRGINIA ZONING ORDINANCE. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED BY OTHER PROFESSIONALS. THE ARCHITECT'S WORK SHALL BE CONSIDERED AS A PROFESSIONAL SERVICE AND SHALL BE SUBJECT TO THE CITY OF FREDERICKSBURG, VIRGINIA ZONING ORDINANCE.

**FOR ILLUSTRATIVE PURPOSES ONLY**



DATE: 07-1-2017	SHEET: 613 OF 7	
DESIGNED BY: [Name]	DRAWN BY: [Name]	
FILE NO. [Number]	PROJECT NO. [Number]	

**INNOVATION TOWN CENTER**

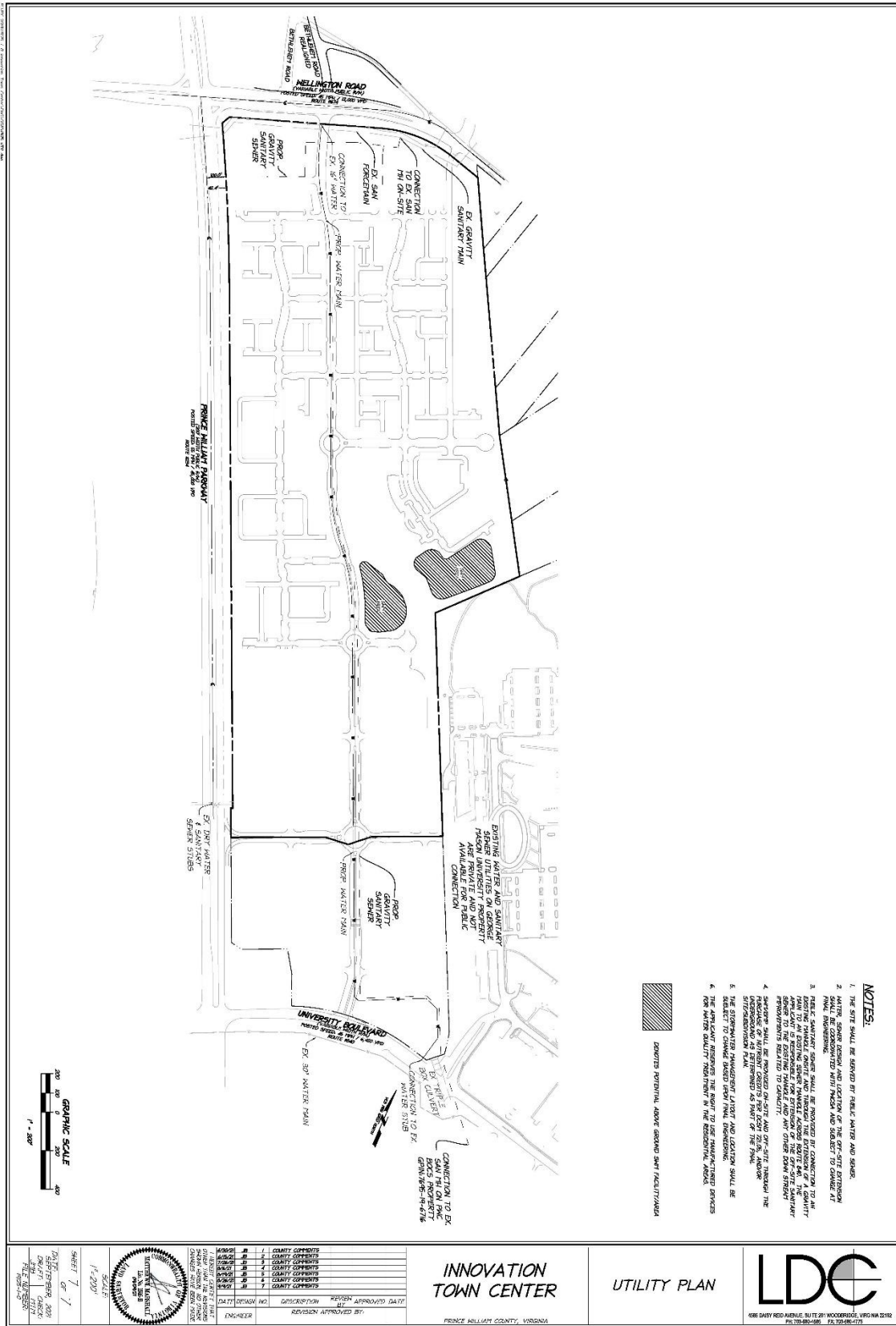
INTERIM PLAZA PLAN

FREDERICKSBURG, VIRGINIA

**LDC**

408 DAILY ROAD AVENUE, SUITE 201 WOODBRIDGE, VIRGINIA 22192  
 PH: 703-890-1886 FAX: 703-890-1776

# Master Zoning Plan



- NOTES:**
1. THE SITE SHALL BE SERVED BY PUBLIC WATER AND SEWER.
  2. WATER SOURCE DESIGN AND LOCATION OF THE OFF-SITE EXTENSION SHALL BE COORDINATED WITH INMCA AND SUBJECT TO CHANGE AT ANY TIME.
  3. PUBLIC SANITARY SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING PUBLIC SANITARY SEWER. THE EXTENSION OF A SANITARY SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING SANITARY SEWER. THE EXTENSION OF A SANITARY SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING SANITARY SEWER. THE EXTENSION OF A SANITARY SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING SANITARY SEWER.
  4. SANITARY SEWER SHALL BE PROVIDED ON-SITE AND OFF-SITE THROUGH THE EXISTING SANITARY SEWER. THE EXTENSION OF A SANITARY SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING SANITARY SEWER.
  5. THE EXISTING SANITARY SEWER AND EXTENSION SHALL BE SUBJECT TO STANDARD SHOWN SHOWN DIMENSIONS.
  6. THE APPLICANT RESERVES THE RIGHT TO USE MANUFACTURED SERVICES FOR WATER QUALITY PROTECTION IN THE RESIDUAL WOODS.

EXISTING UTILITY ABOVE GROUND SHOWN FACILITIES

NO.	DATE	DESCRIPTION	REVISION APPROVED BY
1	11/11/16	ISSUED FOR PERMIT	ENGINEER
2	11/11/16	ISSUED FOR PERMIT	ENGINEER
3	11/11/16	ISSUED FOR PERMIT	ENGINEER
4	11/11/16	ISSUED FOR PERMIT	ENGINEER
5	11/11/16	ISSUED FOR PERMIT	ENGINEER
6	11/11/16	ISSUED FOR PERMIT	ENGINEER
7	11/11/16	ISSUED FOR PERMIT	ENGINEER
8	11/11/16	ISSUED FOR PERMIT	ENGINEER
9	11/11/16	ISSUED FOR PERMIT	ENGINEER
10	11/11/16	ISSUED FOR PERMIT	ENGINEER

**INNOVATION TOWN CENTER**

**UTILITY PLAN**



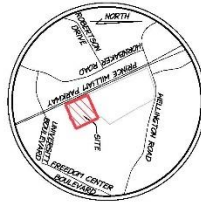
406 DAILY RD AVENUE, SUITE 201 WOODBRIDGE, VA 22192  
 PH: 703-680-8888 FAX: 703-680-4778



# INNOVATION TOWN CENTER

SPECIAL USE PERMIT  
SUP #2016-00031

BRENTSVILLE MAGISTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA



**PROJECT TEAM**

**DEVELOPER/OWNER:**  
STANLEY PARTNERS COMPANIES, LLC  
1400 DARTY ROAD, SUITE 100  
CHANTILLY, VA 20151  
(703) 658-2947  
CONTACT: MRS. TRUETT YOUNG

**CIVIL ENGINEER:**  
LAND DESIGN CONSULTANTS, INC.  
4005 DARTY ROAD, SUITE 201  
CHANTILLY, VA 20151  
(703) 660-6605  
CONTACT: MRS. JESSICA BRIDGEMAN

**LAND USE ATTORNEY:**  
WALSH COLACCI LIBERLETT & WALSH  
850 PRINCE WILLIAM PARKWAY, STE 300  
CHANTILLY, VA 20151  
(703) 600-4664  
CONTACT: MRS. JANELLE CAMPBELL  
MRS. KIMBERLY HARRISON

**TRAFFIC ENGINEER:**  
CONVERSE ASSOCIATES, INC.  
1025 WASHINGTON STREET, SUITE 202  
ALEXANDRIA, VA 22304  
(703) 261-1171  
CONTACT: MRS. CINDY BIRNBO

**ENVIRONMENTAL CONSULTANT:**  
THE ENVIRONMENTAL GROUP  
1700 PARKWAY EAST, SUITE 101  
ALEXANDRIA, VA 22304  
(703) 261-1171  
CONTACT: MRS. AMY SARGENT

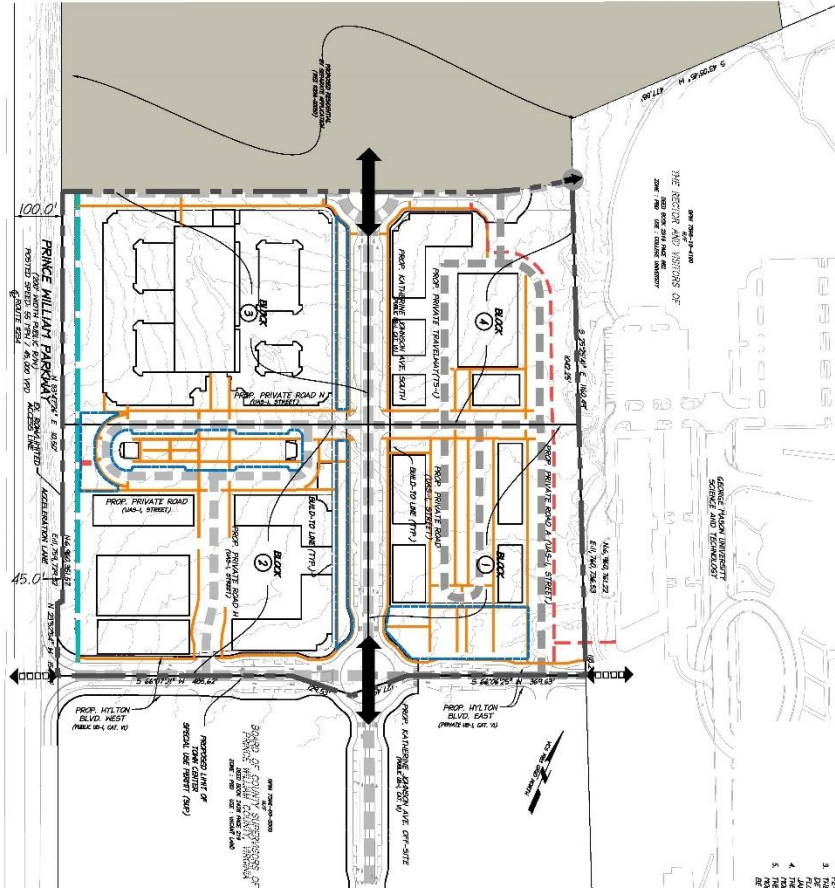
**SHEET INDEX:**

- 1 COVER SHEET
- 2 OVERALL SPECIAL USE PERMIT PLAN
- 3 SCHEMATIC LAND USE PLAN
- 4 UTILITY & INFRASTRUCTURE PLAN
- 5 STREETSCAPE & LANDSCAPE SCHEMATIC PLAN
- 6 STREET GRID PLAN
- 6A STREET SECTIONS
- 7 INFRASTRUCTURE PHASING PLAN

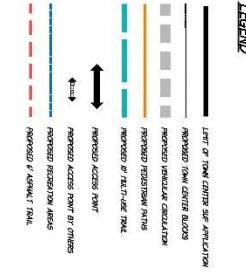
	<p><b>INNOVATION TOWN CENTER</b> <b>SPECIAL USE PERMIT</b> PRINCE WILLIAM COUNTY, VIRGINIA</p>	<p>COVER SHEET</p>	<p>4000 DARTY ROAD AVENUE, SUITE 201 WOODBRIDGE, VIRGINIA 22192 PH: 703-660-6605 FAX: 703-660-6776</p>																																																																																																									
<p>SHEET 1 OF 7 DATE: OCTOBER 2016 DRAWN BY: [Name] CHECKED BY: [Name] SCALE: AS SHOWN</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>APPROVED BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	DATE	DESCRIPTION	BY	APPROVED BY																																														<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>APPROVED BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	DATE	DESCRIPTION	BY	APPROVED BY																																																			<p>DATE: OCTOBER 2016 DRAWN BY: [Name] CHECKED BY: [Name] SCALE: AS SHOWN</p>
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NO.	DATE	DESCRIPTION	BY	APPROVED BY																																																																																																								

# Special Use Permit Plan

**PROPOSED SPECIAL USE PERMIT PLAN**  
**PRINCE WILLIAM COUNTY, VIRGINIA**  
**ZONING B-1 (O.C.), (M.T.) AND (L.P.C.)**



- NOTES:**
1. THE BOUNDARY BETWEEN THIS ZONING DISTRICT AND THE ADJACENT ZONING DISTRICT IS SHOWN BY A DASHED LINE.
  2. THE PROPERTY SHOWN ON THIS PLAN IS LOCATED IN A ZONE "M" AN AREA ALSO REGULATED BY THE COUNTY ZONING ORDINANCES.
  3. THE LAYOUT OF THIS BLOCK AS SHOWN HEREIN, USE, APPROPRIATE AND SUBJECT TO THE LAYOUT OF OTHER BLOCKS BELONGING TO THIS DEVELOPMENT.
  4. THE LAYOUT OF THIS BLOCK IS FOR ILLUSTRATIVE PURPOSES ONLY.
  5. THE LAYOUT OF THIS BLOCK IS FOR ILLUSTRATIVE PURPOSES ONLY.



NO.	DATE	DESCRIPTION	APPROVED BY
1	11/14/2017	ISSUED	
2	11/14/2017	REVISION	

NO.	DATE	DESCRIPTION	APPROVED BY
1	11/14/2017	ISSUED	
2	11/14/2017	REVISION	

**INNOVATION TOWN CENTER SPECIAL USE PERMIT**

**OVERALL SPECIAL USE PERMIT PLAN**



4960 DAVEY ROAD AVENUE, SUITE 201 WOODBRIDGE, VIRGINIA 22192  
 TEL: 703-800-8800 FAX: 703-800-4770

# Special Use Permit Plan

**BLOCK DESIGNATIONS**  
PROPOSED ROADWAY SPACES

GRID #	OWNER	ADDRESS	DC ZONE	TDR
204	OWNERS	(ADDRESS)	A.1	204
205	(OWNER)	(ADDRESS)	A.1	205
206	(OWNER)	(ADDRESS)	A.1	206
207	(OWNER)	(ADDRESS)	A.1	207

BLOCK	DESIGNATION	USE	MAXIMUM DENSITY	MINIMUM LOT SIZE
1	B.1, (DC) ZONE	COMMERCIAL/OFFICE/COMMERCIAL	75 SF	5.1
2	C.1, (DC) ZONE	ECONOMIC/ADULT/CHILDREN/COMMERCIAL	140 SF	7.5
3	B.1, (DC) ZONE	MULTI-FAMILY RESIDENTIAL	200 SF	4.3
4	B.1, (DC) ZONE	ECONOMIC/ADULT/CHILDREN/COMMERCIAL	100 SF	7.1

**LEGEND**

- PROPOSED ACCESS FRONT
- PROPOSED ACCESS FRONT BY OTHERS
- LEFT OF WAY PROVISION
- BLOCK BOUNDARY
- BLOCK DESIGNATION

**GRAPHIC SCALE**  
1" = 100'

**NOTES:**

- THE LAYOUT OF THIS BLOCK AS SHOWN HEREIN, USE APPROPRIATE AND SUBJECT TO APPROVAL BY THE CITY ENGINEER.
- BLOCK DESIGNATIONS AND ZONINGS ARE BASED ON THE CURRENT ZONING MAP OF THE CITY OF CHICAGO AND SHALL BE SUBJECT TO THE CITY ENGINEER'S REVIEW.
- THE LOTS SET IN THE CENTER OF THIS BLOCK ARE RESERVED FOR THE USE OF THE CITY ENGINEER AND SHALL BE RESERVED AT THE CITY ENGINEER'S DISCRETION.
- THE LAYOUT OF THIS BLOCK AS SHOWN HEREIN IS FOR ILLUSTRATIVE PURPOSES ONLY AND SHALL BE SUBJECT TO THE CITY ENGINEER'S REVIEW AND APPROVAL.
- THE LAYOUT OF THIS BLOCK IS FOR ILLUSTRATIVE PURPOSES ONLY AND SHALL BE SUBJECT TO THE CITY ENGINEER'S REVIEW AND APPROVAL.

**INNOVATION TOWN CENTER SPECIAL USE PERMIT**

PLANNING DEPARTMENT

DATE: 01/15/2016

APPROVED BY: [Signature]

REVISION APPROVED BY: [Signature]

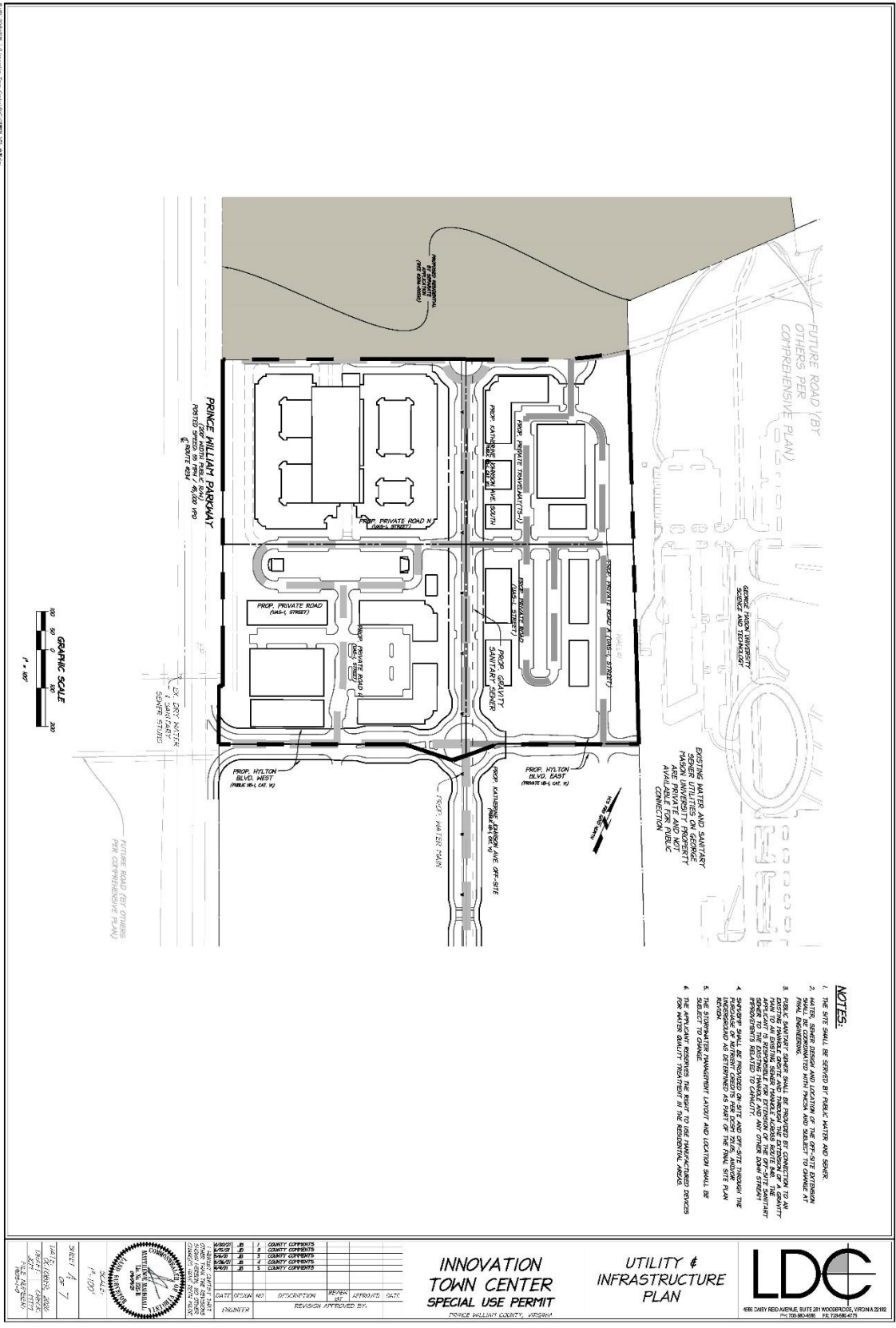
**SCHEMATIC LAND USE PLAN**

**LDC**

696 CHRY RD AVENUE, SUITE 201 WOODRIDGE, VIRGINIA 22191

TEL: 703-943-6411 FAX: 703-943-6471

# Special Use Permit Plan

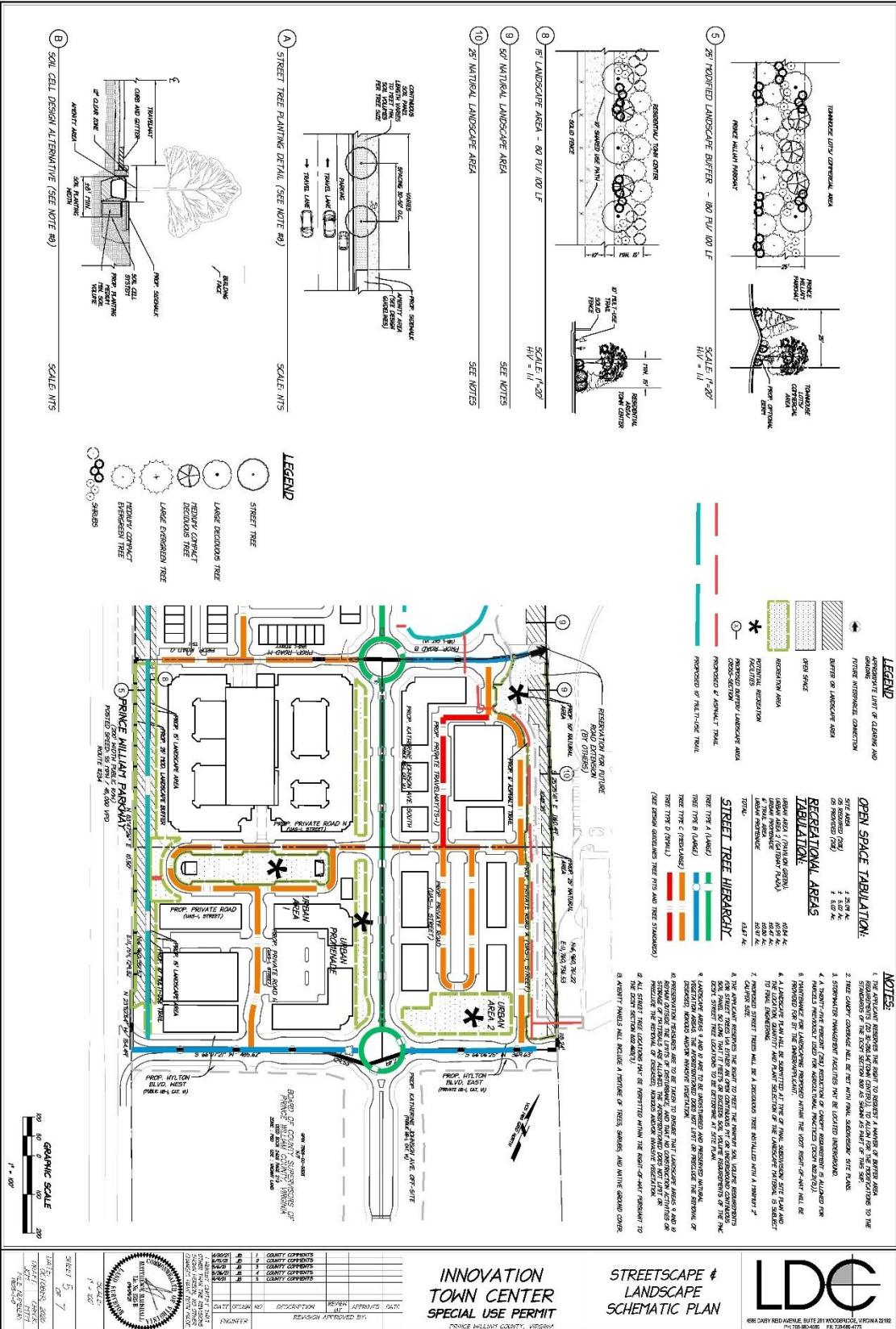


- NOTES:**
1. THE SITE SHALL BE SERVED BY PUBLIC WATER AND SEWER.
  2. WATER, SEWER DESIGN AND LOCATION OF THE OFF-SITE EXTENSION SHALL BE DETERMINED BY THE CITY ENGINEER AND SUBJECT TO OBTAINING THE CITY ENGINEER'S APPROVAL.
  3. PUBLIC WATER/SEWER SHALL BE PROVIDED BY CONNECTION TO AN EXISTING MAIN LINE OR NEW MAIN LINE TO BE INSTALLED AS PART OF THE EXISTING TOWN CENTER AND NOT OTHER EXISTING MAIN LINES OR NEW MAIN LINES.
  4. THE LOCATION OF THE WATER AND SEWER MAINS SHALL BE DETERMINED AS PART OF THE FINAL SITE PLAN AND SHALL BE SHOWN ON THE FINAL SITE PLAN.
  5. THE SEWERAGE MANAGEMENT LAYOUT AND LOCATION SHALL BE SUBJECT TO OBTAINING THE CITY ENGINEER'S APPROVAL.
  6. THE APPLICANT AGREES THE RIGHT TO USE UNUTILIZED DEVICES SHALL BE RESERVED FOR FUTURE USE AND NOT OTHER DEVICES.
  7. THE APPLICANT SHALL MAINTAIN ACCESS TO ALL EXISTING AND FUTURE PUBLIC CONNECTIONS.
  8. THE APPLICANT SHALL MAINTAIN ACCESS TO ALL EXISTING AND FUTURE PUBLIC CONNECTIONS.
  9. THE APPLICANT SHALL MAINTAIN ACCESS TO ALL EXISTING AND FUTURE PUBLIC CONNECTIONS.
  10. THE APPLICANT SHALL MAINTAIN ACCESS TO ALL EXISTING AND FUTURE PUBLIC CONNECTIONS.

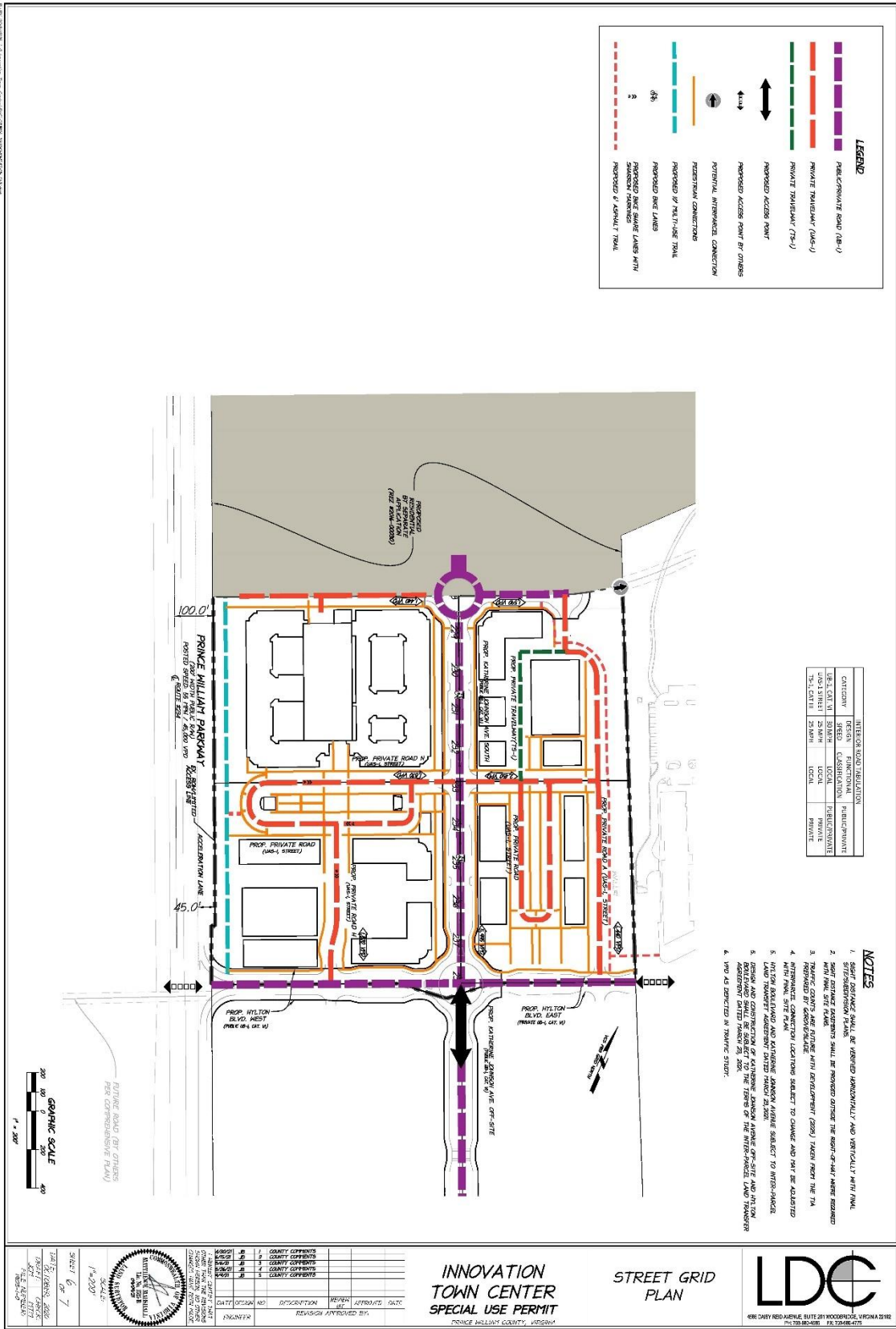
 <p><b>LDC</b> 600 CHERRY RIDGE AVENUE, SUITE 201 WOODBRIDGE, VIRGINIA 22192 TEL: 703-596-6600 FAX: 703-596-4771</p>	<p><b>UTILITY &amp; INFRASTRUCTURE PLAN</b></p> <p><b>INNOVATION TOWN CENTER SPECIAL USE PERMIT</b></p> <p><small>PRINCE WILLIAM COUNTY, VIRGINIA</small></p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>DATE</th> <th>BY</th> <th>REVISIONS</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>COUNTY COMMENTS</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>2</td> <td>COUNTY COMMENTS</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>3</td> <td>COUNTY COMMENTS</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>4</td> <td>COUNTY COMMENTS</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>5</td> <td>COUNTY COMMENTS</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table> <p>APPROVED BY: _____</p> <p>DATE: _____</p> <p>PROJECT: _____</p>	NO.	DESCRIPTION	DATE	BY	REVISIONS	DATE	BY	1	COUNTY COMMENTS						2	COUNTY COMMENTS						3	COUNTY COMMENTS						4	COUNTY COMMENTS						5	COUNTY COMMENTS					
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Special Use Permit Plan



# Special Use Permit Plan



# Special Use Permit Plan

Scale No.	UB-1	COUNTY OF PRINCE WILLIAM VIRGINIA	WOODS FRANCONIA TOWN CENTER 1500-1550 WEST NORTH STREET MIDDLESEX
65015			

**NOTES:**

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND CONDITIONS OF THE SPECIAL USE PERMIT PLAN. THE APPLICANT SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES AND AGENCIES OF STATE AND FEDERAL GOVERNMENT. THE APPLICANT SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES AND AGENCIES OF STATE AND FEDERAL GOVERNMENT.
2. THE APPLICANT SHALL MAINTAIN THE PROPOSED IMPROVEMENTS IN GOOD REPAIR AND CONDITION AT ALL TIMES. THE APPLICANT SHALL BE RESPONSIBLE FOR THE MAINTENANCE AND REPAIR OF ALL IMPROVEMENTS. THE APPLICANT SHALL BE RESPONSIBLE FOR THE MAINTENANCE AND REPAIR OF ALL IMPROVEMENTS.
3. THE APPLICANT SHALL MAINTAIN THE PROPOSED IMPROVEMENTS IN ACCORDANCE WITH THE SPECIFICATIONS AND CONDITIONS OF THE SPECIAL USE PERMIT PLAN. THE APPLICANT SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES AND AGENCIES OF STATE AND FEDERAL GOVERNMENT.

**OPTION WITH CURB EXTENSION (B)**

**OPTION WITHOUT CURB EXTENSION (A)**

DATE	BY	DESCRIPTION	APPROVED	DATE	BY

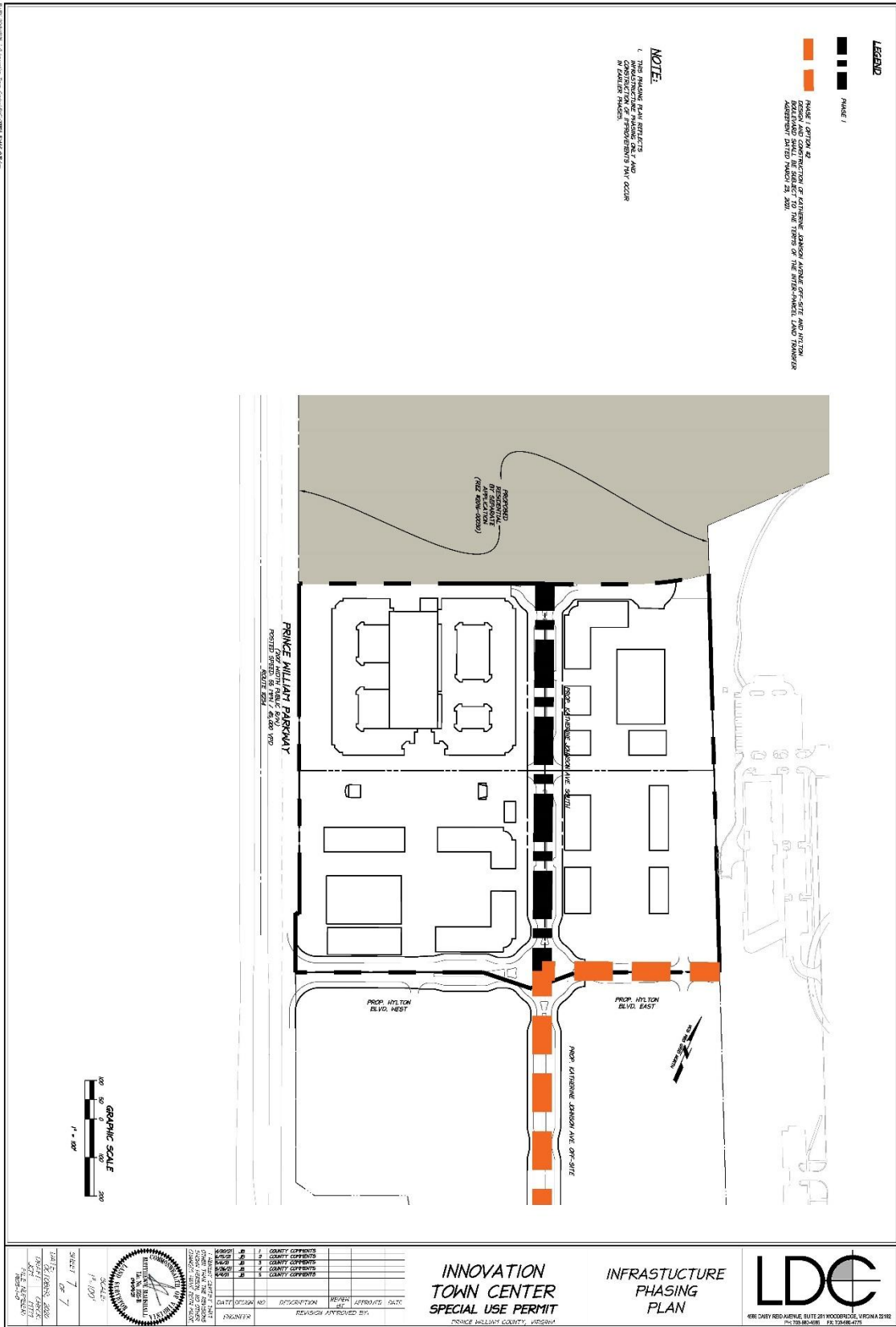


**INNOVATION TOWN CENTER SPECIAL USE PERMIT**  
PRINCE WILLIAM COUNTY, VIRGINIA

STREET SECTIONS



# Special Use Permit Plan



## Waivers and Modifications

**REZ2016-00030**  
**Request for Modification or Waiver of Specific Requirements of the Subdivision Ordinance,**  
**Zoning Ordinance, or DCSM as it Relates to Rezoning Application Pursuant to Sections 32-506.09.1**  
**and 32-700.25 of the Zoning Ordinance**  
**July 26, 2021**

Requested Modification or Waiver	Citation	Applicant's Justification for Request
1. Waiver of the 15-foot wide landscaped area around public use and community recreation sites.	Sections 32-201.18 and DCSM 802.47	Provides flexibility to integrate public uses and community facilities into development design rather than separate out such uses. The Design Guidelines provide for the protection of the view-scape intended by the Code requirement and establish requirements and standards which are tailored to the development.
2. Waiver to allow mobile or modular offices for construction or sales offices to be located on a parcel for which there may not be an approved site plan or building permit.	Sections 32-210.11 and 32-210.12	The dense nature of the development and the Landbay configuration may require that such facilities be located nearby, but not on the specific site which has a final site or subdivision plan where construction has commenced. This is typical of development in more urban areas.
3. Waiver of internal buffers between different uses to be replaced and implemented by the conditions and the Design Guidelines.	Sections 32-250.30, 32-250.31 and 32-250.32; DCSM Sections 802.11 and 802.12	The planning for the mixed-use community and associated Design Guidelines blend the proposed land uses as well as the surrounding uses. This effort provides more appropriate standards consistent with the desired design for this urban area, and the adjacent uses.

## Waivers and Modifications

Requested Modification or Waiver	Citation	Applicant's Justification for Request
<p>4. Modification of the Zoning Ordinance and DCSM requirements for tree canopy and open space to allow calculations based upon the entire Property site instead of each individual site plan.</p>	<p>Section 32-250.40 and DCSM Sections 802.20, 21, 30 and 31</p>	<p>Due to the nature of the overall project and the agreements to not disturb particular areas, and due to the nature of the Town Center, it is not reasonable to require these standards on each site plan. The plan shall provide tree canopy requirements in accordance with the proffers.</p>
<p>5. Modification to permit the following uses on the Property: commercial recreation (outdoor) by-right and drive-through and a motor vehicle fuel sale use on the Property subject to a special use permit as required by the Zoning Ordinance.</p>	<p>Sections 32-280.41.1, 32-405.03.2 and 32-506.04.1</p>	<p>The Town Center will be a robust town center that will offer goods and services to a variety of customers and neighbors. The Applicant would like as much flexibility as possible with proposed uses that may benefit the Town Center customers.</p>
<p>6. Modification of housing types to allow for performance standards as outlined in Exhibit A.</p>	<p>Sections 32-306.12.6.F, 32-306.12.6.G and 32-306.12.6.H</p>	<p>This modification will provide flexibility consistent with urban design guidelines.</p>
<p>7. Waiver to allow the residential use to exceed 35% of the gross land area of the PMD as shown on the Land Use Plan.</p>	<p>Section 32-405.04(7)</p>	<p>The Applicant is proposing a development with a mix of uses both residential and commercial with a town center core. The proposed residential use, with the mix justifies the waiver to allow greater than 35% of gross land area be dedicated to residential to help support the non-residential uses.</p>

## Waivers and Modifications

<b>Requested Modification or Waiver</b>	<b>Citation</b>	<b>Applicant's Justification for Request</b>
<p>8. Modification of the development and setback standards in the Zoning Ordinance to allow for the performance standards as outlined in Exhibit A.</p>	<p>Sections 32-401.14, 32-401.15, 32-403.24, 32-403.25, 32-402.44, 32-402.45, 32-402.14, 32-402.15, 32-402.34, 32-402.35, 32-506.05, 32-506.06, and 32-506.07</p>	<p>This modification will provide flexibility consistent with urban design guidelines.</p>
<p>9. Modification to reduce the perimeter buffer to be as shown on the Open Space/Buffer Plan.</p>	<p>Section 32-405.04(4) and DCSM 802.47.B</p>	<p>The Applicant requests this modification in accordance with Section 32-506.06.1.(c) of the Prince William County Zoning Ordinance. The Design Guidelines provide for the protection of the view-scape intended by the Code requirement and establish requirements and standards which are tailored to the mixed-use development. In addition, the TeOD section provides that a setback is not required in the TM district. The TeOD further states that if there are any conflicts in the Zoning Ordinance section and the TeOD section, the TeOD section prevails. That being said, the TeOD may be eliminated in the future so the Applicant is seeking this modification.</p>
<p>10. Modification requiring a 100 foot setback from all public street rights-of-way classified as interstate/freeway, parkway, and principal arterial and to the requirement for a buffer type C with such setback, as shown on Exhibit A.</p>	<p>Section 32-506.06</p>	<p>Provides flexibility consistent with Urban developments and allows for the "feel" of a compact walkable community.</p>

## Waivers and Modifications

<b>Requested Modification or Waiver</b>	<b>Citation</b>	<b>Applicant's Justification for Request</b>
<p>11. Modification to reduce the required 50' buffer for sides and rears of SFA against Parkway and Principal Arterial to allow for landscaping as depicted on Sheet 4 and Sheet 4A.</p>	<p>DCSM 802.48(B) 32-506.06(1)(A)</p>	<p>This modification is for a few SFA attached units that are also benefiting from the existing grade difference along the Prince William Parkway ROW. The location of the units along with the proposed 25' buffer, a solid fence, a 10' trail, and pockets of 20' landscape areas should provide adequate screening to and from the street.</p>

P1105613.DOCX



**Waivers and Modifications**

**SUP2016-00031**

**Request for Modification or Waiver of Specific Requirements of the Subdivision Ordinance,  
Zoning Ordinance, or DCSM as it Relates to Rezoning Application Pursuant to Section 32-280.51**

**July 26, 2021**

<b>Requested Modification or Waiver</b>	<b>Citation</b>	<b>Applicant's Justification for Request</b>
<p>1. Request to modify or waive various restrictions and limitations on temporary commercial activities to allow for a vibrant Town Center.</p>	<p>Sections 32-210.01(1)(a), (b), (d), (i), (j), 32-210.01(2)(a), (b), (c), (f), 32-210.01(3)(a), (b), (c), (d), 32-210.01(4)(a), (b), 32-210.02(1)(a), 32-210.02(2), 32-210.02(5) and 32-210.02(6)</p>	<p>The waivers and modifications are intended to remove limitations on the number, duration, location and separation of the temporary commercial activities which are expected to help provide the atmosphere needed to achieve and maintain a vibrant Town Center.</p>
<p>2. Waiver request to allow mobile or modular offices for construction or sales offices to be located on a parcel for which there may not be an approved site plan or building permit.</p>	<p>Section 32-210.11 and 32-210.12</p>	<p>The dense nature of the Town Center and the Landbay/Block configuration may require that such facilities be located nearby but not on the specific site which has a final site or subdivision plan where construction has commenced. This is typical of development in more urban areas.</p>
<p>3. Waiver of internal buffers between different uses to be replaced and implemented by the conditions and the Design Guidelines.</p>	<p>Sections 32-250.30, 32-250.31 and 32.250.32; DCSM Sections 802.11 and 802.12</p>	<p>The planning for the mixed-use community and associated Design Guidelines blend the proposed land uses as well as the surrounding uses. This effort provides more appropriate standards consistent with the desired design for this urban area, and the adjacent uses.</p>

## Waivers and Modifications

Requested Modification or Waiver	Citation	Applicant's Justification for Request
4. Modification of the Zoning Ordinance and DCSM requirements for tree canopy and open space to allow calculations based upon the entire Town Center site instead of each individual site plan.	Sections 32-250.40 and DCSM Sections 802.20, 21, 30 and 31	Due to the nature of the overall project and the agreements to not disturb particular areas, and due to the nature of the Town Center, it is not reasonable to require these standards on each site plan. The plan shall provide tree canopy requirements in accordance with the proffers.
5. Modification to allow multiple and/or tiered owner associations.	Sec. 32-280.31.7(a)	
6. Modification to permit the following uses on the Property: commercial recreation (outdoor) by-right and drive-through and a motor vehicle fuel sale use on the Property subject to a special use permit as required by the Zoning Ordinance.	Sections 32-280.41.1, 32-405.03.2 and 32-506.04.1	The Town Center will be a robust town center that will offer goods and services to a variety of customers and neighbors. The Applicant would like as much flexibility as possible with proposed uses that may benefit the Town Center customers.
7. Modification of the Town Center minimum size of 40 acres.	Section 32-280.31.1	The applicant is proposing the Town Center Area in accordance with the Innovation Small Area Plan which depicts only a portion of the recommended Town Center within the site.
8. Modification to allow parking as a principal use on a subdivided lot(s). This modification shall also allow temporary surface parking as provided in the Town Center Conditions.	Section 32-280.31	This would allow for the provision of parking structures to service multiple users on a separate subdivided lot thereby providing more efficient use of parking and resources to provide parking. This modification would allow efficient use of land, limit need to provide structured parking until development requires structured parking.
9. Modification to allow residential units on the first floor.	Sec. 32-280.41.3	This will allow flexibility in the mix of uses. The Applicant would like as much flexibility as possible with proposed unit types.

## Waivers and Modifications

Requested Modification or Waiver	Citation	Applicant's Justification for Request
10. A waiver of the required multifamily or mid- to high-rise buildings to provide landscaped courtyards or plazas.	Section 32-280.51(2)(d)	Provides flexibility to arrange multifamily units in a more urban fashion and to combine and coordinate amenities such as courtyards and plaza areas throughout the Town Center.
11. Modification to reduce the perimeter buffer to be as shown on the Open Space/Buffer Plan.	Section 32-405.04(4) and DCSM 802.47.B	The Applicant requests this modification in accordance with Section 32-506.06.1.(c)of the Prince William County Zoning Ordinance. The Design Guidelines provide for the protection of the view-scape intended by the Code requirement and establish requirements and standards which are tailored to the mixed-use development. In addition, the TeOD section provides that a setback is not required in the TM district. The TeOD further states that if there are any conflicts in the Zoning Ordinance section and the TeOD section, the TeOD section prevails. That being said, the TeOD may be eliminated in the future so the Applicant is seeking this modification.
12. Modification requiring a 100 foot setback from all public street rights-of-way classified as interstate/freeway, parkway, and principal arterial and to the requirement for a buffer type C with such setback.	Section 32-506.06	Provides flexibility consistent with Urban developments and allows for the "feel" of a compact walkable community.

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# PREFACE

The draft Innovation Small Area Plan calls for developing an Innovation Town Center adjacent to the George Mason University (GMU) Science and Technology campus. Innovation Town Center is a 107 acre mixed-use development which will compliment the adjacent 23 acres University Village project to create the long envisioned University Town Center.

Designed to be a high quality community asset offering a humanly satisfying environment, Innovation Town Center responds to the "placemaking" need currently lacking at Innovation Park.

The design guidelines outlined in this document define the qualities of architecture, site design, and open space and serve as a tool for guiding the development towards successful design outcomes.

## PROJECT TEAM

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Chantilly, VA 20151

SEPTEMBER 9, 2021

"dream, create, inspire"



PRINCE WILLIAM COUNTY, VA

DESIGN GUIDELINES

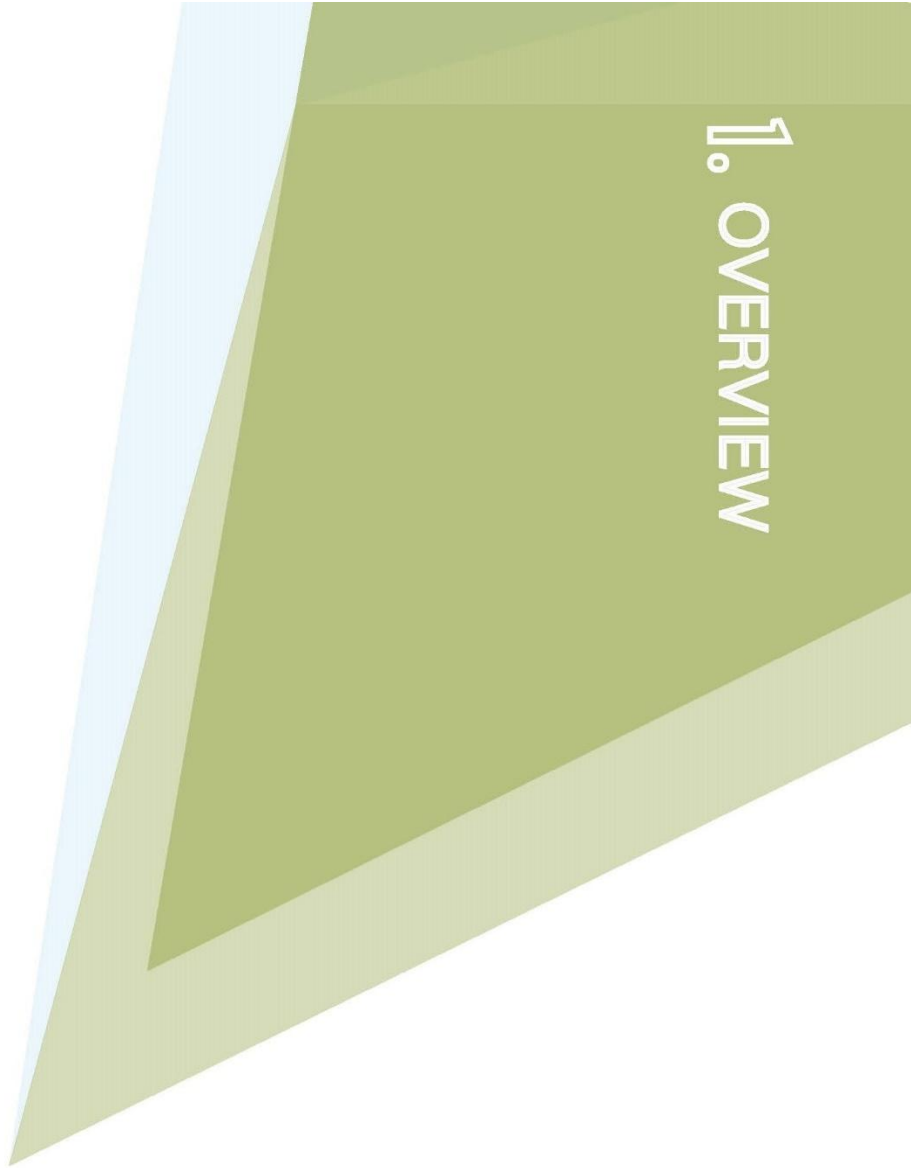
STANLEY MARTIN  
Your Life is our Blueprint



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NOTE:  
This document was prepared to describe the design approach for this community and is subject to final engineering.





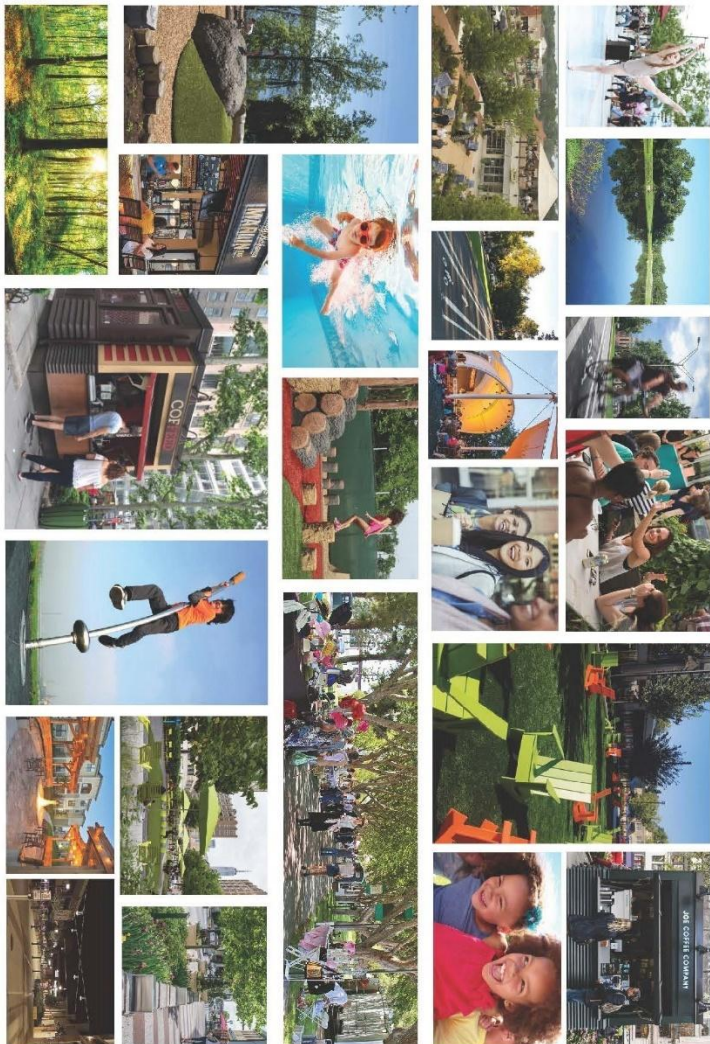
# 1. OVERVIEW

## Project Summary

Located just west of George Mason University Science and Technology Campus, Innovation Town Center is a mixed-use urban enclave - which will integrate functions of work, entertainment and living. Designed as a walkable community with a broad range of housing options combined with office, retail, and recreational areas, Innovation Town Center seeks to create a vibrant neighborhood with a strong sense of place.

The design concepts behind Innovation Town Center follows the recommendations of the Innovation Park Small Area Plan adopted by the Prince William County Board of Supervisors on December 15, 2020. Innovation Town Center incorporates traditional community planning and place making design principles to ensure that the details for street, buildings, parks, and public spaces all work in concert to create a development that produces synergies in the use of the land and fosters positive community spirit.

This document shall work as a tool to help guide the design and development for the entire Innovation Town Center with the goal of fulfilling its vision of creating a long lasting memorable place.





# 1. OVERVIEW

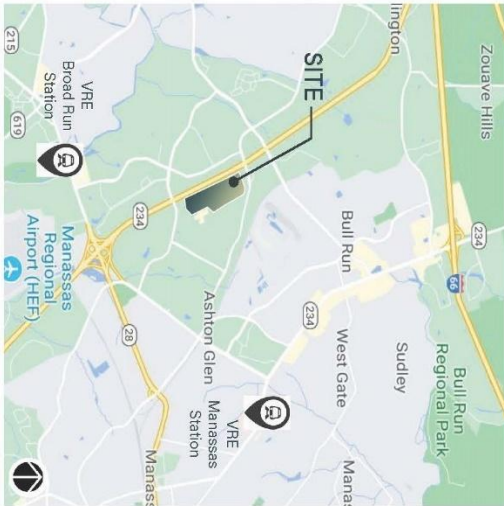
## Site Setting

### LOCATION

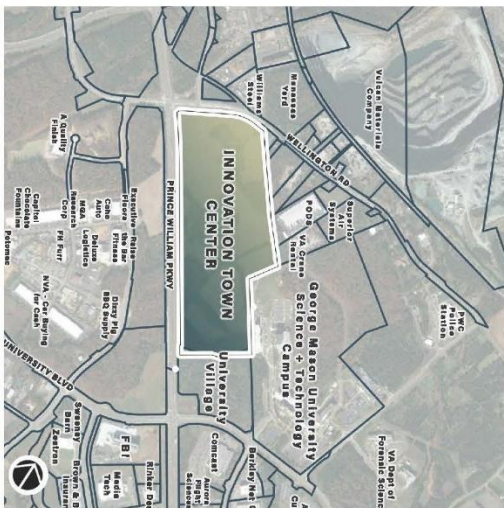
Innovation Town Center is strategically located next to George Mason University's Science and Technology Campus and Hylton Performance Art Center with excellent access to major transportation routes which can provide connectivity to the Manassas Regional Airport and Dulles International Airport. The site also benefits from additional multi-modal connections such as the VRE Manassas Station, as well as the Broad Run Station which provides commuter rail connectivity to the DC metropolitan area's major employment centers. Innovation town Center's prime location presents a unique opportunity to leverage the incredible assets that the SciTech Campus can provide, and serve as the premier catalyst for development.

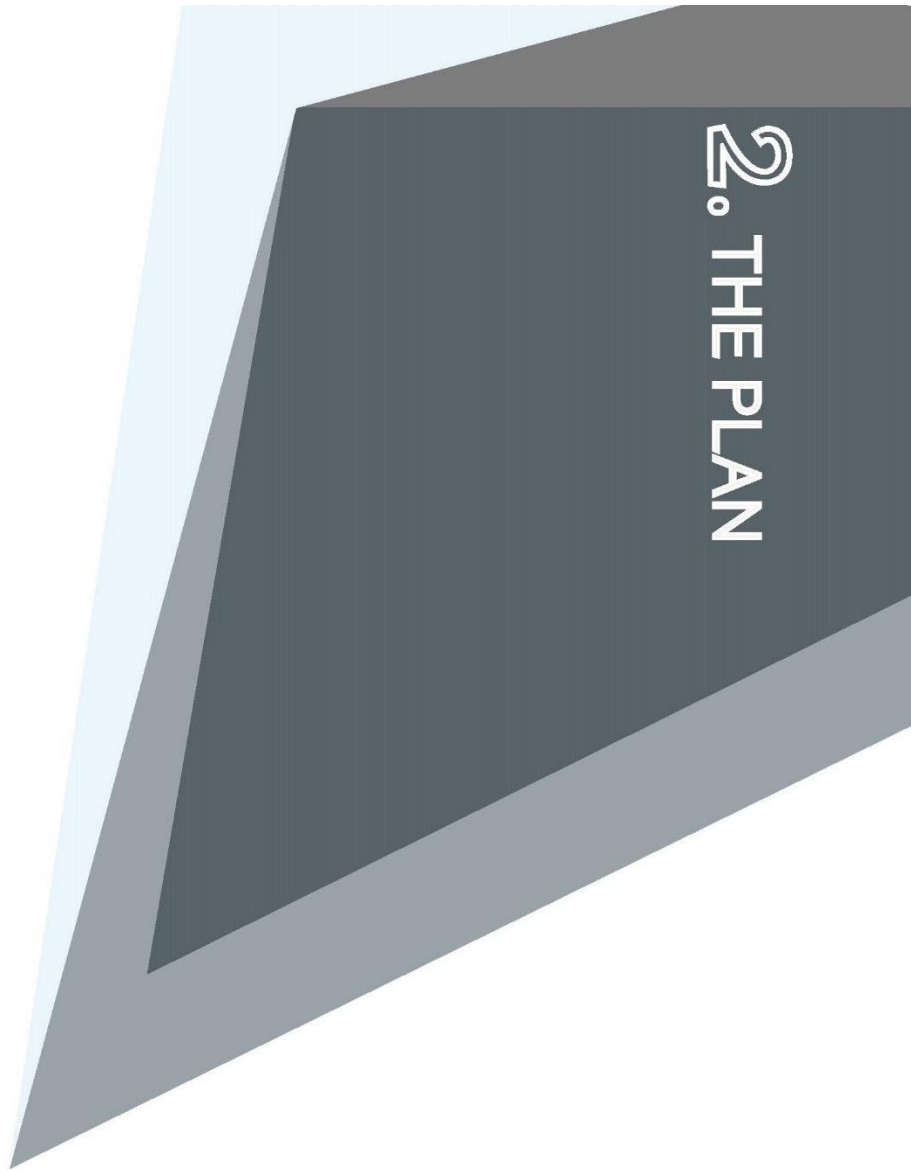
The area is surrounded by companies and government agencies conducting cutting-edge research and also includes an educated workforce that will attract a diverse number of industries which will promote economic growth. In addition, the integration of land uses will help bridge the gap between the current industrial/office park to a mixed use community themed to be set for the future that will provide a much needed mix of uses in the area that will allow for the surrounding businesses to have a place to both live and play.

### REGIONAL



### LOCAL





## 2. THE PLAN

### Illustrative Plan

The development program at Innovation Town Center features a mix of uses to include a variety of housing types. The Town Center area proposes mid-rise multi-family units adjacent to office, retail and a public space set to be the heart of the urban core. Just north of the Town Center, the development proposes multi-family stacked townhouse units. The remaining residential component is composed of a variety of rear load townhouses as well as front loaded townhouse units. All of these different types of housing options will appeal to a broad range of residents creating a lively and diverse community surrounded by a network of recreational opportunities.

Over 400,000 square feet of non-residential uses are being proposed which will include a mix of retail, office and flex-tech uses, among others, creating opportunities to live, work and play at Innovation.

*Live your dreams,  
create your future, and  
inspire others to do the same.*



NOTE: FOR ILLUSTRATIVE PURPOSES ONLY

**INNOVATION**  
DESIGN GUIDELINES

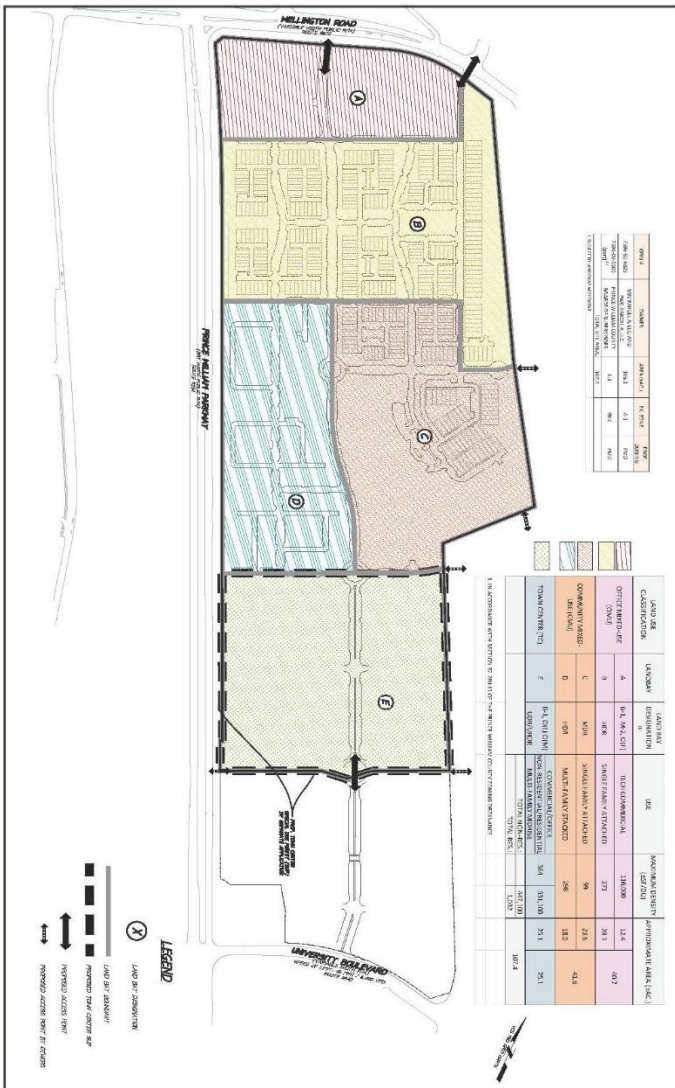


# 2. THE PLAN

## Land Use Plan

The residential landbays are centrally located between the flex-tech and the Town Center landbays. The residential landbay designations range between Medium Density Residential (MDR), adjacent to the natural area, to Urban High Density Residential (UHDR) within the Town Center.

The flex-tech area (Landbay A) may include B-1, M-2, and O(F) uses. The Town Center is the hub of the community with a broad range of B-1, O(L), and O(M) uses which will offer an array of neighborhood serving commercial uses as well as office space conveniently located at a walking distance or a short bike ride from residents.

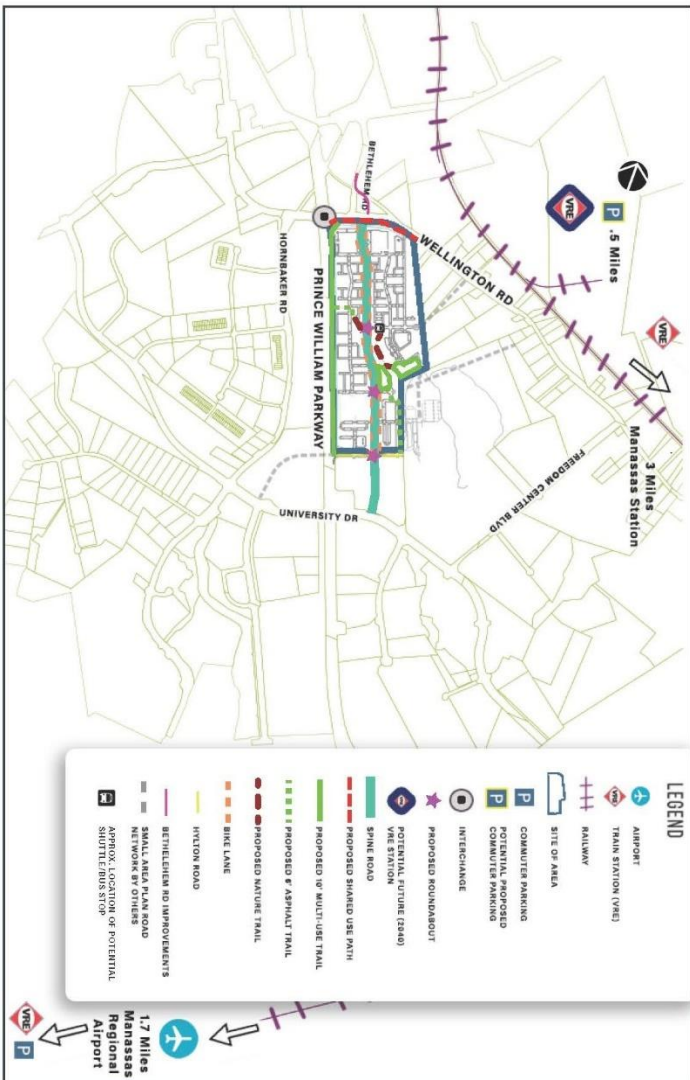


# 2. THE PLAN

## Contextual Connectivity

Existing land use within the vicinity of Innovation Town Center can be broadly categorized as low-density industrial mixed with undeveloped parcels, and some residential subdivisions and auto-oriented uses. The Innovation Park Small Area Plan seeks to implement a multi-modal transportation network that capitalizes on the George Mason University activity center and the potential VRE extension.

Innovation Town Center's geographic location offers an opportunity to weave its proposed extensive grid-like road network not only into the existing road system but also into the County's planned street grid. This creates a viable multi-modal system that will serve residents and visitors and will strengthen community connections.



# 2. THE PLAN

## Street Network & Connectivity

Innovation Town Center's interconnected street grid provides for dispersion of traffic and multiple access points for emergency vehicles. Alleys provide a place for utilities and parking, limit garage views, and promote safer, more social front yards. Local streets encourage walking by providing sidewalks and bike lane connections and by using traffic calming techniques, such as narrow streets and tighter curves.

### ORGANIZATIONAL FRAMEWORK

The community is organized by a spine road from which most of the employment, commercial, residential and recreational opportunities such as parks and playgrounds can be accessed by pedestrians, bicyclists, and vehicles.

### WALKABILITY & BICYCLING

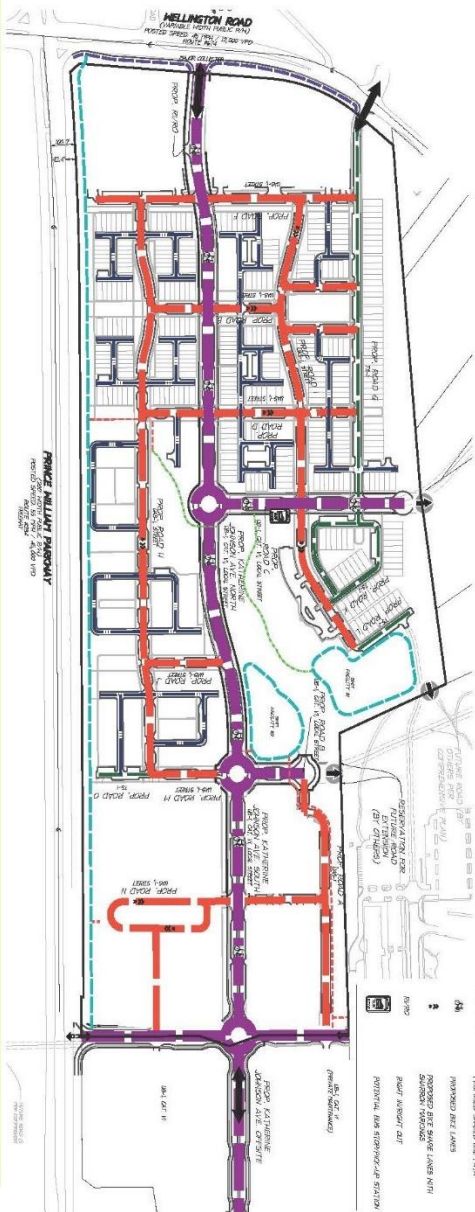
One of Innovation's greatest amenities is the extensive network of pedestrian and bicyclist linkages. Pedestrian facilities have been carefully designed to encourage non-motorized travel. Standard and high visibility crosswalks are proposed all across the community, featuring appropriate signage warning motorists to yield to pedestrians and bicyclists.



### TRAFFIC CALMING MEASURES

The Vision Zero Network is a collaborative campaign helping communities reach their goals of eliminating all traffic fatalities and severe injuries while increasing safe, healthy, equitable mobility for all. Innovation Town Center supports Vision Zero's goals by implementing traffic calming measures to reduce vehicle speeds and improve quality of the pedestrian environment, while conforming to required engineering standards. Typical traffic calming measures shall include:

- Narrow travel lanes
- Sidewalk bulbouts
- Traffic circles
- Accent paving
- Small curb radii





# 2. THE PLAN

## Street Sections

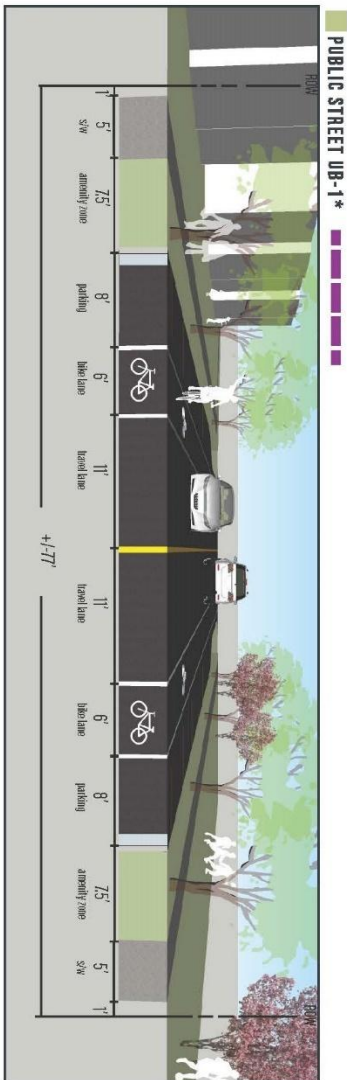
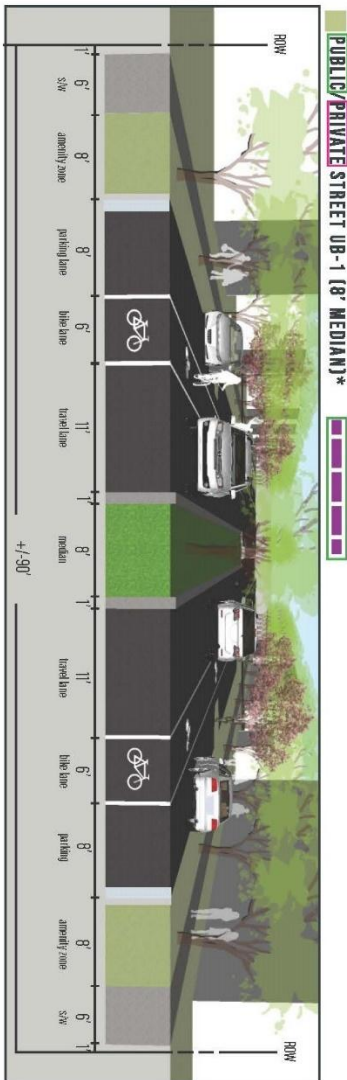
KEY PLAN



**NOTE:**  
All lane widths and improvements may vary subject to final engineering and are depicted herein for illustrative purposes only.

\* The development shall utilize the UB-1 standards as follows:

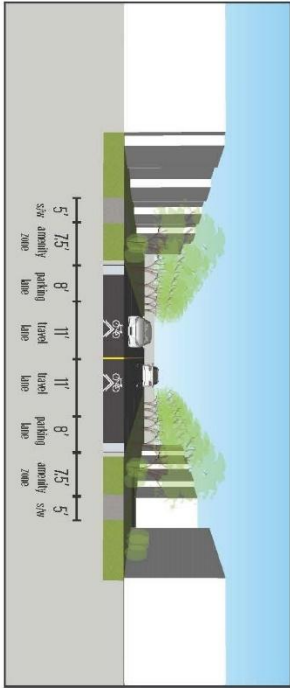
- Land Bay A segment: Modified UB-1 due to lane configurations for turn lanes and transitions from its intersection with Wellington Road.
- Land Bay B, D segment: Standard UB-1
- Land Bay E segment (Town Center): Standard UB-1 with an 8' median, 8' tree pit/amenity panels measured from the face of curb to the sidewalk, 6' sidewalks. Road B and C: Standard UB-1



# 2. THE PLAN

## Street Sections

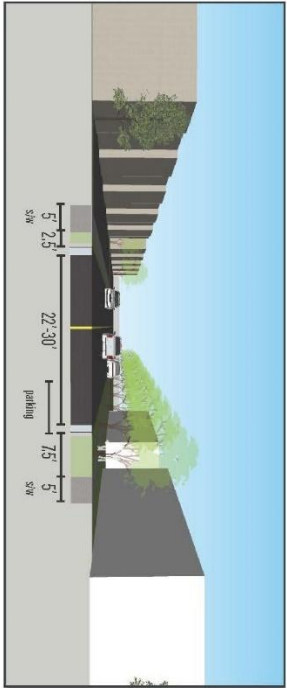
PRIVATE TRAVELWAY UAS-1\*



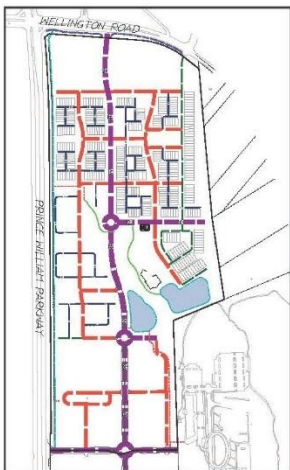
**NOTE:**  
All lane widths and improvements may vary subject to final engineering and are depicted herein for illustrative purposes only.

\*The UAS-1 Standard for the Town Center shall provide 8' tree pit/amenity panels measured from the face of curb to the sidewalk, and 6' sidewalks.

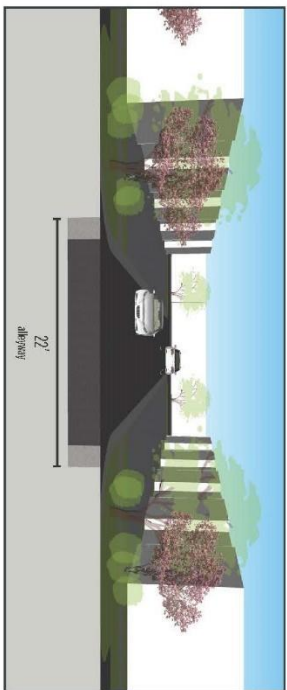
PRIVATE TRAVELWAY TS-1



KEY PLAN



PRIVATE ALLEY UA-1



# 2. THE PLAN

## Open Space & Amenities

### OVERALL AMENITY PLAN

The design concept of Innovation Town Center incorporates a well composed mix of outdoor spaces woven together through a network of pedestrian and vehicular corridors. Many green spaces serve to bring people together and create community. These spaces, which vary in scale from a large nature park & trail to small neighborhood greens or pocket parks, provide a venue for the types of activities essential for a thriving and interactive community.

Potential recreation areas include:

- \*1. Community Gathering Area & Innovation Playground (± 36,000 SF)
- \*2. Neighborhood Promenade (± 29,200 SF)
- 3. Pocket Park 1 (± 3,500 SF)
- 4. Pocket Park 2 (± 15,800 SF)
- \*5. Club House and Community Pool (± 155,200 SF)
- 6. Nature Playground (± 52,800 SF)
- \*7. Nature Park (± 18,700 SF)
- 8. Courtyard Green (± 13,200 SF)
- 9. Linear Courtyard (± 12,600)
- 10. Pocket Park 3 (± 8,800 SF)
- 11. Pavilion Green & Urban Park (± 29,200 SF)
- 12. Bike Path Area (± 7,000 SF)
- \*13. Urban Promenade (± 20,500 SF)
- \*14. Gateway Plaza (± 25,600 SF)
- 15. Stream Preservation Area with Trail (± 193,000 SF)

Total Area: (± 621,100 SF)\* \* Denotes major recreation areas

### INNOVATION DESIGN GUIDELINES



\*These areas are approximate and subject to change at site plan.



# 2. THE PLAN

## Open Space & Amenities

TOWN CENTER



**13 URBAN PROMENADE**  
The Urban Promenade provides an ample attractive path connecting the development to the adjacent University Village project providing a strong visual and pedestrian connection between the two activity nodes.



**14 GATEWAY PLAZA**  
The Gateway Plaza is an urban plaza located along Hyton Boulevard providing ample and inviting sitting areas to support the commercial uses. It will feature some small retail kiosks, public art, and different types of seating areas. During special events the adjacent streets will be closed to vehicular traffic allowing vendors to setup along the parallel parking areas.



**12 BIKE REST AREA**  
The Bike Rest Area will feature a fix-it station, water station, bike racks and a seating area. This area creates an opportunity to implement a bike rental system within the community.



**11 PAVILION GREEN**  
The Pavilion Green will include movable lounging chairs, a lawn area, and a pavilion for concerts and movies.



**URBAN PARK**  
The Urban Park features a series of pocket park spaces that function as urban living rooms with coffee tables and lounging chairs.

# 2. THE PLAN

## Open Space & Amenities

RESIDENTIAL

### 1 COMMUNITY GATHERING AREA

The Community Gathering area is a space that brings people together. It will feature a central fire pit, a shade structure, and movable lounging chairs for residents to congregate and socialize. The space will also include an area with attractive ping pong tables, specialty paving, seating and enhanced landscaping.

### 1 INNOVATION PLAYGROUND

The Innovation Playground sits adjacent to the community gathering area and offers a unique playful area for multi-age children to learn while they have fun. Playground structures will promote education in the form of movement, music, mathematics and science. Innovation Playground leverages from its location next to the George Mason University Science and Technology Campus to shape children's curiosity and appetite for knowledge.

### 5 CLUB HOUSE AND COMMUNITY POOL

The Club House will feature a lap pool as well as a small hot tub with lounging chairs, a trolis and enhanced landscape areas.

### 5 NATURE PLAYGROUND

This Nature Playground is intended to bring a fun and active space which will blend into the existing natural area. The goal is to create a play environment that consists of elements and textures of earth which will include tree stumps, boulders, and tree logs, among others, allowing children to explore, run, climb, crawl, jump and connect with nature.

### 2 NEIGHBORHOOD PROMENADE

The Neighborhood Promenade is a passive recreation green corridor with lawn areas and benches for residents to stroll, rest, and people watch. It encourages people to walk to the community gathering area and Innovation playground as it offers an east-west connection with a ample open space and plenty of pedestrian connections.

### 7 NATURE PARK

Embedded within the site's environmental corridor, this Natural Park includes a nature trail that weaves within the natural open space with potential fitness equipment and a gazebo overlooking the wet ponds.



**INNOVATION**  
DESIGN GUIDELINES



# 2. THE PLAN

## Landscape Scheme Overview

RESIDENTIAL

A variety of ornamental and deciduous trees, native grasses, perennials, groundcovers, and seasonal annuals shall be placed mindfully in proposed landscape beds as well as selected planters in order to define public open space. Landscape shall be planned with the intention of softening the visual impact of large expanses of hardscape, to promote the goals of providing cleaner air, reduce stormwater runoff, reduce the Urban Heat Island effect, and utilize planting strips as storm water features where feasible and appropriate.

### GENERAL LANDSCAPE NOTES

1. Avoid landscaping, which obstructs natural surveillance and creates hiding spaces. Keep shrubbery under 3 feet in height for visibility where appropriate.
2. The selection and spacing of trees and shrubs must be taken into consideration to avoid and minimize conflicts with the landscaping, building envelope and pedestrian corridor. The placement of the plant material will be dependent on the mature size of the tree or shrub. Shrubs will be planted at a minimum distance of half the mature width from the building. Shrubs will not be allowed to grow into the pedestrian walkways. Compact trees will be planted at a minimum distance of 6' from the building. Dependent on the species and its growth habits, this distance may be greater.
3. Pruning maybe done to provide additional clearance and to promote correct structural form. Proper plant spacing from buildings will allow for adequate airflow and ventilation.
4. Street trees with dense low growth foliage should be spaced or their crowns should be raised to avoid a continuous visual barrier.
5. Invasive plant species will not be used. In the design of landscapes for this development. Plant species not acceptable for use are listed in Table I-3, Section 800 of the DCSM.



### LID NOTES

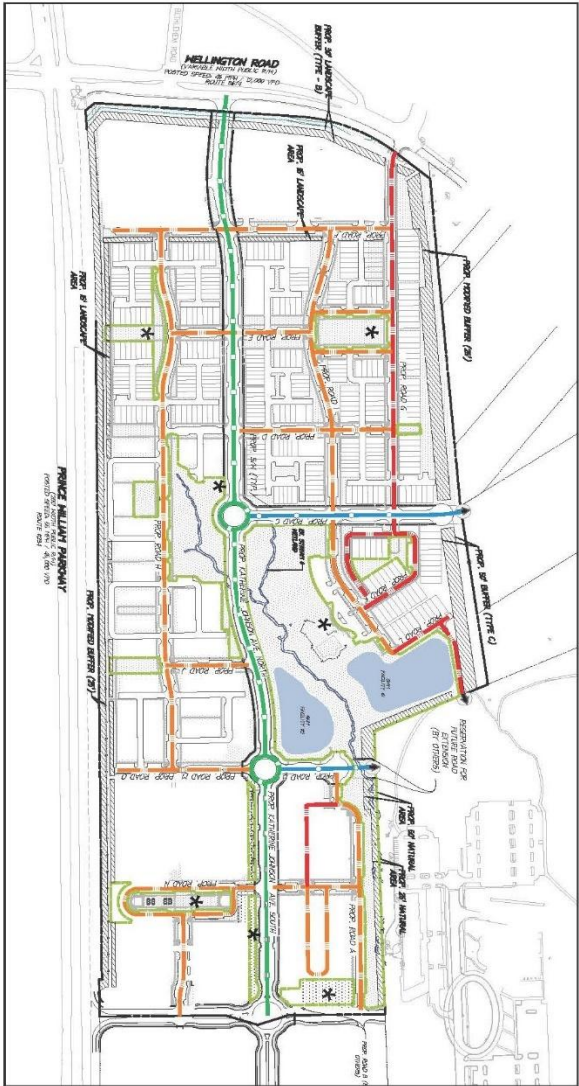
1. Low Impact Development (LID) stormwater management will be incorporated into the overall community design as feasible; possibly and may include but not limited to bio-retention facilities, rain gardens, and permeable pavements.
2. LID maybe utilized community open space, parking lot landscape areas, and amenity areas as possible locations.
3. Unique and innovative design of these facilities is encouraged.
4. Location and design of LID facilities are subject to final engineering and site plan approval.





# 2. THE PLAN

## Street Tree Hierarchy



**OVERVIEW**

Street trees should contribute to the overall unity of the streetscape, through their layout, scale, and character. Careful selection of the tree species will provide scale and visual cohesion to the street.

Deciduous street trees may be provided along all roadways and medians within Innovation Town Center in order to provide shade, as well as to help define the streetscapes.

The species (or cultivar) of tree selected for both along roadways or within medians shall be compatible with the width and function of the street and shall be in accordance with Table I-2, Section 800 of the DCSM.

All street trees along the road shall be single-stemmed unless selected sizes will not conflict with the pedestrian and vehicle movement and at the discretion for the director of Public Works.

All street tree locations may be permitted within the right-of-way if applicable with the DCSM section 802.46B(3).

**LEGEND**

- ROADWAY INTERSECTION CONNECTION
- RAMP OR LANDSCAPE AREA
- DRIVE SHADE
- ADDITIONAL AREA
- PERIODIC RESERVATION FACILITIES
- EXISTING RETAINERS AND FENCES

**STREET TREE HIERARCHY**

- TREE TYPE A (LARGE)
- TREE TYPE B (MEDIUM)
- TREE TYPE C (SMALL)
- TREE TYPE D (SHRUB)

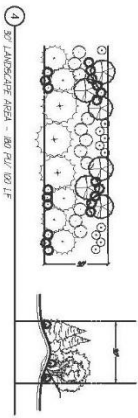
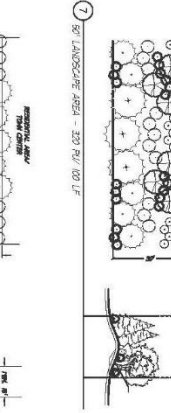
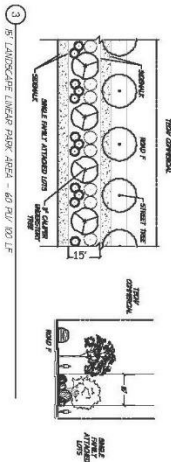
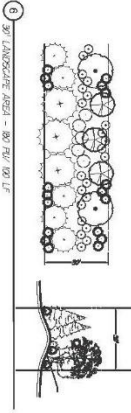
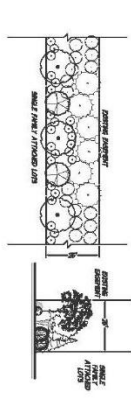
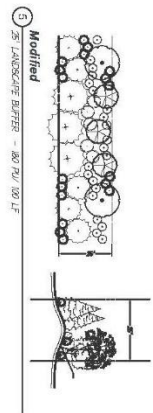
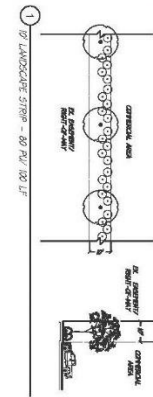
# 2. THE PLAN

## Buffers

### BUFFER DETAILS

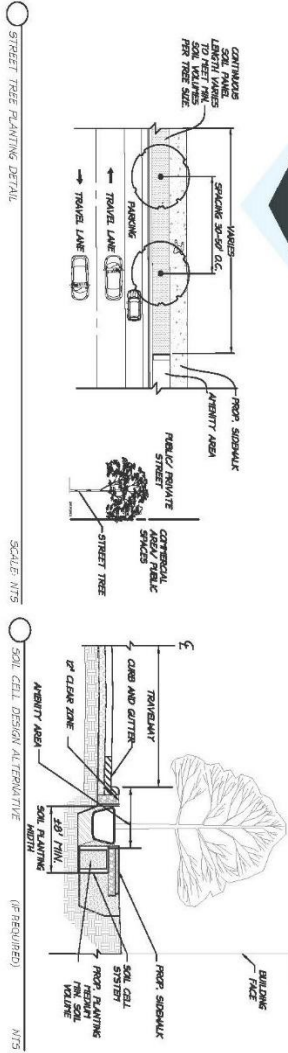
#### BUFFERS

Buffer areas have been provided throughout the community and are intended to provide adequate separation and screening from adjoining uses and to enhance the visual image of Innovation Town Center and promote public health, welfare and safety while still maintaining a compact mixed-use walkable environment.



# 2. THE PLAN

## Tree Pits & Tree Standards



### TREE PITS/AMENITY AREA

Street tree planting shall be in conformance with detail 650.19 of the Design and Construction Standards Manual (DCSM). Tree pits shall provide adequate soil volumes per Table B-8 of the DCSM. In the case where the streetscape design interrupts a continuous soil panel and dimensions to meet the minimum soil volume requirement, an alternate, acceptable design to support the root zone shall be implemented. Amenity panels will include a mixture of trees, shrubs and native ground cover.

### PUBLIC RIGHT OF WAY

Street trees within the public right-of-way to be spaced between 30-50 ft on center. Spacing to be determined upon soil volume calculations and in conformance with DCSM standards for the county. Soils shall meet VDOT requirements and specifications - clear of concrete, debris, gravel, or other foreign materials and shall be loose soils, lightly tamped but uncompacted.

### VERTICAL CLEARANCE

Choosing a tree species that has an appropriate canopy height and growth pattern is a priority. Careful selection prevents obstructing pedestrian clearances. A minimum vertical clearance of 7' is required above sidewalks and 14' above roadways.

### LOCATION AND SPACING

Street tree locations will depend on the size of the street. A spacing shall range from 30' to 50' on center will be required. Tree spacing provides a continuous canopy and acts as a safety green buffer between pedestrians on the sidewalk and the adjacent roadway.










All street tree locations may be permitted within the right-of-way if applicable with the DCSM section 802.46b(3).



### STREET TYPES AND SIZES

Street tree sizes shall follow street type per Detail 650.19, Section 600 of the DCSM. Variations from the street type and size tables at the time of site plan are subject to approval by the Prince William County Department of Transportation and Department of Public Works.

### THREE SPECIES AND CULTVAR SUGGESTIONS:

THREE SPECIES AND CULTVAR SUGGESTIONS:	THREE SPECIES AND CULTVAR SUGGESTIONS:	THREE SPECIES AND CULTVAR SUGGESTIONS:
THREE SPECIES AND CULTVAR SUGGESTIONS:	THREE SPECIES AND CULTVAR SUGGESTIONS:	THREE SPECIES AND CULTVAR SUGGESTIONS:
<b>LARGE TREES</b>	<b>MEDIUM TREES</b>	<b>SMALL TREES</b>
 Londonplane	 Lacebark Elm	 Crape Myrtle
 Red Maple	 Katsura Tree	 Kousa Dogwood
 Thornless Honeylocust	 Kwanzan Cherry	 Thornless Cockspur Hawthorn

NOTE: The Applicant reserves the right to vary street tree sizes from medium to large on avenue streets subject to County approval so long as the minimum soil volumes are met. The Applicant reserves the right to adjust street tree locations outside of the right-of-way if required by final engineering. All changes and alternative designs are subject to approval of the County Arborist and final engineering.



## 2. THE PLAN

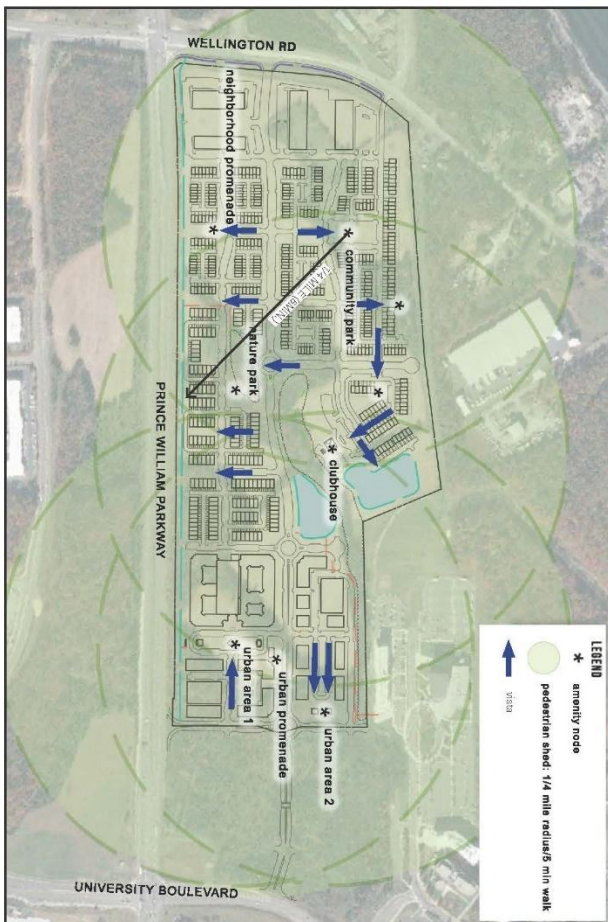
### Vistas & Pedestrian Sheds

#### VISTAS

The street and pedestrian networks at Innovation Town Center have been carefully designed to include terminating vistas in the form of parks, plazas or other public amenity. This is an important method of adding aesthetic appeal and interest to the different corridors within the community.

#### PEDESTRIAN SHEDS

Pedestrians will enjoy sidewalks on both sides of the streets based upon anticipated demand. Recreational and social gathering spaces have been strategically located to serve residents at a walking distance. The main amenity nodes of the site area are located within a typical pedestrian shed (1/4 mile or 5 minute walk) to most dwelling units within the community.



# 2. THE PLAN

## Community Identity & Signage

### COMMUNITY IDENTITY

A good community brand at Innovation Town Center is built over time and capitalizes on the imagery, feeling, and allegiance felt when people see the image of their community whether it is a symbol that evokes a feeling, a tagline that builds pride of place, or colors and typefaces that evoke images or signals sense of place for the community.

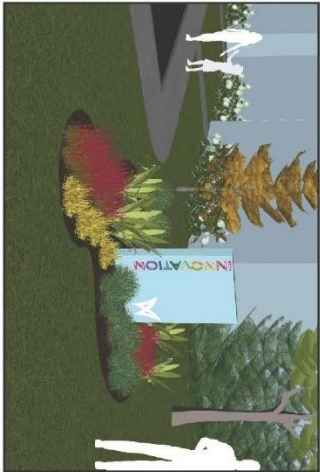


All signage throughout the development, from street name signs to directional signage, shall be uniformed to the project's identity.

ENTRY SIGN #1



ENTRY SIGN #2



*"dream, create, inspire"*



*The rainbow is a perfect harmonic combination which produces a symbol of initiation, transformation, and endless possibilities.*

### SIGNAGE

Innovation Town Center will feature a monument identity sign to welcome residents and visitors. The primary objective of the entry sign is to promote a sense of place and celebrate what is unique about the community.

Entry signage shall incorporate lighting and be visible from the street without encroaching into the public right-of-way. Secondary signs are intended to serve as subtle entry monuments to help define the community and to provide a warm sense of place.

A hierarchy of monument signs has been developed to correspond to the hierarchy of entrance points. Substantial ground mounted signs denote a key entrance while secondary entrances are marked with signs that are smaller but of a consistent design type. The plan shows probable locations for these signs which are subject to change due to final design and engineering.

Entry signs are encouraged to provide enhanced landscaping. Flexibility is allowed if the entry monument is located within an urban plaza area as shown in the left image.



POTENTIAL LOCATIONS OF ENTRY SIGNS

### INNOVATION DESIGN GUIDELINES



# 2. THE PLAN

## Monument Signage Landscape Palette

ENTRY MONUMENT SIGN EXAMPLE PLANT PALETTE:

**VIBRANT PLANTS & PERFECT PLACEMENT**

The plant palette will be vibrant and eye-catching - consisting of specimen trees, sub-canopy (shrubs), perennials, ornamental grasses and groundcovers. Natives are strongly encouraged, but county approved plants may also be used. Beyond creating structural hierarchy of plant material, the design must ensure it is a product of careful planning.

Blue fescue

Carex

Baptisia Cherries Jubilee

Smokeshrub

Blood grass

Deutzia gracilis Duncan

SECONDARY MONUMENT SIGN EXAMPLE PLANT PALETTE:

Witch hazel

False Indigo

Blood grass

Carex

For example, when selecting the palette for the entry monument sign, bloom durations should be taken into consideration to ensure continuous visual interest and fullfill color scheme coordination. Beyond blooms, fruiting durations should also be acknowledged as many shrubs and ornamental trees produce berries in the fall and winter months when perennials are in their dormant stage. Further, plant material with blooms and berries help establish a harmonious ecosystem for pollinators and wildlife which then fosters a resilient landscape.

**ENTRY SIGNS LANDSCAPING**

dwarf mondo grass

blue fescue

coneflower

Japanese bloodgrass

juniper

rosemary

carex

yarrow



## 2. THE PLAN Interpretive Plan

### PRESERVING HISTORY

An interpretive history plan for the Nature Trail and the Town Center shall be prepared in consultation with County Archaeologist and/or Historical Commission.

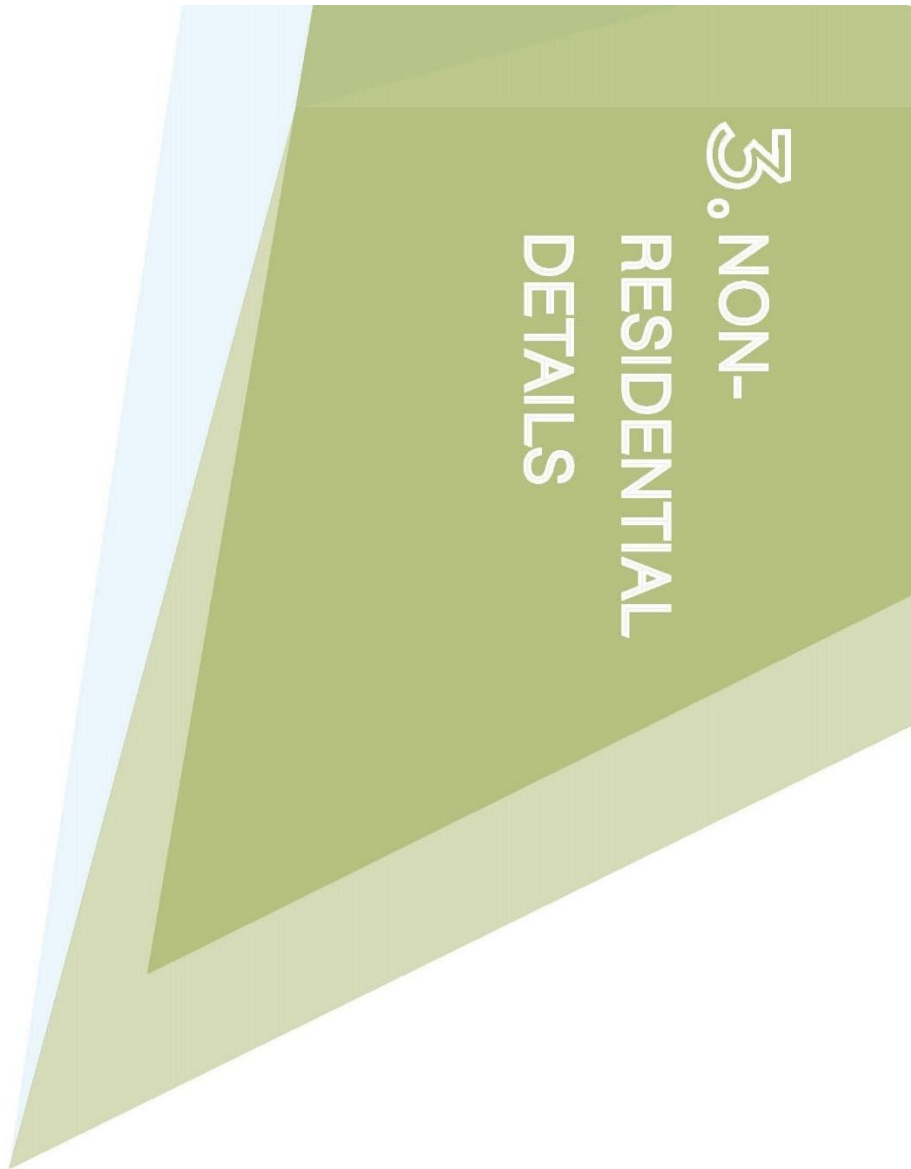
The interpretive history plan shall consist of interpretive signs which describe Twentieth Century farming in Prince William County, Dairy Farming in Prince William County, or sharecropping and pre-contact (Native American) history.

The design of the signs shall fit and complement its surroundings and be creative, durable, and attractive.



The interpretive signage leads users of the space on a journey of discovery about the region, its heritage, history, and its environmental significance.





# 3.1 NON-RESIDENTIAL DETAILS

## Town Center Overview

TOWN CENTER

Innovation Town Center envisions an array of mixed-uses coexisting together adjacent to a thriving central public space. Office with first floor retail as well as freestanding retail structures will dominate the area. The proposed retail kiosk areas will offer leasing opportunities to a diverse group of vendors and help bring synergy to the central square.

Mid-rise multi-family residential units will help support commercial uses and provide housing opportunities for the George Mason University (GMU) Campus.

Pedestrian and bicycle connections are paramount to the design to ensure the most adequate circulation patterns that include not only internal connections but also pathways to GMU and the University Village project.



FOR ILLUSTRATIVE PURPOSES ONLY AND SUBJECT TO CHANGE WITH FINAL DESIGN AND ENGINEERING.

Legend	
<span style="color: red;">■</span>	Residential/Non-Residential
<span style="color: blue;">■</span>	Office/Commercial
<span style="color: yellow;">■</span>	Residential
<span style="color: orange;">■</span>	Non-Residential/Mixed-Use
<span style="color: purple;">■</span>	Retail Kiosk
<span style="color: grey;">■</span>	Parking Structure



# 3.1 NON-RESIDENTIAL DETAILS

## Town Center Amenity Details

TOWN CENTER

### AMENITY AREAS

The Town Center proposes several amenity areas that provide opportunities for social interactions such as, but not limited to:

- Pavilion for entertainment;
- Flex-community lawn space;
- Trail;
- Bicycle rest area;
- Lounging style seating areas;
- Bistro style seating areas;
- Movable and fixed seating;
- Retail kiosk with plaza area and seating
- Public art; and
- Fire feature

### PLACEMAKING ELEMENTS

The amenities should provide a placemaking formula to include, but not limited to:

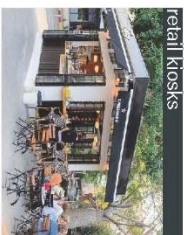
- Proper physical form & human scale;
- Proper mix of land uses and functions;
- Proper mix of social opportunity; and
- Programmed special events

They should include elements such as, but limited to:

- Quality public spaces;
- Attractive urban streetscape;
- Arts, culture and creativity;
- Green Spaces, and
- Recreation

### AMENITY LANDSCAPING

Landscaping throughout the Town Center amenities shall provide year-round seasonal interest, as well as help to define and frame community areas. A mix of evergreen and deciduous, predominately native and drought resistant plant material will be used to promote a sustainable landscape. Thoughtfully selected planters, benches, lighting poles with hanging planters and tree lined streets will visually enhance connections between spaces and provide unifying design elements.



retail kiosks



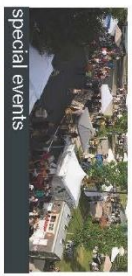
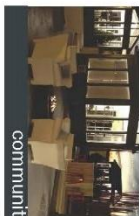
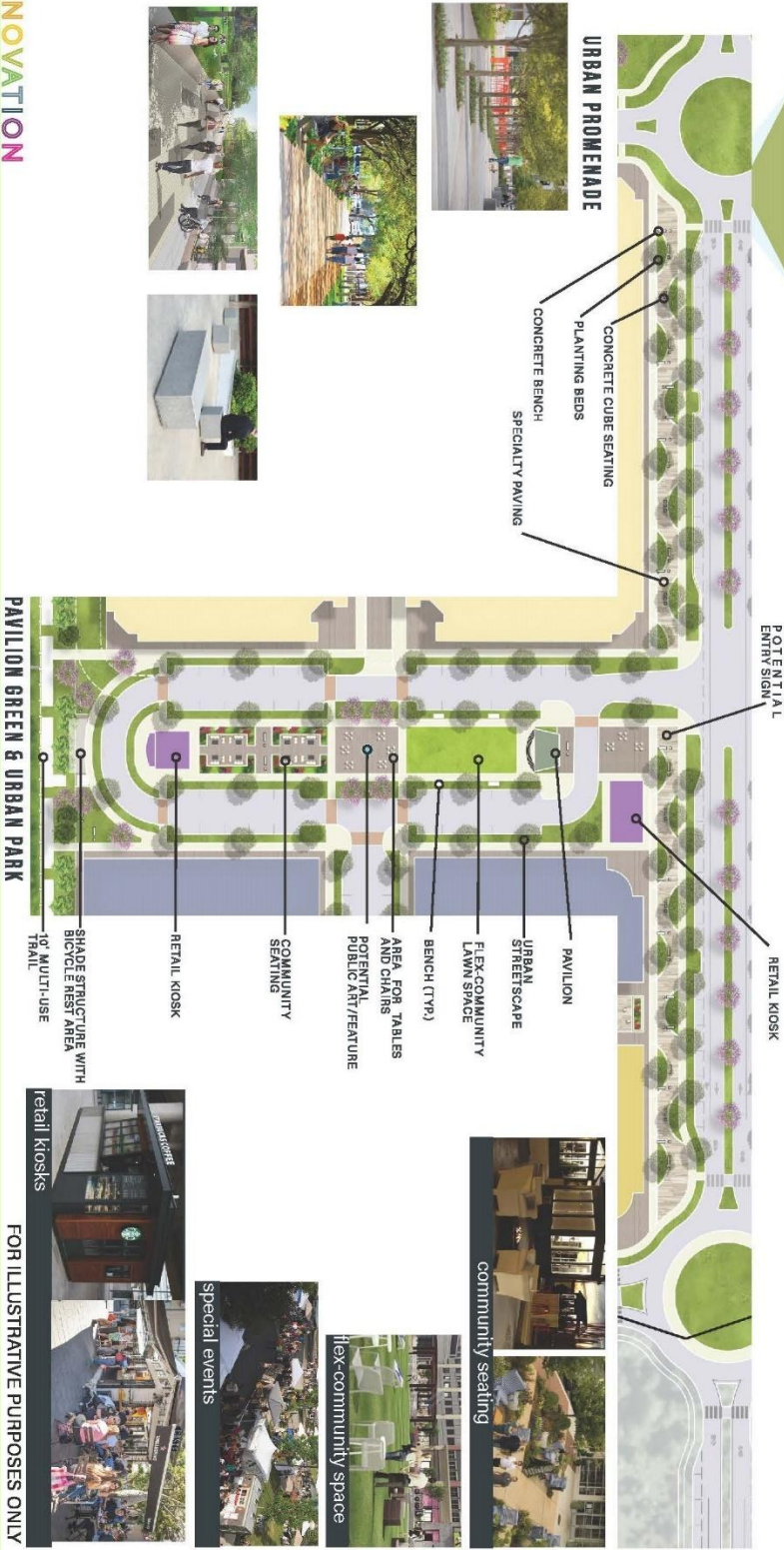
retail kiosks

FOR ILLUSTRATIVE PURPOSES ONLY

# 3.1 NON-RESIDENTIAL DETAILS

## Town Center Amenity Details

TOWN CENTER



FOR ILLUSTRATIVE PURPOSES ONLY



# 3.1 NON-RESIDENTIAL DETAILS

## Streetscape Elements

TOWN CENTER

### STREETSCAPE ELEMENTS

The streetscape shall separate public sidewalks into functional spaces as they relate to three independent zones: building frontage, pedestrian travel, and fixture planting.



NOTE: If the streetscape design interrupts a continuous soil panel and dimensions to meet the minimum soil volume requirement, another acceptable design to support root zone growth shall be implemented.

### INNOVATION DESIGN GUIDELINES

#### 3 AMENITY AREA

The amenity zone is the area adjacent to the street in which street furniture, street lights, signage, tree pit, and other public realm amenities are located. In the Town Center, the amenity area shall be 8' measured from the face of curb to the sidewalk.

Variations in the street tree pattern to acknowledge building or store entrances and other significant features shall be allowed. Flowering trees and fruit bearing trees should be avoided near pedestrian sidewalks to maintain clear passageways.



#### b PEDESTRIAN ZONE

Safe, accessible and well-designed sidewalks and crosswalks are essential for an active Town Center setting, allowing people to "park once" and easily visit area businesses on foot. Pedestrian walkways in the Town Center shall have a minimum width of 6 feet. Paving should be simple and consistent throughout the Town Center area to allow for seamless connectivity between destinations and nearby parking. Pedestrian crosswalks shall be clearly marked with contrasting surface materials that compliments the sidewalk paving. High visibility crosswalk or stamped asphalt crosswalks shall be provided in areas of high pedestrian traffic (subject to VDOT approval).



#### c BUILDING ZONE

The building zone (0-6 feet) is the area immediately adjacent to a building where building entrances are located and where activities such as outdoor dining and retail browsing occur. Awnings and architectural canopies may project above the building zone at building entrances and windows. In-ground planting and planted containers may also be located in the building zone but should be carefully situated as to not block pedestrian or visual access into doors and retail or commercial windows. Outdoor dining should be located in the building zone.



# 3.1 NON-RESIDENTIAL DETAILS

## Site Furniture

TOWN CENTER

### SITE FURNITURE

Site furnishings shall play an important role in the overall design of the Town Center. Careful consideration must be given to the selection of site furnishings. Coordination of materials and colors, functionality, durability and handicap accessibility are paramount to the selection and implementation of furnishings.

#### Recommended Colors:

- matte black
- vibrant colors
- silver/gray

#### Recommended Materials:

- Aluminum Cast Aluminum/Powder Coated Aluminum
- Cast Iron (with epoxy based paint)
- Wrought metal

### TRASH RECEPTACLES

Trash receptacles shall be surface mounted and located at pedestrian intersections, where people congregate or other areas of high pedestrian traffic. Trash receptacles need to be easily visible by users and designed in a way where trash is protected from the rain but guests should not have to touch the waste receptacle or push open a door to use it. Trash receptacles should not interfere with the pedestrian traffic, and therefore should be located within the planting zone near or along curbs for easy maintenance access.



### BIKE RACKS

Bike racks need to support the entirety of the bike - not just one wheel to allow the use of a U-lock or cable. The racks shall be installed on a wide sidewalk. The racks shall be a minimum of four (4) feet from fire hydrants, curb ramps, and building entrances. Bike parking and quantities shall be determined at site plan per DCSM standards.



### BIKE SHELTER

A bike shelter, if provided or constructed, will be constructed of structurally sound material and provide visibility of passengers to passing traffic and pedestrians. All transparent materials will be shatter proof.



### BIKE STATION

The bike rest area will include a water station and a bike pump station for users.



### BOLLARDS

Bollards shall complement the architectural style of the streetscape and shall be both functional as well as aesthetically pleasing. When necessary, bollards can provide light to pedestrian paths and sidewalks.



### PLANTERS

Planters are to be placed throughout the office, retail, pop up retail, and urban and amenity node areas. Units to be either surface mounted or with adjustable feet for easy mobility. Pots shall be used to visually enhance the space and provide areas for landscape relief, as well as to reduce or accent an architectural mass. Material to be consistent with site furnishings and preferably consist of powder coated aluminum or oxidized zinc patina finishes.



### ELECTRIC VEHICLE CHARGING STATIONS

Charging stations, if provided, should not be placed in a location that would cause a cord to be a tripping hazard. Location should be easily accessible and centrally located adjacent to the gateway plaza and the pavilion green. Site choices should consider building entry ways, pathways, street crossings and meeting points so as not to impede pedestrians.





# 3.1 NON-RESIDENTIAL DETAILS

## Site Furniture

TOWN CENTER

### SITE FURNITURE - SEATING AREAS

The location and type of seating provided within the public realm is paramount to the establishment of lively social environments. Seating is a critical part of the daily social life and allows people to loosely belong within the flow of urban life, to see and be seen. Innovation Town Center provides an array of seating opportunities to meet the needs of its diverse user base and may include, but are not limited to the following furniture types:

#### BENCHES

Benches should be placed outside of the main pedestrian flow, but within close proximity to pedestrian zones. They should be placed in shaded open spaces; along trails, plazas, parks and bus stops; to invite longer stays. Benches shall be consistent with site furnishings implemented throughout the Town Center. Commercial advertising on benches is not permitted.



#### TABLES AND CHAIRS

Bistro style seating areas located on plazas provide spaces for permanence and generate the lively environments needed to support local businesses.



#### MOVABLE SEATING

Plazas and lawn areas may include movable seating elements to allow flexibility in the user experience. Seating blocks or lounging chairs are welcoming and invite people to stay and enjoy the public realm. Fun and bright colors that portray the developer's theme are recommended.



#### SCULPTURAL SEATING

Sculptural seating shall provide a dual purpose of adding a modern aesthetic to the urban centers while inviting residents and visitors to lounge and enjoy their atmosphere. Furnishings that mirror natural silhouettes from nature complement the surrounding landscape scheme. Their abstract form also acts as an element of public art for the community.



# 3.1 NON-RESIDENTIAL DETAILS

## Building & Architectural Standards

TOWN CENTER

Building arrangements, massing, facades, and heights define the character of a community. Buildings that frame the street with entrances and transparent glass at the ground floor are street oriented and create a more urban character.

The design guidelines on this sheet promote street oriented development to encourage walking and to enhance community social life.



### MASSING & FORM

Building height, scale, and massing can be used to emphasize important corners, designate points of entry, and create a visible roofline silhouette. The primary mass of structures is to be designed to include secondary projections that reduce the apparent scale, creates visual interest, and promotes compatibility with adjacent uses.

Buildings should create a contextual fit with articulated building form including strong massing and horizontal division (base, middle, top).

Massing and height shall be arranged to emphasize the central public space with shorter buildings framing the public plaza and higher structures behind them.

Buildings shall be placed along both sides of a street to create building walls that define the main street corridors and public sidewalks.

Local parking, service and loading areas along alleys or to the rear of the buildings not along primary retail street frontage.



### FACADES

Building design shall incorporate a 350-degree design philosophy understanding that all of the built environment should be well designed not just that visible from public areas.

Utilize building setbacks and arceded spaces as an extension of the sidewalk to provide adequate space for pedestrian movement and activity. This space can be used for outdoor seating, street furniture, landscaping, and public art that can enliven the streetscape.

Create visual interest in building facades and break up the mass of large-scale buildings with articulation in form, architectural details, and changes in materials and colors. Non-residential facade colors, styles, and materials should be harmonious with the proposed architectural styles of the residential component utilizing similar high-quality materials such as brick and stone.

Entry facades shall orient towards the primary street active pedestrian zone within the Town Center to create an inviting image and consistent front and street edge definition.

Recess entrances to increase legibility and emphasize storefronts and bay windows. Corner entrances shall be emphasized through incorporation of a building recess, projection, canopy, or similar design element.

### RETAIL FRONTAGE

Well-designed storefronts including windows, doors, wall composition, colors, and materials should be incorporated to create a sense of entry at a pedestrian scale.

Retail and restaurant storefronts shall be designed with a significant amount of transparency, to promote business and activate the street.

Design store signage as an integral element of the building facade such as a panel on the building's cornice, or as a vertical hanging banner sign.

Ground-floor design shall be high quality and pedestrian oriented and Storefront configurations and details should provide a sense of human scale, variety, and interest within the overall context of the buildings.

Entrances shall be placed close together along primary retail streets to activate the street and provide convenience.



### BUILDING SIGNAGE

Signage must be scaled appropriately to appeal to both pedestrians walking on the adjacent sidewalks and to vehicles driving at reduced speeds.

On all street frontages, signage material shall be integrated into the overall design of the building.

Signs shall be located to complement the architectural features of a building such as above the building entrance, storefront opening, or other similar feature.





# 3.1 NON-RESIDENTIAL DETAILS

## Building & Architectural Standards

TOWN CENTER

### FENESTRATION

The size, frequency, and location of windows will be one of the primary visual characteristics of each building. Building fenestration should be appropriately proportioned for the building's scale and function.

Window and door placement shall provide a high degree of transparency at the lower levels of the building, maximize visibility of pedestrian active uses, provide a human-scaled architectural pattern along the street and establish a pattern of individual windows and exterior openings within building facades that provides a greater variety of scale through material variation, detail and surface relief.

Windows should be grouped to establish rhythms and hierarchies at important places on the facade.



### PARKING STRUCTURES

Architectural treatments, artwork, facades and other design features are encouraged for use on parking structures where blank walls occur.

The design of all parking structures should be more than a rectangular utilitarian box composed of concrete or steel beams and columns. Variation in forms and detail along with high quality facing materials are expected where viewable by project users and the general public.



### MECHANICAL EQUIPMENT

Mechanical equipment or other utility hardware on roofs, ground, or buildings shall be screened from public view with architectural and/or landscape materials harmonious with the building, or they shall be located so as not to be visible from any public space.



### PARKING & SCREENING

Parking areas should provide pedestrian linkages to the retail structures, and include crosswalks where appropriate.

Parking areas and dumpster pads shall be screened from the principal street by a building or appropriate landscaping.

Loading dock, outdoor storage, trash receptacles etc. should be located and screened in such a way that they are not viewed from the street.





# 3.1 NON-RESIDENTIAL DETAILS

## Lighting

TOWN CENTER

### TOWN CENTER LIGHTING OPTIONS



#### STREETLIGHTS

Street lights shall be located and mounted to ensure safety and to minimize maintenance burdens. Contemporary full cutoff lighting fixtures shall be used and coordinated from the parking lot, to the pedestrian areas, to the walls of the buildings. Such lighting shall be designated at site plan and shall follow all Federal and Local standards for placement and intensity.

Other street lights not identified herein may be installed along public streets in accordance with DCSM.



#### STRING LIGHTS

String lights help define a space and provide ambience lighting between building structures.



#### SEATING LIGHTING

Seating lighting can create interesting effects in the evening and help provide a contemporary feeling within the space.



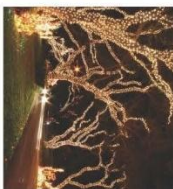
#### LANDSCAPE LIGHTING

Landscape uplights create a dramatic effect at dusk and provide soft lighting to public spaces and pathways.



#### SEASONAL LIGHTING

Seasonal lighting provides a festive environment within the Town Center. It is a welcoming a soothing element to maintain the vitality during the winter season.



#### PAVING LIGHTING

Interesting patterns can be created by utilizing lighting within the pathways or to illuminate a specific corridor.



# 3.1 NON-RESIDENTIAL DETAILS

## Landscape Scheme

TOWN CENTER

Innovation Town Center's landscape scheme will promote and foster a sense of community within a lush, well maintained, contemporary environment. Where the combined commercial, retail, and urban setting meets nature, pedestrians will be able to enjoy a variety of native grasses, shrubs, and perennials. The design intent will be to seamlessly introduce a resilient and sustainable greenspace to encourage visitors to explore their surroundings while shopping, dining out, or participating in community events outside. Sculpting the built environment to be in tandem with nature brings about a synergy of the living environment, urban scale and built form to a comfortable human scale and connected spaces.

Enhanced landscaping will be created in a variety of key locations with high pedestrian traffic or visibility. Annals shall be replaced seasonally within moveable planters as well as certain focal points of interest around larger native perennial planting beds. Hanging baskets will also be featured along light posts to add visual interest and a touch of color to seasonal events. Hardscape elements will be clean, minimalist, and modern to reflect the forward-looking aesthetic of the Town Center.



# 3.1 NON-RESIDENTIAL DETAILS - Town Center

Signage & Public Art

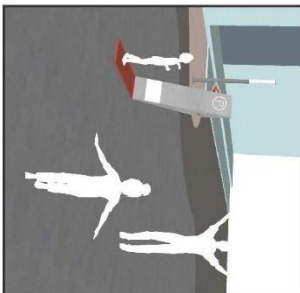
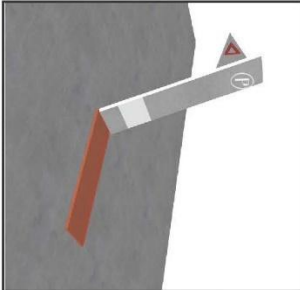
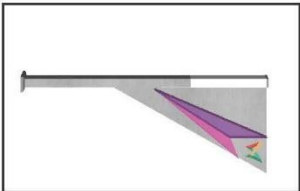
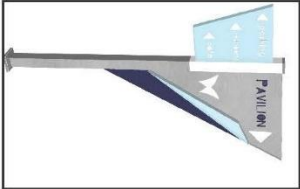
TOWN CENTER

## WAYFINDING

A comprehensive sign program will contribute to the Innovation Town Center identity branding. The process shall identify the need for specific wayfinding signage required to navigate the public space and help people orient to where they are and understand where they need to go.

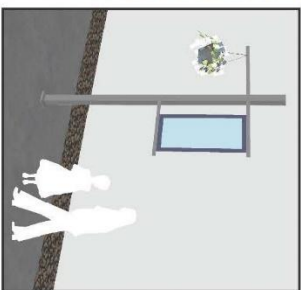


## WAYFINDING SIGNAGE ALTERNATIVES



## POLE BANNER SIGNAGE

Vibrant pole banner that can be also used for seasonal promotions around special events shall be displayed between two poles on lamp posts and light posts on main roads. Additional brackets to support planting pots are often utilize to boost the streetscape appeal.



## POLE BANNER SIGNAGE



# 3.1 NON-RESIDENTIAL DETAILS - Town Center

## Public Art

TOWN CENTER

**PUBLIC ART**

Achieving a sense of place is accomplished not only from a physical perspective but also from a cultural one. To that end, Innovation Town Center may include rich and varied cultural and educational experiences. This goal can be achieved in part by promoting art in public spaces.

Public art can take any shape or form, it can be either permanent or temporary, located indoors or outdoors, and be integrated into the architecture and site design or stand alone.

Public Art can be located at prominent locations, vista termination points, central gathering places, or along pathways, building faces or even benches. The placement of public art at Innovation Town Center shall be carefully reviewed during the design process to ensure the most optimal location for its success in providing a rich cultural experience for the community.

General examples of public art include, but are not limited to:

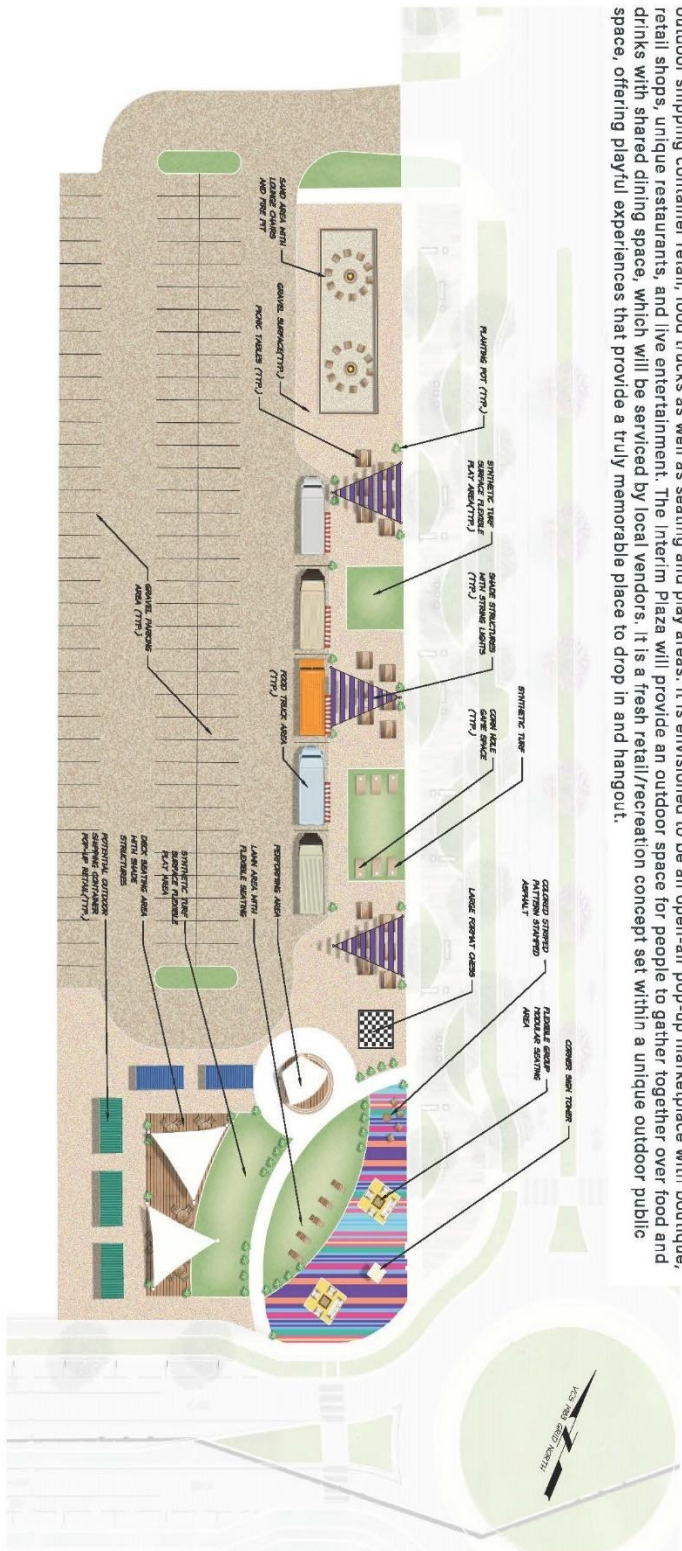
- Sculpture: in the round, bas relief, kinetic works (mobiles), electronic works, light works; figurative, abstract, statuary; formed from any material that provides the type of durability required for the project;
- Mosaics: including engravings, carvings, frescoes;
- Fountains or water elements;
- Fine arts/crafts: clay, fiber (tapestries), textiles, wood, metal, plastics, stained glass;
- Mixed-media video and computer-generated works, collage, photography;
- Decorative, ornamental, or functional elements designed by an artist;
- Murals, drawings, and paintings; and
- Monuments



# NON-RESIDENTIAL DETAILS - Town Center Interim Plaza

TOWN CENTER

During the build out of the project, the Applicant will construct an Interim Plaza, with the intention of creating an entainment/dining/recreation destination that will enhance the gateway into Innovation Town Center. The Interim Plaza will include a number of customized features, such as outdoor shipping container retail, food trucks as well as seating and play areas. It is envisioned to be an open-air pop-up marketplace with boutique, retail shops, unique restaurants, and live entertainment. The Interim Plaza will provide an outdoor space for people to gather together over food and drinks with shared dining space, which will be serviced by local vendors. It is a fresh retail/recreation concept set within a unique outdoor public space, offering playful experiences that provide a truly memorable place to drop in and hangout.





# 3.1 NON-RESIDENTIAL DETAILS - Town Center

Interim Plaza Character Images

TOWN CENTER

**1 FOOD TRUCK EXAMPLES**

**2 PERFORMING AREA EXAMPLE**

**3 DINING AREAS EXAMPLES**

**4 FLEXIBLE GROUP SEATING EXAMPLES**

**5 SHIPPING CONTAINER POP-UP RETAIL EXAMPLES**

**6 COLOR PAVEMENT**

**7 DINING AREAS EXAMPLES**

**8 PERFORMING AREA EXAMPLE**

**9 FLEXIBLE GROUP SEATING EXAMPLES**

**10 SHIPPING CONTAINER POP-UP RETAIL EXAMPLES**

**INNOVATION**  
DESIGN GUIDELINES

# 3.2 NON-RESIDENTIAL DETAILS

## Flex-Tech Overview

FLEX-TECH

### FLEX-TECH - LANDBAY A

Flex-tech building structures have the increased flexibility to offer the freedom to modify and customize office layouts, provide additional room for expansion and have the capability for on-site storage. The opportunities flex tech buildings propose make these building useful and valuable assets in today's real estate markets. Innovation Town Center capitalizes on this opportunity to ensure viability and economic growth for the community.

Some examples of potential flex tech buildings could include:

- Commercial indoor or outdoor recreation
- Crossfit/Garage/Group fitness gyms
- Storage facilities
- Business parks



FOR ILLUSTRATIVE PURPOSES ONLY.



# 3.2 NON-RESIDENTIAL DETAILS

## Site Furniture

FLEX-TECH

### FLEX-TECH DESIGN STANDARDS

Site furnishings shall play an important role in the overall character of the flex-tech commercial area. Careful consideration must be given to the selection of site furnishings, such as those shown to the left. Coordination of materials and colors, functionality, durability and handicap accessibility are paramount to the selection and implementation of furnishings.

#### Recommended Colors:

- charcoal gray
- crimson red
- silver

#### Recommended Materials:

- Aluminum/Cast Aluminum/Powder Coated Aluminum
- Cast Iron (with epoxy based paint)

### BENCHES

Seating shall be consistent with site furnishings implemented throughout the flex-tech area.



### LIGHTING

Street Lights shall be located and mounted to ensure safety and to minimize maintenance burdens. Contemporary full cutoff lighting fixtures shall be used and coordinated from the parking lot to the pedestrian areas to the walls of the buildings through the transect zone.



### BIKE RACKS

Bike racks need to support the use of a U-lock or cable. The racks shall be installed on area clear of the sidewalk space. Bike parking and quantities shall be determined at site plan per DCSM standards.



### PLANTERS

Planters to be placed strategically along main entryways of commercial buildings. Units to be either surface mounted or with adjustable feet for easy mobility. Pots may be used to visually enhance the space and provide areas for landscape relief as well as to reduce or accent an architectural mass. Material to be consistent with site furnishings and preferably consist of powder coated aluminum.





# 3.2 NON-RESIDENTIAL DETAILS

## Building & Architectural Standards

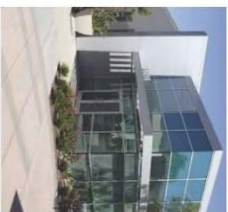
FLEX-TECH

### BUILDING STANDARDS

Flex-Tech Buildings within Landbay A will feature facilities designed to accommodate a wide range of businesses. These flex-tech buildings will house an assortment of unrelated businesses in separate suites. As such, a flex-tech building must be flexible enough to support a wide range of business activities. It will incorporate the features of other types of buildings such as the storage space of a warehouse, utilities infrastructure for call centers or data centers, and superior exterior finish and architectural design to complement the adjacent residential area.

### FLEXIBILITY

Flex buildings are, by design, "flexible" structures that allow for a wide range of office and warehouse uses. They can be used for many purposes and are easier to retrofit to meet a company's needs than typical warehouse buildings. This flexibility is ideal for a wide range of companies that need office space with a warehouse component. Because flex-tech buildings are commonly used for businesses that require both office and/or retail paired with manufacturing, storage, high-bay storage or production uses, these structures shall have an attractive appearance to the front of the building with very utilitarian/functional spaces in the back.



### LOADING & SCREENING

Flex buildings may have some type of overhead loading doors, to ensure the loading situation works for some uses that may require this. The loading areas in flex buildings can be dock high, or grade-level, and can also accommodate smaller box trucks and vans. Special attention is to be given to screen these areas from the residential use through landscaping measures or decorative walls that serve as extensions of the building face.



Raised planters



Decorative walls

# 3.2 NON-RESIDENTIAL DETAILS

## Landscape Scheme & Signage

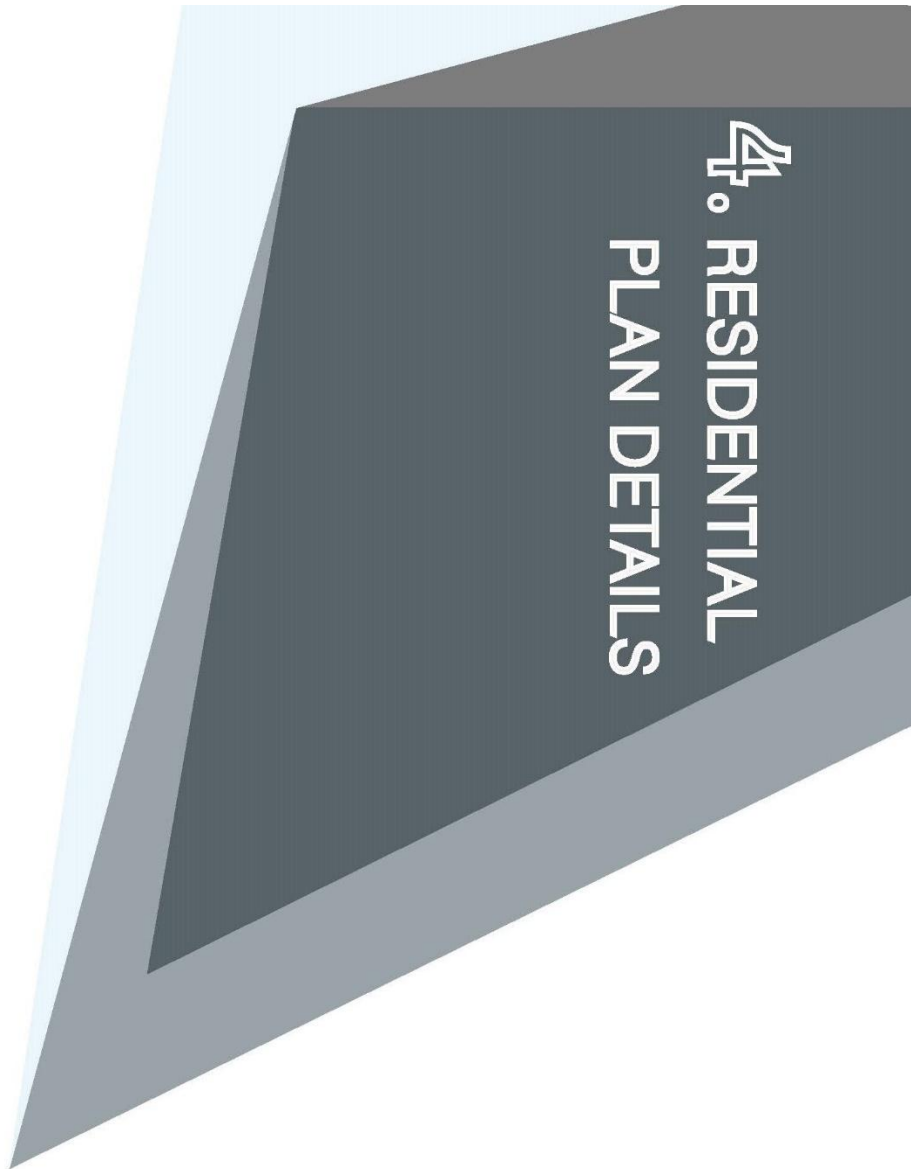
FLEX-TECH

The flex-tech commercial area shall be landscaped with a variety of native shrubs, perennials, ornamental trees and grasses. A minimalist theme will further create a landscape that can be easy on the eyes as well as easily maintained by landscape crews. The intent shall be to bring about an environment that softens the traditional heavily landscaped flex tech setting.

Enhanced landscaping will be created in a variety of key locations with high pedestrian traffic or visibility - around important signage, entryways to buildings, etc.







# 4. RESIDENTIAL PLAN DETAILS

## Residential Overview

The Innovation Town Center residential component offers a community with a varied selection of home types. The residential housing mix is composed of mid-rise multi-family units within the Town Center. Then it transitions north to stacked townhouses also arranged in a neo-traditional urban pattern. An environmental amenity corridor which includes a clubhouse, a pool, a nature park and playground, as well as a trail, separates this neighborhood with the other area that offers single family detached townhouse. The development proposes an array of lot sizes and home choices catering to the different needs of potential home buyers.

Each unit type within the community is strategically located to front a street or a public space. The alleyways have been located in a manner in which they are not directly visible from another street.

Residential areas also provide a significant amount of active and passive recreational areas strategically located within walking distance from the community.



FOR ILLUSTRATIVE PURPOSES ONLY

# 4.1 RESIDENTIAL PLAN DETAILS

## Major Neighborhood Amenity Details

### RECREATIONAL AREAS

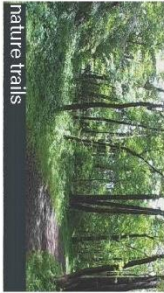
The residential component of the development proposes an array of active and passive recreational opportunities that may include, but are not limited to:

- Nature trails
- Preservation areas
- Multi-use trails
- Clubhouse
- Swimming pool
- Ping pong tables
- Pavilion/Shelters
- Playgrounds
- Fire pit with movable chairs
- Fitness Stations

### PLACEMAKING ELEMENTS

The amenities should provide key placemaking strategies that result in quality spaces such as, but not limited to:

- Provide access and linkages;
- Offer choices in recreation;
- Are attractive, safe, comfortable, green; promote sociability, pride, and a sense of place.



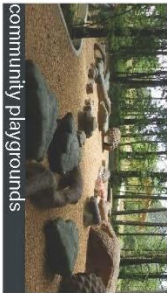
nature trails



club house amenities



club house amenities



community playgrounds



club house & community playground

### AMENITY LANDSCAPING

Landscaping within the amenities shall provide year-round seasonal interest, as well as complement and enhance the surrounding native vegetation. A mix of evergreen and deciduous, predominately native and drought resistant plant material will be used to promote a sustainable landscape. Canopy trees are to be used throughout the design to provide natural shaded areas. Images shown are subject to final design and engineering.



nature park



innovation playground



community gathering area



neighborhood promenade



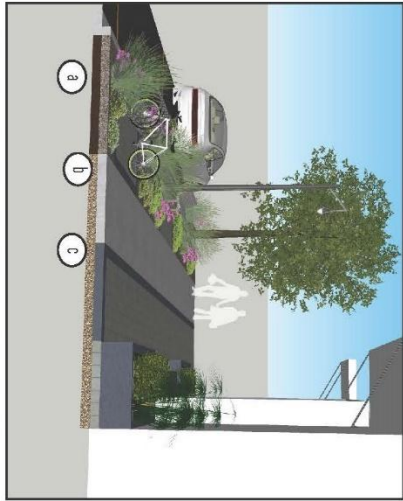
# 4.1 RESIDENTIAL PLAN DETAILS

## Streetscape Elements

TOWN CENTER MULTI-FAMILY

### STREETSCAPE ELEMENTS

Site furnishings shall play an important role in the overall character of the town center multi-family area. Careful consideration must be given to the selection of site furnishings, such as those shown. Coordination of materials and colors, functionality, durability and handicap accessibility are paramount to the selection and implementation of furnishings.



**a AMENITY AREA**  
The amenity zone shall feature site furnishings such as bench seating, bike racks, street lighting and plant groupings to serve as a green buffer from the road. Tree guards may be used to protect street trees and their critical root zones.



**b PEDESTRIAN ZONE**  
Sidewalks should have a "through pedestrian zone" that is kept clear of street furniture, landscape features, and other fixtures/obstructions. A minimum of 6' feet should be reserved to allow for two people to walk comfortably side by side.



**c BUILDING ZONE**  
Access points should have ADA accessible surfaces that are attractively incorporated into the streetscape by inserting the streetscape paving materials into the access doors to minimize their appearance. Awnings, canopies and architectural elements over doorways and windows that project into the building zone may be incorporated to accentuate these spaces. These elements provide protection from the weather and assist in way-finding for pedestrians.



NOTE: If the streetscape design interrupts a continuous soil panel and dimensions to meet the minimum soil volume requirement, another acceptable design to support root zone growth shall be implemented.

# 4.1 RESIDENTIAL PLAN DETAILS

## Site Furniture

TOWN CENTER MULTI-FAMILY

### SITE FURNITURE

Site furnishings is an important element in the overall design of the Town Center. Careful consideration must be given to the selection of site furnishings, such as, but not limited to those shown to the left. Coordination of materials and colors, functionality, durability and handicap accessibility are paramount to the selection and implementation of furnishings.

#### Recommended Colors:

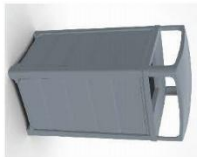
- matte black
- vibrant colors
- silver/gray

#### Recommended Materials:

- Aluminum/Cast Aluminum/Powder Coated Aluminum
- Cast Iron (with epoxy based paint)
- Wrought metal

### BENCHES AND TRASH RECEPTACLES

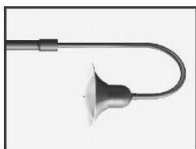
Benches should be tucked within the amenity or building zone and shall not interfere with the pedestrian clear zone. Trash receptacles may be located within the amenity zone and shall complement the other site furniture and architecture.



### LIGHTING

One of the most important and effective ways to unify the public realm is through lighting. The lighting style selected for the residential component within the Town Center needs blend well with the lighting within the public areas. Lighting levels should be sufficient for the safety of site occupants and visitors but should not spill onto adjacent properties.

Other street lights not identified herein may be installed along public streets in accordance with DCSM.



### BIKE RACKS

Short term bicycle parking for residents and visitors shall be located in close proximity to the buildings access point.





# 4.1 RESIDENTIAL PLAN DETAILS

## Building & Architectural Standards

TOWN CENTER MULTI-FAMILY

Successful mid-rise buildings employ design strategies such as street-oriented character, massing that responds to all frontages, a variety of architectural detail and context-sensitive massing. The design of residential buildings within the Town Center must be mindful of creating lively pedestrian environments through the careful use of scale, setbacks and step-backs, if needed.



**MASSING & FORM**  
Multi-family buildings should be arranged to create functional public and private outdoor spaces.

At-grade residential uses are permitted, however the design of ground floors should provide adequate public/private transition, through setbacks and other methods, and allow for future conversion to retail uses. Floor heights for commercial uses are generally higher than a typical residential floor. A taller floor-to-floor height at grade will provide for flexibility of grade level uses and increase the marketability of potential future retail spaces.

For at-grade residential units facing the streetscape, the design of the ground floor should allow for adequate separation from the sidewalk to provide transition from the public sidewalk to private residences. A setback is required beyond the sidewalk zone and should contain a raised planter, low fencing and/or landscape buffers. The ground floor of the residential units may have individual entrances and can be level with the sidewalk.

Areas between buildings should be programmed as attractive courtyards that could serve as an amenity to residents.

**FACADES & SIGNAGE**  
Building facades should be articulated or "broken up" to ensure that facades are not overly long.

Balconies or other permanent building elements should not encroach into the public right of way or setback.

Buildings should utilize high-quality materials selected for their permanence and durability.

Multi-family buildings shall be designed to support the public and commercial function of the streetscape through well articulated and appropriately scaled facades.

Residential shall provide appropriately scaled and attractive signage that fits with the character of the Town Center.

Facade building materials shall include brick, glass, sign board trim and similar product in selected areas.

Facades may utilize a variety of architectural elements such as balconies, railings, window boxes, mullions and cornices to add a level of dimensional detail to the elevations.

**PARKING & SCREENING**  
Loading, servicing, and other vehicular related functions should not detract from the use or attractiveness of the pedestrian realm.

Design of mechanical areas and penthouses shall use materials that complement the architecture of the building.

Parking areas should provide pedestrian linkages.

Parking areas and dumpster pads shall be screened from the principal street by a building or appropriate landscaping.

# 4.2 RESIDENTIAL PLAN DETAILS

## Streetscape Elements

MULTI-FAMILY STACKED

### STREETSCAPE ELEMENTS

#### MULTI-FAMILY STACKED STREETSCAPE



**NOTES:**  
Mailbox strategically located for easy mail truck delivery access.  
Streetscape shown for illustrative purposes only and is subject to change with final engineering. If the streetscape design interrupts a continuous soil panel and dimensions to meet the minimum soil volume requirement, another acceptable design to support root zone growth shall be implemented.

#### a AMENITY AREA

A mix of native, low-growing ground covers can provide an alternative to grass that will not need moving nor frequent watering to survive and remain attractive. Trees and landscaping should be kept out of the edge zone to protect them from car doors and overhangs and allow pedestrians to access their vehicles without conflict. Care should be taken to choose plants whose growth will not create obstructions for the pedestrian nor damage the sidewalk. Planting strips are also useful for street signs, cluster mail boxes, fire hydrants, and street lights, among others. The approach to the design and planting of this strip must acknowledge its multi-purpose nature.  
Low shrubs and street trees to further enhance the pathway and the parking. Examples may include but are not limited to:



Kentucky Coffee Tree  
Gymnocladus dioica  
"Espresso"



Lilyturf  
Liriope muscari 'Big Blue'



Deutzia  
Deutzia gracilis 'Duncan'  
Chardonnay Pearls®



#### b PEDESTRIAN ZONE

An unobstructed 5' sidewalk shall be provided to ensure two pedestrians can walk side by side.



#### c BUILDING ZONE

Foundation planting and shrubs to be maintained by HOA. Examples may include but are not limited to:



Crepe Myrtle  
Lagerströmia indica



Inkberry  
Ilex glabra



Blue Fescue Grass  
Festuca glauca 'Elijah Blue'

# 4.2 RESIDENTIAL PLAN DETAILS

## Site Furniture

MULTI-FAMILY STACKED

### SITE FURNITURE

Site furnishings shall play an important role. Careful consideration must be given to the selection of site furnishings, such as those shown. Coordination of materials and colors, functionality, durability and handicap accessibility are paramount to the selection and implementation of furnishings.

#### Recommended Colors:

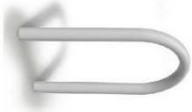
- matte black
- vibrant colors
- silver/gray

#### Recommended Materials:

- Aluminum/Cast Aluminum/powder Coated Aluminum
- Cast Iron (with epoxy based paint)
- Wrought metal

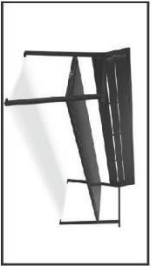
### BIKE RACKS

Bike racks within the multi-family stacked areas shall be strategically located adjacent to open space destination areas that serve the residents and their visitors.



### BENCHES

Benches should be located within pocket parks and at sidewalk intersections. Amenity zones may include benches adjacent to cluster mailboxes.



### LIGHTING

Streetscape lighting fixtures should illuminate both the roadway as well as the streetscape and pedestrian realm. These fixtures may be consistent throughout the length of the street.



### TRASH RECEPTACLE

Trash receptacles should be located in proximity to parks and pathways and shall complement the other site furniture and architecture.





# 4.2 RESIDENTIAL PLAN DETAILS

## Building & Architectural Standards

MULTI-FAMILY STACKED

### BUILDING PLACEMENT

- Multi-family stacked townhouses share a sidewalk and have 2 units stacked vertically. They have a front oriented towards the street or an open space area and a back for vehicle access, parking, and site utility servicing infrastructure. The building placement shall:
- Provide appropriate setbacks from the streets to allow for transition from the public realm to private space.
  - Maximize and create high-quality landscaped open space on the site. Opportunities may include hard and soft landscaped features such as courtyards and children's play space.
  - Provide direct visual and easily accessible physical connections to streets, and walkways and create attractive views and community focal points
  - Use the building architectural elements or landscape design to screen vehicular access and alleyways.



TYPICAL REAR ELEVATION  
OFF-TOWNHOUSE VERSION



TYPICAL REAR ELEVATION  
STANDARD VERSION



ELEVATION A  
STANDARD VERSION



ELEVATION B  
OFF-TOWNHOUSE VERSION



ELEVATION K  
OFF-TOWNHOUSE VERSION



ELEVATION D  
STANDARD VERSION



ELEVATION L  
OFF-TOWNHOUSE VERSION



TYPICAL SIDE ELEVATION  
OFF-TOWNHOUSE VERSION



TYPICAL SIDE ELEVATION WITH UTILITY SHED  
OFF-TOWNHOUSE VERSION

### ARCHITECTURAL STANDARDS

Facades shall be well articulated to provide visual interest and definition through a combination of architectural features. Multiple building materials also add visual interest to facades and create individuality.

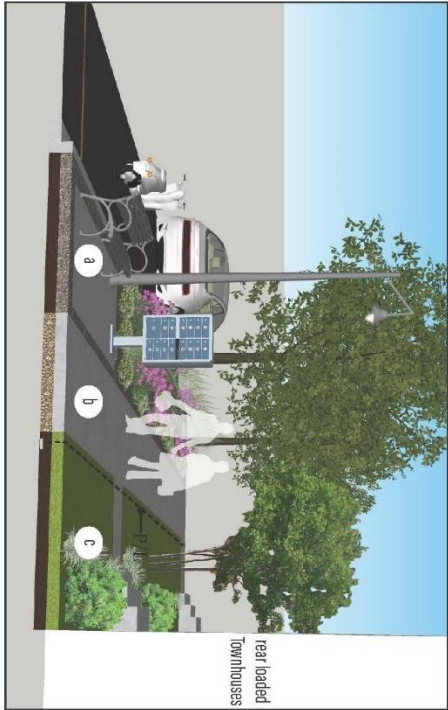
# 4.3 RESIDENTIAL PLAN DETAILS

## Streetscape Elements

REAR LOADED TOWNHOUSE

### STREETSCAPE ELEMENTS

#### REAR LOADED TOWNHOUSE STREETSCAPE



NOTE: Streetscape shown for illustrative purposes only and is subject to change with final engineering. If the streetscape design interrupts a continuous soil panel and dimensions to meet the minimum soil volume requirement, another acceptable design to support root zone growth shall be implemented.

#### 3 AMENITY AREA

Planting strips provide a landscaped buffer to improve pedestrian safety by separating the pathway from the street. This area shall primarily contain landscaping such as a continuous planting strip in residential areas. Care should be taken to choose plants whose growth will not create obstructions for the pedestrian nor damage the sidewalk. Low shrubs and street trees to further enhance the pathway and the parking. Examples may include but are not limited to:



Littleleaf Linden  
Tilia cordata



Coneflower  
Echinacea purpurea



Blue Grama Grass  
Bouteloua gracilis  
'Blonde Ambition'



Black Mondo Grass  
Ophiopogon planiscapus  
'Nigrescens'



#### D PEDESTRIAN ZONE

An unobstructed sidewalk shall be provided. Sometimes known as the "pedestrian clear zone," this is the walking zone on the sidewalk that must remain clear, both horizontally and vertically.

#### C FRONT YARD

Foundation planting and shrubs examples may include but are not limited to:



Crepe Myrtle  
Lagerströmia



Inkberry  
Ilex glabra



Blue Fescue Grass  
Festuca glauca 'Elijah Blue'



False Indigo  
Baptisia Decandollea 'Queen's Jubilee'



# 4.3 RESIDENTIAL PLAN DETAILS

## Site Furniture

REAR LOADED TOWNHOUSE

### SITE FURNITURE

The design of the streetscape, particularly paving, street furniture, landscaping, and lighting, should contribute to the evolving sense of place and character of the residential areas.

Recommended Colors:

- matte black
- vibrant colors
- silver/gray

Recommended Materials:

- Aluminum/Cast Aluminum/Powder Coated Aluminum
- Cast Iron (with epoxy based paint)

### BENCHES & TRASH RECEPTACLES

Benches should be located within pocket parks, linear parks, recreation areas, and at sidewalk intersections.



### LIGHTING

Streetscape lighting fixtures should illuminate both the roadway as well as the streetscape and pedestrian realm. These fixtures should be consistent throughout the length of the street.



### BIKE RACKS

Bike racks within the townhouse areas shall be strategically located adjacent to open space destination areas that serve the residents and their visitors to allow for parking.



# 4.4 RESIDENTIAL PLAN DETAILS

## Building & Architectural Standards

TOWNHOUSE OVERALL STANDARDS

### BUILDING PLACEMENT

Garage placement dictates driveway location, front yard setbacks, and opportunities for landscaping. It is one of the primary determinants of streetscape appearance and overall community character. It is recommended that, where feasible, rear-loaded garages should be utilized for townhouses. Townhouse buildings shall follow these general recommendations:

- Build parallel to the street and extend the building the length of the block along the edges of streets, parks, and open space with front doors on the primary facade facing these areas.
- Provide soft landscaping, walkways and projecting elements such as canopies within the front setback area.
- Front yard landscaping includes ornamental trees and foundation planting

Where rear-loaded garages are not practical, front-loaded garages may be used. Driveway lengths shall be limited to prevent cars from parking one behind the other, which can result in overhang onto the sidewalk.

### ARCHITECTURAL STANDARDS

By utilizing a combination of building materials, colors, and other facade features, townhouse units can appear as unique homes within a cohesive whole. Facade offsets and a mix of building materials window treatments, and protrusions are recommended to enhance flat facades.

Architecture design elements shall use proper proportion and sizes. The design should focus on the appropriate use of materials and how these work harmoniously with other facades. Architectural features shall be adequately sized and proportionally designed. In addition, specific attention should be placed on choosing the right blend of colors so they can work together to create a beautiful exterior.



TYPICAL TOWNHOUSE FACADE EXAMPLE

TYPICAL TOWNHOUSE FACADE DIFFERENCE



TYPICAL TOWNHOUSE



TYPICAL TOWNHOUSE

# 4.5 RESIDENTIAL PLAN DETAILS

## High Visibility Side Elevations

Exterior detailing on the side of units designated as "High Visibility" shall contain a brick, masonry, natural stone or cultured stone (excluding windows and doors), two windows and two architectural features similar to the front of the particular unit. The use of additional windows will help break up these sides.



Multi-Family Stacked High Visibility Side Elevation



Rear Loaded Townhouse High Visibility Side Elevation



\* Denotes High Visibility Units.

SEPTEMBER 9, 2021

"dream, create, inspire"



PRINCE WILLIAM COUNTY, VA  
**INNOVATION**



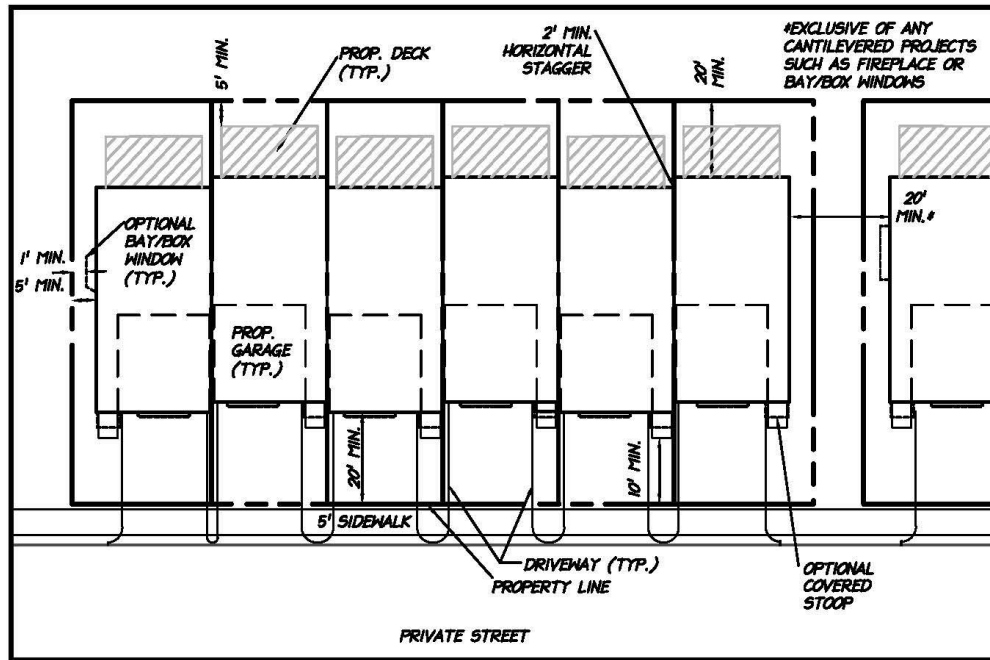
# Performance Standards

## Exhibit A – Residential and Non-Residential Performance Standards

RESIDENTIAL DEVELOPMENT STANDARDS		
PROPOSED TOWNHOUSE DEVELOPMENT STANDARDS		
MAX NUMBER OF DWELLING UNITS IN A GROUP	9	
MIN. LOT WIDTH	16 FT	
GROUP SETBACK	20 FT	
MAX BUILDING HEIGHT	35 FT	
MIN. SETBACKS		
	FRONT (WITH OF STREET PARKING)	5 FT
	FRONT (WITH GARAGE)	20 FT
	SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FT
	SIDE (END UNIT)	5 FT
	REAR (SHALL NOT APPLY TO BACK-TO-BACK TOWN HOUSE)	20 FT
MIN. SETBACK FOR COVERED STOOPS, UNROOFED DECKS, LANDINGS, AND SIMILAR FEATURES		
	FRONT (WITH OF STREET PARKING)	0 FT
	FRONT (WITH GARAGE)	10 FT
	SIDE (END UNIT)	1 FT
	REAR	5 FT
MIN. BUILDING FOOTPRINT	640 SQ FT	
OPEN SPACE	20% PROJECT WIDE	

**NOTES:**

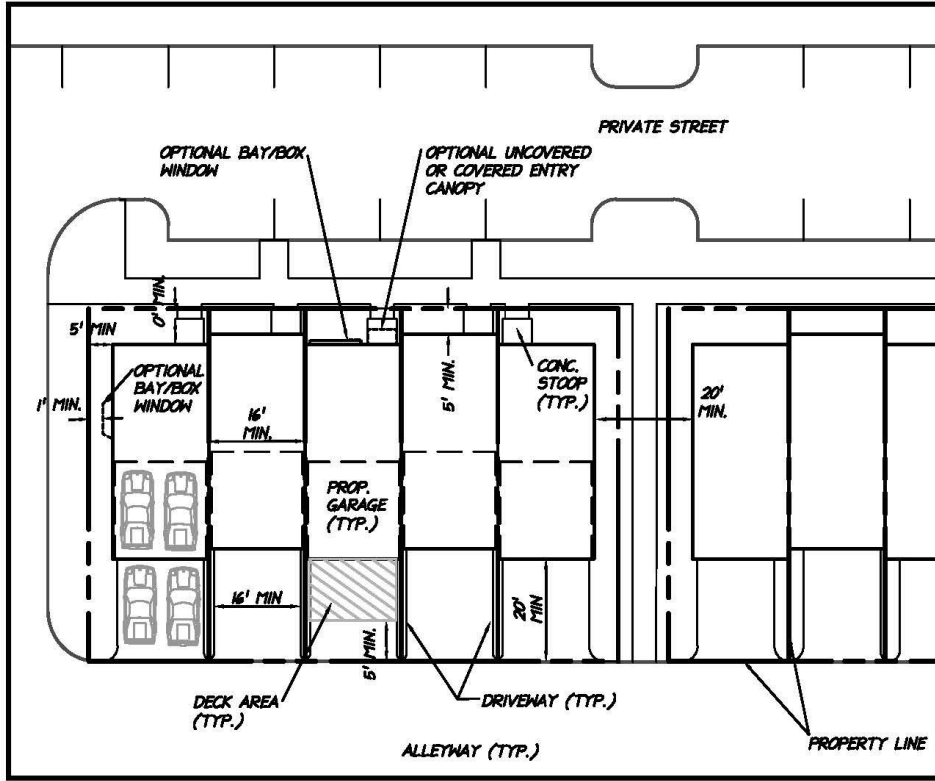
1. Setbacks shall be varied at least two feet for all townhouse units within a group, except that two abutting units may have the same setback, provided no more than four units in the group have the same setback.
2. Architectural treatment shall vary so that no more than two abutting units are substantially the same, and so that no more than six units in any group are substantially the same.



**FRONT-LOADED TOWNHOUSE**

**NOT TO SCALE**





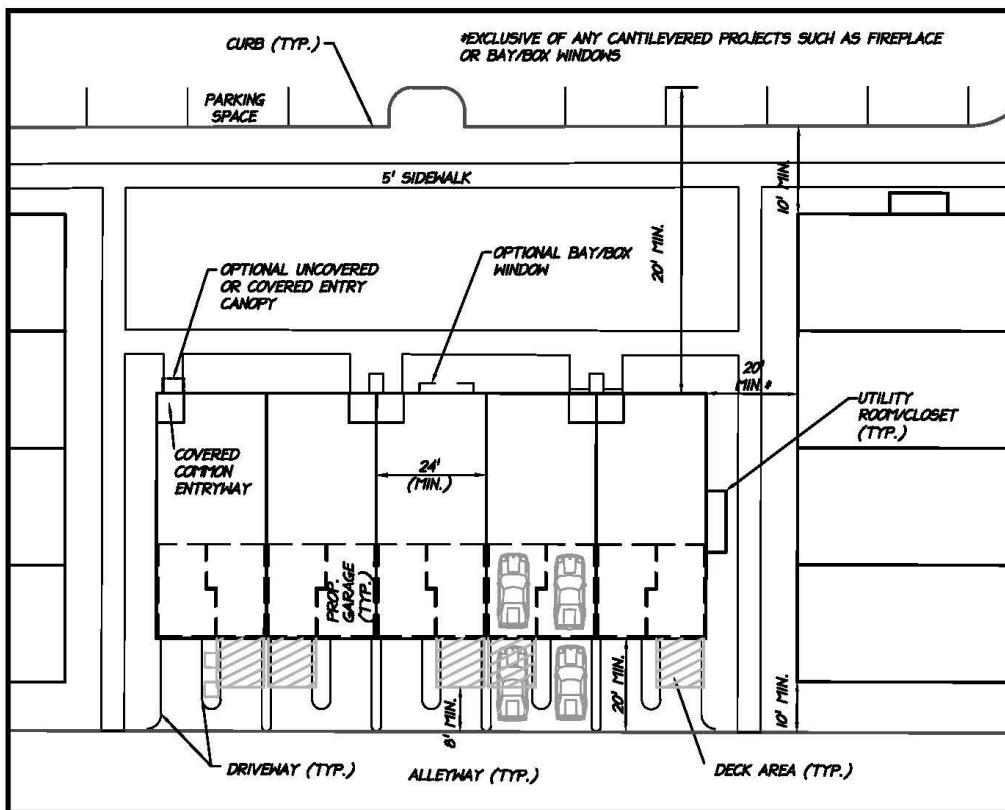
REAR-LOADED TOWNHOUSE

NOT TO SCALE

# Performance Standards

RESIDENTIAL DEVELOPMENT STANDARDS	
PROPOSED MULTIFAMILY STACKED DEVELOPMENT STANDARDS	
MINIMUM LOT AREA IN SQ. FT.(PER DWELLING UNIT)	2,000 (1 story) 1,725 (2 story) 1,450 (3 story) 1,450 (4 story)
MAXIMUM LOT COVERAGE	0.8
MAX BUILDING HEIGHT	50 FT
MIN. SETBACKS	
FRONT YARD (FROM TRAVELED PORTION OF ACCESS EASEMENT)	20 FT
REAR	20 FT
SIDE	10 FT
MINIMUM BUILDING SEPARATION	20 FT*
STRUCTURE PARKING TO DWELLING	20 FT
PARKING LOT TO DWELLING	10 FT
MIN. SETBACK FOR BALCONIES AND DECKS	8 FT

\* NOT INCLUDING UTILITY CLOSET OR ROOM PROJECTIONS, WHICH MAY BE LOCATED WITHIN THE 20' SEPARATION AREA.



MULTIFAMILY STACKED

NOT TO SCALE

## Performance Standards

RESIDENTIAL DEVELOPMENT STANDARDS	
PROPOSED MULTIFAMILY MID-RISE DEVELOPMENT STANDARDS	
MINIMUM LOT AREA IN SQ. FT. (PER DWELLING UNIT)	2,500 (Buildings up to 16 units) 1,800 (Buildings up to 26 units) 1,200 (Buildings up to 36 units) 1,000 (Buildings up to 46 units) 900 (Buildings greater than 46 units)
MAXIMUM LOT COVERAGE	0.90
MAX BUILDING HEIGHT	100 FT
MIN. YARDS AND SETBACKS	
STREET TO DWELLING	20 FT
PARKING TO DWELLING	
1. STRUCTURED LOT	0 FT
2. LOT	0 FT
SIDE	0 FT
REAR	0 FT
MIN. LOT WIDTH PER STRUCTURE	NONE
MIN. DISTANCE BETWEEN BUILDINGS	20 FT

PROPOSED NON-RESIDENTIAL DEVELOPMENT STANDARDS		
STANDARD	PROPOSED LAND BAY A-D	PROPOSED LAND BAY E (TOWN CENTER SUP)
MAX. FAR	0.65	3.00
MAX. LOT COVERAGE	90% (10% OPEN SPACE)	90% (10% OPEN SPACE)
MIN. LOT SIZE, WIDTH, AND DEPTH	NONE	NONE
MAX. BUILDING HEIGHT	70'	100'
MIN. BUILDING HEIGHT	NONE	15'
MIN. BUILDING SEPARATION	20'	20'
MIN. YARD LINES AND SETBACKS		
FRONT	10'	0'
SIDE	5'	
REAR	10'	
MIN. SETBACK FROM STREET RIGHTS-OF-WAY	10'	5'
MIN. SETBACK FROM RESIDENTIAL DISTRICT PROPERTY LINE	10'	NONE
OFF-STREET PARKING MIN. SETBACK	8'	8'









# Transportation Demand Management (TDM) Guidelines



## INNOVATION TOWN CENTER (REZ 2016-00030)

### TRANSPORTATION DEMAND MANAGEMENT (TDM) GUIDELINES June 15, 2021

Transportation Demand Management (TDM) is the establishment of measures to influence travel behavior by mode, frequency, time, route, or trip length in order to achieve a maximally efficient use of transportation facilities. The proposed Innovation Town Center development is located approximately 30 miles southwest of Washington, DC. The proposed development is situated in close proximity to three major commuter corridors: Nokesville Road (Route 28), Prince William Parkway (Route 234), and Interstate 66 (I-66). It is also within a two-mile radius of the terminal Broad Run Station served by the VRE Manassas Line, which primarily provides commuter service to and from Union Station in Washington, DC, with a connection to the VRE Fredericksburg Line at the Alexandria Station. The proposed development is also within a 2.5-mile radius of the Manassas Regional Airport (HEF).

The proposed development will be constructed in two phases. For the purposes of the analysis, Phase 1 is projected to consist of approximately 100 townhomes, 150 2-over-2's, 124 apartment units, 61.6 kSF of general office uses, and 26.8 kSF of retail uses. Phase 2 (full buildout) is projected to consist of an additional 281 townhomes, 138 2-over-2's, 100 apartment units, 118 kSF of office-flex uses, 203.4 kSF of general office uses, and 123.2 kSF of retail uses for a full buildout of approximately 381 townhomes, 288 2-over-2's, 224 apartment units, 118 kSF of office-flex uses, 265 kSF of general office uses, and 150 kSF of retail uses.

The guidelines contained herein provide a variety of strategies that can be incorporated into a Transportation Demand Management (TDM) plan for the site. Although a TDM plan is not required for this development per Section 602.02.2 of the Prince William County Design and Construction Standards Manual (DCSM), as no trip generation reductions are being requested in conjunction with the proposed development's TDM plan, it is being established due to the development's location within the Innovation Town Center, which is part of the Innovation Park Small Area – University Center Subdistrict. In accordance with the same DCSM Section, strategies identified here shall be measurable, monitorable, and enforceable, and implementation shall be designated on the plan and proffered.

The TDM plan will have many components that are tailored to accommodate the Innovation Town Center. Once implemented, the TDM program will be monitored and adjusted as needed to continually create opportunities to reduce the amount of traffic generated by the site. Traffic calming measures along the proposed Katherine Johnson Avenue are desirable but should be designed and implemented so as to maintain Katherine Johnson Avenue as the proposed primary vehicular route providing access to the Town Center.

Major components of carrying out a TDM plan include (1) transit/VRE promotion, (2) ridesharing promotion, (3) parking management, (4) on-site construction measures, (5) lease agreements, and (6) monitoring and compliance.

The results sought in the Innovation Town Center TDM plan are:

1. Maintain peak hour level of service at major intersections at or above Level of Service D.
2. Maximize transportation alternatives.
3. Utilize transportation facilities efficiently.
4. Encourage modes of transportation that focus on moving people, not vehicles.

# Transportation Demand Management (TDM) Guidelines

5. Increase convenience of intermodal transfers between the commuter rail system and feeder/distributor systems.
6. Encourage carpooling for trips generated by the development.
7. Utilize public transportation efficiently, through optimized frequencies, routing, connections, and technologies.
8. Implement improved communication and dissemination of transit information to enable people to make the most efficient use of the transportation system.
9. Encourage innovative technologies.
10. Encourage group riding.
11. Reduce vehicle-generated air pollution.
12. Review transportation management plans during the site development process.

The site's close location to the terminal Broad Run VRE Station, the I-66 High Occupancy Vehicle (HOV) lanes (and High Occupancy Toll (HOT) lanes now under construction), and proximate park and ride lots allows for a TDM program that may include, but not be limited to, the following strategies:

### ***Participate in a Transportation Management Association (TMA)***

Transportation Management Associations (TMA) are partnerships between businesses and local government, created to help solve transportation problems. TMAs provide a unified voice and forum for discussing local transportation issues and priorities, and enable developers and employers to pool resources and address problems on a joint basis.

If the users (including commercial building management or community of homeowners associations (HOAs), if applicable) comprising the Innovation Park Small Area decide to set up and designate a Transportation Management Association (TMA) for either the entire area or each subdistrict, then the Innovation Town users will be a participant for the larger area's TMA. The TMA, if established, will function as the primary point of contact with the County and undertake the implementing, coordinating, and managing of all TDM obligations. Additionally, the TMA will oversee the enforcement and monitoring of the strategies included in the TDM plan.

### ***Active Marketing***

A designated transportation coordinator will serve as the focal point for all commuter transportation initiatives. If a TMA is established, the TMA would designate a transportation coordinator for the entire Innovation Park Small Area. The transportation coordinator's name and contact information is posted in employee break areas, housing and rental leasing company websites, and other locations in the Innovation Town Center. Additionally, the transportation coordinator establishes a calendar of events, provides regular employee and resident communications, conducts targeted vanpool and transit marketing and other outreach to keep commuting at the forefront of the employee and resident environment. The transportation coordinator also supports bicycling efforts by sponsoring events such as "Bike to Work" and participating in "Air Quality Action Days".

### ***Progressive Employee Policies***

Alternative work schedules, such as flex-time, variable work hours and the compressed work week have been successful in spreading peak hour traffic volumes over the peak period and reducing traffic volumes during the week. Flex-time includes staggered and flexible work hours which allow employees to arrive or leave before or after the normal congested commuting period. Flexible working arrangements increase the opportunity of prospective rideshare employees having similar core period working hours. The compressed work week typically includes working four 10-hour days (4/10), or nine days over the two-week period (5/4/9). Generally, with the 5/4/9 plan, employees are off every other Friday or Monday. Depending upon the category of development, the transportation coordinator could require tenants to support innovative work scheduling to help reduce peak period vehicle travel.

# Transportation Demand Management (TDM) Guidelines

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Recent advances in electronic communication, in light of COVID-19 work-from-home policies, and the vision of the future, suggest a substantial growth in telecommuting, decreasing the number of employees who must commute to or from a site on a daily basis. The "smart" office building of the future will be constructed to facilitate telecommuting by tenants. The transportation coordinator will encourage employers to implement flexible work schedules by distributing literature about the advantages of teleworking and flex time.

### ***Interactive Intranet and Internet Resources***

As appropriate, the transportation coordinator can sponsor development of an intra/internet website that provides commuting information, linked to external transportation services. Additionally, through the dedicated transportation coordinator, the website can sponsor/promote partnerships in a regional self-directed commuter ride matching system.

### ***Public Transportation***

Innovation Town Center is within a two-mile radius of the terminal Broad Run Station served by the VRE Manassas Line, which primarily provides commuter service to and from Union Station in Washington, DC, with a connection to the VRE Fredericksburg Line at the Alexandria Station. As part of the Innovation Park Small Area Plan (yet to be approved), there is the potential for a possible area shuttle (project currently undergoing feasibility studies, as arranged by the Prince William County Board of County Supervisors), which would connect to the train station to encourage office employees, hotel patrons, and residents to use the transit facility, which in turn will reduce the number of vehicle trips.

The Innovation Park Small Area Plan also calls for shuttle routes throughout the Town Center and beyond, along with stops, and a potential for a commuter bus stop between the Innovation Town Center and the SciTech Campus, as depicted in Figure 2. The connections to the other sections of Innovation Park could provide opportunities for lunch, dinner, and recreation throughout the Innovation Park Small Area, which would further enhance the vibrancy and connectivity of Innovation Park.

### ***Transit Program***

In addition to increasing the cost of commuting to work in a single occupant vehicle (through charging market-based parking rates), partially subsidizing an employee's transit costs can often make the monetary difference required to cause a mode shift in an employee's commuting habits. Transit subsidy programs may include the regional Metrorail/Metro bus system, commuter rail (VRE) and commuter bus (OmniRide) systems. Depending upon the category of development, various transit program subsidies may be implemented.

VRE fare products, such as VRE-TLC (Transit Link Card) passes, are promoted on-site to provide the most convenient form of transit fare medium and make it easier to ride transit. Employers with employees who either ride transit or vanpool (or vanpool to transit stations) are encouraged to enroll in a *SmartBenefits* pre-tax benefits program to help them take advantage of available automated and convenient tax savings on their transit costs.

### ***On-site Amenities***

On-site retail and retail service amenities are expected to be included as part of the Town Center development as well as the planned adjacent development. Additionally, the site will be designed to support and encourage multi-destination travel with such amenities and incorporate pedestrian facilities as an integral part of the development. These amenities will help reduce the need for mid-day employee vehicular travel, thus not adding to off-peak traffic (i.e., lunch hour rush, etc.). The mixed-use environment provides a space for residents and non-residents alike to live, work (study), and play without having to drive, which will promote pedestrian activity and alternative non-vehicular transportation options.

### ***Bike/Walk Programs***

The development is planning to provide pedestrian and bicycle facilities and clear paths throughout the property that provide circulation throughout the site and the adjacent parcels (surrounding development). Beyond the existing shared use path and



# Transportation Demand Management (TDM) Guidelines

sidewalks along the existing road network (see Figure 1), the facilities may include the addition of sidewalks, shared-use paths, trails, bike lanes (along the proposed Katherine Johnson Avenue), pedestrian and cyclist bridges, marked crosswalks, and pedestrian signals in and around the site (see Figure 2). Additionally, secure bicycle parking/storage facilities, and showers and clothing lockers could be provided to support bicycle and walk commuters. The applicant plans to construct the base network of bicycle and pedestrian facilities within Innovation Town Center in accordance with the current MUTCD, VDOT, and ADA standards.

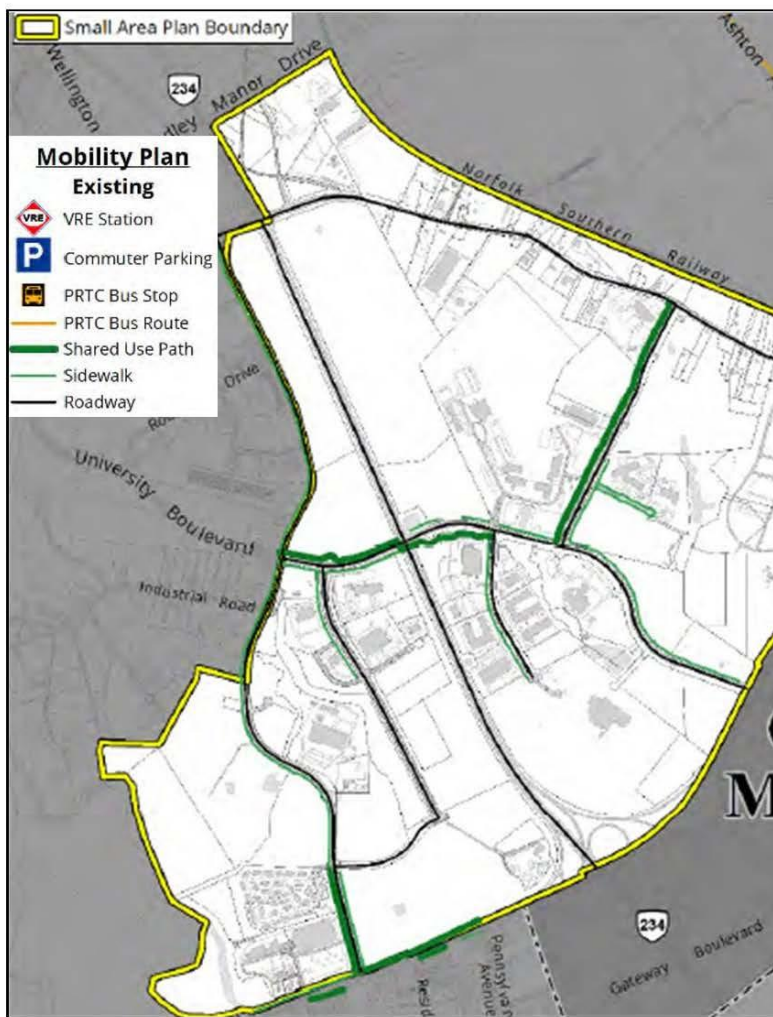


Figure 1: Existing Facilities (Source: Innovation Park Small Area Plan)

# Transportation Demand Management (TDM) Guidelines

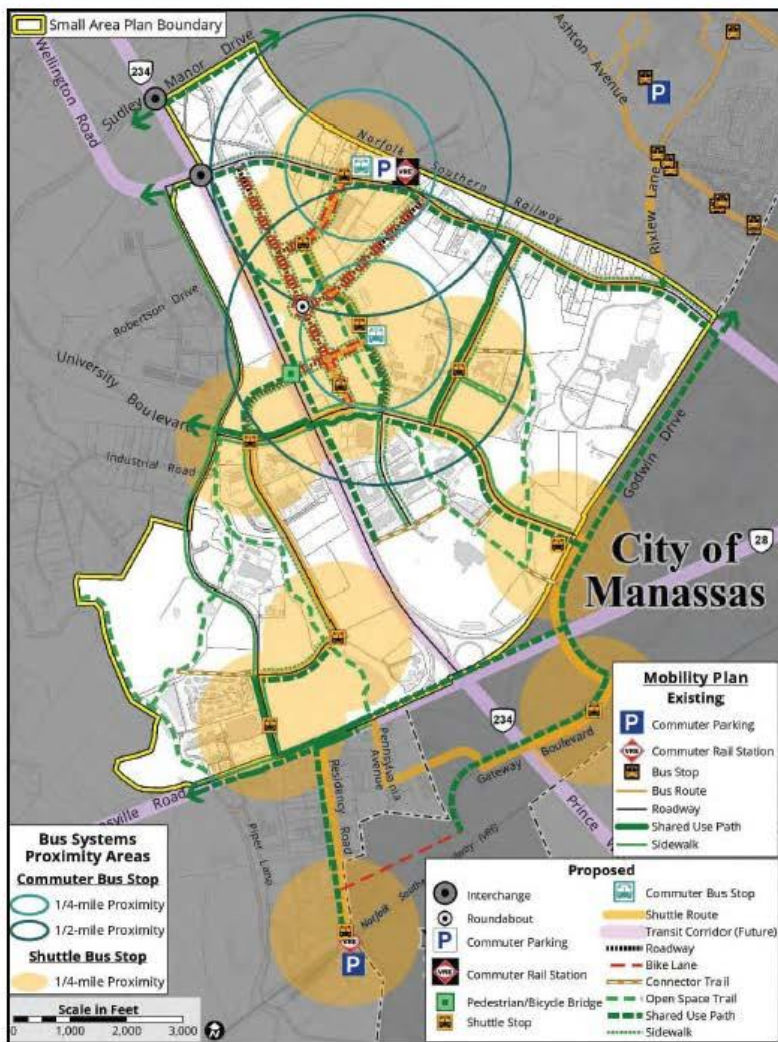


Figure 2: Planned Facilities (Source: Innovation Park Small Area Plan)

## Ridesharing Marketing

Encourage and promote rideshare matching and incentives for car/vanpooling for both Innovation Town Center residents and (non-resident) employees. Strategies include transportation fairs, distribution of ridesharing marketing material to residents and employees, and displaying information material, such as posters, brochures, etc., in common areas like hallways, elevators, restrooms, water fountains, building management offices and public space notice boards.

A reason often cited to not participate in ridesharing arrangements is the need to have a personal vehicle at hand for emergency situations. A number of innovative programs have been developed which provide emergency transportation to one's home or child's school, daycare, etc. Programs include a limited tax/bus fare subsidy, relaxed company vehicle policies, as well as the



# Transportation Demand Management (TDM) Guidelines

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Commuter Connections Guaranteed Ride Home (GRH) program which provides commuters who regularly carpool, vanpool, bike, walk or take transit to work with a free and reliable ride home when unexpected emergencies arise. Commuters may take advantage of GRH up to four times per year and is designed to rescue commuters who are worried about how they'll get home when an emergency arises. Knowing there is a GRH allows commuters to consider other options like transit and carpools with peace of mind and confidence.

## ***Transportation Surveys***

Surveys are useful in determining commuting patterns, mode split, average commute distance and travel times, employee and resident attitudes, needs, and willingness to switch modes. In order to set objectives and monitor performance, resident and (non-resident) employee transportation surveys should be conducted every two years. The data is useful in developing successful transportation programs, such as transit subsidies, and car and vanpool programs.

For example, the results of the transportation survey may identify an opportunity to operate a vanpool program. Depending upon site location, the program could entail a shuttle bus system, as proposed in the Innovative Park Small Area Plan and currently undergoing feasibility studies, or company vans to provide group transportation from other developments to the Innovation Town Center.

Vanpools generally need to operate at full occupancy (14 riders) to cover their operating expenses. To assist new or potential operators, a number of state and local governments provide startup seed money to vanpool drivers. The programs include interest free loans for a specified period of time and passenger subsidies. Developer assistance may include additional loan programs, outright purchase, matching or doubling passenger subsidy programs and backup vehicles.

## ***Connections to Area Destinations***

In addition to the potential area shuttle (the project currently undergoing feasibility studies) serving the Broad Run Station as outlined in the Innovation Park Small Area Plan, the shuttle service could be offered to serve old Town Manassas, the Manassas Regional Airport (HEF), and the nearest Park and Ride lot, other George Mason University campuses, or other desirable destinations (see Figure 3). The shuttle could provide alternative transportation methods for the region and have the potential to reduce vehicular trips and parking demand at the site.

# Transportation Demand Management (TDM) Guidelines

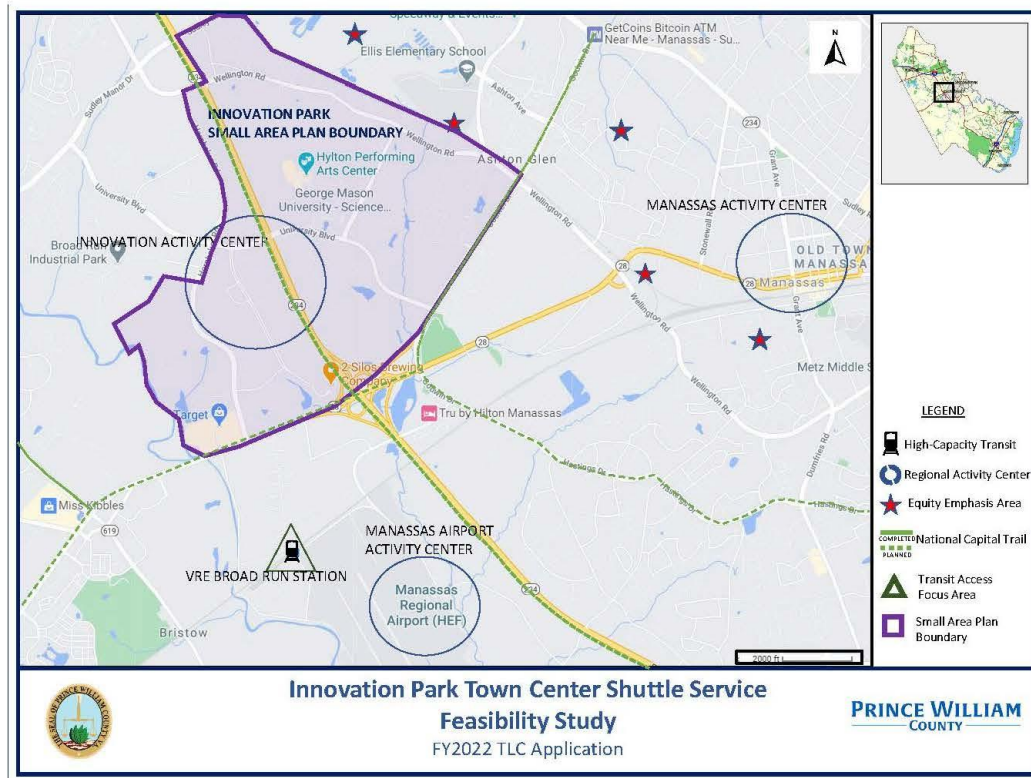


Figure 3: Shuttle Feasibility Study Area (Source: Innovation Park Town Center Shuttle Service Feasibility Study Application for Funding)

### Car Share Programs

Car sharing refers to a short-term automobile rental service available to the general public. Car sharing operators provide a fleet of vehicles that are placed across defined geographic areas and can be used by members for personal trips that are typically short in length and duration. Carsharing programs allow residents to give up their cars and can reduce some of the parking demand.

Parking spaces for increasingly popular car share programs such as Zipcar could be reserved throughout the site, providing a convenience for employees who carpool but also need a vehicle during the mid-day to attend off-site meetings, or for residents and students, as needed.

### Managed/Preferential Parking

Office tenant employees could be required to participate in a parking management and registration program. As such, reserved parking spaces would be provided for tenant employee carpoolers and vanpoolers that are conveniently located with respect to the elevators serving the buildings. These convenient spaces can also support environmental awareness and be reserved for residents or employees who have fuel-efficient vehicles and/or have electric cars. Charging stations could also be made available at these locations.

# Transportation Demand Management (TDM) Guidelines

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The spaces will be clearly marked "Reserved for car and vanpools." The number of spaces set aside could be unlimited, depending upon the success of the Transit Management plan. Five to ten percent of the commercial parking supply should be reserved for eligible vehicles. This amount can be adjusted based on demand and popularity of the carpooling program.

To encourage group riding, the preferential parking program may include a parking rate agreement charging market (or above market) rates for single occupant vehicles. As a part of the biennial transportation survey, a parking survey should be conducted to determine local area price structures for determining appropriate "market rates".

In conjunction with establishing market parking rates in private parking lots/garages and streets, subsidies are encouraged to promote group riding by both car and vanpools. Depending upon the category of development and the need to reduce single occupant vehicle trips to the site, parking rate subsidies in private parking lots/garages and streets for car and vanpools may range from full subsidy to full market rate.

### ***Monitoring and Compliance***

The transportation coordinator will organize a periodic traffic monitoring program. The transportation coordinator shall submit reports to the Prince William County Department of Transportation every five (5) years until such time that the project is built out and the TDM program has stabilized as determined by the Prince William County Department of Transportation. The purpose of the monitoring program is to evaluate whether the project is generating the traffic as anticipated in the corresponding traffic impact study (*Innovation Town Center Traffic Impact Study*, Gorove Slade, revised March 29, 2021). If the traffic volumes collected (weekday morning peak hour, weekday afternoon peak hour, daily traffic) are equivalent or less than the traffic volumes reflected in the corresponding traffic impact study, no further action is needed until the next monitoring date. If the traffic is more than reflected within the study, the transportation coordinator shall work with the local authorities to mitigate the impacts with adjustments to the demand management program to reduce traffic.

INNOVATION TOWN CENTER  
RESIDENTIAL DEVELOPMENT  
PRINCE WILLIAM COUNTY, VA

PROFFER JUSTIFICATION NARRATIVE

AUGUST 23, 2021

PREPARED BY:

MUNICAP, INC.  
— PUBLIC FINANCE —

**INNOVATION TOWN CENTER  
RESIDENTIAL DEVELOPMENT  
PRINCE WILLIAM COUNTY, VA**

**PROFFER JUSTIFICATION NARRATIVE**

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## *I. Introduction*

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**The purpose of this document is to update the previously submitted *SB 549 Proffer Justification Narrative* dated August 11, 2021 by MuniCap, Inc. in order to update the proffer impact analysis for the addition of 36 multi-family units.**

The purpose of this *Proffer Impact Analysis* is to satisfy portions of the County’s requirements as they relate to the 2016 legislation (as subsequently described, and as subsequently amended) for the proposed residential component of the Innovation Town Center Residential Development (the “Residential Development”). More specifically, this document addresses legislative requirements and County policy related to “proffers” that the applicant has elected to propose with the rezoning for the Residential Development.

### LEGISLATION PERTAINING TO RESIDENTIAL PROFFERS

Section 15.2-2303.4 of the Code of Virginia (the “Residential Proffer Legislation”), as it was amended effective July 1, 2019, places certain limitations on proffers for residential rezoning cases filed after July 1, 2016, or July 1, 2019. As stipulated by the Residential Proffer Legislation, and unless an applicant elects to apply for a rezoning pursuant to Subsection D of that statute, a local government may only request or accept a proffer if it addresses an impact that is specifically attributable to a proposed new residential development, and, if it is an offsite proffer, it addresses an impact to an offsite public facility, such that (a) (i) the new residential development creates a need, or an identifiable portion of a need, for one or more public facility improvements in excess of existing public facility capacity at the time of the rezoning, and (b) (ii) each such new residential development applied for receives a direct and material benefit from a proffer made with respect to any such public facility improvements. For the purposes of the statute, a locality may base its assessment of public facility capacity on the projected impacts specifically attributable to the new residential development.

The rezoning application was filed prior to July 1, 2016. Subsequent to the filing of the rezoning application, the County eliminated their level of service policy and initiated a zoning text amendment to create the Innovation Small Area Plan. This plan was approved by the Board of County Supervisors’ in December 2020. As such, the Applicant (as subsequently described) is submitting this application pursuant to Section 15.2-2303.4D.

The Residential Proffer Legislation designates four categories of public improvements and facilities, which are as follows:

- **Public school facility improvements:** construction of new primary and secondary public schools or expansion of existing primary and secondary schools, to include all buildings, structures, parking, and other costs directly related thereto;
- **Public safety facility improvements:** construction of new law enforcement, fire, emergency, medical, and rescue facilities or expansion of existing public facilities, to include all buildings, structures, parking and other costs directly related thereto;

- **Public park facility improvements:** construction of public parks or improvements and/or expansion of existing public parks, with “public parks” including playgrounds and other recreational facilities; and
- **Public transportation facility improvements:** construction of new roads; improvement or expansion of existing roads and related appurtenances as required by applicable standards of the Virginia Department of Transportation, or the applicable standards of a locality; and construction, improvement, or expansion of buildings, structures, parking, and other costs directly related to transit.

According to the statute, expenses of an existing public facility, such as ordinary maintenance or repair, or any capital improvement to an existing public facility, such as a renovation or technology upgrade, that does not expand the capacity of such facility shall not be included. In addition, all proffers will be deemed unreasonable unless the proffer addresses an impact to public facilities that is specifically attributable to the proposed residential development and for which there will not be adequate existing capacity for the proposed residential development.

This document addresses the projected impact of the Residential Development on the foregoing infrastructure categories to which residential proffers may be directed. A separate traffic impact analysis (the “Traffic Impact Analysis”) was also prepared by Gorove Slade. This document summarizes the estimated traffic impacts from the proposed development. Additionally, while not required by the Residential Proffer Legislation, this document addresses the projected impact of the Residential Development on public library facilities.

### PROFFER JUSTIFICATION NARRATIVE REQUIREMENT

In response to the Residential Proffer Legislation, the County adopted policies to ensure any proffer requested or accepted meets the standards mandated by it. Among them is the requirement that any residential rezoning or proffer amendment application subject to the residential proffer legislation include a justification narrative identifying impacts to public facility improvements. The requirement further states that the justification narrative must, in detail:

- Identify all of the impacts of the proposed rezoning/proffer amendment;
- Propose specific and detailed mitigation strategies and measures to address all of the impacts of the proposed rezoning/proffer amendment;
- Address whether all of the mitigation strategies and measures are consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation; and
- Demonstrate the sufficiency and validity of those mitigation strategies using professional best accepted practices and criteria, including all data, records, and information used by the applicant or its employees or agents in identifying any impacts and developing any proposed mitigation strategies and measures.

## Proffer Justification Narrative

*This document focuses on the identification of potential impacts to public facility improvements resulting from the proposed Residential Development.*

Subsequent sections of this document provide a detailed description of the Residential Development and its potential impacts on public facility improvements. This document also provides a detailed explanation of the methodology employed in calculating these impacts.

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*II. The Residential Development*

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THE RESIDENTIAL DEVELOPMENT

As proposed by Stanley Martin Homes (the “Developer,” or the “Applicant”), the Residential Development consists of 370 for-sale single-family attached townhome units and 662 multi-family units on an approximately 106-acre site. The Residential Development is part of a larger, mixed-use development, the plans for which also include approximately 533,000 square feet of commercial space.

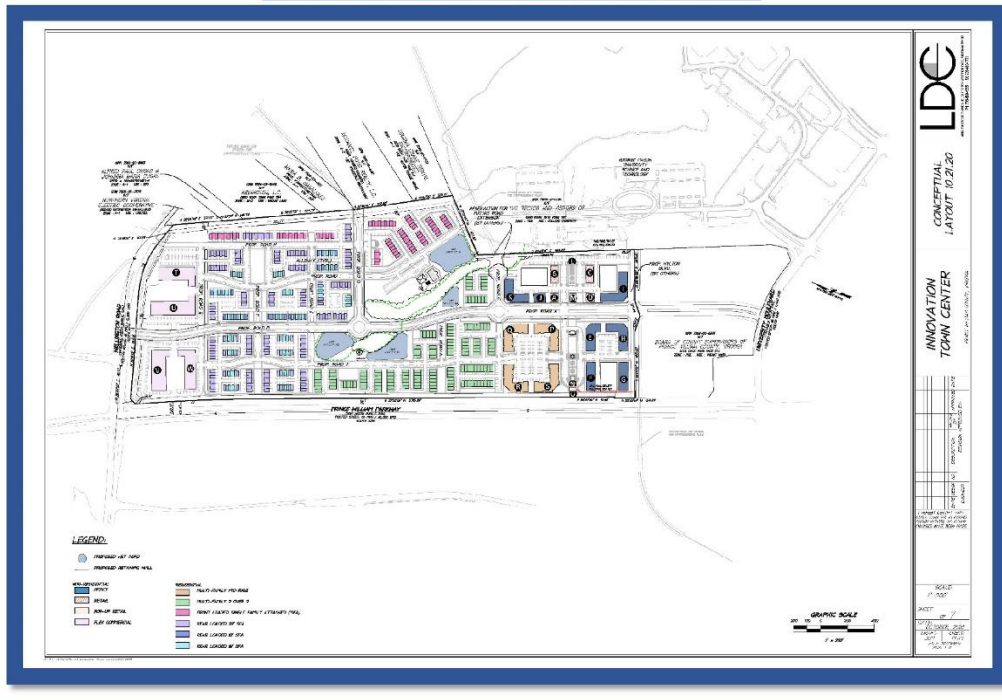
The site of the proposed Residential Development is located on the east side of Prince William Parkway and south of Wellington Road. The site is expected to include parcel 7596-92-6825 and a small portion of parcel 7596-00-5505.

The Residential Development site is currently zoned A-1(Agricultural, which allows one detached single-family unit per ten acres) and PBD (Planned Business District). The maximum residential development allowed “by-right” under current zoning is estimated to be 10 single-family detached units. Accordingly impacts herein are estimated for those units net of the 10 “by-right” single-family detached units. According to County Assessor records, there are no existing improvements on the site.

The site (see Exhibit A) is included in the Innovation Park Small Area, which is dedicated to “support and protect the presence of the university and to encourage economic development opportunities.” This project represents a part of the Town Center Mixed Use subdistrict, which is anticipated to “reflect urban design characteristics,” as defined in the Innovation Park Small Area Plan. The Small Area Plan also designates portions of the property as Community Mixed Use (CMU) and Technology Flex (TF).

# Proffer Justification Narrative

EXHIBIT A: INNOVATION TOWN CENTER SITE PLAN



MuniCap | 5



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## *III. Public Facility Improvement Impacts*

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### OVERVIEW

As mentioned, this document includes a calculation of public facility impacts, which are detailed in the subsequent sub-sections. Included in each section is a discussion of the methodology employed in estimating impacts. The included subsections are as follows:

- **Public school facility improvements** – In keeping with County practices, separate impacts are calculated for elementary, middle, and high schools, and are based on projected incremental additional students that will result from the Residential Development.
- **Public library facility improvements** – Impacts are based on projected incremental additional residents that will result from the Residential Development.
- **Public safety facility improvements** – In keeping with County practices, impacts are calculated for both police services and fire and rescue services; impacts are based on projected incremental additional residents that will result from the Residential Development.
- **Public park facility improvements** – Impacts are based on projected incremental additional residents that will result from the Residential Development.
- **Transportation facility improvements** – Impacts are based on the projected percentage of total site trips at intersections from the proposed development, as described in the Traffic Impact Analysis.

*It should be noted that level of service (“LOS”) standards shown herein represent the County standards as described in the County Comprehensive Plan and the Innovation Park Small Area Plan. In many cases, the current LOS provided to this consultant by the County is significantly less than the stated LOS standard. Any calculation of proffers will take into account the LOS standard as set out in the plans, the current County LOS, and the amount pledged in the County’s Capital Improvement Plan to raise the current County LOS to meet the planned LOS standard.*

**III-A. Public School Facility Improvement Impacts**

**METHODOLOGY**

To project impacts to public school facility improvements, MuniCap used generation factors used by Prince William County Public Schools and confirmed by the Developer. These factors are calculated separately by school type (elementary, middle, and high school) and by unit type (single-family, townhouse, and multi-family). Historical student generation factors are shown below in Table III-A.1.

**TABLE III-A.1  
Current and Historical Student Generation Factors**

Historical Data											
Year	School Type	Unit Type			Total	Year	School Type	Unit Type			Total
		Single Family	Townhouse	Multi-Family				Single Family	Townhouse	Multi-Family	
2020-21	Elementary	0.273	0.278	0.160	0.251	2014-15	Elementary	0.294	0.285	0.176	0.272
	Middle	0.159	0.143	0.073	0.138		Middle	0.156	0.128	0.070	0.134
	High	0.228	0.187	0.087	0.189		High	0.206	0.155	0.085	0.172
	Total	0.660	0.608	0.320	0.578		Total	0.656	0.569	0.331	0.578
2019-20	Elementary	0.292	0.289	0.172	0.267	2012-13	Elementary	0.302	0.287	0.184	0.279
	Middle	0.164	0.145	0.076	0.142		Middle	0.156	0.120	0.075	0.133
	High	0.230	0.185	0.088	0.189		High	0.205	0.147	0.083	0.169
	Total	0.686	0.619	0.336	0.598		Total	0.662	0.554	0.342	0.582
2018-19	Elementary	0.292	0.288	0.175	0.268	2010-11	Elementary	0.301	0.258	0.167	0.268
	Middle	0.163	0.144	0.075	0.140		Middle	0.152	0.111	0.067	0.127
	High	0.224	0.179	0.085	0.185		High	0.202	0.139	0.072	0.164
	Total	0.680	0.611	0.335	0.592		Total	0.655	0.509	0.306	0.560
2016-17	Elementary	0.300	0.313	0.163	0.273	2008-09	Elementary	0.298	0.245	0.142	0.258
	Middle	0.159	0.145	0.064	0.135		Middle	0.148	0.107	0.055	0.122
	High	0.220	0.184	0.082	0.181		High	0.206	0.139	0.069	0.166
	Total	0.679	0.642 <sup>1</sup>	0.309 <sup>1</sup>	0.590		Total	0.652	0.491	0.265	0.546

Source: *Student Generation Factors by School Level and Housing Unit Type, Prince William County Public Schools (2020-21)*.

MuniCap then applied these student generation factors to the proposed units within the Residential Development that are in excess of the development that would be allowed under the current zoning designation. For purposes of this exercise, it is assumed that all of the projected students are new to the County, rather than relocated from elsewhere within the Prince William County Public Schools system.

Finally, MuniCap identified the schools that will be impacted by the Residential Development based on school boundaries and researched the projected capacity at each applicable school. MuniCap then determined whether the projected net student impacts represented a burden beyond projected school capacity.

PROJECTED NET STUDENT IMPACTS

As previously described, the Residential Development includes 370 single-family attached (“townhome”) units and 662 multi-family units. Based on projected development and the student generation factors identified in Table III-A.1, the proposed development will generate an estimated 437 total students, 7 of whom are estimated to be generated by-right. As shown in Table III-A.2 below, the Residential development is estimated to create 430 new students, which is the total estimated number of students generated less the estimated number of by-right students

TABLE III-A.2  
 Projected Student Generation – Innovation Town Center Residential Development

<i>School Type</i>	<i>Units<sup>(a)</sup></i>	<i>Unit Type</i>	<i>Generation Factor<sup>(b)</sup></i>	<i>Total Projected Students</i>
Elementary	370	Townhome	0.278	<b>102.86</b>
Middle	370	Townhome	0.143	<b>52.91</b>
High	370	Townhome	0.187	<b>69.19</b>
<i>Subtotal - Townhome:</i>				<i>224.96</i>
Elementary	662	Multi-family	0.160	<b>105.92</b>
Middle	662	Multi-family	0.073	<b>48.33</b>
High	662	Multi-family	0.087	<b>57.59</b>
<i>Subtotal - Multi-family:</i>				<i>211.84</i>
<b>Elementary</b>	<b>1032</b>		0.202	<b>208.78</b>
<b>Middle</b>	<b>1032</b>		0.098	<b>101.24</b>
<b>High</b>	<b>1032</b>		0.123	<b>126.78</b>
<b>Total proposed</b>				<b>436.80</b>
Elementary	10	Single-family detached	0.273	<b>(2.73)</b>
Middle	10	Single-family detached	0.159	<b>(1.59)</b>
High	10	Single-family detached	0.228	<b>(2.28)</b>
<b>Less: Total-by-right</b>				<b>(6.60)</b>
Elementary	1032			<b>206.05</b>
Middle	1032			<b>99.65</b>
High	1032			<b>124.50</b>
<b>Total</b>				<b>430.20</b>
(a) Source: Stanley Martin Homes.				
(b) See Table III-A.1.				

**CURRENT CAPACITY OF PUBLIC SCHOOL FACILITIES**

The public school facilities potentially impacted by the Residential Development are: Ellis Elementary School, Marstellar Middle School, and Unity Reed High School. Table III-A.3 below shows the projected capacity and enrollment at each school for the 2023-24 school year, which represents the year development is assumed to be completed.

**TABLE III-A.3**  
**County School Facilities – Current Capacity and Enrollment**

<i>School</i>	<i>Projected Capacity<sup>(a)</sup></i>	<i>Projected Enrollment<sup>(a)</sup></i>	<i>Excess Capacity</i>
Ellis Elementary School	492	443	49
Marstellar Middle School	1,233	1,149	84
Unity Reed High School <sup>(b)</sup>	2,409	2,052	357
<b>Total</b>	<b>4,134</b>	<b>3,644</b>	<b>490</b>
(a) Capacity and enrollment shown is for 2023-2024, which represents the year development is assumed to be completed. Source: Prince William County Public Schools Enrollment Forecast. (b) Stonewall Jackson High School was renamed Unity Reed High school in the summer of 2020.			

**Elementary School Facilities**

The Residential Development site is located within the Ellis Elementary School boundaries (see Exhibit C). According to Prince William County Public Schools, the school has a planning capacity of 492 students and a projected enrollment of 443 students, meaning that the school has projected excess capacity of 49 students. The 206 projected elementary school students that will be created by the Residential Development are in excess of projected capacity and represent an additional need for Prince William County Public Schools facilities.

Additionally, Ellis Elementary School is located within the Sudley planning area. According to school enrollment and capacity projections provided by the County (see Appendix A), by 2023-24 the Sudley planning area will have excess capacity for 49 students. The 206 projected elementary school students will also be in excess of projected capacity at the planning area level.

**Middle School Facilities**

The Residential Development site is located within the Marstellar Middle School boundaries (see Exhibit D). According to Prince William County Public Schools, the school has a planning capacity of 1,233 students and a projected enrollment of 1,149 students, meaning that the school has projected excess capacity of 84 students. The 100 projected middle school students that will be created by the Residential Development are in excess of projected capacity and represent an additional need for Prince William County Public Schools facilities.

**High School Facilities**

The Residential Development site is located within the Unity Reed High School boundaries (see Exhibit E). According to Prince William County Public Schools, the school has a planning capacity of 2,409 students and a projected enrollment of 2,052 students, meaning that the school has projected

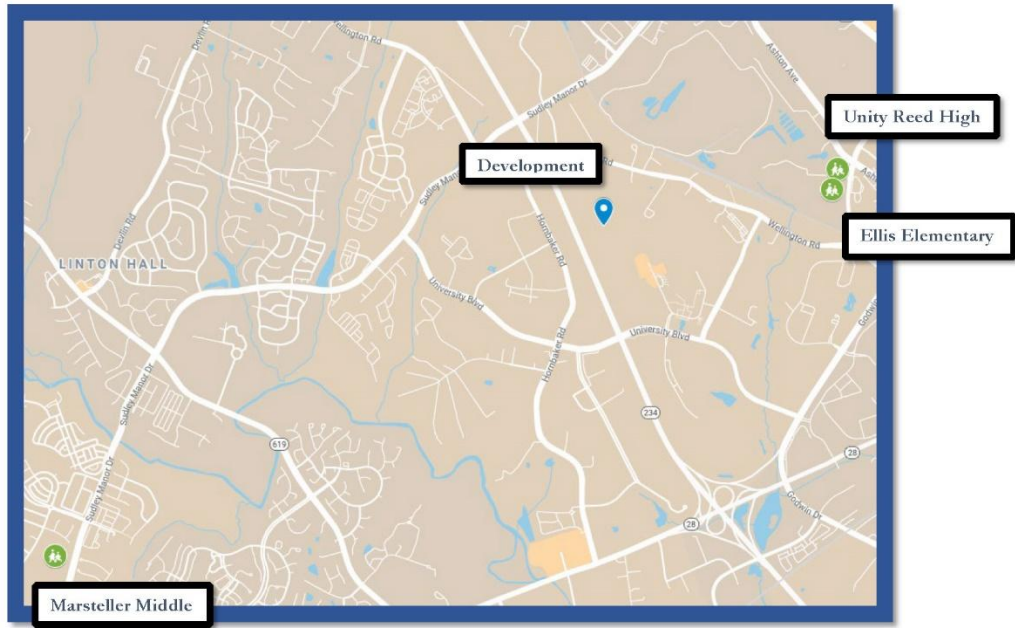
## Proffer Justification Narrative

excess capacity of 357 students. Therefore, the 125 projected high school students that will be created by the Residential Development are not in excess of projected capacity and do not represent an additional need for Prince William County Public Schools facilities.



# Proffer Justification Narrative

EXHIBIT B: AREA MAP (DEVELOPMENT SITE & SCHOOL FACILITIES)

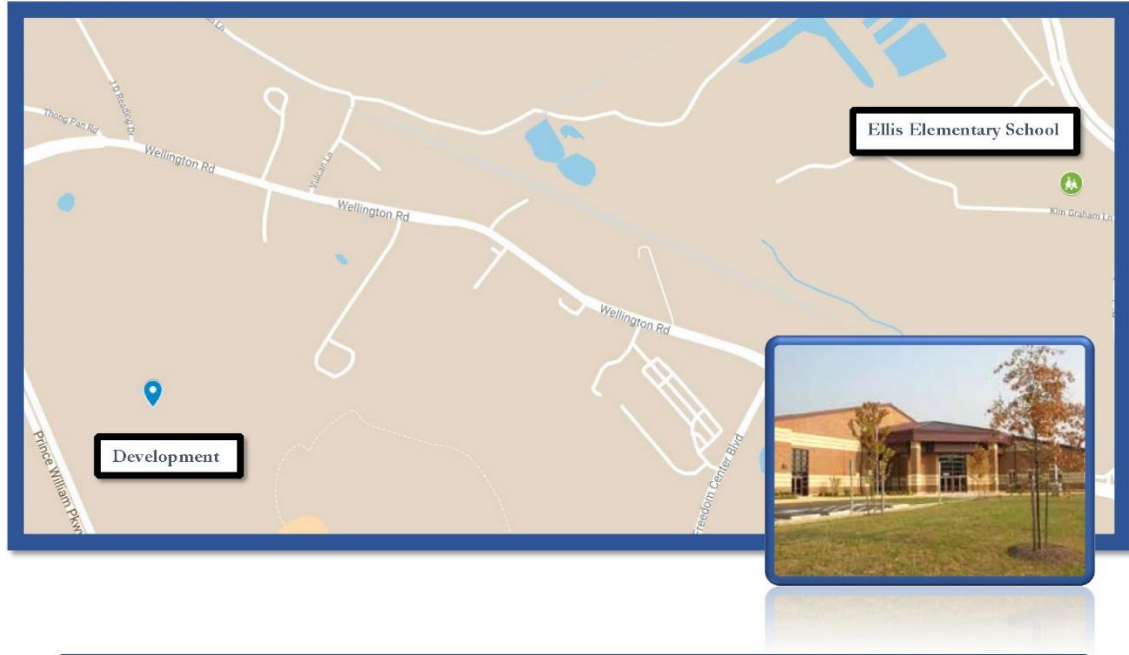


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EXHIBIT C: AREA MAP (DEVELOPMENT SITE, ELLIS ELEMENTARY SCHOOL)



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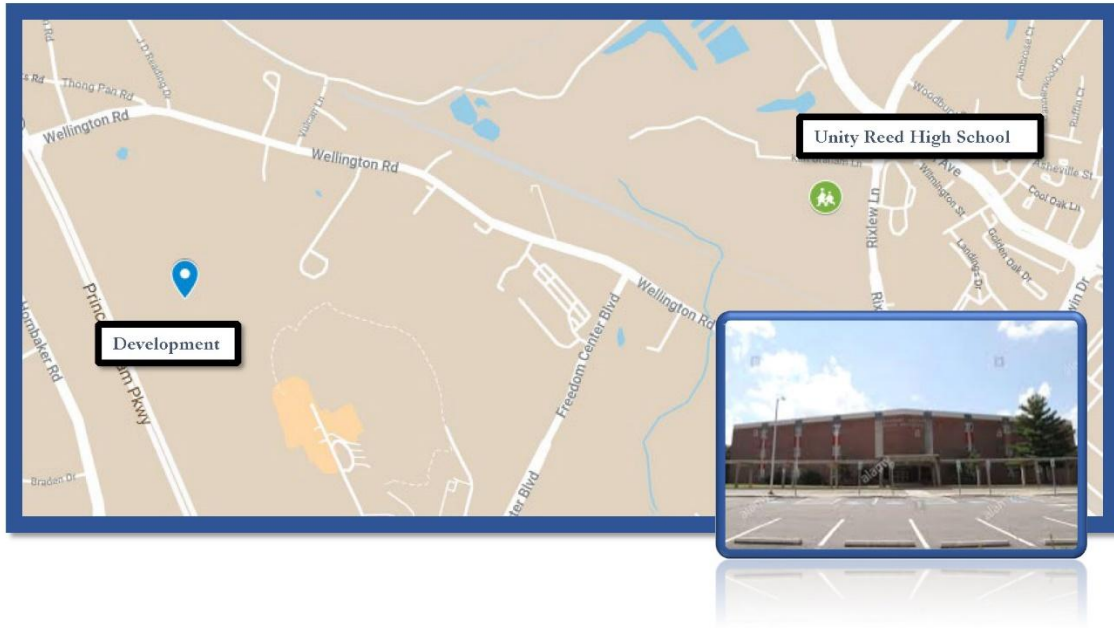
EXHIBIT D: AREA MAP (DEVELOPMENT SITE, MARSTELLER MIDDLE SCHOOL)



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EXHIBIT E: AREA MAP (DEVELOPMENT SITE, UNITY REED HIGH SCHOOL)



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MITIGATION STRATEGIES

As previously mentioned, the projected students resulting from the Residential Development are in excess of projected capacity at Ellis Elementary School and Marsteller Middle School. Accordingly, the estimated cost of public school facilities resulting from the additional elementary and middle school students is shown in Table III-A.4 below and on the following page.

**TABLE III-A.4**  
**Projected School District Impact**

<i>School Impact for Proposed Zoning Reclassification</i>	
<b><u>Elementary School</u></b>	
(a) Estimated cost of new elementary school <sup>(a)</sup>	\$57,077,041
(b) Small Area Plan's additional need percentage <sup>(b)</sup>	47%
(c) Cost of school allocated to Small Area Plan (a × b)	\$26,826,209
(d) Projected students at Development after by-right allocation <sup>(c)</sup>	206
(e) Elementary school excess capacity <sup>(d)</sup>	49
(f) Projected students above school capacity (d - e)	157
(g) Total projected elementary school students in Small Area Plan <sup>(e)</sup>	739
(h) Development's percentage of total students in Small Area Plan (f ÷ e)	21%
<b>(i) Sub-total: elementary school proffer contribution for Development (c × h)</b>	<b>\$5,701,023.24</b>

*Table III-A.4 continues on the following page.*



## Proffer Justification Narrative

<b>Middle School</b>	
(j) Estimated cost of new middle school <sup>(f)</sup>	\$58,119,000
(k) Small Area Plan's additional need percentage <sup>(b)</sup>	13%
(l) Cost of school allocated to Small Area Plan (j × k)	\$7,555,470
(m) Projected students at Development after by-right allocation <sup>(c)</sup>	100
(n) Middle school excess capacity <sup>(d)</sup>	84
(o) Projected students above school capacity (m - n)	16
(p) Total projected middle school students in Small Area Plan <sup>(e)</sup>	268
(q) Development's percentage of total students in Small Area Plan (o ÷ p)	6%
<b>(r) Sub-total: middle school proffer contribution for Development (l × q)</b>	<b>\$441,092.85</b>
<b>(s) Total Development Proffer Contribution: Public School Facilities (i + r)</b>	<b>\$6,142,116.09</b>
<b>(t) Percentage of total students generated: townhome units</b>	<b>52%</b>
<b>(u) Percentage of total students generated: multi-family units</b>	<b>48%</b>
<b>Proffer contribution: per townhome unit (s × t ÷ 370)</b>	<b>\$8,549.47</b>
<b>Proffer contribution: per multi-family unit (s × u ÷ 662)</b>	<b>\$4,499.72</b>
<p>(a) Cost of new elementary school is based on the estimated per student cost (\$61,772) of the Occoquan/Woodbridge Area Elementary School (\$40,584,000 ÷ 657 students) as described in the draft Prince William County Public Schools Capital Improvements Program Fiscal Years 2022-2031. This cost per student is multiplied by the projected student capacity of a new elementary school (924). Source for student capacity: Innovation Park Small Area Plan LOS case file provided by Prince William County.</p> <p>(b) Source: page 206 of the Innovation Park Small Area Plan. This figure indicates the percentage of a new school that is needed to serve the students in the Small Area Plan.</p> <p>(c) See Table III-A.2.</p> <p>(d) See Table III-A.3.</p> <p>(e) Source: Innovation Park Small Area Plan LOS case file provided by Prince William County.</p> <p>(f) Source: Prince William County Public Schools Capital Improvements Program Fiscal Years 2022-31. Based on Potomac Shores Middle School.</p>	

The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.

**III-B. Public Library Facility Improvement Impacts**

**METHODOLOGY**

To estimate impacts on public library facilities, MuniCap first estimated the total population that will reside within the proposed Residential Development based on residential unit generation factors from the Prince William County Geographic Information Systems Demographics Quarterly Estimates as of December 31, 2020.

MuniCap then applied the LOS standards for public library services as identified in the County Comprehensive Plan (Community Education), approved March 10, 2020, to calculate the impact of the Residential Development on public library services. MuniCap then compared the existing capacity at the relevant public library facilities to the forecasted increase in required services resulting from the proposed development and determined whether the projected demand exceeded current capacity.

**PROJECTED NET RESIDENT IMPACTS**

As previously described, the Residential Development includes of 370 for-sale single-family attached townhome units and 662 multi-family units. Based on projected development and the average occupancy of residential units in the County, the proposed development will house an estimated 2,671 residents above by-right, as shown below in Table III-B.1.

**TABLE III-B.1  
Projected Residents – Innovation Town Center Residential Development**

<i>Unit Type</i>	<i>Units<sup>(a)</sup></i>	<i>Residents Per Unit</i>	<i>Total Projected Residents</i>
(a) Townhome units <sup>(a)</sup>	370	3.16	1,169
(b) Multi-family units	662	2.32	1,536
(b) Less single-family detached units allowed by-right <sup>(b)</sup>	10	3.37	(34)
<b>Total: Above by-right (a-b)</b>	<b>1,022</b>	<b>2.61</b>	<b>2,671</b>
(a) Source: Stanley Martin Homes			
(b) Residential unit generation factors. Source: Prince William County Geographic Information Systems Demographics Quarterly Estimates (as of 12/31/2020).			

**CURRENT CAPACITY OF PUBLIC LIBRARY FACILITIES**

The County LOS standards for library facilities are broken down into travel time, square footage needs, and collection sizes. Table III-B.2 on the following page summarizes the LOS standard for travel time according to the County Comprehensive Plan.

TABLE III-B.2  
Library Level of Service Standards – Driving Time

<i>Library Facility Type</i>	<i>Urban (Development Area) Drive Time</i>
Regional Library	20 Minutes
Community Library	10 Minutes
Neighborhood Library	5 Minutes

Based on the location of the Residential Development and the available library facilities, the Residential Development will have access to the following libraries from each classification (see Exhibit F), as shown in Table III-B.3 below. The estimated driving time to each library facility from the Residential Development is also provided below.

TABLE III-B.3  
Libraries Serving Development – Drive Time

<i>Library Facility</i>	<i>Classification</i>	<i>Distance from Development</i>	<i>Drive Time Estimate</i>
Bull Run Regional Library	Regional	2.2 miles	6 minutes
Central Community Library	Community	5.9 miles	16 minutes
Nokesville Neighborhood Library	Neighborhood	6.5 miles	12 minutes

As shown by Table III-B.3 above, the Residential Development is not within the specified travel times for both community and neighborhood libraries. Further, as described in the Innovation Park Small Area Plan, the Small Area will likely require an additional community or neighborhood library. Therefore, the projected impact on public library facilities that will be generated by the Residential Development represents a requirement beyond existing capacity.

County LOS standards for library facilities also include facility requirements of 0.6 square feet per capita and 2.5 materials (books) per capita. The projected demand created by the Residential Development is shown below and on the following page in Table III-B.4 and Table III-B.5.

TABLE III-B.4  
Projected Library Facility Impacts – Square Feet

<i>Projected Resident Impact</i>	<i>Square Feet per Capita</i>	<i>Additional Facility Sq. Ft. Requirement</i>
2,671	0.6	1,603

TABLE III-B.5  
Projected Library Facility Impacts – Materials

<i>Projected Resident Impact</i>	<i>Materials per Capita</i>	<i>Additional Materials Requirement</i>
2,671	2.5	6,678

As shown in Tables III-B.4 and III-B.5, the projected demand created by the Residential Development is an additional 1,603 in library square feet and 6,678 in additional materials.

EXHIBIT F: AREA MAP (DEVELOPMENT SITE & LIBRARY FACILITIES)



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# Proffer Justification Narrative

## MITIGATION STRATEGIES

Any proffer related to public libraries must only mitigate the costs reasonably attributable to an increase in population. The County Capital Improvement plan does not include planned capital improvements that increase capacity. However, the Applicant has decided to make a proffer contribution based on the Residential Development's impact on public library facilities.

Proffers for public library facilities are calculated in accordance with County LOS standards and shown below in Table III-B.6. The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.

**TABLE III-B.6**  
**Proffer Estimates - Projected Public Library Facility Impacts**

<b>Public Library Impact for the Proposed Zoning Reclassification</b>	
<b>Library - Square Feet</b>	
(a) Required additional square feet <sup>(a)</sup>	1,603
(b) Estimated cost per square foot of a community library <sup>(b)</sup>	\$252
<b>(c) Library (square feet) proffer required for Residential Development (a × b)</b>	<b>\$403,378.68</b>
<b>Library - Collection Size</b>	
(d) Required additional books <sup>(c)</sup>	6,678
(e) Estimated cost per book <sup>(d)</sup>	\$30
<b>(f) Library (collection size) proffer required for Residential Development (d × e)</b>	<b>\$200,327.54</b>
<b>(g) Total proffer for development in units in excess of by-right (c + f)</b>	<b>\$603,704.22</b>
<b>(h) Percentage of total residents generated: townhome units</b>	<b>43%</b>
<b>(i) Percentage of total residents generated: multi-family units</b>	<b>57%</b>
<b>Proffer contribution: Per townhome unit (g × h ÷ 370)</b>	<b>\$705.24</b>
<b>Proffer contribution: Per multi-family unit (g × i ÷ 662)</b>	<b>\$517.77</b>
(a) See Table III-B.4.	
(b) Represents the estimated cost per square foot of a 20,000 square foot community library. Cost estimate was calculated using Marshall & Swift "Commercial Estimator 7" software.	
(c) See Table III-B.5.	
(d) Cost per book was determined by taking the estimated total book cost (\$418,000) and dividing it by the total required additional books (13,935), as outlined on page 207 of the Innovation Park Small Area Plan.	

**III-C. Public Safety Facility Improvement Impacts**

**METHODOLOGY**

MuniCap applied the LOS standards for various public safety services as identified in the County Comprehensive Plan, approved June 18, 2019, to calculate the impact of the Residential Development on public safety services. MuniCap then compared the existing capacity at the relevant public safety facilities to the forecasted increase in required services resulting from the proposed development and determined whether the projected demand exceeded current capacity.

**CURRENT CAPACITY OF PUBLIC SAFETY FACILITIES**

**Police Facilities**

The County LOS standards for police work suggest two sworn officers per 1,000 residents. In addition, the facility requirements for the Prince William County Police Department are 250 square feet per sworn officer with a building minimum size of 50,000 square feet. Therefore, the projected impact created by the additional 2,671 residents estimated for the Residential Development is 1,336 square feet, as shown below in Table III-C.1.

**TABLE III-C.1  
Projected Police Station Facility Impacts**

<i>Projected Resident Impact</i>	<i>Officers Per 1,000 Residents</i>	<i>Officer Requirement</i>	<i>Facility Sq. Ft. Per Officer</i>	<i>Additional Facility Sq. Ft. Requirement</i>
2,671	2	5	250	1,336

The project site is within the Innovation Park Small Area, which is serviced by the Western District Police Station (see Exhibit G). According to the Prince William County Police Department, there are currently 129 sworn officers employed at the Western District Police Station. Based on Prince William County Assessor records, the station is 51,137 square feet, implying a capacity to accommodate 205 officers (51,137 total square feet ÷ 250 feet per officer). This means that the station currently has the excess capacity for 76 additional sworn officers, representing 18,887 square feet of facility space (76 officers × 250 square feet). Therefore, the projected residents associated with the Residential Development are not anticipated to place demands on police station facilities in excess of current capacity.

County LOS standards for police facilities also include requirements for animal control, training, and administrative support facilities. The projected demand created by the Residential Development is shown on the following page in Table III-C.2.

TABLE III-C.2  
Other Projected Police Facility Impacts

<i>Facility Type</i>	<i>Projected Resident Impact</i>	<i>Sq. Ft. Required per 1,000 Residents</i>	<i>Additional Facility Sq. Ft. Requirement</i>
Animal control	2,671	67	179
Training	2,671	324	866
Administrative support	2,671	274	732

Source: Prince William County Comprehensive Plan Safety and Secure Community.

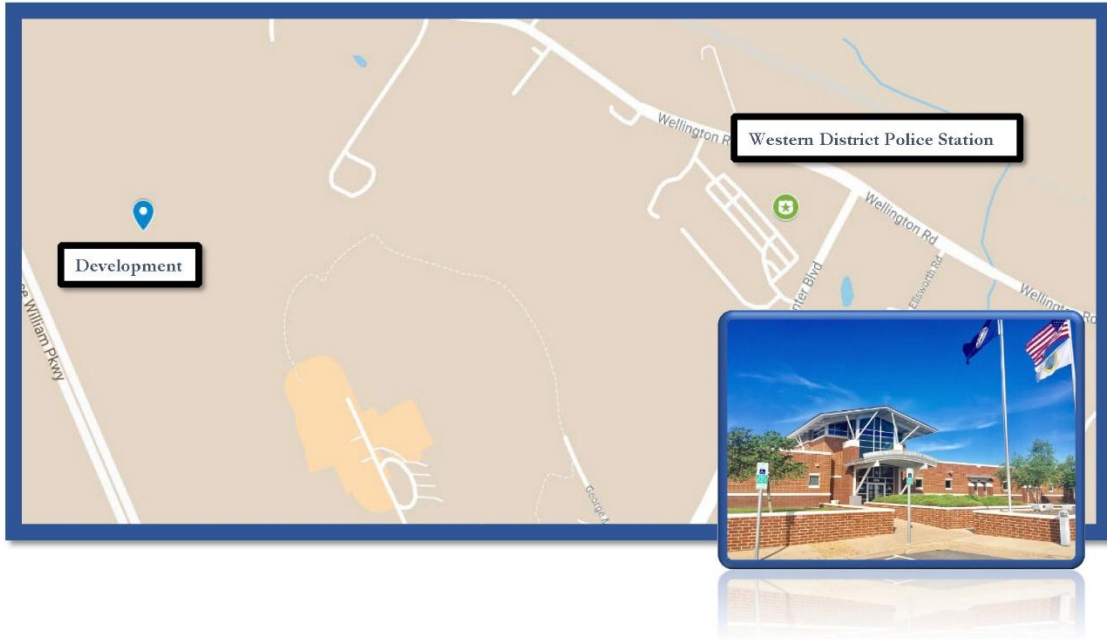
The County LOS standard for animal control facilities is 67 square feet per 1,000 residents. According to the Prince William County Geographic Information Systems Demographics Quarterly Estimates, the total population of Prince William County is estimated at 469,236 people as of December 31, 2020. This translates to a need for 31,439 square feet of animal control facility space (67 square feet per thousand residents × 469,236 thousand residents). Based on County Assessor data, the existing Prince William County Animal Shelter includes 8,032 square feet of animal control facility space, implying that the shelter is already over capacity and cannot accommodate any additional demand. Therefore, the projected impact of 179 square feet in necessary animal control facility space that will be generated by the Residential Development represents a requirement in excess of current capacity. According to the County Capital Improvement Program for Fiscal Years 2021 – 26, there is currently a project in place to renovate the animal control facility. The estimated costs of this expansion were used as the basis of cost for impacts as described on the following pages.

The County LOS standard for police training facilities is 324 square feet per 1,000 residents. Therefore, the County’s current population of 469,236 creates a need for 152,032 square feet of police training facility space (324 square feet per thousand residents × 469,236 thousand residents). Based on County Assessor data, the existing County Public Safety Training Center includes 54,651 square feet of space. This suggests that existing police training facility space is inadequate and cannot accommodate any additional demand. Therefore, the projected impact of 866 square feet in necessary police training facility space that will be generated by the Residential Development represents a requirement in excess of current capacity. According to the Capital Improvement Program for Fiscal Years 2021 – 26, there is currently a project in place to expand the Training Center. Impact costs were estimated based on a shared portion of the cost of this project as described below.

The County LOS standard for police administrative support facilities is 274 square feet per 1,000 residents. Therefore, the County’s current population of 469,236 creates a need for 128,571 square feet of police administrative facility space (274 square feet per thousand residents × 469,236 thousand residents). At the time of this writing, total existing administrative support facility space was unavailable, as much of this space is leased and not consolidated with other Police Department operations. The Applicant will coordinate with appropriate County staff to determine whether the projected impact of 732 square feet in necessary police administrative support facility space that will be generated by the Residential Development represents a requirement that exceeds current capacity.

# Proffer Justification Narrative

EXHIBIT G: AREA MAP (DEVELOPMENT SITE & POLICE DEPARTMENT FACILITY)



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**Fire and Rescue Facilities**

The County LOS standards for fire and rescue facilities servicing residential development are broken down into incident capacity and travel times. Table III-C.3 summarizes the LOS standards according to the County Comprehensive Plan.

**TABLE III-C.3  
Prince William County Fire and Rescue Level of Service Standards**

**A. Travel Times**

Area	First Unit Travel Time in Minutes
Fire Suppression Emergency Standard - (Countywide)	4.0
Basic Life Support (BLS) Emergency Standard - (Countywide)	4.0
Advanced Life Support (ALS) Emergency Standard (Countywide)	8.0
(a) Source: Prince William County Comprehensive Plan Safety and Secure Community Table 4.	

**B. Workload**

Factor	Standard
Responses per Tactical Unit	2,000 per year
(a) Source: Prince William County Comprehensive Plan Safety and Secure Community Table 5.	

According to the County Fire Department, the fire and rescue facility that primarily serves the Residential Development’s location is Station 25, located approximately 3.12 miles away and estimated to be a travel time of five minutes using Prince William County Fire & Rescue Station Finder. According to the Innovation Park Small Area Plan, the Small Area is currently not within the four-minute travel time for fire suppression and basic life support standard for any nearby stations.

Workload statistics specific to Station 25 indicate that the station serves over 30,000 residents and answered 3,700 calls in 2019, which suggests a call volume per resident of 0.12 (calculated as 3,700 calls ÷ 30,000 residents). Using this call volume per resident, the 2,671 projected residents in units above by-right at the Residential Development would increase overall call volume by 329 calls, as shown in Table III-C.4 on the following page.

It should be noted that there is a new fire station that is anticipated to open in March 2021 that may relieve pressure on Station 25. Nevertheless, this analysis does not assume any changes in call volume for Station 25 as a result of this new station.



TABLE III-C.4  
Projected Fire and Rescue Facility Impacts

<i>Projected Resident Impact <sup>(a)</sup></i>	<i>Average Annual Incident Rate <sup>(b)</sup></i>	<i>Projected Annual Increase</i>
2,671	0.12	329
(a) See Table III-B.1.		
(b) Source: Nokesville Volunteer Fire Department.		

As the development is outside of the response time of existing fire stations, incidents attributable to the development are assumed to be in-excess of existing capacity.

# Proffer Justification Narrative

EXHIBIT H: AREA MAP (DEVELOPMENT SITE & FIRE STATION #25 FACILITY)



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## MITIGATION STRATEGIES

Because the excess capacities at the Western District Police Station exceed the projected increase in service demand by the Residential Development, any proffer related to police station facility costs would be inappropriate under the Residential Proffer Legislation. Also, the County Capital Improvement Plan does not include any capital improvements to Police Administrative facilities that increase capacity in the Residential Development's service area. As such, any proffer related to such facilities would be inappropriate under the Residential Proffer Legislation.

As previously described, the incident rate of Station 25 was used to estimate the incident increase attributable to the Residential Development. As development is outside of the response time of existing fire stations, incidents attributable to the development are assumed to be in excess of existing capacity and considered eligible for proffers.

Proffers for eligible public safety facilities are calculated in accordance with County LOS standards and shown on the following page in Table III-C.5. The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.

# Proffer Justification Narrative

**TABLE III-C.5**  
**Proffer Estimates - Projected Public Safety Facility Impacts**

<b>Public Safety Impact for the Proposed Zoning Reclassification</b>	
<b>Police - Animal Control</b>	
(a) Current population of Prince William County <sup>(a)</sup>	469,236
(b) Projected residents above by-right <sup>(b)</sup>	2,671
(c) Total projected County population (a + b)	471,907
(d) Total cost of Animal Shelter Expansion/Renovation <sup>(c)</sup>	\$5,370,000
<b>(e) Building cost per capita (d ÷ c)</b>	<b>\$11.38</b>
<b>(f) Animal Control Proffer required for Residential Development (e × b)</b>	<b>\$30,398.12</b>
<b>Police - Training</b>	
(g) Current population of Prince William County <sup>(a)</sup>	469,236
(h) Projected residents above by-right <sup>(b)</sup>	2,671
(i) Total projected County population above by-right (g + h)	471,907
(j) Total cost of Public Safety Training Center Master Plan <sup>(c)</sup>	\$35,800,000
<b>(k) Project cost per capita (j ÷ i)</b>	<b>\$75.86</b>
<b>(l) Training Proffer required for Residential Development (k × h)</b>	<b>\$202,654.13</b>
<b>Fire &amp; Rescue</b>	
(m) Expected workload capacity of new station (2 tactical units) <sup>(c)</sup>	4,000
(n) Projected incident impact of Development <sup>(d)</sup>	329
(o) Total cost of new station <sup>(c)</sup>	\$14,000,000
(p) Project cost per incident (o ÷ m)	\$3,500.00
<b>(q) Fire &amp; Rescue Proffer required for development (p × n)<sup>(c)</sup></b>	<b>\$1,153,128.43</b>
<b>(r) Fire &amp; Rescue cost per capita (q ÷ b)</b>	<b>\$431.67</b>
<b>Estimated Cost Per Home</b>	
(s) Gross cost per capita (e + k + r)	\$518.91
(t) Residents in development above by-right <sup>(d)</sup>	2,671
<b>(u) Total proffer for development in units in excess of by-right (s × t)</b>	<b>\$1,386,180.68</b>
(v) Proffer credit (1,500 square feet donated to Police) <sup>(f)</sup>	\$290,430.00
<b>(w) Net proffer for development in units in excess of by-right (u -v)</b>	<b>\$1,095,750.68</b>
(x) Percentage of total residents generated: townhome units	43%
(y) Percentage of total residents generated: multi-family units	57%
<b>Proffer contribution: Per townhome unit (w × x ÷ 370)</b>	<b>\$1,280.04</b>
<b>Proffer contribution: Per multi-family unit (w × y ÷ 662)</b>	<b>\$939.78</b>
(a) Source: Prince William County Geographic Information Systems Demographics Quarterly Estimates (as of 12/31/2020).	
(b) See Table III-B.1.	
(c) Source: Prince William County Capital Improvements Program Fiscal Years 2021-26. Amount represents the portion of the \$16.725MM expansion cost balance to be financed.	
(d) See Table III-C.4.	
(e) Fire Station 29 is anticipated to be complete in 2021, which will relieve some of the pressure on Fire Station 25.	
(f) Represents the estimated cost of the 1,500 square feet that will be donated to the Police Department. Cost estimate was calculated using Marshall & Swift "Commercial Estimator 7" software.	

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**III-D. Public Parks Facility Improvement Impacts**

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**METHODOLOGY**

MuniCap applied the LOS standards for public parks as identified in the County Comprehensive Plan – (Parks Recreation and Tourism approved March 10, 2020) and the Innovation Park Small Area Plan to calculate the impact of the Residential Development on public parks services. MuniCap then compared the existing capacity at the relevant public parks facilities to the forecasted increase in required services resulting from the proposed development and determined whether the projected demand exceeded current capacity.

**PROJECTED NET RESIDENT IMPACTS**

As previously described, the Residential Development includes 370 for-sale single-family attached townhome units and 662 multi-family units. Based on projected development and the average occupancy of residential units in the County, the proposed development will house an estimated 2,671 residents above by-right, as shown in Table III-B.1.

**CURRENT CAPACITY OF PUBLIC PARKS FACILITIES**

Based on the County’s established Park Planning Districts, the Residential Development falls within Park Planning District 4. In order to show that the Residential Development’s impacts on the parks system, service area and LOS quality were taken into account, Table III-D.1 below shows the LOS standard for parks and recreation service area requirements.

**TABLE III-D.1**  
Prince William County Parks and Recreation Service Area Standards

<b>PARK TYPE</b>	<b>WALK/BIKE SERVICE AREA</b>	<b>DRIVE TIME SERVICE AREA</b>
Neighborhood	5 to 10-minute walk/bike time; bus stop within ¼-mile, preferred	Less than 10 minutes
Community	10 to 15-minute walk/bike time	10 to 20-minute drive time
Regional	Greater than 15-minute walk/bike time	20 to 30-minute drive time
School/Community-Use	5 to 10-minute walk/bike time	Less than 10 minutes

As described in the Innovation Park Small Area Plan, the small area is not within the service area of any existing neighborhood parks nor is it within the walking/biking range of a community park. The County also identified the importance of linear/greenway trails as a critical level of service metric. As such, the



County has identified neighborhood parks, community parks, and linear/greenway trail improvements as high community priorities for the Innovation Park Small Area.

## MITIGATION STRATEGIES

Any proffer related to public parks must only mitigate the costs reasonably attributable to an increase in population. As previously described, the Innovation Park Small Area Plan identifies park improvements as a high community priority, especially with regard to the construction of neighborhood and community parks. Proffers have been estimated accordingly based on the construction of each type of park within the LOS specified distance from the development. A summary of mitigation strategies follows for the varying park types.

### Neighborhood/School Community Use Park

Neighborhood parks have been identified as a high priority within the Innovation Park Small Area Plan. Generally, neighborhood parks are the smallest parks in size ranging from ¼-acre to 20 acres (less for urban neighborhood parks) and serve the community within a 5 to 10-minute walk/bike time and less than 10-minute drive time. Amenities typically include singular field/open play areas with no standard dimensions, singular courts, picnic pavilion and playgrounds. In conclusion, the Development is creating the desired space and providing amenities to meet the needs of neighborhood parks. As a result of these contributions, no proffer contribution is contemplated. The following neighborhood park amenities will be provided within the development:

- 3,500 square foot pocket park.
- 15,800 square foot pocket park.
- 52,800 square foot nature playground.
- 18,700 square feet nature park.
- 13,200 square foot courtyard green.
- 8,800 square foot pocket park.
- 7,000 square foot bike rest area.
- 193,000 square foot stream preservation area with trail.

### Community Park

Community parks have been identified as a high priority within the Innovation Park Small Area Plan. Community parks are larger than neighborhood parks, usually ranging from 20-100 acres and serve larger areas of the County. Community parks level of service generally include a 10 to 15-minute walk/bike time and 10 to 20-minute drive time. Amenities typically offer multiple recreation fields, courts, picnic pavilions and playgrounds and are generally 50% active and 50% passive. There are currently no community parks within Park Planning District 4, though the Rollins Ford Park in Park Planning District 3 satisfies the level of service requirement for drive time from the Development. Given the needs, the developer has calculated a proffer contribution and believes the development will further supplement the needs of a community park by providing both the amenities as noted under neighborhood parks but also additional amenities as noted below.

The community park proffer contribution is provided in Table III-D.2 and is based on the community park needs for the Brentsville Magisterial District. The Rollins Ford Community Park is being developed

in Park Planning District 3 and will service Brentsville Magisterial District and surrounding communities. Rollins Ford Community Park is within a 10 to 20-minute driving time to the project, which is within the guidelines for proximity per LOS park standards. As such, Rollins Ford Community Park is assumed to impact Park Planning District 4. To account for both PPD needs and service requirements for the nearby Rollins Ford Community Park, it is appropriate to consider the entire district that will be impacted for our analysis. Additional amenities will be provided to meet the community park needs as follows:

- 36,000 square foot community gathering area and innovation playground.
- 29,000 square foot community promenade.
- 155,200 square foot club house and community pool.
- 12,600 square foot linear courtyard.
- 29,200 square foot pavilion green and urban park.
- 20,500 square foot urban promenade.
- 25,600 square foot gateway plaza.

### **Linear/Greenway Parks**

Linear and Greenway parks are a critical level of service metric for the Innovation Small Area Plan. The Small Area Plan wants to ensure connectivity and accessibility between the community and surrounding communities. The linear park/greenway proffer contribution provided in Table III-D.2 on the following page is based on the Broad Run Linear Park/Greenway. The Broad Run Linear Park/Greenway will provide a connection from the Development to the Rollins Ford Community Park, which as previously described will also service the Development. Additional linear park/greenway amenities that are provided in the new development plan include the following:

- 3,500 linear foot trail along the Prince William Parkway from Hylton Boulevard to Wellington Road.
- 1,200 linear foot trail along Wellington Road.
- Trails around the perimeters of two ponds.
- 750 linear foot trail along Spine Road.
- 500 linear foot trail along Western Spine Road.
- Trails within the nature preservation.
- 193,000 square foot stream preservation area.
- Trail connections from George Mason University to the Development.

### **Regional Park**

Any proffer related to public parks must only mitigate the costs reasonably attributable to an increase in population. Moreover, the County Capital Improvement Plan does not include any capital improvements to Park Planning District 4 that increase regional park capacity in the Residential Development's service area. As such, any proffer related to such facilities would be inappropriate under the Residential Proffer Legislation.

# Proffer Justification Narrative

TABLE III-D.2  
Projected Proffer Costs – Public Park Facility Improvements

<b>Public Park Facilities</b>	
<b>Community Park</b>	
(a) Cost to construct Rollins Ford Community Park <sup>(a)</sup>	\$7,300,000
(b) Current population of Brentsville Magisterial District <sup>(b)</sup>	71,414
(c) Projected residents above by-right <sup>(c)</sup>	2,671
(d) Total project projected residents <sup>(c)</sup>	2,705
(e) Total projected District population (b + d)	74,119
(f) Project cost per capita (a ÷ e)	\$98.49
<b>(g) Sub-total: community park proffer contribution for Development (c × f)</b>	<b>\$263,100.84</b>
<b>Linear Park/Greenway</b>	
(h) Cost to construct Broad Run Linear Park/Greenway <sup>(d)</sup>	\$11,000,000
(i) Current population of Brentsville Magisterial District <sup>(b)</sup>	71,414
(j) Projected Residents above by-right <sup>(c)</sup>	2,671
(k) Total project projected residents <sup>(c)</sup>	2,705
(l) Total projected District population (i + k)	74,119
(m) Project cost per capita (h ÷ l)	\$148.41
<b>(n) Sub-total: linear/park greenway proffer contribution for Development (j × m)</b>	<b>\$396,453.33</b>
<b>(o) Total Development Proffer Contribution: Public Parks (f + g)</b>	<b>\$659,554.17</b>
<b>(p) Proffer contribution per resident per unit (f + m × 3.16): townhome unit</b>	<b>\$780.20</b>
<b>(q) Proffer contribution per resident per unit (f + m × 2.32): multi-family unit</b>	<b>\$572.81</b>
<b>(r) Single-family detached units by-right (f + m × 3.37)</b>	<b>\$832.05</b>
<b>Proffer contribution: Total townhome contribution (p × 370)</b>	<b>\$288,675.62</b>
<b>Proffer contribution: Total multi-family contribution (q × 662)</b>	<b>\$379,199.08</b>
<b>Less single-family detached per capita contributions allowed by-right (r × 10)</b>	<b>(\$8,320.53)</b>
<b>Total Contribution for Development:</b>	<b>\$659,554.17</b>
<b>Townhouse share of contribution</b>	<b>\$285,079.24</b>
<b>Townhouse contribution per unit</b>	<b>\$770.48</b>
<b>Multi-family share of contribution</b>	<b>\$374,474.94</b>
<b>Multi-family contribution per unit</b>	<b>\$565.67</b>
<sup>(a)</sup> Source: Prince William County Capital Improvements Program Fiscal Years 2021-26. Based on estimated costs for Rollins Ford Park. Comments received from the Prince William County Parks and Recreation Department listed Rollins Ford Park within service area of the Development.	
<sup>(b)</sup> Source: Prince William County Geographic Information Systems Demographics Annual Estimates (as of 12/31/2020). The population is based on the entire Brentsville Magisterial District which the park will be serving.	
<sup>(c)</sup> See Table III-B.1.	
<sup>(d)</sup> Source: Prince William County Parks, Recreation & Open Space Master Plan 2020. Based on the estimated costs for Broad Run Linear Park/Greenway.	

EXHIBIT I: INNOVATION TOWN CENTER SITE PLAN WITH AMENITIES



## Proffer Justification Narrative

The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable laws, including, but not limited to, the Residential Proffer Legislation.

In addition to the estimated proffers, the Residential Development is planned to include the previously mentioned amenities and park space to reasonably offset any increase in demand on existing facilities and to address the Innovation Park Small Area Plan community needs. The total improvements will add approximately 621,000 square feet or 14.26 acres of park space for the community (see Exhibit I).

Proffers for eligible public park facilities are calculated in accordance with County LOS standards and shown on the previous page in Table III-D.2. The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.



**III-E. Public Transportation Facility Improvement Impacts**

**METHODOLOGY**

MuniCap relied on the Traffic Impact Analysis (prepared by Gorove Slade on November 6, 2020) and the planned transportation improvements outlined in the Innovation Park Small Area Plan to calculate the impact of the projected development on public transportation facilities. MuniCap then compared the Developer’s estimated public transportation contributions to the estimated costs of the transportation impacts from the projected development.

**PLANNED TRANSPORTATION IMPROVEMENTS**

MuniCap reviewed information provided by Gorove Slade, the transportation consultant, to determine the total planned transportation improvements within or near Innovation Park. A portion of these costs will be by the Applicant’s contribution. Table III-E.1 below provides the estimated costs of the transportation improvements that are planned by Prince William County and the portion to be supported by the Applicant.

**TABLE III-E.1  
Innovation Park Small Area Plan – Planned Capital and Infrastructure Improvements**

<i>Small Area Plan Identified Capital and Infrastructure Improvements<sup>(a)</sup></i>	<i>Cost<sup>(b)</sup></i>
<b>Capital Improvements</b>	
Rt. 28 phase III (Linton Hall Road to Pennsylvania Avenue)	\$36,200,000
Intersection improvements at Prince William Parkway and University Boulevard	\$24,200,000
Sub-total capital improvements	\$60,400,000
<b>Infrastructure Improvements</b>	
Construct new roads within the town center	\$17,466,488
Pedestrian/bicyclist bridge across Prince William Parkway north of the Discovery Blvd intersection	\$1,000,000
Sudley Manor/Wellington Road innovative intersections/interchange	\$120,000,000
Sub-total infrastructure improvements	\$138,466,488
<b>Total identified Small Area Plan costs</b>	<b>\$198,866,488</b>
<b>Innovation Town Center supported costs<sup>(c)</sup></b>	<b>\$16,990,612</b>
<sup>(a)</sup> Source: Prince William County Innovation Park Small Area Plan.	
<sup>(b)</sup> Cost information was collated by Gorove Slade. Costs are based on the Prince William County Capital Improvements Program Fiscal Years 2022-27, as well as similar capital improvements.	
<sup>(c)</sup> See table table III-E.2	

The Applicant contribution of \$16.99 million is further detailed in Table III-E.2 on the following page.

TABLE III-E.2  
 Developer Contributions – Planned Capital and Infrastructure Improvements

<i>Developer Contributions - Capital and Infrastructure Improvements</i>	<i>Cost<sup>(a)</sup></i>
<b>Town Center Roads</b>	
Katherine Johnson BLVD, North Segment	\$8,552,500
Road "B"	\$605,000
Road "C"	\$1,320,000
15% Contingency	\$1,571,625
<i>Sub-total town center roads</i>	\$12,049,125
<b>Sudley Manor Interchange</b>	
Thong Pan Road realignment	\$561,875
Bethlehem Road realignment	\$2,735,070
Traffic Signal at Wellington Road at Hornbaker Road	\$500,000
Traffic Signal at Wellington Road at relocated Bethlehem Road	\$500,000
15% Contingency	\$644,542
<i>Sub-total Sudley Manor Interchange</i>	\$4,941,487
<b>Total Developer Contributions</b>	<b>\$16,990,612</b>

<sup>(a)</sup> Source: Gorove Slade.

To determine the projected development’s transportation proffer impacts, MuniCap reviewed the traffic impacts described in the Traffic Impact Analysis.

**ESTIMATED TRAFFIC IMPACTS**

Gorove Slade estimated traffic volumes for key intersections under various development scenarios. For purposes of estimating the Residential Development’s impact on traffic volume, MuniCap used the Traffic Impact Analysis’ full development scenario, which also includes the University Village at Innovation (the “Full Development”). This scenario reflects a more holistic representation of the future Innovation Small Area traffic volumes. Please refer to the Traffic Impact Analysis for a comprehensive description of the assumptions and methodologies used for estimating traffic volume in this scenario.

Table III-E.3 on the following page provides the Full Development’s portion of total site trips at key intersections. The proposed development, under the full development scenario, represents a minimal percentage of the total estimated intersection trips.

TABLE III-E.3  
Percentage of Total Site Trips – Exit Nodes (Full Development Scenario)

<i>Traffic Volume Percentages at Exit Nodes<sup>(a)</sup></i>	<i>AM Demand</i>	<i>PM Demand</i>	<i>Average of AM/PM</i>
East exit (Wellington Road & Site Entrance/JD Reading Drive)	4.6%	4.7%	4.7%
West exit (Wellington Road and Hornbaker Road)	3.1%	3.1%	3.1%
North exit (Wellington Road and Prince William Parkway)	7.3%	9.3%	8.3%
South exit (University Boulevard and Prince William Parkway)	5.1%	7.3%	6.2%
<b>Innovation Town Center average traffic volume percentage</b>			<b>5.6%</b>
<small><sup>(a)</sup> Source: Transportation Impact Study prepared by Gorove Slade. Represents the share of traffic volume attributable to Innovation Town Center.</small>			

As shown above, the average percentage of total trips from the Full Development is 5.6%. This percentage is then applied to determine the projected development’s portion of the total planned capital and infrastructure improvements in the Small Area Plan.

**MITIGATION STRATEGIES**

Proffers for transportation facilities have been estimated based on the total planned transportation improvements in the Small Area Plan, the estimated traffic volume generated by the Full Development, and the Applicant’s contribution of transportation improvements. Table III-E.4 below shows the methodology used to determine the proffer contribution required after Applicant contributions, if required.

TABLE III-E.4  
Proffer Estimates – Excess Developer Contribution

<i>Estimated Proffer Contribution</i>	
<b>Transportation Impacts</b>	
(a) Total Small Area Plan transportation improvement costs <sup>(a)</sup>	\$198,866,488
(b) Innovation Town Center average traffic volume percentage <sup>(b)</sup>	5.6%
<b>(c) Required Small Area Plan proffer (a × b)</b>	<b>\$11,061,948</b>
<b>(d) Developer credit for infrastructure improvements<sup>(c)</sup></b>	<b>(\$16,990,612)</b>
<b>(e) Excess Developer contribution (c - d)</b>	<b>\$5,928,663</b>
<small><sup>(a)</sup> See Table III-E.1</small>	
<small><sup>(b)</sup> See Table III-E.3.</small>	
<small><sup>(c)</sup> See Table III-E.2.</small>	

As shown in Table III-E.4, the Applicant contribution exceeds the required Small Area Plan proffer. Table III-E.5 on the following page provides a comparison between the required Small Area Plan contribution calculated on a per unit basis and the Applicant contribution calculated on a per unit basis. Additionally, this table shows the remaining proffer mitigation required from the Development. Due to the Applicant contribution exceeding the required Small Area Plan contribution any additional proffer mitigation would be deemed unreasonable.

## Proffer Justification Narrative

Table III-E.5  
Proffer Estimates – Projected Transportation Facility Impacts

<i>Estimated Proffer Contribution</i>	<i>Small Area Plan Proffer</i>	<i>Developer Contribution</i>	<i>Proffer Mitigation</i>
<b><u>Transportation Impacts</u></b>			
(a) Proffer credit (Developer transportation contributions) <sup>(a)</sup>	\$11,061,948	\$16,990,612	\$0
(b) Percentage of total residents generated: townhome units	43%	43%	43%
(c) Percentage of total residents generated: multi-family units	57%	57%	57%
Proffer contribution: per townhome unit (a × b ÷ 370)	\$12,922.45	\$19,848.26	\$0.00
Proffer contribution: per multi-family unit (a × c ÷ 662)	\$9,487.37	\$14,572.14	\$0.00
<sup>(a)</sup> See Table III-E.4.			

The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.

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*IV. Conclusions, Assumptions, and Limitations*

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The preceding narrative provides projections of impacts to public facility improvements as mandated by the Prince William County Justification Narrative Requirement and has been updated based on comments received by County staff. This narrative is being resubmitted for review. Upon receipt of such review and any additional commentary, the Applicant will further augment this submission with specific mitigation strategies as appropriate.

**SUMMARY OF ANALYSIS**

Based on MuniCap's analysis, the estimated cash proffer that may be collected from the Residential Development is as shown in Table IV-A on the following page.



## Proffer Justification Narrative

TABLE IV-A  
Summary of Analysis

<i>Public Facilities</i>	<i>Estimated Proffer per Dwelling Unit</i>
<b>a) Public school facilities total proffer cost<sup>(a)</sup></b>	
i) Townhome unit	\$8,549.47
ii) Multi-family unit	\$4,499.72
<b>b) Public library facilities<sup>(b)</sup></b>	
i) Townhome unit	\$705.24
ii) Multi-family unit	\$517.77
<b>c) Public safety facilities<sup>(c)</sup></b>	
i) Townhome unit	\$1,280.04
ii) Multi-family unit	\$939.78
<b>d) Public parks facilities cost per unit<sup>(d)</sup></b>	
i) Townhome unit	\$770.48
ii) Multi-family unit	\$565.67
<b>e) Public transportation facilities<sup>(e)</sup></b>	
i) Townhome unit	\$0.00
ii) Multi-family unit	\$0.00
<b>f) Total estimated proffer per unit</b>	
i) Townhome unit	\$11,305.24
ii) Multi-family unit	\$6,522.94
<b>g) Proposed residential units<sup>(f)</sup></b>	
i) Townhome unit	370
ii) Multi-family unit	662
<b>Total Development Proffer Contribution ((f(i) × g(i))) + (f(ii) × g(ii))</b>	<b>\$8,501,125.16</b>
(a) See Table III-A.4. (b) See Table III-B.6. (c) See Table III-C.5. (d) See Table III-D.2. (e) See Table III-E.4. (f) See Table III-B.1.	

## ASSUMPTIONS AND LIMITATIONS

This narrative relies on information from the Traffic Impact Analysis. This document should be reviewed in conjunction with the Traffic Impact Analysis.

MuniCap obtained the information presented and used in this narrative from multiple sources. While these sources are believed to be reliable, MuniCap has not undertaken any efforts to independently verify the veracity of any such information.

While the methodology employed, and the content provided herein, are believed to be consistent with applicable law, including the Residential Proffer Legislation, none of the statements in this document should be construed as legal advice.

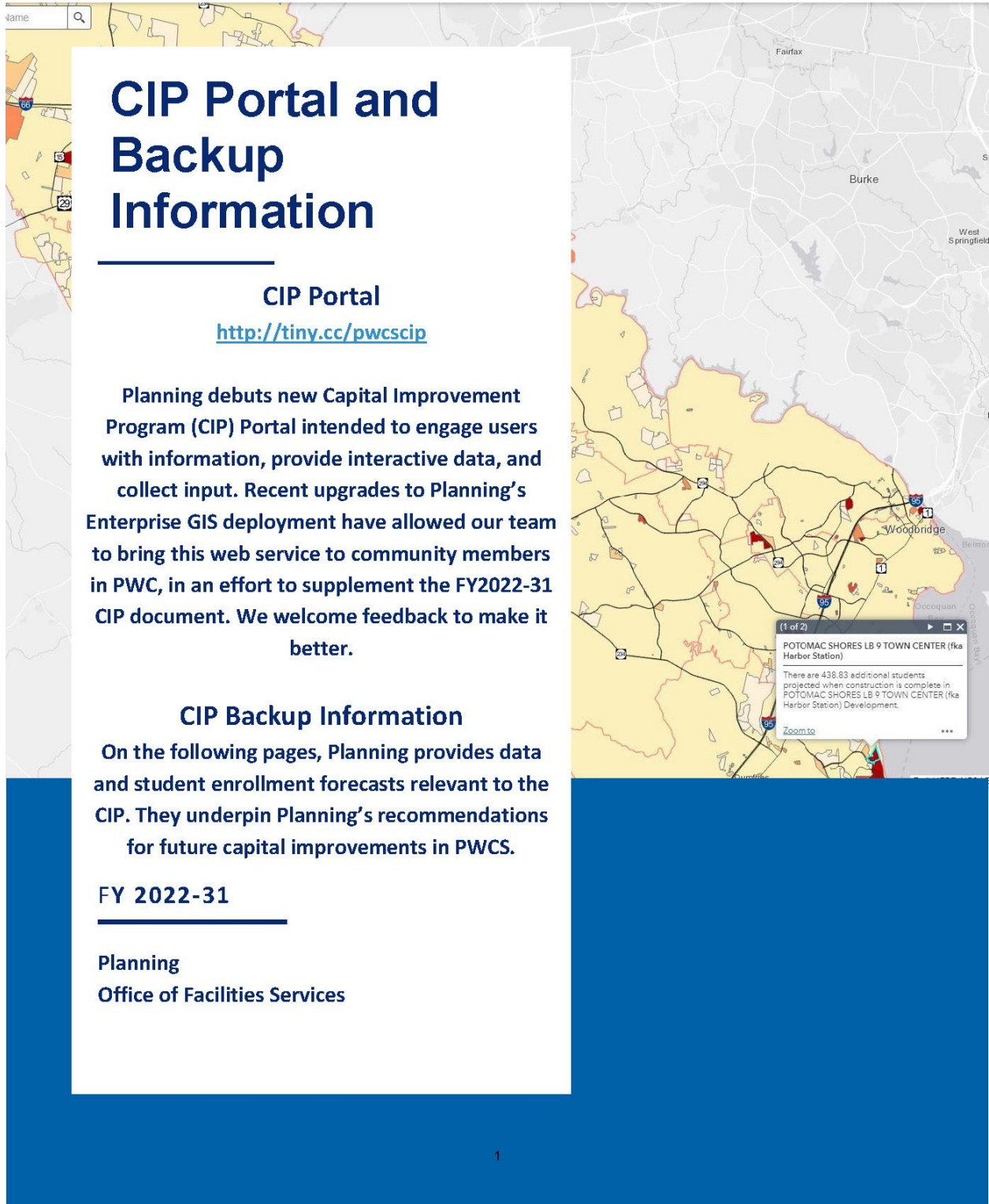
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*Appendix A: School Capacity and Enrollment Projections*

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**CIP Portal and Backup Information**

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**CIP Portal**  
<http://tiny.cc/pwscscip>

Planning debuts new Capital Improvement Program (CIP) Portal intended to engage users with information, provide interactive data, and collect input. Recent upgrades to Planning’s Enterprise GIS deployment have allowed our team to bring this web service to community members in PWC, in an effort to supplement the FY2022-31 CIP document. We welcome feedback to make it better.

**CIP Backup Information**

On the following pages, Planning provides data and student enrollment forecasts relevant to the CIP. They underpin Planning’s recommendations for future capital improvements in PWCS.

**FY 2022-31**

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Planning  
Office of Facilities Services

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# Proffer Justification Narrative

Table 1  
Student Enrollment Data: Historical, Current, and Projected Enrollment  
1990 - 2030

Year	Elementary								PWCS Enrollment								Special		Total Enrollment				
	KG	1st			2nd			Total	% Chg	Middle				High				#	#	% Chg	% Chg		
		1st	2nd	3rd	4th	5th	6th			7th	8th	Total	% Chg	9th	10th	11th	12th					Total	% Chg
1990	3,294	3,517	3,422	3,482	3,397	3,443	20,702		3,086	3,145	3,079	9,320		3,226	3,016	2,654	2,742	11,638		686	42,346		
1991	3,345	3,595	3,526	3,508	3,506	3,396	20,984	1.4%	3,491	3,199	3,171	9,861	5.8%	3,565	2,902	2,772	2,607	11,846	1.8%	709	43,400	1,054	2.5%
1992	3,483	3,794	3,666	3,623	3,580	3,607	21,768	3.7%	3,443	3,523	3,246	10,212	3.6%	3,746	3,047	2,619	2,766	12,178	2.8%	712	44,870	1,470	3.4%
1993	3,240	3,704	3,707	3,664	3,678	3,595	21,605	-0.7%	3,618	3,530	3,506	10,654	4.3%	3,866	3,149	2,753	2,733	12,501	2.7%	779	45,539	669	1.5%
1994	3,447	3,551	3,715	3,665	3,684	3,764	21,826	1.0%	3,625	3,624	3,484	10,733	0.7%	3,996	3,135	2,821	2,870	12,822	2.6%	900	46,281	742	1.6%
1995	3,677	3,818	3,682	3,737	3,752	3,807	22,473	3.0%	3,782	3,666	3,637	11,085	3.3%	4,281	3,304	2,867	2,877	13,329	4.0%	825	47,712	1,431	3.1%
1996	3,611	4,051	3,881	3,781	3,775	3,784	22,886	1.8%	3,790	3,857	3,680	11,327	2.2%	4,328	3,511	2,980	3,009	13,828	4.7%	760	48,801	1,089	2.3%
1997	3,634	4,103	4,046	3,983	3,909	3,854	23,329	2.8%	3,892	3,830	3,914	11,616	2.7%	4,453	3,574	3,187	3,152	14,366	3.9%	751	50,282	1,481	3.0%
1998	3,531	4,119	4,189	4,118	4,105	3,982	24,044	2.2%	3,888	3,957	3,899	11,744	0.9%	4,286	3,893	3,401	3,259	14,839	3.3%	994	51,621	1,339	2.7%
1999	3,545	4,188	4,244	4,280	4,254	4,179	24,690	2.7%	4,066	4,024	4,122	12,212	4.0%	4,367	4,022	3,506	3,397	15,292	3.1%	868	53,062	1,441	2.8%
2000	3,615	4,303	4,347	4,456	4,461	4,471	25,633	3.9%	4,348	4,184	4,242	12,774	4.6%	4,926	3,995	3,696	3,462	16,079	5.1%	633	55,139	2,077	3.9%
2001	3,986	4,494	4,416	4,596	4,640	4,702	26,814	4.5%	4,581	4,525	4,339	13,445	5.3%	5,482	4,082	3,702	3,615	16,881	5.0%	683	57,823	2,684	4.9%
2002	4,072	4,772	4,597	4,625	4,815	4,836	27,717	3.4%	4,912	4,705	4,639	14,256	6.0%	5,471	4,503	3,841	3,743	17,558	4.0%	698	60,229	2,406	4.2%
2003	4,431	4,901	4,885	4,806	4,845	4,952	28,820	4.0%	5,074	5,063	4,898	15,035	5.5%	5,979	4,546	4,074	3,926	18,525	5.5%	719	63,099	2,870	4.8%
2004	4,607	5,315	5,066	5,042	5,034	5,037	30,101	4.4%	5,119	5,197	5,262	15,580	3.6%	6,143	4,975	4,300	4,227	19,652	6.1%	760	66,093	2,994	4.7%
2005	5,098	5,507	5,270	5,133	5,161	5,157	31,286	3.9%	5,091	5,236	5,296	15,623	0.3%	6,477	5,084	4,574	4,306	20,448	4.1%	877	68,234	2,141	3.2%
2006	5,298	5,844	5,564	5,345	5,333	5,320	32,704	4.5%	5,262	5,155	5,331	15,748	0.8%	6,433	5,498	4,751	4,546	21,228	3.8%	1,003	70,683	2,449	3.6%
2007	5,711	5,824	5,833	5,576	5,492	5,422	33,858	3.5%	5,406	5,367	5,267	16,040	1.9%	6,450	5,498	5,051	4,793	21,792	2.7%	964	72,654	1,971	2.8%
2008	5,641	6,001	5,857	5,756	5,652	5,521	34,428	1.7%	5,428	5,509	5,336	16,273	1.5%	6,276	5,509	5,091	5,054	21,930	0.6%	1,027	73,657	1,003	1.4%
2009	5,925	6,065	6,181	6,003	6,008	5,873	36,055	4.7%	5,697	5,663	5,674	17,034	4.7%	6,428	5,514	5,189	5,197	22,328	1.8%	1,239	76,656	2,999	4.1%
2010	5,986	6,377	6,198	6,326	6,192	6,182	37,261	3.3%	6,027	5,877	5,796	17,700	3.9%	6,453	5,752	5,323	5,329	22,857	2.4%	1,298	79,115	2,459	3.2%
2011	6,439	6,405	6,482	6,300	6,467	6,343	38,436	3.2%	6,254	6,125	5,972	18,351	3.7%	6,675	5,989	5,485	5,351	23,500	2.8%	1,348	81,635	2,520	3.2%
2012	6,459	6,829	6,472	6,552	6,375	6,382	39,269	2.2%	6,417	6,208	6,217	18,902	3.0%	6,734	6,104	5,666	5,511	24,015	2.2%	1,365	83,551	1,916	2.3%
2013	6,360	6,806	6,888	6,524	6,619	6,451	39,538	0.7%	6,621	6,444	6,408	19,473	3.0%	6,884	6,227	5,827	5,727	24,665	2.7%	1,379	85,055	1,504	1.8%
2014	6,365	6,529	6,888	6,913	6,515	6,680	39,920	1.0%	6,441	6,505	6,557	19,703	1.2%	7,240	6,420	5,706	5,883	25,249	2.4%	1,337	86,209	1,154	1.4%
2015	6,271	6,675	6,630	6,899	6,953	6,642	40,070	0.4%	6,724	6,483	6,797	20,004	1.5%	7,162	6,840	6,016	5,843	25,861	2.4%	1,319	87,253	1,045	1.2%
2016	6,256	6,611	6,778	6,783	7,071	7,070	40,569	1.2%	6,692	6,839	6,564	20,095	0.5%	7,514	6,957	6,350	6,103	26,923	4.1%	1,333	88,920	1,667	1.9%
2017	6,435	6,548	6,675	6,860	6,862	7,102	40,482	-0.2%	7,086	6,748	6,879	20,713	3.1%	7,192	7,291	6,470	6,308	27,261	1.3%	1,405	89,861	941	1.1%
2018	6,254	6,691	6,623	6,701	6,913	6,928	40,110	-0.9%	7,112	7,147	6,738	20,997	1.4%	7,392	6,981	6,789	6,448	27,610	1.3%	1,486	90,203	342	0.4%
2019	6,303	6,639	6,806	6,745	6,854	7,075	40,422	0.8%	6,960	7,172	7,225	21,357	1.7%	7,497	7,213	6,507	6,841	28,058	1.6%	1,689	91,526	1,323	1.5%
2020	5,592	6,312	6,521	6,601	6,629	6,735	38,390	-5.0%	6,955	6,894	7,128	20,978	-1.8%	7,532	7,257	6,899	6,656	28,343	1.0%	1,365	89,076	-2,450	-2.7%
2021	6,086	6,464	6,527	6,691	6,849	6,786	39,403	2.6%	6,876	7,045	6,968	20,889	-0.4%	7,905	7,675	6,793	6,800	29,173	2.9%	1,526	90,991	1,915	2.2%
2022	6,195	6,602	6,784	6,841	7,001	7,183	40,606	3.1%	7,009	7,091	7,241	21,341	2.2%	7,816	7,742	7,248	6,902	29,709	1.8%	1,618	93,274	2,283	2.5%
2023	6,185	6,520	6,730	6,908	6,959	7,144	40,447	-0.4%	7,217	7,094	7,155	21,466	0.6%	7,974	7,613	7,270	7,319	30,176	1.6%	1,624	93,713	439	0.5%
2024	6,076	6,505	6,653	6,860	7,034	7,108	40,237	-0.5%	7,184	7,310	7,164	21,659	0.9%	7,887	7,773	7,155	7,348	30,164	0.0%	1,644	93,704	-9	0.0%
2025	6,159	6,394	6,640	6,783	6,988	7,188	40,153	-0.2%	7,151	7,280	7,386	21,817	0.7%	7,900	7,692	7,309	7,235	30,136	-0.1%	1,659	93,766	62	0.1%
2026	6,199	6,487	6,532	6,777	6,917	7,147	40,059	-0.2%	7,237	7,253	7,363	21,853	0.2%	8,152	7,712	7,239	7,397	30,500	1.2%	1,675	94,087	321	0.3%
2027	6,304	6,544	6,643	6,683	6,927	7,091	40,192	0.3%	7,214	7,358	7,353	21,925	0.3%	8,146	7,977	7,275	7,344	30,741	0.8%	1,691	94,549	462	0.5%
2028	6,420	6,665	6,712	6,806	6,810	7,111	40,554	0.9%	7,167	7,345	7,470	21,981	0.3%	8,146	7,982	7,536	7,391	31,054	1.0%	1,707	95,296	746	0.8%
2029	6,541	6,790	6,838	6,879	6,969	7,025	41,041	1.2%	7,190	7,300	7,459	21,950	-0.1%	8,279	7,985	7,543	7,658	31,466	1.3%	1,722	96,179	884	0.9%
2030	6,667	6,921	6,969	7,011	7,046	7,160	41,774	1.8%	7,106	7,327	7,417	21,850	-0.3%	8,271	8,119	7,550	7,669	31,609	0.5%	1,738	96,972	792	0.8%



# Proffer Justification Narrative

Table 2  
**Student Enrollment Data: Current and Projected Enrollment**  
 Elementary School, 2020-2030

Elementary School	Program Capacity	Funds/Class Issues	2020-21			2021-22			2022-23			2023-24			2025-26			2030-31		
			Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization
Abey	838	0	498	340	59.4%	467	371	55.7%	447	391	53.4%	409	429	48.9%	366	472	43.0%	365	473	43.0%
Antietam	939	0	773	166	82.3%	791	148	84.2%	810	129	86.3%	804	135	85.0%	812	127	86.4%	810	129	86.3%
Ashland	960	0	784	176	81.7%	782	178	81.5%	762	198	79.4%	741	219	77.2%	669	291	69.7%	668	292	69.6%
Bel Air	392	2	383	9	97.7%	387	5	98.7%	386	6	98.4%	383	9	97.6%	351	41	89.4%	349	43	89.1%
Belmont	536	0	499	37	93.1%	496	40	92.5%	537	-1	100.2%	532	4	99.3%	517	19	96.5%	546	-10	101.9%
Bennett	828	2	705	123	85.1%	772	56	93.2%	830	-2	100.3%	849	-21	102.6%	878	-50	106.0%	901	-73	108.8%
Bristow Run	828	0	574	254	69.3%	596	232	72.0%	586	242	70.8%	586	242	70.8%	605	223	73.1%	604	224	73.0%
Buckland Mills	919	0	705	214	76.7%	741	178	80.6%	778	141	84.7%	793	126	86.2%	802	117	87.2%	807	112	87.8%
Cedar Point	838	0	516	322	61.6%	511	327	61.0%	525	313	62.7%	504	334	60.1%	483	355	57.7%	499	339	59.6%
Coles	368	3	408	-40	110.9%	379	-11	103.0%	388	-20	105.5%	363	5	98.7%	310	58	84.2%	315	53	85.6%
Covington-Harper	783	0	704	79	89.9%	826	-43	105.5%	940	-157	120.1%	1,021	-238	130.3%	1,177	-394	150.3%	1,439	-656	183.8%
Dale City	371	3	415	-44	111.9%	422	-51	113.7%	448	-77	120.9%	454	-83	122.4%	464	-93	125.2%	463	-92	124.9%
Dumfries	379	4	418	-39	110.3%	439	-60	115.8%	470	-91	124.1%	495	-116	130.5%	614	-235	162.1%	732	-353	193.0%
Ellis	492	3	436	56	88.6%	438	54	89.0%	440	52	89.5%	443	49	90.0%	436	56	88.6%	517	-25	103.1%
Enterprise	398	6	376	22	94.5%	393	5	98.7%	413	-15	103.7%	412	-14	103.6%	439	-41	110.2%	439	-41	110.2%
Featherstone	415	3	470	-55	113.3%	486	-71	117.1%	496	-81	119.6%	488	-73	117.7%	485	-70	116.9%	489	-74	117.8%
Fitzgerald	773	1	829	-56	107.2%	872	-99	112.8%	921	-148	119.2%	919	-146	118.9%	916	-143	118.6%	970	-197	125.5%
Glenkirk	823	0	654	169	79.5%	667	156	81.0%	680	143	82.6%	670	153	81.4%	671	152	81.5%	669	154	81.3%
Gravelly	936	0	755	181	80.7%	751	185	80.2%	736	200	78.0%	699	237	74.7%	618	318	66.0%	717	219	76.6%
Haymarket	944	2	814	130	86.2%	817	127	86.5%	807	137	85.5%	791	150	84.1%	756	188	80.1%	796	148	84.4%
Henderson	915	0	752	163	82.2%	787	128	86.0%	819	96	89.0%	823	92	90.0%	837	78	91.5%	840	75	91.8%
Jenkins	662	0	582	80	87.9%	569	93	86.0%	573	89	86.6%	575	87	86.9%	556	106	84.0%	616	46	93.0%
Kerrydale	355	1	338	17	95.2%	341	14	96.1%	353	2	99.6%	354	1	99.8%	344	11	97.0%	343	12	96.6%
Kilby	684	0	597	87	87.3%	610	74	89.2%	656	28	95.8%	663	21	96.9%	655	29	95.8%	727	-43	106.3%
King	430	0	433	-3	100.7%	439	-9	102.1%	450	-20	104.7%	454	-24	105.7%	454	-24	105.6%	453	-23	103.4%
Lake Ridge	871	0	632	239	72.6%	636	235	73.0%	628	243	72.1%	593	278	68.1%	540	331	62.0%	540	331	62.0%
Leesylvania	760	0	684	76	90.0%	726	34	95.5%	746	14	98.2%	764	-4	100.5%	761	-1	100.1%	765	-5	100.6%
Lech Leonard	441	6	526	-85	119.3%	561	-120	127.2%	589	-148	133.4%	609	-168	138.2%	615	-174	139.6%	615	-174	139.5%
Marshall	738	0	637	101	86.3%	619	119	83.9%	609	129	82.5%	582	156	78.8%	558	180	75.0%	564	174	76.4%
Manassas Hills	579	4	558	21	96.4%	564	15	97.4%	582	-3	100.6%	574	5	99.2%	542	37	93.6%	574	5	99.1%
McAuliffe	446	2	398	48	89.2%	397	49	89.0%	404	42	90.6%	402	44	90.1%	398	48	89.3%	397	49	89.1%
Minnieville	612	0	529	83	86.4%	572	40	93.5%	602	10	98.3%	622	-10	101.7%	592	20	96.7%	590	22	96.5%
Montclair	592	2	638	-46	107.8%	668	-76	112.8%	709	-117	119.7%	710	-118	120.0%	693	-101	117.1%	693	-101	117.1%
Mountain View	721	0	458	263	63.5%	419	302	58.1%	392	329	54.3%	356	365	49.4%	305	416	42.3%	304	417	42.2%
Mullen	709	1	704	5	99.3%	748	-39	105.5%	771	-62	108.8%	788	-79	111.1%	797	-88	112.4%	801	-92	113.0%
Neasco	716	0	643	73	89.8%	668	48	93.3%	689	27	96.3%	690	26	96.4%	715	1	99.9%	714	2	99.7%
Nokesville School, The	641	0	671	-30	104.7%	718	-77	112.0%	763	-122	119.0%	788	-147	123.0%	849	-208	132.5%	970	-329	151.3%
Occoquan	498	4	617	-119	123.9%	643	-145	129.1%	674	-176	135.2%	672	-174	134.9%	679	-181	136.3%	705	-207	141.5%
Old Bridge	532	0	476	56	89.5%	464	68	87.2%	459	73	86.3%	433	99	81.3%	409	123	76.8%	408	124	76.8%
Parks	851	0	587	264	69.0%	576	275	67.7%	583	268	68.0%	593	258	69.6%	546	305	64.1%	590	261	69.4%
Partie	785	0	660	125	84.1%	686	99	87.4%	692	93	88.1%	670	115	85.3%	696	89	88.7%	697	88	88.8%
Perin	663	0	672	-9	101.4%	694	-31	104.7%	730	-67	110.1%	725	-62	109.3%	712	-49	107.4%	713	-50	107.6%
Perinington (ES)	364	0	405	-41	111.3%	405	-41	111.3%	405	-41	111.3%	405	-41	111.3%	405	-41	111.3%	405	-41	111.3%

# Proffer Justification Narrative

Table 2  
**Student Enrollment Data: Current and Projected Enrollment**  
 Elementary School, 2020-2030

Elementary School	Program Capacity	Furniture Class Issues	2020-21			2021-22			2022-23			2023-24			2025-26			2030-31		
			Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization	Students	Space	Utilization
Piney Branch	931	0	724	207	77.8%	730	201	78.4%	733	198	78.7%	723	208	77.7%	692	239	74.3%	691	240	74.2%
Porter (ES)	356	0	419	-63	117.7%	420	-64	118.0%	420	-64	118.0%	420	-64	118.0%	420	-64	118.0%	420	-64	118.0%
Potomac View	516	2	510	6	98.8%	544	-28	105.4%	570	-54	110.5%	559	-43	108.3%	551	-35	106.8%	574	-58	111.2%
River Oaks	612	1	583	29	95.3%	602	10	98.4%	635	-23	103.8%	626	-14	102.4%	650	-38	106.2%	649	-37	106.1%
Rockledge	522	1	488	34	93.5%	489	33	93.7%	504	18	96.6%	481	41	92.2%	455	67	87.2%	483	39	92.6%
Signal Hill	819	0	663	156	81.0%	689	130	84.1%	725	94	88.5%	724	95	88.4%	759	60	92.7%	804	15	98.2%
Simclair	662	3	772	-110	116.6%	799	-137	120.7%	842	-180	127.2%	855	-193	129.2%	848	-186	128.1%	847	-185	128.0%
Springwoods	946	0	759	187	80.2%	776	170	82.0%	772	174	81.6%	748	198	79.1%	712	234	75.2%	711	235	75.1%
Sudley	639	1	615	24	96.2%	628	11	98.3%	652	-13	102.1%	636	3	99.5%	619	20	96.9%	618	21	96.7%
Swans Creek	632	1	621	11	98.3%	627	5	99.2%	660	-28	104.4%	656	-24	103.8%	661	-29	104.0%	693	-61	109.6%
Triangle	752	0	710	42	94.4%	743	9	98.8%	788	-36	104.8%	810	-58	107.7%	800	-48	106.3%	865	-113	115.0%
Tyler	574	0	443	131	77.2%	465	109	81.0%	480	94	83.7%	500	74	87.1%	511	63	89.1%	563	11	98.1%
Vaughan	585	3	555	30	94.9%	576	9	98.5%	602	-17	102.9%	619	-34	105.8%	596	-11	101.8%	594	-9	101.6%
Victory	874	0	589	285	67.4%	610	264	69.8%	631	243	72.2%	610	264	69.8%	590	284	67.5%	607	267	69.5%
West Gate	497	4	545	-48	109.7%	580	-83	116.7%	620	-123	124.8%	629	-132	126.0%	638	-141	128.4%	660	-163	132.8%
Westridge	709	0	660	49	93.1%	667	42	94.1%	693	16	97.7%	672	37	94.8%	659	50	92.9%	657	52	92.7%
Williams	696	1	668	28	96.0%	662	34	95.1%	659	37	94.6%	639	57	91.8%	631	65	90.7%	630	66	90.5%
Wilson	872	0	846	26	97.0%	852	20	97.7%	873	-1	100.1%	854	18	97.9%	835	37	95.8%	851	21	97.6%
Wood	950	0	858	92	90.3%	880	70	92.6%	899	51	94.6%	885	65	93.2%	896	54	94.3%	900	50	94.7%
Yorkshire	745	0	710	35	95.3%	722	23	96.9%	735	10	98.6%	722	23	97.0%	719	26	96.5%	825	-80	110.7%
Yung	839	0	739	100	88.1%	799	40	95.2%	803	36	95.8%	839	0	100.0%	849	-10	101.2%	976	-137	116.3%
<b>Total</b>	<b>43,053</b>	<b>66</b>	<b>38,390</b>	<b>4,663</b>	<b>89.2%</b>	<b>39,403</b>	<b>3,650</b>	<b>91.5%</b>	<b>40,577</b>	<b>2,476</b>	<b>94.2%</b>	<b>40,413</b>	<b>2,640</b>	<b>93.0%</b>	<b>40,118</b>	<b>2,935</b>	<b>93.2%</b>	<b>41,740</b>	<b>1,313</b>	<b>96.9%</b>

Notes:  
 (A) Table displays rounded figures; thus their summation may not equal the displayed total.

# Proffer Justification Narrative

Table 3  
Student Enrollment Data: Current and Projected Enrollment  
Middle School, 2020-2030

Middle School	Planning Capacity	Program Capacity	Permits Characteristics	2020-21			2021-22			2022-23			2023-24			2025-26			2030-31		
				Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization
Berrien	1,464	1,436	-	1,344	120	91.8%	1,341	95	93.4%	1,306	130	90.9%	1,298	138	90.4%	1,267	169	88.2%	1,056	380	73.5%
Besseville	1,191	1,256	-	1,072	118	90.0%	1,216	40	96.8%	1,252	4	99.7%	1,268	-42	101.0%	1,280	-4	100.3%	1,231	25	98.0%
Bill Dora	1,233	1,153	-	1,157	76	93.8%	1,080	73	93.3%	1,121	32	97.2%	1,141	12	99.0%	1,126	27	97.9%	1,050	103	91.1%
Gainesville	1,233	1,209	7	1,307	-164	113.3%	1,370	-161	113.3%	1,421	-212	117.5%	1,385	-176	114.6%	1,371	-162	113.4%	1,407	-198	116.4%
Gratham Park	867	863	8	978	-111	112.8%	747	116	86.6%	769	94	89.1%	770	93	89.2%	867	+4	100.5%	982	-119	113.8%
Hampton	582	1,039	2	1,011	-29	103.0%	975	64	93.8%	983	56	94.6%	957	82	92.1%	1,044	-5	100.5%	1,134	-95	109.1%
Lake Ridge	1,464	1,495	-	1,478	-11	101.0%	1,455	40	97.3%	1,439	56	96.3%	1,484	11	99.3%	1,465	30	98.0%	1,395	300	86.6%
Lynn	1,170	1,139	14	1,366	-196	116.8%	1,035	161	90.9%	1,058	81	92.9%	1,142	-3	100.3%	1,287	-148	113.0%	1,238	-99	108.7%
Marsteller	1,233	1,217	5	1,246	-13	101.1%	1,202	15	98.8%	1,163	54	95.6%	1,149	68	94.4%	1,141	76	93.8%	1,140	77	93.7%
Nokaville School, The (MS)	439	448	-	391	48	89.1%	380	68	84.8%	417	31	93.1%	443	5	98.9%	474	-26	105.8%	538	-90	120.1%
Parkside	1,433	1,381	4	1,509	-56	103.9%	1,576	-145	110.9%	1,570	-189	113.7%	1,575	-194	114.0%	1,590	-209	115.1%	1,742	-361	126.1%
Pennington (MS)	243	243	-	252	-9	103.7%	243	0	100.0%	243	0	100.0%	243	0	100.0%	243	0	100.0%	243	0	100.0%
Perier (MS)	252	252	-	263	-11	104.4%	252	0	100.0%	252	0	100.0%	252	0	100.0%	252	0	100.0%	252	0	100.0%
Potomac	1,464	1,409	-	1,213	251	82.9%	1,311	198	85.9%	1,268	141	90.0%	1,285	124	91.2%	1,219	190	86.5%	1,272	137	90.3%
Potomac Shores	-	1,462	-	-	-	-	949	513	64.9%	1,014	448	69.4%	1,085	377	74.2%	1,118	344	76.5%	1,292	176	88.4%
Reagan	1,233	1,236	5	1,382	-149	112.1%	1,378	-142	111.8%	1,390	-154	112.5%	1,347	-111	109.0%	1,341	-105	108.5%	1,209	27	97.8%
Rippon	1,380	1,394	2	1,360	30	97.8%	1,187	207	85.2%	1,216	178	87.2%	1,224	170	87.8%	1,312	82	94.1%	1,460	34	97.6%
Sandfork	1,212	1,244	2	1,212	0	100.0%	1,168	76	93.9%	1,196	48	96.1%	1,145	98	92.0%	1,091	153	87.7%	1,015	229	81.6%
Union-Baxton	1,360	1,307	-	1,178	333	82.9%	1,149	158	87.9%	1,233	84	93.6%	1,214	93	92.9%	1,292	15	98.9%	1,359	-52	104.0%
Woodbridge	1,466	1,081	9	1,219	-153	114.4%	1,025	56	94.8%	1,020	61	94.4%	1,047	34	96.9%	1,046	35	96.8%	1,016	65	94.6%
<b>Total</b>	<b>20,949</b>	<b>22,264</b>	<b>58</b>	<b>20,978</b>	<b>-29</b>	<b>100.1%</b>	<b>20,889</b>	<b>1,375</b>	<b>93.8%</b>	<b>21,323</b>	<b>941</b>	<b>95.8%</b>	<b>21,452</b>	<b>812</b>	<b>96.4%</b>	<b>21,805</b>	<b>459</b>	<b>97.9%</b>	<b>21,832</b>	<b>432</b>	<b>98.1%</b>

Note: \* Planning Capacity used for the 2020-21 School Year.  
\* Program Capacity will be updated Planning Capacity starting in the 2021-22 School Year. The numbers in the table reflect the change starting in 2021-22.

Table 4  
High School, 2020-2030

High School	Planning Capacity	Permits Characteristics	2020-21			2021-22			2022-23			2023-24			2025-26			2030-31			
			Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	Students	Spaced	Utilization	
Battlefield	2,053	21	2,908	-853	141.6%	2,338	-285	113.9%	2,063	-10	100.5%	1,924	129	93.7%	1,754	299	85.4%	1,489	564	72.5%	
Breansville	1,110	-	976	134	87.9%	1,010	100	91.0%	1,043	67	94.0%	1,121	-11	101.0%	1,159	-49	104.4%	1,334	-224	120.2%	
Colgan	2,053	7	2,909	-856	141.7%	2,860	-807	139.3%	2,834	-782	138.1%	2,788	-755	135.8%	2,615	-563	127.4%	2,331	-278	113.5%	
Forest Park	2,053	-	2,226	-173	108.4%	2,253	-200	109.7%	2,277	-224	110.9%	2,248	-195	109.5%	2,439	-386	118.8%	2,665	-612	129.8%	
Freedom	2,053	8	2,168	-115	105.6%	2,214	-161	107.8%	2,398	-345	116.8%	2,461	-408	119.9%	2,517	-464	122.6%	2,848	-795	138.7%	
Gainesville	2,557	-	-	-	-	1,613	944	63.1%	2,230	327	87.2%	2,575	-18	100.7%	2,610	-53	102.1%	2,748	-179	107.0%	
Garfield	2,839	-	2,380	459	80.3%	2,387	452	84.1%	2,433	406	85.7%	2,436	403	85.8%	2,428	411	85.8%	2,607	143	95.0%	
Hillen	2,053	-	2,130	-77	103.8%	2,178	-125	106.1%	2,126	-73	103.6%	2,180	-427	106.2%	2,088	-35	101.7%	1,948	105	94.9%	
Odyssey Park	2,430	4	2,598	-168	106.9%	2,948	-518	121.3%	3,114	-684	128.1%	3,227	-797	132.8%	3,186	-756	131.1%	3,591	-1,461	147.8%	
Parrot	2,053	20	2,817	-764	137.2%	2,376	-233	110.9%	2,020	33	98.4%	1,938	115	94.4%	1,831	222	89.2%	1,688	365	83.2%	
Potomac	2,357	-	1,899	458	80.6%	1,979	378	84.0%	2,095	262	88.9%	2,172	185	92.2%	2,310	47	98.0%	2,581	-274	109.5%	
Unity Road	2,409	7	2,662	-253	110.8%	2,221	188	92.2%	2,096	313	87.0%	2,052	357	85.2%	2,143	266	89.0%	2,504	-95	103.9%	
Woodbridge	-	-	2,731	-83	101.4%	2,896	-162	105.9%	2,979	-245	109.0%	3,055	-321	111.7%	3,057	-323	111.8%	3,196	-464	117.0%	
<b>Total HIS</b>	<b>26,197*</b>	<b>28,754*</b>	<b>67</b>	<b>28,343</b>	<b>-2,146</b>	<b>108.2%</b>	<b>29,173</b>	<b>419</b>	<b>101.5%</b>	<b>29,708</b>	<b>-954</b>	<b>103.3%</b>	<b>30,176</b>	<b>-4,422</b>	<b>104.9%</b>	<b>30,136</b>	<b>-1,382</b>	<b>104.8%</b>	<b>31,609</b>	<b>-2,855</b>	<b>109.9%</b>

Note:  
(A) All tables displayed on this page employ rounded figures, thus their sum may not equal the displayed total.  
(B) Student enrollment projections for 2021-22 reflect the opening of Germantown High and the migration of new students from its 6th housing market. Using 10 and 100 grade students will be transferred. 11th grade students will have a one-time projection to transfer to their currently assigned school as transfer students, and rising 12th grade students will not be assigned. Germantown will open without a 12th grade class for the opening year only.  
\* Capacity on which available space is calculated for the 2020-21 school year.  
\* Capacity on which available space is calculated for the 2021-22 through 2030-31 school year.



# Proffer Justification Narrative

## Elementary School Space with Approved CIP Solutions School Years 2020-30

Planning Area	Elementary School	2020 Program Capacity	Portable Classrooms	Capacity w/ Portables	Space Available (based on Permanent Capacity)										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
Cardinal	Total	3,779	6	3,890	228	71	-87	-112	-83	-96	-66	-57	-68	-98	-147
	Bel Air	392	2	427	9	5	6	9	31	41	45	48	48	46	43
	Dale City	371	3	427	-44	-51	-77	-83	-92	-93	-88	-85	-85	-87	-92
	Fitzgerald	773	1	793	-56	-99	-148	-146	-141	-143	-144	-151	-166	-181	-197
	Henderson	915		915	163	128	96	92	105	78	84	88	89	84	75
	Minnieville	612		612	83	40	10	-10	5	20	27	31	31	28	22
	Neabsco	716		716	73	48	27	26	9	1	9	13	14	10	2
	<b>Total (After CIP)</b>				<b>228</b>	<b>71</b>	<b>-87</b>	<b>-112</b>	<b>-83</b>	<b>-96</b>	<b>-66</b>	<b>-57</b>	<b>-68</b>	<b>-98</b>	<b>-147</b>
	Percentage Utilization				94.0%	98.1%	102.3%	103.0%	102.2%	102.6%	101.7%	101.5%	101.8%	102.6%	103.9%
	New School - Add'l Space														
Add'l Space															
Add'l Space Location															
Cherry Hill	Total	4,639	7	4,770	275	54	-205	-278	-408	-591	-694	-819	-951	-996	-1,065
	Covington-Harper	783		783	79	-43	-157	-238	-315	-394	-463	-532	-629	-640	-656
	Dumfries	379	4	450	-39	-60	-91	-116	-178	-235	-288	-320	-337	-341	-353
	Pattie	785		785	125	99	93	115	109	89	96	100	101	95	88
	River Oaks	612	1	631	29	10	-23	-14	-21	-38	-30	-27	-26	-30	-37
	Swans Creek	632	1	653	11	5	-28	-24	-10	-29	-32	-41	-46	-52	-61
	Triangle	752		752	42	9	-36	-58	-41	-48	-59	-75	-91	-102	-113
	Williams	696	1	717	28	34	37	57	48	65	72	76	77	73	66
	<b>Total (After CIP)</b>				<b>275</b>	<b>54</b>	<b>-205</b>	<b>-278</b>	<b>318</b>	<b>135</b>	<b>32</b>	<b>-93</b>	<b>-225</b>	<b>456</b>	<b>387</b>
	Percentage Utilization				94.1%	98.8%	104.4%	106.0%	94.1%	97.5%	99.4%	101.7%	104.2%	92.5%	93.6%
New School - Add'l Space								726	726	726	726	726	1,452	1,452	
Add'l Space															
Add'l Space Location								Potomac Shores ES #2	Potomac Shores ES #2				Route 1 South Area ES		



# Proffer Justification Narrative

## Elementary School Space with Approved CIP Solutions School Years 2020-30

Planning Area	Elementary School	2020 Program Capacity	Portable Classrooms	Capacity w/ Portables	Space Available (based on Permanent Capacity)										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
Ferazzo	Total	4,917	13	5,180	447	431	335	388	474	573	615	632	634	578	511
	Ashland	960		960	176	178	198	219	241	291	298	302	303	299	292
	Coles	368	3	430	-40	-11	-20	5	26	58	60	61	61	58	53
	Enterprise	398	6	516	22	5	-15	-14	-28	-41	-36	-33	-33	-36	-41
	King	430		430	-3	-9	-20	-24	-22	-24	-19	-16	-16	-18	-23
	McAuliffe	446	2	487	48	49	42	44	46	48	52	55	55	53	49
	Montclair	592	2	633	-46	-76	-117	-118	-101	-101	-93	-89	-88	-93	-101
	Parks	851		851	264	275	268	258	288	305	312	315	316	285	261
	Wilson	872		872	26	20	-1	18	25	37	40	38	35	30	21
	<b>Total (After CIP)</b>				<b>447</b>	<b>431</b>	<b>335</b>	<b>388</b>	<b>474</b>	<b>573</b>	<b>615</b>	<b>632</b>	<b>634</b>	<b>578</b>	<b>511</b>
	Percentage Utilization				90.9%	91.2%	93.2%	92.1%	90.4%	88.4%	87.5%	87.1%	87.1%	88.2%	89.6%
	New School - Add'l Space														
	Addition - Add'l Space														
Add'l Space Location															
Haymarket	Total	4,932	2	4,983	1,259	1,272	1,292	1,380	1,524	1,575	1,589	1,576	1,542	1,461	1,380
	Alvey	838		838	340	371	391	429	468	472	477	479	479	477	473
	Buckland Mills	919		919	214	178	141	126	122	117	125	129	126	121	112
	Gravelly	936		936	181	185	200	237	290	318	311	293	271	245	219
	Haymarket	944	2	995	130	127	137	150	187	188	189	188	181	170	148
	Mountain View	721		721	263	302	329	365	404	416	420	421	422	420	417
	Tyler	574		574	131	109	94	74	54	63	68	67	62	27	11
	<b>Total (After CIP)</b>				<b>1,259</b>	<b>1,272</b>	<b>1,292</b>	<b>1,380</b>	<b>1,524</b>	<b>1,575</b>	<b>1,589</b>	<b>1,576</b>	<b>1,542</b>	<b>1,461</b>	<b>1,380</b>
	Percentage Utilization				74.5%	74.2%	73.8%	72.0%	69.1%	68.1%	67.8%	68.0%	68.7%	70.4%	72.0%
	New School - Add'l Space														
	Addition - Add'l Space														
	Add'l Space Location														



# Proffer Justification Narrative

## Elementary School Space with Approved CIP Solutions School Years 2020-30

Planning Area	Elementary School	2020 Program Capacity	Portable Classrooms	Capacity w/ Portables	Space Available (based on Permanent Capacity)										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
Lake Ridge	<b>Total</b>	<b>5,017</b>	<b>5</b>	<b>5,116</b>	<b>612</b>	<b>551</b>	<b>478</b>	<b>615</b>	<b>681</b>	<b>752</b>	<b>788</b>	<b>798</b>	<b>779</b>	<b>753</b>	<b>702</b>
	Antietam	939		939	166	148	129	135	129	127	137	141	142	138	129
	Lake Ridge	871		871	239	235	243	278	307	331	337	340	340	337	331
	Ocoquan	498	4	577	-119	-145	-176	-174	-181	-181	-181	-185	-192	-196	-207
	Old Bridge	532		532	56	68	73	99	114	123	128	130	130	128	124
	Rockledge	522	1	543	34	33	18	41	51	67	67	64	49	44	39
	Springwoods	946		946	187	170	174	198	215	234	242	246	247	243	235
	Westridge	709		709	49	42	16	37	46	50	58	62	62	59	52
	<b>Total (After CIP)</b>				<b>612</b>	<b>551</b>	<b>478</b>	<b>615</b>	<b>681</b>	<b>978</b>	<b>1,014</b>	<b>1,024</b>	<b>1,005</b>	<b>979</b>	<b>928</b>
	Percentage Utilization				87.8%	89.0%	90.5%	87.7%	86.4%	81.3%	80.7%	80.5%	80.8%	81.3%	82.3%
	New School - Add'l Space									226	226	226	226	226	226
Add'l Space Location									Occ ES - Replacement						
Linton Hall	<b>Total</b>	<b>6,724</b>		<b>6,724</b>	<b>1,399</b>	<b>1,213</b>	<b>1,104</b>	<b>1,117</b>	<b>1,106</b>	<b>1,089</b>	<b>1,086</b>	<b>1,053</b>	<b>962</b>	<b>908</b>	<b>807</b>
	Bristow Run	828		828	254	232	242	242	234	223	229	233	233	230	224
	Cedar Point	838		838	322	327	313	334	327	355	360	363	364	350	339
	Glenkirk	823		823	169	156	143	153	159	152	160	164	165	161	154
	Nokesville School (ES)	641		641	-30	-77	-122	-147	-179	-208	-236	-262	-281	-293	-329
	Piney Branch	931		931	207	201	198	208	225	239	247	251	251	248	240
	Victory	874		874	285	264	243	264	281	284	286	282	277	273	267
	Wood	950		950	92	70	51	65	63	54	63	68	69	64	50
	Yung	839		839	100	40	36	0	-5	-10	-23	-45	-115	-126	-137
	<b>Total (After CIP)</b>				<b>1,399</b>	<b>1,213</b>	<b>1,104</b>	<b>1,117</b>	<b>1,106</b>	<b>1,089</b>	<b>1,086</b>	<b>1,053</b>	<b>962</b>	<b>908</b>	<b>807</b>
	Percentage Utilization				79.2%	82.0%	83.6%	83.4%	83.6%	83.8%	83.9%	84.3%	85.7%	86.5%	88.0%
New School - Add'l Space															
Add'l Space Location															

# Proffer Justification Narrative

## Elementary School Space with Approved CIP Solutions School Years 2020-30

Planning Area	Elementary School	2020 Program Capacity	Portable Classrooms	Capacity w/ Portables	Space Available (based on Permanent Capacity)										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
Mid-County	Total	1,756	1	1,775	109	102	64	96	132	142	157	163	163	153	135
	Kerrydale	355	1	374	17	14	2	1	6	11	15	17	18	16	12
	Marshall	738		738	101	119	129	156	173	180	184	185	184	180	174
	Penn	663		663	-9	-31	-67	-62	-47	-49	-42	-39	-39	-43	-50
	Total (After CIP)				109	102	64	96	132	142	157	163	163	153	135
	Percentage Utilization				93.8%	94.2%	96.4%	94.6%	92.5%	91.9%	91.0%	90.7%	90.7%	91.3%	92.3%
	New School - Add'l Space														
Add'l Space Location															
Neabsco Mills / Northern Route 1	Total	4,737	12	4,952	282	166	-26	-37	13	74	89	72	23	-52	-148
	Belmont	536		536	37	40	-1	4	6	19	23	24	22	4	-10
	Featherstone	415	3	468	-55	-71	-81	-73	-65	-70	-64	-62	-61	-66	-74
	Jenkins	662		662	80	93	89	87	115	106	108	106	106	82	46
	Kilby	684		684	87	74	28	21	25	29	15	-6	-32	-35	-43
	Leesylvania	760		760	76	34	14	-4	-20	-1	6	9	8	4	-5
	Marumsco Hills	579	4	654	21	15	-3	5	15	37	39	36	23	11	5
	Potomac View	516	2	547	6	-28	-54	-43	-44	-35	-34	-36	-44	-49	-58
	Vaughan	585	3	641	30	9	-17	-34	-19	-11	-3	0	1	-3	-9
	Total (After CIP)				282	166	-26	-37	653	714	729	712	663	588	492
	Percentage Utilization				94.0%	96.5%	100.5%	100.8%	87.9%	86.7%	86.4%	86.8%	87.7%	89.1%	90.8%
	New School - Add'l Space								640	640	640	640	640	640	640
	Add'l Space Location								Occ/ Woodbdg ES - [Site Needed]	Occ/ Woodbdg ES - [Site Needed]					
Sudley	Total	2,999	12	3,209	-73	-194	-327	-352	-362	-339	-301	-282	-279	-303	-445
	Ellis	492	3	548	56	54	52	49	55	56	61	63	64	62	-25
	Mullen	709	1	728	5	-39	-62	-79	-88	-88	-79	-74	-73	-84	-92
	Sinclair	662	3	715	-110	-137	-180	-193	-195	-186	-176	-172	-171	-176	-185
	Sudley	639	1	659	24	11	-13	3	11	20	27	30	31	28	21
	West Gate	497	4	560	-48	-83	-123	-132	-144	-141	-134	-130	-129	-133	-163
	Total (After CIP)				-73	-194	94	69	59	83	120	139	142	119	327
	Percentage Utilization				102.4%	106.5%	97.2%	98.0%	98.3%	97.6%	96.5%	95.9%	95.8%	96.5%	91.3%
	New School - Add'l Space						421	421	421	421	421	421	421	421	772
Add'l Space Location						Rosemount Lewis ES (60%)						Yorkshire Area ES (50%) - Site Needed		Yorkshire Area ES (50%) - Site Needed	

# Proffer Justification Narrative

## Elementary School Space with Approved CIP Solutions School Years 2020-30

Planning Area	Elementary School	2020 Program Capacity	Portable Classrooms	Capacity w/ Portables	Space Available (based on Permanent Capacity)										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
Yorkshire	Total	2,833	8	2,990	229	89	-45	-72	-123	-138	-131	-138	-166	-254	-312
	Bennett	828	2	879	123	56	-2	-21	-33	-50	-47	-50	-53	-59	-73
	Loch Lomond	441	6	547	-85	-120	-148	-168	-189	-174	-168	-164	-164	-167	-174
	Signal Hill	819		819	156	130	94	95	80	60	61	59	57	44	15
	Yorkshire	745		745	35	23	10	23	18	26	24	17	-6	-71	-80
	Total (After CIP)				229	89	235	209	158	142	150	142	114	27	320
	Percentage Utilization				91.9%	96.9%	92.4%	93.3%	94.9%	95.4%	95.2%	95.4%	96.3%	99.1%	90.8%
	New School - Add'l Space							281	281		281	281	281	281	632
Add'l Space Location							Rosemount Lewis ES (40%)						Yorkshire Area ES (50%) Site Needed	Yorkshire Area ES (50%) Site Needed	
Elementary School Total	Total (prior to CIP measures)	42,333	66	43,588	4,767	3,755	2,581	2,745	2,955	3,040	3,133	3,000	2,638	2,150	1,418
	Total (including CIP measures)				4,767	3,755	3,283	3,447	4,383	5,334	5,427	5,294	4,932	5,170	5,140
	Percentage Utilization				88.7%	91.1%	92.4%	92.0%	90.0%	88.0%	87.8%	88.1%	88.9%	88.6%	88.8%
	New School Space				0	0	702	702	1,428	2,294	2,294	2,294	2,294	3,020	3,722
	Addition Space				0	0	0	0	0	0	0	0	0	0	0
	Addition Classrooms				0	0	0	0	0	0	0	0	0	0	0

Notes:

- 1) Target utilization level is 95% of capacity.
- 2) Projected Program Capacity of schools to be built in out years is calculated by the following equation: Avg. Net Effective K-5 Class Size of Planning Region \* (46 classrooms \* (1-Avg. Pct. Of Reg. Rooms for Specialized Use))

# Proffer Justification Narrative

## Middle School Space with Approved CIP Solutions School Years 2020-30

Middle School	2020-21 Capacity	Program Capacity (SY2021+)	Portable Class-rooms	Program Capacity w/ Portables	Space Available										
					2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
<b>Total</b>	<b>20,949</b>	<b>22,264</b>	<b>58</b>	<b>23,482</b>	<b>-29</b>	<b>1,375</b>	<b>941</b>	<b>812</b>	<b>618</b>	<b>459</b>	<b>427</b>	<b>356</b>	<b>300</b>	<b>333</b>	<b>432</b>
Reagan	1,233	1,236	5	1,341	-149	-142	-154	-111	-152	-105	-62	25	36	30	27
Bull Run	1,233	1,153		1,153	76	73	32	12	-6	27	60	89	110	98	103
Gainesville	1,233	1,209	7	1,356	-164	-161	-212	-176	-198	-162	-183	-179	-223	-215	-198
Marsteller	1,233	1,217	5	1,322	-13	15	54	68	93	76	117	106	121	119	77
Nokesville School (MS)	439	448		448	48	68	31	5	-4	-26	-41	-69	-81	-81	-90
Unity Braxton	1,360	1,307		1,307	232	158	84	93	64	15	-35	-71	-70	-59	-52
Parkside	1,453	1,381	4	1,465	-56	-145	-189	-194	-174	-209	-242	-323	-356	-369	-361
Benton	1,464	1,436		1,436	120	95	130	138	193	169	236	274	361	368	380
Saunders	1,212	1,244	2	1,266	0	76	48	99	133	153	176	196	227	224	229
Beville	1,191	1,256		1,256	119	40	4	-12	-37	-4	-5	37	30	27	26
Hampton	982	1,039	2	1,081	-29	64	56	82	19	-5	-90	-94	-117	-109	-95
Lake Ridge	1,464	1,495		1,495	-14	40	56	11	32	30	105	130	173	184	200
Lynn	1,170	1,139	14	1,433	-196	104	81	-3	-61	-148	-136	-138	-112	-109	-99
Woodbridge	1,066	1,081	9	1,270	-153	56	61	34	43	35	43	59	45	52	65
Rippon	1,390	1,394	2	1,436	30	207	178	170	126	82	51	14	27	24	34
Potomac	1,464	1,409		1,409	251	198	141	124	155	190	203	162	111	121	137
Potomac Shores	N/A	1,462		1,462		513	448	377	345	344	310	249	147	155	170
Graham Park	867	863	8	1,031	-111	116	94	93	46	-4	-80	-110	-131	-126	-119
Pennington (MS)	243	243		243	-9	0	0	0	0	0	0	0	0	0	0
Porter (MS)	252	252		252	-11	0	0	0	0	0	0	0	0	0	0
<b>Total (including CIP measures)</b>					<b>-29</b>	<b>1,375</b>	<b>1,298</b>	<b>1,169</b>	<b>975</b>	<b>816</b>	<b>784</b>	<b>713</b>	<b>657</b>	<b>690</b>	<b>789</b>
<b>Percentage Utilization<sup>1</sup></b>					<b>100.1%</b>	<b>93.8%</b>	<b>94.3%</b>	<b>94.8%</b>	<b>95.7%</b>	<b>96.4%</b>	<b>96.5%</b>	<b>96.8%</b>	<b>97.1%</b>	<b>96.9%</b>	<b>96.5%</b>

Proposed and Approved CIP Solutions												
New Schools	Location											
	Add'l Space											
Additions	Location				11-room at Gainesville and 6-room at Reagan							
	Add'l Space				357	357	357	357	357	357	357	357

<sup>1</sup> Target utilization level is 95% of capacity.

<sup>2</sup> 2020-21 space available calculated on planning capacity, 2021-2030 space available calculated on program capacity.



# Proffer Justification Narrative

## High School Space with Approved CIP Solutions School Years 2020-30

High School	Capacity	Portable Class-rooms	Capacity w/ Portables	Space Available										
				2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28	2028-29	2029-30	2030-31
<b>Total</b>	<b>28,754</b>	<b>67</b>	<b>30,161</b>	<b>-2,146</b>	<b>-419</b>	<b>-954</b>	<b>-1,422</b>	<b>-1,409</b>	<b>-1,382</b>	<b>-1,746</b>	<b>-1,988</b>	<b>-2,301</b>	<b>-2,711</b>	<b>-2,855</b>
Battlefield	2,053	21	2,494	-855	-285	-10	129	232	299	364	337	379	485	564
Gainesville	2,557		2,557	944	327	-18	-46	-53	-35	-60	-119	-137	-179	
Patriot	2,053	20	2,473	-764	-223	33	115	162	222	243	336	360	356	365
Brentsville	1,110		1,110	134	100	67	-11	-34	-49	-136	-147	-170	-206	-224
Unity Reed	2,409	7	2,556	-253	188	313	357	332	266	186	170	86	22	-95
Osborn Park	2,430	4	2,514	-168	-518	-684	-797	-782	-756	-859	-866	-948	-1,102	-1,161
Colgan	2,053	7	2,200	-856	-807	-782	-735	-617	-562	-537	-493	-443	-389	-278
Hylton	2,053		2,053	-77	-125	-73	-127	-135	-35	0	78	116	115	105
Forest Park	2,053		2,053	-173	-200	-224	-195	-296	-386	-480	-543	-549	-576	-612
Woodbridge	2,734		2,734	-37	-162	-245	-321	-346	-323	-412	-484	-531	-564	-464
Gar-Field	2,839		2,839	560	452	406	403	428	411	419	329	302	246	142
Freedom	2,053	8	2,221	-115	-161	-345	-408	-388	-464	-481	-547	-681	-763	-795
Potomac	2,357		2,357	458	378	262	185	81	47	-17	-97	-103	-197	-224
<b>Total (including CIP measures)</b>				<b>-2,146</b>	<b>-419</b>	<b>-954</b>	<b>-1,422</b>	<b>-1,409</b>	<b>-1,382</b>	<b>811</b>	<b>569</b>	<b>256</b>	<b>-154</b>	<b>-298</b>
<b>Percentage Utilization<sup>1</sup></b>				<b>108.2%</b>	<b>101.6%</b>	<b>103.3%</b>	<b>104.9%</b>	<b>104.9%</b>	<b>104.8%</b>	<b>97.4%</b>	<b>98.2%</b>	<b>99.2%</b>	<b>100.5%</b>	<b>101.0%</b>

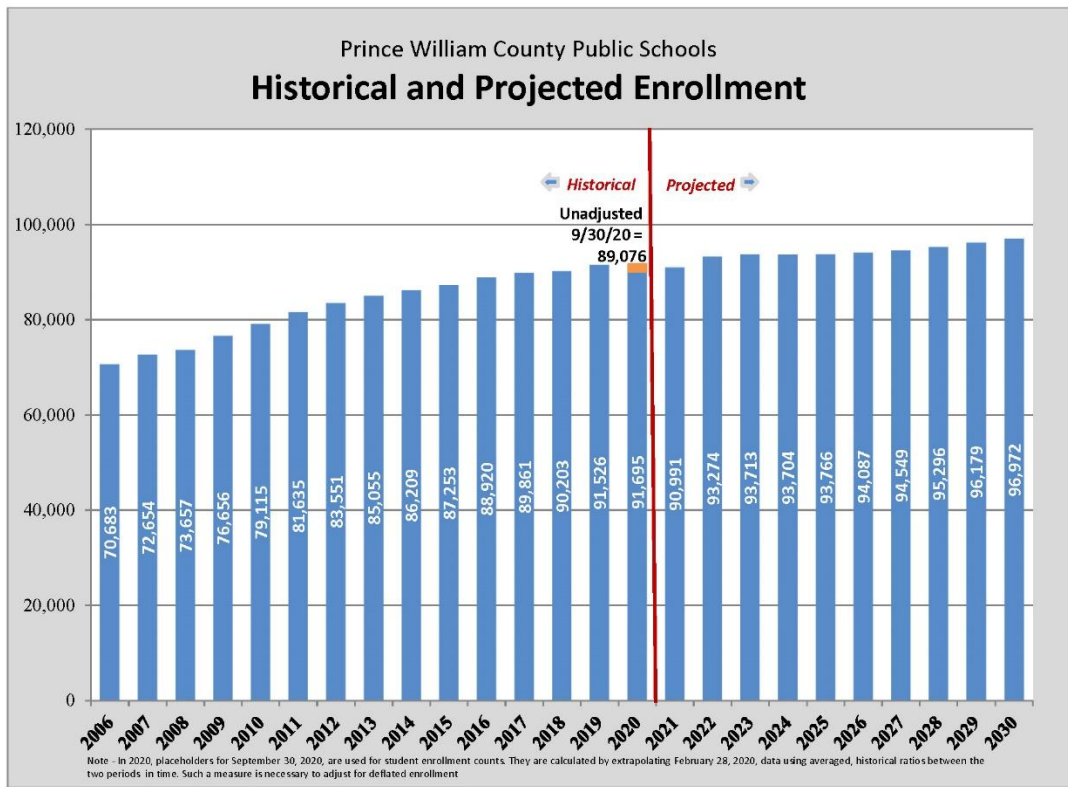
Proposed and Approved CIP Solutions														
New Schools	Location								44th HS (Mid-County)	14th HS (Mid-County)				
	Add'l Space									2,557	2,557	2,557	2,557	2,557
Additions	Location													
	Add'l Space													

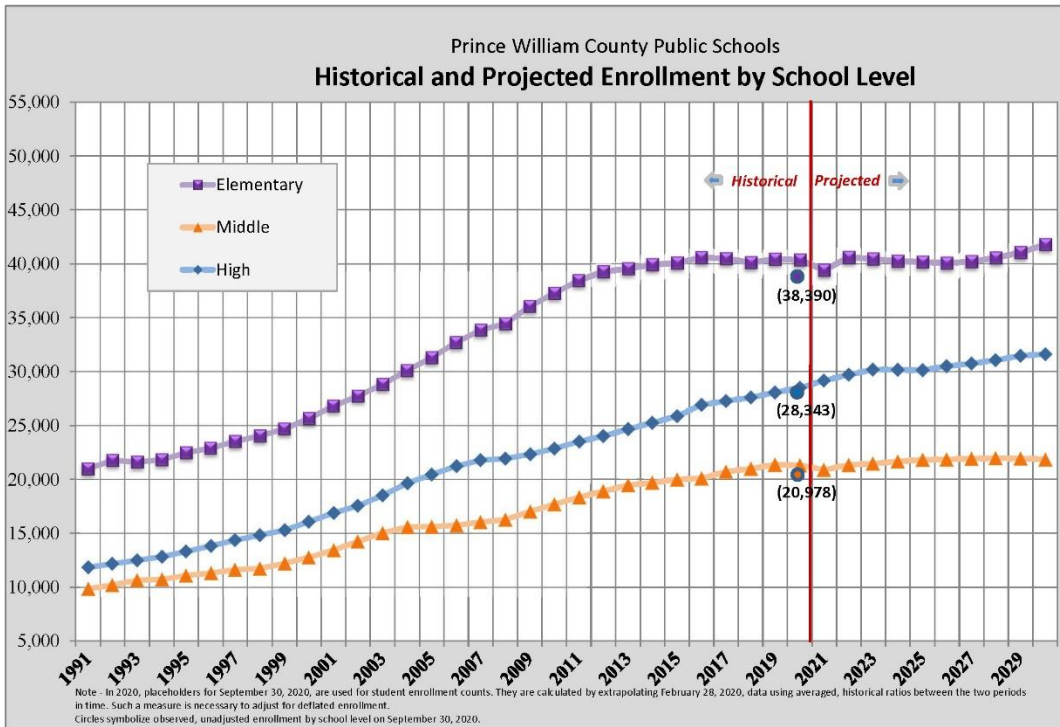
<sup>1</sup> Target utilization level is 95% of capacity.

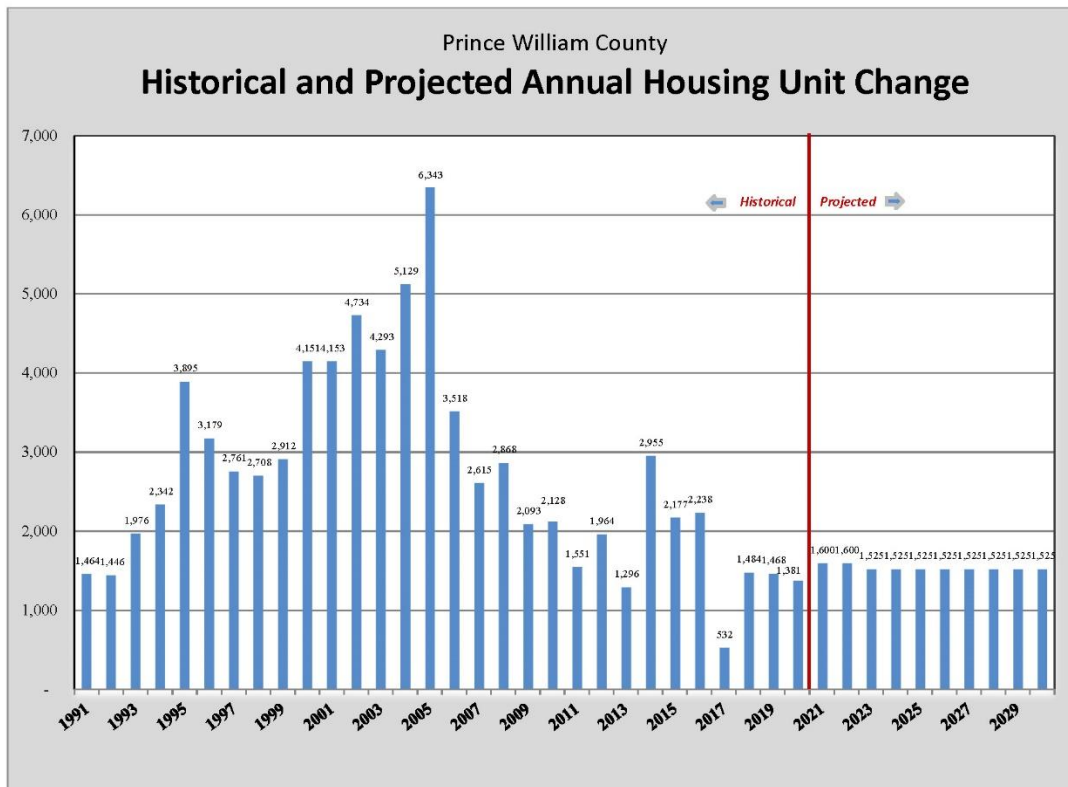
<sup>2</sup> Calculations including Gainesville High School begin in 2021-22 school year.



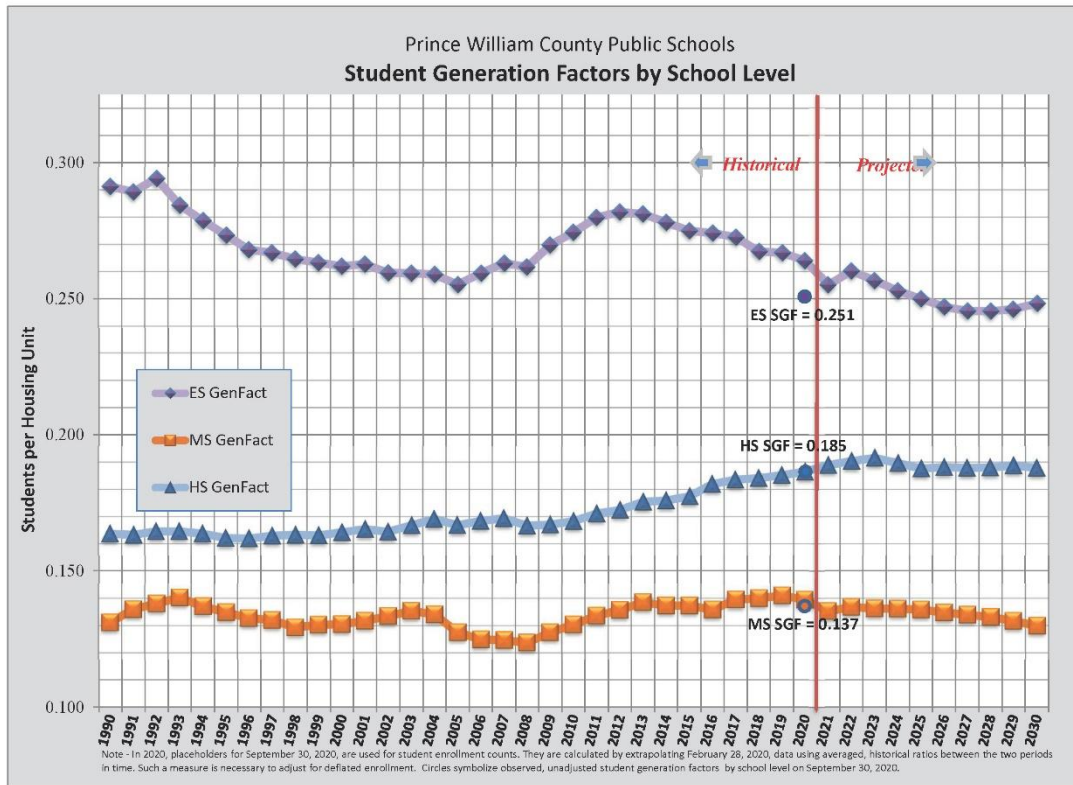








# Proffer Justification Narrative



# Proffer Justification Narrative

Prince William County Public Schools  
**Change in Enrollment Projections**  
 2019-20 to 2020-21

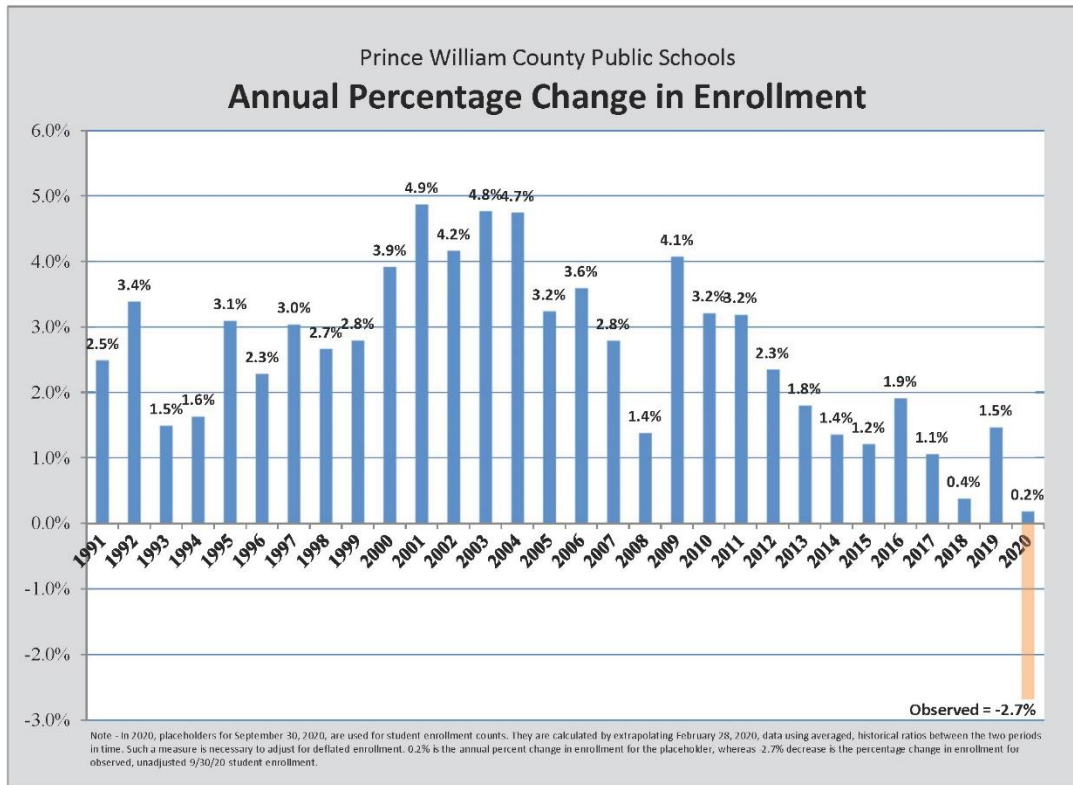
Year	2019 Enrollment Projections						Year	2020 Enrollment Projections						Year	Change 2019 to 2020						
	Housing Units	ES	MS	HS	SE	Total		Housing Units	ES	MS	HS	SE	Total		Housing Units	ES	MS	HS	SE	Total	Annual Incremental Change
2020	153,055	40,513	21,372	28,562	1,600	92,048	2020	152,836	40,348	21,315	28,505	1,527	91,695	2020	(219)	(185)	(57)	(57)	(73)	(353)	
2021	154,655	40,703	21,292	29,396	1,639	92,989	2021	154,436	39,403	20,889	29,173	1,526	90,991	2021	(219)	(1,300)	(403)	(183)	(113)	(1,699)	(1,645)
2022	156,255	40,933	21,383	29,731	1,663	93,709	2022	156,036	40,606	21,341	29,709	1,618	93,274	2022	(219)	(327)	(42)	(22)	(45)	(434)	1,563
2023	157,780	41,034	21,485	30,169	1,672	94,360	2023	157,561	40,447	21,466	30,176	1,624	93,713	2023	(219)	(587)	(20)	8	(48)	(647)	(212)
2024	159,305	41,268	21,610	30,078	1,690	94,644	2024	159,086	40,237	21,659	30,164	1,644	93,704	2024	(219)	(1,028)	49	86	(46)	(847)	(283)
2025	160,830	41,648	21,787	30,067	1,707	95,208	2025	160,611	40,153	21,817	30,136	1,659	93,766	2025	(219)	(1,495)	31	69	(47)	(1,442)	(503)
2026	162,355	42,154	21,828	30,453	1,722	96,157	2026	162,136	40,059	21,853	30,500	1,675	94,087	2026	(219)	(2,095)	25	47	(48)	(2,077)	(628)
2027	163,880	42,763	21,971	30,639	1,739	97,111	2027	163,661	40,192	21,925	30,741	1,691	94,549	2027	(219)	(2,570)	(46)	103	(48)	(2,509)	(491)
2028	165,405	43,484	22,220	31,001	1,755	98,460	2028	165,186	40,554	21,981	31,054	1,707	95,296	2028	(219)	(2,931)	(238)	53	(48)	(3,149)	(603)
2029	166,930	44,280	22,567	31,405	1,771	100,024	2029	166,711	41,041	21,950	31,466	1,722	96,179	2029	(219)	(3,239)	(618)	61	(49)	(3,844)	(680)
2030							2030	168,236	41,774	21,850	31,609	1,738	96,972								

Note: 2020 figures are placeholders for September 30, 2020, student enrollment counts. They are calculated by extrapolating February 28, 2020, data using averaged, historical ratios between the two periods in time. Such a measure is necessary to adjust for deflated enrollment.

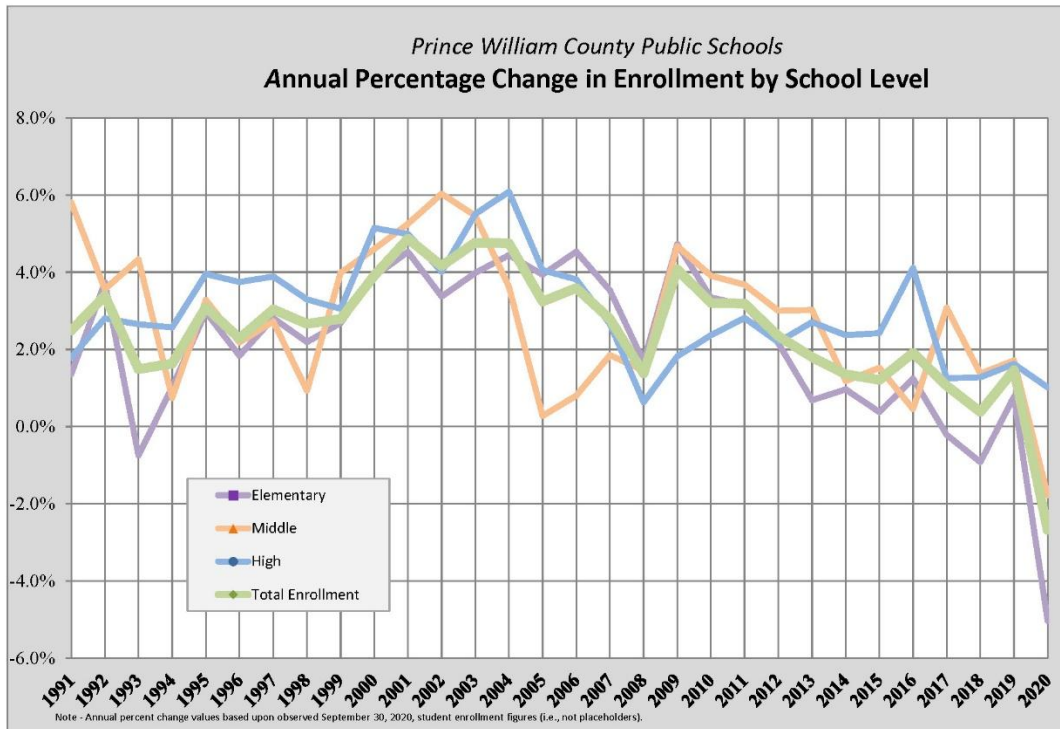
Equivalent Schools	(3.17)	(0.16)	0.02
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# Proffer Justification Narrative



# Proffer Justification Narrative



# Proffer Justification Narrative

## Portable Classrooms Totals

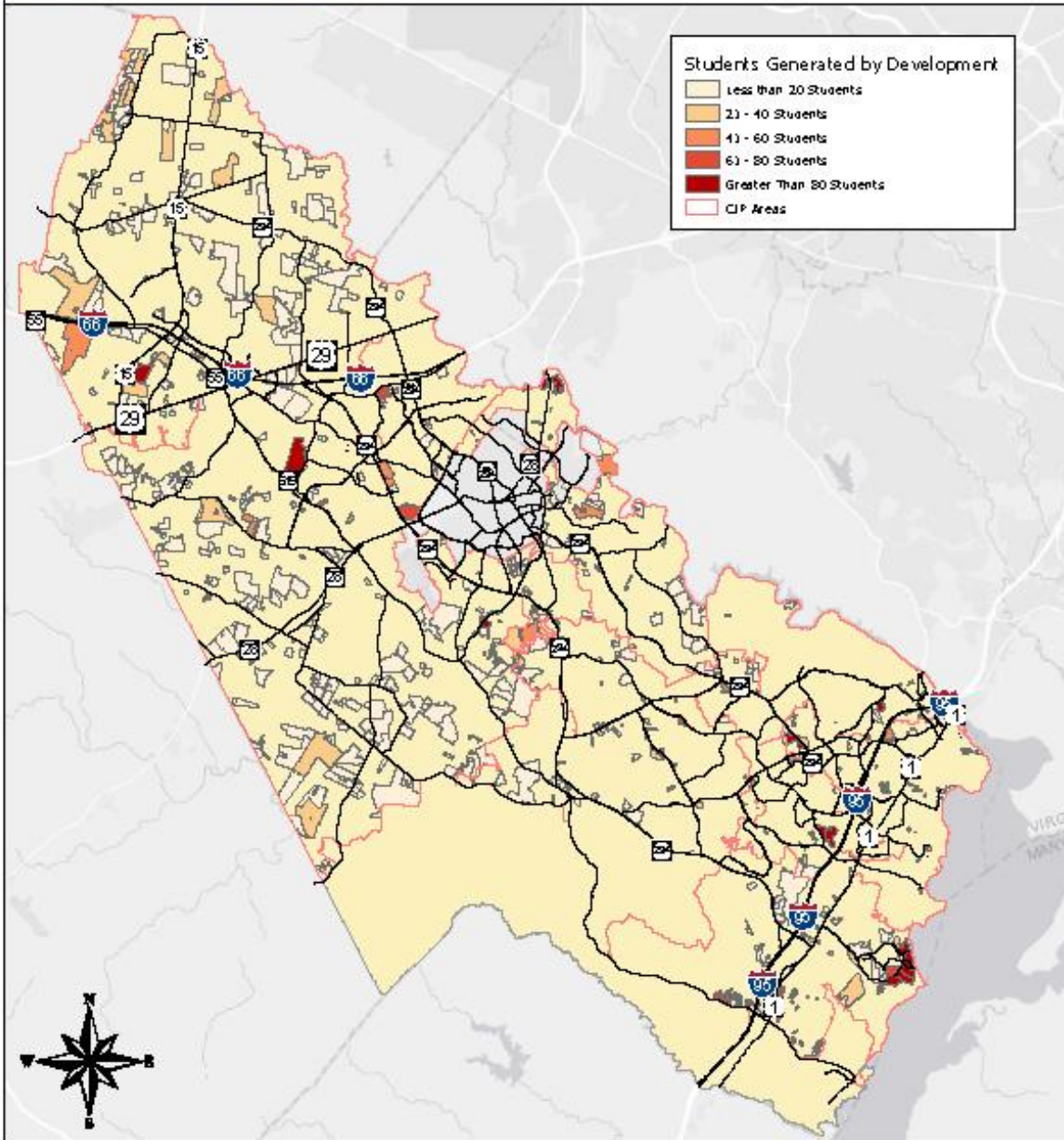
By Year and School Type

Year	Elementary	Middle	High	Alternative	Special Education	Total
1998	101	21	28	0	5	155
1999	109	25	25	0	5	164
2000	118	31	5	10	5	169
2001	122	37	2	10	5	176
2002	130	26	2	10	5	173
2003	141	16	2	10	5	174
2004	168	19	2	10	5	204
2005	185	40	5	10	5	245
2006	179	27	8	10	5	229
2007	184	20	14	10	10	238
2008	171	20	21	10	10	232
2009	150	21	36	10	7	224
2010	145	21	37	10	10	223
2011	132	31	21	10	10	204
2012	136	29	18	10	9	202
2013	126	23	30	11	9	199
2014	121	22	37	11	9	200
2015	122	27	46	11	9	215
2016	128	30	41	11	1	211
2017	110	36	49	11	1	207
2018	107	47	51	0	1	206
2019	72	50	61	0	0	183
2020	66	58	67	0	0	191

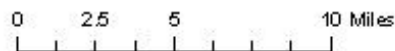
*Note: Portable classrooms used for instruction.*

# Students Generated

per Built-Out Development



Data:  
Road\_Shield  
CIP\_Areas  
Developments



Prepared by Sean O'Reilly, GIS Analyst  
Prince William County Public Schools  
Office of Facilities Services  
January 2021

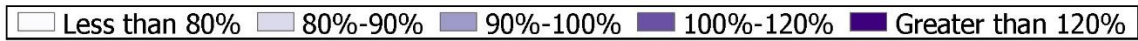
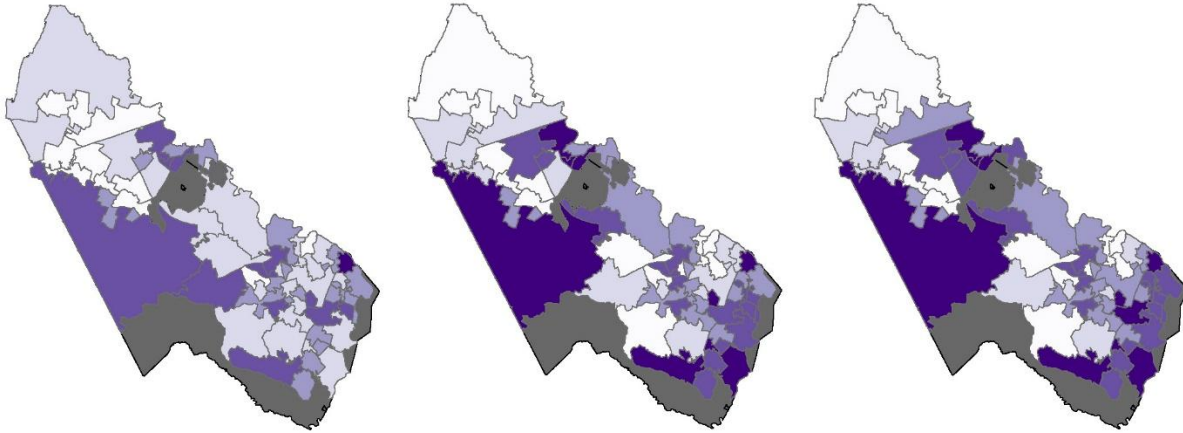


# Elementary School Capacity Utilization

2020

2025

2030



Data:  
ES20b  
School\_Cap

Path: C:\Users\orahilly\Documents\ArcGIS\Projects\ES\_School\_Capacity\_Util\ES\_School\_Capacity\_Util.aprx  
22

Prepared by Sean O'Rahilly, GIS Analyst  
Prince William County Public Schools  
Office of Facilities Services  
January 2021



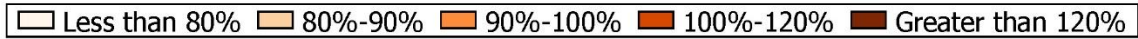
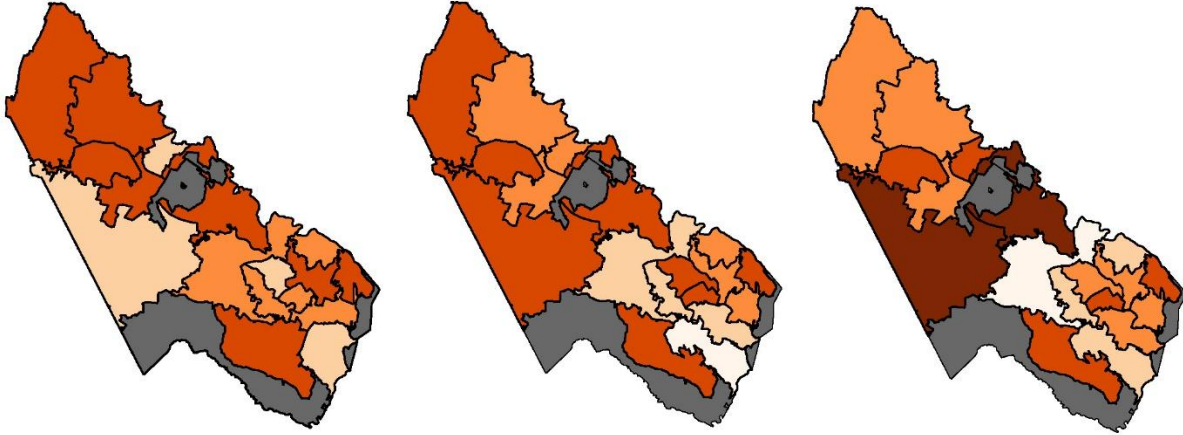


### Middle School Capacity Utilization

2020

2025

2020



Data:  
MS20b  
MS21a  
School\_Cap

Path: C:\Users\orahilly\Documents\ArcGIS\Projects\MS\_School\_Capacity\_Util\MS\_School\_Capacity\_Util.aprx  
23

Prepared by Sean O'Rahilly, GIS Analyst  
Prince William County Public Schools  
Office of Facilities Services  
January 2021

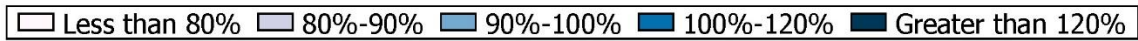
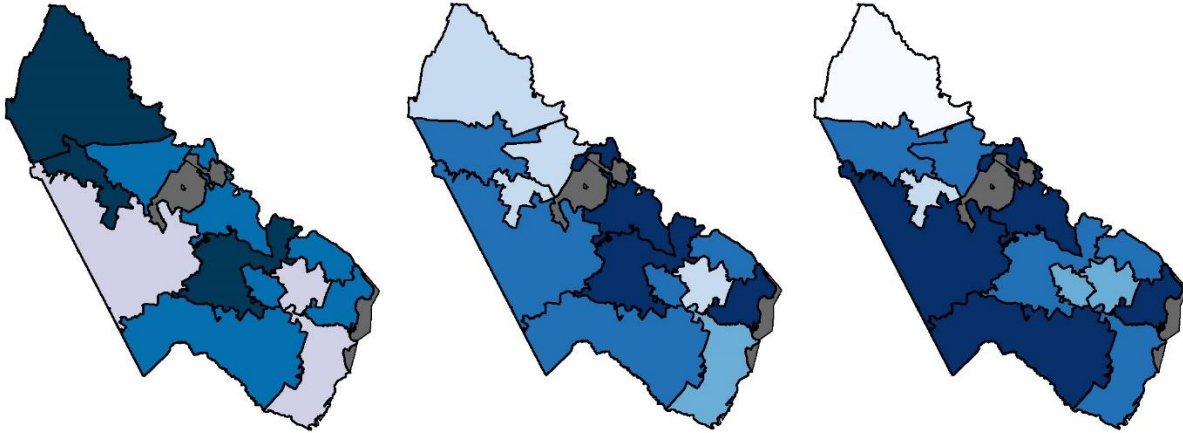


# High School Capacity Utilization

2020

2025

2030



Data:  
HS20b  
HS21a  
School\_Cap

Path: C:\Users\ora\hild\Documents\ArcGIS\Projects\HS\_School\_Capacity\_Util\HS\_School\_Capacity\_Util.aprx  
24

Prepared by Sean O'Rahilly, GIS Analyst  
Prince William County Public Schools  
Office of Facilities Services  
January 2021

### Agency Comments

On August 13, 2021, the Applicant indicated via email, additional willingness to revise the proposal to improve the mix of uses, building heights, and phasing within the Town Center designation of the Innovation Park Small Area Plan. Revised plans and the proffers that include these changes have not been submitted to the Planning Office at the time of publication of this staff report, and have not been reviewed by all relevant agencies. After the revised drawings are submitted, additional review from various agencies will be required.

The following agencies have reviewed the proposal and their comments have been summarized in relevant comprehensive plan chapters of this report. Individual comments are in the case file in the Planning Office:

- City of Manassas Airport
- PWC Archaeologist
- PWC Economic Development
- PWC Fire Marshal Office
- PWC Historical Commission
- PWC Housing & Community Development
- PWC Libraries
- PWC Parks, Recreation, and Tourism
- PWC Planning Office
- PWC Police / Crime Prevention
- PWC Public Works – Watershed / Environmental / Arborist
- PWC School Board
- PWC Service Authority
- PWC Transportation
- Virginia Department of Transportation (VDOT)
- Virginia Railway Express

For convenience, recent and relevant Agency Comments are also attached to this section of the report.



# City of Manassas Airport Director's Office

July 16, 2021

**TO:** Planning Office  
**FROM:** Jolene Berry, Sr. Airport Operations  
**SUBJECT:** Innovation Town Center

This is in response to the site plan review. We have the following comments:

- The developer should be mindful of standing water and trash as they can be an attractant for birds. An increase in bird activity can be hazardous to transient aircraft.
- The developer should use best practices in regards to dust control. Large dust plumes can reduce a pilot's visibility and can potentially damage aircraft engines.
- If a crane will be used during the course of construction, the developer is required to submit a 7460-1 form to the Federal Aviation Administration (FAA) no less than 45 days prior to construction. Forms can be found at <http://oeaaa.faa.gov>. The purpose of this form is to (14 CFR Part 77.5c):
  1. Protect the developer from liability
  2. Evaluate the effect of the construction or alteration on operating procedures
  3. Determine the potential hazardous effect of the proposed construction on air navigation
  4. Identify mitigating measures to enhance safe air navigation
  5. Place new objects on navigational charts
- Prior to erecting a crane, please notify Airport Operations (703-257-8270) at least 72 hours in advance with the crane's operating hours, location, and maximum boom height. All cranes must be flagged and/or lighted appropriately.
- Stormwater detention ponds should be designed, engineered, constructed, and maintained for a maximum 48-hour detention period after the design storm and remain completely dry between storms (FAA Advisory Circular 150/5200-33B, FAA Advisory Circular 150/5320-5D).
- Due to the site's proximity to the airport, attached to these comments is the Airport's Foliage Guide.

If you have any questions regarding these comments, please feel free to contact me.



Planning Office  
Rebecca Horner, AICP, CZA  
Director of Planning

January 28, 2021

To: Meika Daus  
Randy Thornton

From: Justin Patton

Re: Comments Submission No. 3  
REZ2016-00030, Innovation Town Center  
SUP2016-00031, Innovation Town Center

The Innovation Small Area Plan was approved on December 15, 2020, and both of the applications referenced above need to be consistent with the policies and action strategies therein. The applicant has conducted archaeological and architectural survey of the project area and proffered to curate archaeological artifacts with the County. Below is an excerpt of portion of those policies and action strategies that likely still apply.

Policy: Interpret the Small Area Plan's history to citizens and visitors

- Continue to conduct research and install historical markers and interpretive kiosks. Where possible collocate with planned open space and parks.
- Where appropriate, partner with developers and property managers to install historical markers and interpretive kiosks in consultation with the Historical Commission, the Planning Office and the Historic Preservation Division.
- Include interpretation of the Small Area Plan's history in planned open spaces.
- Require developers to use the Plan Area's history in placemaking.
- Where appropriate, plan and install interpretive trails and connect trails to commercial and residential areas outside of the Small Area Plan

The small area plan has a number of action strategies that call for interpreting the plan area's history in public spaces. Two spaces that are recommended are along the proffered Nature Trail and in the Town Center. Given each of the two areas above have distinctly different settings and purposes and the full details of those areas are not yet fully planned, it is recommended the applicant develop an interpretive plan for the Nature Trail and the Town Center, approved by the Planning Office and the Historic Preservation Division. The Interpretive Plan shall consist of at a minimum Interpretive Themes, Spaces, Interpretive Media, and management. Interpretive themes could include but are not limited to Twentieth Century farming in Prince William County, Dairy Farming in Prince William County, sharecropping and pre-contact (Native American) history. The Interpretive Plan shall include interpretive signs (multiple) and, or historical markers (both, to be purchased and installed by the applicant) but can also include other interpretive media such as brochures, etc. as long as they are permanent. The interpretive plan shall be prepared in consultation with and approved by the Planning Office and other entities as identified by the Planning Office. The Planning Office shall receive electronic copies of all final interpretive elements.



## Agency Comments

### Economic Development Comments

Tom Flynn

December 30, 2020

The Department of Economic Development has reviewed the Innovation Town Center REZ 2016-00030 and SUP 2016-00031. The Department supports these rezoning and special use requests. This rezoning will allow a mixed-use development that compliments the proposed University Village Center, and together with the University Village project will create the University Town Center long envisioned in plans for Innovation Park.

This Town Center project will provide needed services in a mixed-use environment to the growing business community in Innovation Park. Businesses looking for space in Innovation Park are often concerned about the lack of services and "place" for their employees. The Town Center will help provide the services and "place" that is now lacking at Innovation Park. It is important that this rezoning be coordinated with the University Village rezoning to maximize the placemaking opportunity and the economic development impact of these two projects.

The residential component of the project is needed to support the retail uses and create a critical mass planned for the project. The residential component may also provide housing for George Mason University faculty, staff, and researchers in a walkable community. The proposed rezoning also includes a tech commercial area in Land Bay A. More of this type of flex/office/industrial product is needed in Innovation to support the small business and technology companies wanting space in the Innovation Park area and continue to diversify our economic business base

Phasing of the project should promote the earliest possible development of commercial and/or mixed-use buildings in Land Bay E to promote the integration of the project with University Village and create the mixed-use environment missing in Innovation Park.

Let me know if you have any questions.

Thanks,

Tom Flynn, CEcD  
Director, Business Development and Investment  
Prince William County  
Department of Economic Development  
13575 Heathcote Boulevard, Suite 240  
Gainesville, VA 20155

Office: 703-792-5517  
Cell: 571-839-6371

web: [PwcEconDev.org](http://PwcEconDev.org)  
twitter: [@pwcded](https://twitter.com/pwcded)

## Agency Comments



FIRE & RESCUE SYSTEM  
Chief Timothy L. Keen

May 11, 2021

**TO:** Randy Thornton  
Office of Planning

**FROM:** Ernest H. Little, Fire Plans Reviewer  
Fire Marshal's Office

**SUBJECT:** REZ2016-00030 – Innovation Town Center – 8226 Wellington Road - Rezoning- Mixed Use - submission 4

As requested, the Prince William County Department of Fire and Rescue has reviewed a copy of the subject application, proposed proffers, and site plan, and offers the following comments:

**Conditions:**  
None

**Corrections:**  
None

**Recommendations:**

4.01- Fire/Rescue Station 22 (Groveton) went in service January, 2021 and is now the first due Fire/Rescue resource to this property. Due to the timeframe involved we do not have work load information available except that the station will be able to support 4,000 incidents per year.

4.02- Noting the change in phase of construction proposed if there are areas that would or may require Fire/Rescue services where emergency apparatus would need to respond through, the street system through the affected area should be in a state to make the such response possible.



# Agency Comments

February 9, 2021  
Regular Meeting  
Res. No. 21-008  
Page 2

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
REZ2021-00010	Old Carolina Overlook	Table

**Votes:**

**Ayes:** by acclamation

**Nays:** None

**Absent from Vote:** None

**Absent from Meeting:** Davis, Duley, Spinks

**MOTION CARRIED**

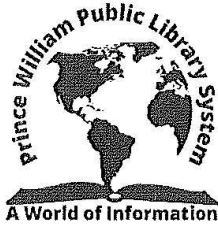
ATTEST:  \_\_\_\_\_  
Secretary to the Commission

## Agency Comments

Land Development Comments  
Tom Shrewsbury  
May 24, 2021

I see a modification request to reduce the 100' setback from the Parkway to a 50' landscape buffer. There is a 100' buffer along that side of the Parkway for all of the industrial uses. I would recommend that this remain a 100' planted buffer to be consistent with the Innovation buffer. No other comments.





## Prince William Public Library System

Administrative Support Center  
13083 Chinn Park Drive, Prince William, Virginia 22192-5073  
703-792-6100 | TTY 703-792-6163 | FAX 703-792-4875  
Director: Constance W. Gilman, Ph.D

[www.pwcgov.org/library](http://www.pwcgov.org/library)

August 25, 2016

**TO:** Steve Donohoe, Development Services Technician  
Planning Office

**FROM:** Connie Gilman, Library System Director *Connie Gilman*

**RE:** Innovation Town Center REZ PLN2016-00030 & SUP2016-00031  
Brentsville Magisterial District

The Prince William County Public Library System has reviewed a copy of the subject rezoning application and offers the following comments, relevant to the level of service (LOS) standards contained in the Library Plan, 2008 Comprehensive Plan.

### APPLICATION SUMMARY

The application is a Rezoning for Innovation Town Center in the Brentsville Magisterial District. The applicant is requesting to rezone approximately 106.047 acres from A-1, Agricultural to PMD, Planned Mixed District and approximately 23.745 acres from PBD, Planned Business District to PMD, Planned Mixed District, to allow for the development of a town center with a mix of retail, office, and multi-family residential uses. The overall town center proposal includes 343,000 square feet of nonresidential development, a hotel, and a maximum of 1,416 residential units, which include 200 age-restricted units. The site is identified on County maps as GPIN 7696-00-5505 and 7596-92-6825.

The applicant is also requesting a Special Use Permit to allow for the development of a town center that will contain a mix of retail, office, and multi-family residential uses. The overall town center proposal includes 343,000 square feet of nonresidential development, a hotel, and a maximum of 1,416 residential units, which includes 200 age-restricted units.

### LIBRARY NEAR THE PROJECT AREA

The library nearest the proposed project area is Bull Run Regional Library located at 8051 Ashton Avenue, Manassas, VA 20109.

Innovation Town Center  
REZ PLN2016-00030  
SUP2016-00031  
Page 2

**LEVEL OF SERVICE ANALYSIS**

The 2008 Comprehensive Plan contains level of service (LOS) standards for library facilities and prescribes strategies for achieving and maintaining those standards through the development review process. The LOS standards are based upon existing conditions within the County and industry standards. LOS standards for the Prince William Public Library System consist of the building size and number of volumes (periodicals and books) needed to meet national standards for suburban jurisdictions.

These LOS standards quantify the monetary costs for providing County-wide library service at LOS standards levels where new development affects library services and facilities. Doing so covers the additional resource needs for books and other materials necessary to offset the impact of added library users, as a result of development, within the County-wide Library System. The County-wide standard is important, as library users have access to all facilities, in any location, not just near their neighborhood. The LOS standards, as currently outlined in the 2008 Comprehensive Plan, determine that there should be a total of 8 full-service libraries (the 2 existing Regional Libraries plus 6 full-service Community Libraries) by 2030.

The applicant has proposed the following:

**LIBRARIES**

The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$805.00 per single family attached residential unit and \$597.00 multi-family residential unit constructed on the Property to be used for library purposes. Said contribution shall be paid prior to and as a condition of occupancy permit issuance for each said unit constructed on the Property.

As a result of applying the established LOS standards for libraries to the subject application, and in light of the applicant's proffered mitigation measures, this application meets the current LOS standards.

CWG/djc/Rezoning/FY2017/Innovation Town Center REZ PLN2016-00030 & SUP2016-00031

## Agency Comments

Long Range Land Use Comments  
REZ2016-00030  
Innovation Town Center  
Third Submission  
Connie M. Dalton Senior Planner  
January 4, 2021  
**Fourth Submission**  
**May 17, 2021**

This is a request to rezone ±106.8 acres from A-1, Agricultural, **to PMD**, Planned Mixed Use District, and ±1.32 acres from PBD, Planning Business District, to PMD, Planned Mixed Use District, to allow for a development of a mixed-use project that will include up to **996 residential units and ±447,100 sq. ft. of nonresidential uses and associated waivers and modifications**. The Special Use Permit is for ±26.42 acres to support the Town Center of the development and associated modifications and waivers.

The site is located on the east side of the Prince William Parkway (Route 234 By-Pass) between University Blvd., and Wellington Road.

The property is identified on County maps as GPINs 7596-92-6825 and 7696-00-5505 (pt.); is located within the Airport Safety and Technology Overlay Districts. The site is located in the Innovation Small Area Plan which was adopted by the BOCS on 12-15-2020. Brentsville Magisterial District

Project Name: Innovation Town Center  
Project Number: REZ2016-00030  
Magisterial District: Brentsville

The MZP Plan submitted for review is dated April 30, 2021, the associated proffers are dated April 30, 2021 and the updated Design guidelines are dated April 30, 2021.

In addition to the original comments provided below here are some general comments:

**DG – Design Guidelines:**

**Page 6** – Local Map – preference is to provide exhibit due north (the North arrow appears to be shown incorrectly)

**Page 29** – Trash Receptacles – consideration should be given to solar powered and receptacles that allow for recycling options to promote sustainability and environmental goals in the county. Bike Shelter – last sentence is incomplete. Could solar panels be provided to allow for lighting at night?

**Electric** Charging Stations, as discussed in the design guidelines, support the environmental concerns of the county and consideration should be given to providing stations near multi-family housing where a specific need could be met.

**Page 30** – Site Furniture – the numerous options should compliment and support the project; consideration should also be given to providing benches along the trail system to support the needs of all residents.

**Page 32**- The innovative and contemporary design options for lightning and other amenities support the potential branding efforts for the Innovation Small Area and springboard off of the GMU Science and Technology campus. Using alternative power sources would support the initiative to go green, to be resilient and sustainable.



Police Department  
Jarad L. Phelps | Acting Chief of Police



December 23, 2020

TO: Meika Fields Daus  
Planning Office, ZIP DS940

FROM: M.P.O. Jason Alicie / 1352 JWA  
Crime Prevention Unit

RE: REZ2016-00030, Innovation Town Center 2<sup>nd</sup> Submission

The Crime Prevention Unit has reviewed the application for REZ2016-00030, Innovation Town Center 2<sup>nd</sup> Submission. The applicant is requesting to develop a town center with a mix of retail, office, and multi-family residential uses. The overall town center proposal includes 440,000 square feet of non-residential development, a hotel, and a maximum of 1,108 residential units, which includes 200 age-restricted units. The provided plans do integrate Crime Prevention Through Environmental Design (CPTED). The applicant is encouraged to contact and work with the Crime Prevention Office within the Police Department for crime prevention techniques and CPTED concepts and strategies related to this site.

**Comments**

1. Parking

- The police department's experience that the lack of adequate parking within the residential community will increase the number of calls for service regarding parking complaints. This issue generally creates a significant quality-of-life concern within the community.

2. Pedestrian Safety

- While the intent is to create a walkable area, vehicles will inevitably be present. Therefore enhanced pedestrian crossing lighting, signs, and signals should be installed to maintain pedestrian safety.
- Roadway lighting has often focused on the motorist's needs and not necessarily the safety of the pedestrian. However, it is important to consider lighting that illuminates pedestrian crosswalks and reduces glare to motorists. Pedestrian fatalities occur disproportionately during dark conditions. Adequate roadway lighting enhances the safety of all roadway users. In contrast, pedestrian-scale lighting improves nighttime security and enhances commercial districts. Appropriate quality and placement of lighting can enhance an environment and increase comfort and safety. Pedestrians may assume that their ability to see oncoming headlights means motorists can see them at night; however, without sufficient lighting, motorists may not be able to see pedestrians in time to stop.

Considerations

## Agency Comments

- Install lighting on both sides of wide streets and streets in commercial districts.
- Use uniform lighting levels.
- Place lights in advance of midblock and intersection crosswalks on both approaches to illuminate the pedestrian's front and avoid creating a silhouette.

### 3. Landscaping

- Careful consideration should be given to the placement landscaping, in its current and mature form, as well as any other element that may have the potential for blocking light.
  - Avoid landscaping, which obstructs natural surveillance and creates hiding spaces. Keep shrubbery under 3 feet in height for visibility.
  - Trees with dense low growth foliage should be spaced, or their crowns should be raised to avoid a continuous barrier.
4. At this time, the Police Department does not believe this application will significantly impact police calls for service. However, as additional burdens of increased population, traffic congestion, demands for police services, complexities of calls for service, and the increased call volume may increase response time and officers' workload.

A copy of this memo will remain on file for future reference. If there are any questions or comments, please contact the Prince William County Police Crime Prevention Unit at 703-792-7270.

JWA/jwa

*1st Sgt Kitchens 12/23/20*



**WATERSHED MANAGEMENT COMMENTS**

**PROJECT:** Innovation Town Center  
**PROJECT#:** REZ2016-00030  
**FROM:** Benjamin Eib, Assistant Chief of Watershed Management Branch  
**REVIEWERS:** Julia Flanagan (Arborist), Clay Morris (Environmental Engineer)  
**DATE:** June 24, 2021 (5<sup>th</sup> submission)

**REQUEST:** Rezone 107 acres from A-1 to Planned Mixed Development (PMD) and 23 acres from Planned Business District (PBD) to PMD to allow development of a town center with a mix of retail, office, and up to 996 residential units and 447,100sf of non-residential uses. There are associated waivers and modification requests.

The subject parcel is currently planned for Office Mixed-Use (OMU) at the northern end, Community Mixed-Use (CMU) in the middle, and a Town Center (TM) at the southern end. The associated SUP case (SUP2016-00031) is for the proposed town center.

**SITE:** Site is predominantly agricultural fields, mixed hardwood forests and old field successional forest. There is no RPA onsite. There are jurisdictional wetlands and two forested intermittent stream channels. There are 13 specimen trees. This site is within the TEOD and the Innovation Small Area Plan with only the southern third of the site located within the Town Center Subdistrict (TM).

**COMMENTS:**  
**I. Anticipated Impacts on Goals, Policies and Action Strategies of the Comprehensive Plan Natural Resources & Sustainability**

5.1 (Repeat Comment) EN-10.1 calls for allowing reductions in setbacks and such to be permitted to increase the amount of protection and preservation of natural features and mature trees. The Applicant is requesting numerous waivers to increase density without significant preservation of natural features. See Comment below for recommended actions.

5.2 (Repeat Comment) A specific objective of planned developments is to “preserve the existing sensitive environmental areas” and “promote sustainable development patterns”, see ZO 32-280.01. The request zoning of Plan Mixed Development (PMD) requires a 50’ perimeter buffer that preserves forest where it exists. The Comprehensive Plan calls for protecting and preserving existing woodlands, specimen trees and wooded streams (DES 12.1-.3; EN-5.1; EN-10). The recently approved Innovation Small Area Plan calls for the preservation of open space and natural beauty through a compact design for the purpose of creating environmentally friendly development.

The Applicant has revised the limit of clearing to preserve some natural open space along the primary intermittent stream. While this is an improvement from the 0% preservation

Innovation Town Center REZ2016-00030  
June 24, 2021  
Page 2 of 7

previously proposed, the current proposal preserves only 4% of the total site area and only 7% of the total existing onsite forest across this 107 acre site.

There is a good quality, wooded intermittent stream/wetland system that bisects the site and continues along the eastern property boundary. There is also good quality mature hardwood forest along the Parkway in the area of the proposed town center. These should be used to create substantial natural open space that may also serve as a valuable amenity to the site. Even in town centers and urban development, the Comprehensive Plan calls for Applicants to recognize the value of these natural resource and community assets and preserve them.

Staff recommends the Applicant:

- a. Provide a minimum 50' wide undisturbed buffer on each side of the primary intermittent stream that runs along the eastern boundary of the town center and enters the proposed residential portion of the site. Currently as little as 10' is proposed in some areas.
- b. Commit to meeting their tree cover requirement through the preservation of onsite forest cover. This preservation should be focused along the intermittent stream, in the required 50' perimeter buffers per the PMD, and in the mature hardwood forest onsite. The newest design indicates developed open space proposed near one of the largest areas of forest onsite. Shifting the location of the open space and going from developed to preserved open space would both improve community design.
- c. These design elements will need to be proffered.

5.3 The limits of clearing and grading have been revised to preserve 6 of the 13 specimen trees located onsite. Proffer #10.d addresses their preservation. Please address the following:

- a. (Repeat Comment) Revise the proffer to read "...County DCSSM elements outlined in the Plant Selection Guide, Paragraph III."
- b. As proposed the Critical Root Zone (CRZ) for Specimen Trees T-2, T-3 and T-193 will be disturbed. The CRZ is the area of root zone that it is imperative to preserve in order to preserve these massive trees. In these same areas the minimum 50' undisturbed area recommended in the Comprehensive Plan for intermittent streams is also not met. Please revise the plan to remove all proposed disturbance from the CRZ and meet the minimum 50' undisturbed buffer along the intermittent stream.

## **Landscaping**

5.4 (Repeat Comment) Staff did not find proposed designs to integrate project entrance signs into the site frontage landscaping. Please provide such designs. A conceptual showing the general location and how it integrates into proposed road frontage landscaping will suffice. The Design Guidelines (See Page 23 of the DG) do not adequately address this provision as no quantitative or enforceable design elements are offered. Staff recommends it be addressed in the Proffers and SUP Conditions and graphically on the MZP. (DES-1.1, 1.6)

5.5 (Repeat Comment) Regarding the portion of the site, approximately 82 acres, outside the town center: This area lies within the Innovation TEOD and has extensive frontage on Prince William Parkway and Wellington Road. The Zoning Ordinance requires a 100' wide landscaped setback for the Parkway and a 50' landscape setback for Wellington Road. See ZO 32-506.06.

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With this submission, the Applicant proposes to modify this Zoning Ordinance requirement down to a 25' wide landscape buffer along Prince William Parkway (100' required) with some sections expanded to 40' wide. Along Wellington Road they propose only a 30' landscape strip (50' required) which is almost entirely occupied by existing utilities. This proposal does not upgrade the visual quality of these major travelways and it is far from meeting the minimum standards for Innovation. It would result in a significant downgrading of the quality and character expected of Innovation (DES-4.1, 4.3, EN-4.8).

Staff recommends the Applicant provide and commit to the following on the MZP and in the proffers:

1. The minimum required 100' landscape setback along the Parkway for residential landbays,
2. A minimum 30' wide setback along Wellington Road;

Planting within the setbacks should be conditioned to meet the standards as follows:

1. For the 100' wide buffers, planting equivalent to 2 Type C buffers,
2. For the 30' wide buffers, planting equivalent to a Type B buffer,
3. Proffers that no sidewalks or trails or utilities, except for minimal crossings will be allowed in these areas except as minimal crossings.

### **Stormwater Management**

5.6 (Repeat Comment) This proposal includes a high percentage of impervious surfaces including surface parking and does not utilize porous pavement to minimize impervious surfaces. Staff recommends the Applicant incorporate porous pavement type elements to create greater open spaces and reduce impervious surfaces. (DES-11.1; EN-8.6)

## **II. Site Specific Concerns:**

5.7 Regarding proposed Proffers:

- a. (Updated Comment) The Applicant has not proffered to retain the undisturbed open space shown on the MZP. These areas would preserve forested areas onsite. Once this plan is revised to improve the amount of preservation as recommended herein, a proffer is needed to commit to the "Approximate limits of clearing and grading" shown on the "Landscape Buffer Plan".
- b. (Updated Comment) Regarding Proffer #10.a.:
  - i. This proffer only commits "generally" to the landscaping shown on Sheet 4 and 4a. General conformance does not guarantee the Applicant will provide the buffers and landscape areas shown on the MZP. Please revise the proffer and commit to "substantial conformance" with these specific elements and revise the landscaping/buffering to conform with the minimum standards noted herein.
  - ii. The proffer refers to "Natural Landscape Area" buffers shown on Sheet 4 and 4A. Portions of these areas contain no forest or tree cover. Please clarify in the proffer that any areas so labeled will be supplemented with plantings to meet the minimum standards of the DCSM for that buffer size.
- c. (Updated Comment) Because the DG are only guidelines and include little to no enforceable design details for street landscapes, staff recommends that various features of the site be specified in the rezoning proffers as follows:

Innovation Town Center REZ2016-00030  
June 24, 2021  
Page 4 of 7

- i. Urban street designs for tree pits/panels. See, for example, DCSM Detail 650.19 for appropriate designs.
  - ii. The minimum widths for tree lawns in urban streets per DCSM standards.
  - iii. The use of Silva Cells, or similar system, for providing adequate minimum soil volumes under paved areas such as plazas.
  - iv. Provision of street trees, including spacing and how a hierarchy of streets will be achieved.
  - v. Perimeter buffers along Prince William Parkway and Wellington Road.
  - vi. Measures to improve stormwater infiltration.
- d. Staff recommends the Applicant agree to the exclusive use of native species in the replanting of this project to help compensate for the loss of native forest. (DES-13.1; EN-5.4)
  - e. With this submission Proffer #6 has been added that would clear and grade existing forest cover without a user for the site. This is opposed to multiple policies of the Comprehensive Plan and the Design and Construction Standards Manual (See DCSM 710.01.D) and opposed to sustainable design concepts. Staff recommends this proffer be deleted.
  - f. A proffer is needed to guarantee that all proposed buffers and landscape areas shall be provided outside of all sidewalks, utilities and their associated easements, except for minimal crossings.
  - g. Proffer #10.d requires a tree preservation plan only for specimen trees. Given that the site contains mature hardwoods that will be impacted by construction but are not specimen trees, staff recommends the Applicant make this proffer apply to all trees save areas.

5.8 Regarding proposed waivers. The Applicant is proposing at least 5 waivers or modifications to the County's minimum landscaping and buffering standards. Staff notes this will result in a virtual waiving or modification (and mostly waiving) of all minimum landscaping and buffering requirements. Overall, the Applicant has not justified these requests or provided alternative landscape designs suitable to substitute for them.

- a. (Repeat Comment) Proffers #38.a proposes to waive the 15' wide landscape area around recreational facilities. The Applicant has provided no information on an alternative plan for creating beautifully landscaped recreational amenities onsite. The required landscape area is minimal and provides for an aesthetically attractive facilities and parks, which is lacking in the proposal. Staff recommends this waiver be withdrawn or denied.
- b. (Updated Comment) Proffer #38.d is to allow tree cover required for each section of the development to be met instead by the development as a whole. Staff would be agreeable to this modification when the adjustments to the stream corridor and preservation within buffers as commented on elsewhere are met.
- c. Proffer #38.d also modifies DCSM 802.30 and 802.31 which have to do with minimum areas needed to effectively preserve trees. These are based on laws of nature that if ignored will result in failure to preserve trees. They are unrelated to the Applicant's desire to calculate tree cover for the overall project rather than on a section by section basis. Please delete these references.

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- d. Proffer #38.i is to reduce the required 50' buffer for residential units that back or side to a major roadway to 30'. Staff recommends this be deleted and the Applicant agree to the 100' TEOD buffer along the Parkway referenced earlier, or at a minimum the required 50' PMD buffer.
  - e. Proffer #38.j is to modify the TEOD buffer along the Parkway from 100' wide to 25' wide. The language references a 50' Type C buffer, but this is not what is proposed on the MZP. The reduction to 25' would result in a significant downgrading of the quality and character expected of Innovation and for residences along this major roadway. Also, "Exhibit A" is referenced. Staff could not find this exhibit in the review package received. Please provide it for review.
- 5.9 Regarding the proposed Design Guidelines (DG):
- a. Regarding proposed Tree Pits, the response letter states the Applicant is no longer proposing tree pits, however illustrations in the DG show tree pits.
    - i. (Repeat Comment) Tree pit details have been added to Page 20 of the DG. Language in the DG suggests "acceptable" alternative designs may be approved. Please include language that ensures the County Arborist's approval will be required for any alternative design.
    - ii. Soils in the tree pits are to meet VDOT standards. What are these standards? Please proffer to meet the following:  
*"Soils shall be free of all foreign materials (asphalt, concrete, rock, gravel, debris, etc.) and loose to a depth of a minimum of 3'. A top dressing of 4" to 6" of clean topsoil shall be provided through the pit. This topsoil shall be a loam, sandy loam, clay loam, silt loam, or sandy clay loam. The topsoil shall not be a mixture of or contain contrasting textured subsoils. The topsoil shall contain less than 5% by volume of cinders, stones, slag, coarse fragments, sticks, roots, trash, or other materials larger than 1" in diameter and shall not contain gravel. The topsoil shall contain a minimum of 5% natural fine organic matter, such as leaf mold, peat moss, etc."*
  - b. (Repeat Comment) The aerial space designed for tree canopies from the trunk to the buildings is unclear in the town center as well as the residential areas. The DG provides no information on this. Please provide details showing how residential and town center planting areas are designed to provide sustainable landscaping and avoids/minimizes conflicts with building structures. Page 17 does not adequately address this.
  - c. (Repeat Comment) The DG used to reference the potential to utilize planting strips as storm water features, but this appears to have been deleted. Staff recommends the Applicant commit to incorporating such features into the overall design, particularly within parking lots and provide examples/details in the DG or on the MZP.
  - d. The detail on Page 20 shows only one soil cell used under a portion of a sidewalk. Where this could be applied to plaza and such more sustainable volumes of soil are possible and should be shown graphically. Please revise the detail to allow for this.
  - e. Japanese Blood Grass (*Imperata cylindrica*) is listed as a noxious weed on the U.S. Federal Noxious Weed list. Please remove it from the DG.



Innovation Town Center REZ2016-00030  
June 24, 2021  
Page 6 of 7

- 5.10 (Repeat Comment) On the MZP Plan please show:
- a. (Repeat Comment) All streams and wetlands. The streams are not legible.
  - b. An accurate width of the buffers. The 50' buffer along the northern portion of the eastern property line actually measures about 36' in width. The required 50' buffer appears to need a shift in the location of the residential units shown. Please shift the units accordingly.

### **III. Conflicts with Minimum Development Standards:**

5.11 Regarding Buffers: The Applicant is proposing to waive or modify all required buffers. These proposals will reduce the quality of the development, eliminating green space, clear forests, reducing stormwater infiltration and property values.

- a. (Updated Comment) To be in compliance with the TEOD regulations, a 100' wide buffer is required along Prince William Parkway (ZO 32-506.06.1(a)). Proffer #38.j proposes to reduce this to a 25' wide buffer. Staff recommends the delete this proffer and meet the minimum standard of the TEOD along the Parkway or at a minimum meet the 50' wide PMD buffer.
- b. A perimeter 50' buffer that retains existing forest cover (where present) is required around the PMD. The Applicant is proposing the following changes:
  - i. (Repeat Comment) Reduce this buffer to 0' on the southern side of the town center. This is reduced to a simple streetscape containing only scattered shade trees. Staff recommends this boundary of the town center be modification be revised to show an improved streetscape a minimum of 10' in width and containing a mixture of shade and ornamental trees, shrubs and native ground covers.
  - ii. (Repeat Comment) A 25' landscape area along the eastern boundary in the town center. Staff recommends this area preserve the intermittent stream with a minimum 50' wide undisturbed area extending from the edge of the intermittent stream.
  - iii. (Repeat Comment) A 25' wide landscape area along a portion of the residences abutting an electrical substation on the eastern boundary. The DG do not provide for adequate buffering of industrial uses external to the site. Staff recommends the full 50' buffer be provided.
  - iv. (Repeat Comment) Along Wellington Road a 30' buffer is proposed that would almost entirely be occupied by utilities. This is substandard. Staff recommends the Applicant provide the 30' buffer outside of all utilities, allowing for minimum perpendicular crossing.
  - v. Along the Parkway only an approximately 25' wide buffer is proposed with a parallel FIOS utility next to it. As noted above, staff recommends the required 100' TEOD landscaped setback be provided along all residential landbays and 50' buffer in the Town center landbays. Where this contains existing hardwood forest the forest should be preserved, as the ZO states.
- c. A 50' wide buffer is required along the Parkway for all residences that back or side to the roadway. The TEOD 100' buffer would exceed this, but the Applicant is proposing to modify both below either minimum standard. Staff recommends this waiver (Proffer #38.i) be denied and the Applicant met staff's recommendations above.

## Agency Comments

Innovation Town Center REZ2016-00030  
June 24, 2021  
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d. (Repeat Comment) The Applicant should proffer to provide these buffers outside of all proposed and existing utilities except for perpendicular crossings.

5.12 (Repeat Comment) It is unclear if the 30 landscape area shown along Wellington Road is co-located with existing utility/water/sewer lines. An approximately 13' wide area of easement is proposed to run parallel in the 30' buffer. Please show the required buffer outside of all existing and proposed utilities, as required. (ZO 32-250.32.2; DCSM 802.10.F)

5.13 (Repeat Comment) The Innovation DG contain higher standards for interior parking lot landscaping. The Applicant states they are seeking to be released from the Innovation Declarations. This will result in lower quality to the overall site. Staff recommends the Applicant proffer to meet these standards without reduction. (Innovation DG; TEOD; DCSM 802.44)



**Prince William County**  
**PUBLIC SCHOOLS**  
*Providing A World-Class Education*

**Prince William County School Board – Impact Statement**

Date:	September 8, 2021		
Case Number:	REZ2016-00030		
Case Name:	INNOVATION TOWN CENTER		
Magisterial District:	Brentsville		
Description:	Rezone 107.4 acres from A-1, Agricultural and PBD, Planned Business District, to PMD, Planned Mixed Use District		
Proffer Evaluation Category:	<input type="checkbox"/> Pre-2016	<input type="checkbox"/> 2016-2019	<input checked="" type="checkbox"/> Post-2019

Proposed Residential Rezoning (number of units)	Student Generation for Proposed Rezoning																				
<table border="1" style="margin: auto;"> <thead> <tr> <th colspan="2" style="background-color: #0056b3; color: white;">Housing Units Proposed</th> </tr> </thead> <tbody> <tr> <td>Single-Family</td> <td style="text-align: center;">0</td> </tr> <tr> <td>Townhouse</td> <td style="text-align: center;">370</td> </tr> <tr> <td>Multi-family</td> <td style="text-align: center;">662</td> </tr> <tr> <td><b>Total</b></td> <td style="text-align: center;"><b>1032</b></td> </tr> </tbody> </table>	Housing Units Proposed		Single-Family	0	Townhouse	370	Multi-family	662	<b>Total</b>	<b>1032</b>	<table border="1" style="margin: auto;"> <thead> <tr> <th colspan="2" style="background-color: #0056b3; color: white;">Students Generated</th> </tr> </thead> <tbody> <tr> <td>Elementary</td> <td style="text-align: center;">206</td> </tr> <tr> <td>Middle</td> <td style="text-align: center;">99</td> </tr> <tr> <td>High</td> <td style="text-align: center;">125</td> </tr> <tr> <td><b>Total</b></td> <td style="text-align: center;"><b>430</b></td> </tr> </tbody> </table>	Students Generated		Elementary	206	Middle	99	High	125	<b>Total</b>	<b>430</b>
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<b>Total</b>	<b>430</b>																				

Developer Proposed Mitigation			
Monetary proffers are consistent with Monetary Policy Guide (for cases prior to July 1, 2016)?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input checked="" type="checkbox"/> N/A
School site, if offered, addresses a need identified in the School Division’s CIP?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input checked="" type="checkbox"/> N/A
The location and size of the school site, if offered, is acceptable to the School Division?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input checked="" type="checkbox"/> N/A
For cases July 1, 2016 to present			
The student generation methodology in the developer's impact analysis is acceptable?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No*	<input type="checkbox"/> N/A
*If No, what is the correct student generation?	Elementary School		Total Students
	Middle School		
	High School		
Monetary proffers, if offered, are based on adopted CIP projects, in terms of cost and in the geographic area of the rezoning, in the developer impact statement?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A

**Developer Proposed Mitigation**

- The application was filed prior to July 1, 2016. Proffer legislation was amended July 1, 2019 and the applicant has elected to submit this application pursuant to Subsection D of amended statute. For purposes of the statute, assessment of public facility capacity is based on the projected impact specifically attributable to the new residential development.
- The applicant’s Proffer Statement dated August 26, 2021, indicates a monetary contribution of \$8,549.47 per townhome unit and \$4,499.72 per multi-family unit, generating approximately \$6,142,118.30.
- The applicant has increased the overall number of planned units from 996 units to 1,032 total units, thereby increasing the number of proposed student generation from 425 students to 430 students.
- The applicant states the residential development allowed ‘by-right’ under the current zoning is estimated to be 10 single-family units, therefore student generation is calculated less the total ‘by-right’ student generation. (seven students)

**Countywide Current and Projected Student Enrollment & Capacity Utilization**

School Level	Available Space		2020-21			2025-26			2030-31		
	Capacity	Portable Classrooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Elementary School	43,053	66	38,390	4,663	89.2%	40,118	2,935	93.2%	41,740	-1,313	96.9%
Middle School	20,949 <sup>1</sup>	58	20,978	-29	100.1%	21,805	459	97.9%	21,832	432	98.1%
	22,264 <sup>2</sup>										
High School	26,197 <sup>3</sup>	67	28,343	-2,146	108.2%	30,136	-1,382	104.8%	31,609	-2,855	109.9%
	28,754 <sup>4</sup>										

<sup>1</sup> (MS) Planning Capacity is used for the 2020-21 school year.  
<sup>2</sup> (MS) Program Capacity will be replacing Planning Capacity in the 2021-22 school year. The numbers in the table reflect the change starting in 2021-22.  
<sup>3</sup> (HS) Capacity on which available space is calculated for the 2020-21 school year.  
<sup>4</sup> (HS) Capacity on which available space is calculated for the 2021-22 through 2030-31 school years.

**Current and Projected Student Enrollment & Capacity Utilization  
- Schools in same attendance area as Proposed Rezoning**

Under the School Division’s 2021-22 school attendance area assignments, students generated from the Proposed Rezoning will attend the following schools:

School Level	Available Space			2020-21			2025-26			2030-31		
	Planning Capacity <sup>1</sup>	Program Capacity <sup>2</sup>	Portable Classrooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Ellis ES	492	--	3	436	56	88.6%	436	56	88.6%	517	-25	105.1%
Marsteller MS	1,233	1,217	5	1,246	-13	101.1%	1,141	76	93.8%	1,140	77	93.7%
Unity Reed HS	2,409	---	7	2,662	-253	110.5%	2,143	266	89.0%	2,504	-95	103.9%

<sup>1</sup> (MS) Planning Capacity is used for the 2020-21 school year.  
<sup>2</sup> (MS) Program Capacity will be replacing Planning Capacity in the 2021-22 school year. The numbers in the table reflect the change starting in 2021-22.

**Current and Projected Student Enrollment**  
– Schools in same attendance area as Proposed Rezoning, including the effect of students generated from Proposed Rezoning

School Level	Available Space			2020-21			2025-26			2030-31		
	Planning Capacity <sup>1</sup>	Program Capacity <sup>2</sup>	Portable Classrooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Ellis ES	492	--	3	436	56	88.6%	642	-150	130.4%	723	-231	146.9%
Marsteller MS	1,233	1,217	5	1,246	-13	101.1%	1,240	-23	101.8%	1,239	-22	101.8%
Unity Reed HS	2,409	---	7	2,662	-253	110.5%	2,268	141	94.1%	2,629	-220	109.1%

<sup>1</sup> (MS) Planning Capacity is used for the 2020-21 school year.

<sup>2</sup> (MS) Program Capacity will be replacing Planning Capacity in the 2021-22 school year. The numbers in the table reflect the change starting in 2021-22.

**Schools Capital Improvements Program (CIP) Projects that may impact schools in attendance areas of the Proposed Rezoning (with year anticipated)**

Elementary School	Rosemount Lewis Elementary School (2023)
Middle School	Reagan Middle School 6-room addition (2022); Gainesville Middle School 11-room addition (2022)
High School	Gainesville High School (2021)

Note: The capacity utilization of an individual school due to the impact of future Schools CIP projects will vary based upon the attendance area modifications approved by the School Board.

**School Board Comments and Concerns**

- Current enrollment exceeds capacity at the assigned middle school (Marsteller) and assigned high school (Unity Reed). Projections for the assigned high school has capacity with the opening of Gainesville High School.
- As indicated above, the assigned elementary school and middle school are expected to exceed capacity within five years with the additional students under this application. However, Ellis Elementary School should see overcrowding relief with the opening of the “Rosemount Lewis” Elementary School in 2023 and the additions at Reagan Middle School and Gainesville Middle School in 2022 will provide overcrowding relief to Marsteller Middle School. The applicant is proffering to contribute \$6,142,118.30 to Schools as mitigation strategy.
- For these reasons, the School Board is not opposed to the subject application.



4 County Complex Court  
Woodbridge, VA 22192

Phone (703) 335-7900  
www.pwcsa.org

Division of Engineering & Planning  
Samer S. Beidas, P.E., CCM, Director



December 23, 2020

**MEMORANDUM**

To: Randy Thornton  
PWC Office of Planning

From: David L. Guerra, P.E. **David Guerra**  
Digitally signed by David Guerra  
DN: cn=David Guerra, o=pwcsa.org  
Reason: your signing reason here  
Location: your signing location here  
Date: 2020.12.23 15:43:43  
Full PhotonPDF Version: 9.7.1

Re: **REZ2016-00030 & SUP2016-00031, Innovation Town Center – 3<sup>rd</sup> Submission**

GPIN (s): 7696-00-5505, 7596-92-6825

The subject property is within the Development Area of the County and is thereby required to utilize public water and sewer to develop.

The Service Authority’s comments regarding this application are as follows:

1. No oils, fuels, anti-freeze, solvents or other pollutants or flammable substances shall be discharged into the public sewer system.
2. Applicant shall size, design and install a Service Authority (or PWC) approved grease trap on-site, if required by the Service Authority. The applicant shall properly maintain the grease trap to prevent grease build-up in the force main or gravity sewer.
3. Fire sprinkler systems shall incorporate a county approved backflow prevention device and be designed to eliminate water hammer.
4. Grinder pumps in the sanitary sewer system may be required.
5. The applicant shall install a county approved, adequately sized backflow prevention device on the water service line. This device shall be on the customer side of the water meter and before any point of use fixture of the on-site plumbing system.
6. For any proposed landscape irrigation system, the applicant shall demonstrate to the Service Authority that there is no detrimental effect on the Service Authority’s water distribution system and service pressure to the community. Irrigation systems shall be represented as a collective maximum hour demand for the hydraulic modeling of the proposed water system, both with and without a simultaneous fire flow event.

## Agency Comments

PWC Office of Planning  
Page 2 of 2  
REZ2016-00030 & SUP2016-00031

7. All on-site and off-site water system improvements necessary to mitigate the impact of the proposed irrigation system demands shall be the responsibility of the applicant.
8. The Service Authority has an existing 16-inch stub-out located on parcel 7596-92-6825, an existing 8-inch water main on the adjacent property east of parcel 7596-92-6825, and an existing 12-inch stub-out located on the southeast corner of parcel 7696-00-5505, with availability of capacity determined in conjunction with plan submission. The developer shall be required to install a minimum 12-inch diameter main through the site connecting the two specified stub-outs to provide a system loop closure for increased redundancy and water quality. In addition, connections to the existing 16-inch dry main located on the southwest corner of parcel 7596-92-6825 and the existing 12-inch water main along George Mason Circle shall be required. All connections to the public water system shall be in accordance with the Service Authority's USM requirements and restrictions.
9. The Service Authority has an existing 30-inch gravity sewer main located along the northern boundary of parcel 7596-92-6825 and an existing 16-inch gravity sewer main located east of the intersection of University Boulevard and Cannon Creek Lane, with availability of capacity determined in conjunction with plan submission. All connections to the public sewer system shall be in accordance with the Service Authority's USM requirements and restrictions.
10. Depending on the final configuration of any proposed on-site water mains, additional water main extensions may be required by the Service Authority to provide adequate fire protection or satisfy water quality requirements.
11. The applicant shall design and construct all new on-site and off-site water and sanitary sewer utility improvements necessary to develop the subject property and the above listed requirements in accordance with the Service Authority's USM, and County and State requirements, standards and regulations. The sizing and configuration of on-site and off-site utility system improvements will be determined during the preliminary and final plan review process, based on existing and proposed zonings of surrounding properties and the policies of the County Comprehensive Plan and Service Authority planning documents. The design shall be supported by appropriate engineering analysis/modeling of affected existing utility systems and the proposed new facilities.
12. Approval of a Special Use Permit or the rezoning of a property does not guarantee or assure water and sanitary sewer capacity availability for development of said property. Available utility system capacities are allocated on a first-come-first-served basis to zoned properties having approved final site/subdivision plans upon filing the required application and full payment of all associated utility fees/charges.

PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required		COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS	
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051		DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
ITEM No.	DWG. No. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE: August 10, 2021	FINAL DISPOSITION(3)		
6.01	SUP Plan and MZP	Parking Tabs must be included on both plan sets.	1	Parking Tabs have been included on both the MZP and SUP Plan Sets	The Parking tabs provided on the SUP Plan and MZP are insufficient. They must list the type of development, the square footage/type and number of residential units and the parking required and provided for each.	This comment has been satisfied.	
6.02	Proffer 27 a, iii, c	If the CTB does not approve the connection from PW Pkwy to Hylton Blvd., the Applicant must submit a revised TIA that shows how much development can be constructed on the property either with the approved proffered improvements or with additional mitigating measures.	1	As discussed with Staff, Proffer 27 has been revised to include a commitment to prepare a revised TIA if the CTB does not approve the connection from PW Pkwy to Hylton Blvd.	This comment has been satisfied.	This comment has been satisfied.	
6.03	Proffer 37	There are two Proffer 37s.	1	Proffer numbering has been corrected. Thank you for this comment.	This comment has been satisfied.	This comment has been satisfied.	
6.04	Proffer 35	Eminent Domain - the pedestrian facilities are not specifically proffered but are included on the MZP so they will be required. The BOCs has just recently been willing to use eminent domain for pedestrian facilities so it is recommended that sidewalks/shared use	1	Proffer 35 has been edited to include "associated pedestrian facilities".	This comment has been satisfied.	This comment has been satisfied.	

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
(2) The PWC reviewer is responsible for the final disposition of all comments.

Note: This form is to be used by the PWC Transportation Planning to provide comments or concerns associated with the rezoning applications, site plans, special use permit applications or any other plans when requested by or the applicants.  
Revised March, 2017

**PRINCE WILLIAM COUNTY  
DEPARTMENT OF TRANSPORTATION  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

TIA Required

**COMMENT CATEGORIES:**  
1. CORRECTIONS  
2. RECOMMENDATIONS

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COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC	REVIEWER(S): ELIZABETH SCULLIN 703-792-4051	DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER			
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE: AUGUST 10, 2021	FINAL DISPOSITION (3)
6.05	Sheet 6	This phasing plan does not make sense. Proffer 24 states that Katherine Johnson South must be constructed prior to the first occupancy permit on the property. This connection is essential for development of the Town Center on Katherine Johnson North - which is shown as Phase 1. Katherine Johnson North of the Town Center is shown as Phase 2 and proffer 25 states that this road would be constructed prior to the 500 <sup>th</sup> residential unit but there would be no connection to the residential units until this road is completed. Similarly, the phasing of the pad sites is based on the number of residential units but the connection to the residential units isn't proffered until the 500 <sup>th</sup> unit. The TIA doesn't propose improvements based on the amount of development so it's difficult to determine if the phasing of the pad sites is reasonable unless the phasing is a Planning/Economic Development request.		As discussed with Staff, the Applicant will make clarifications to the Phasing Plan shown on Sheet 6 of the MZP, and will provide the updated phasing plan to Staff between the Planning Commission and Board public hearings.	The Phasing Plan must be tied to the TIA and the capacity of the roadway network to handle the traffic associated with each of the buildings/residential units. The Phasing Plan must be submitted to PW/CDOT 3 weeks before the Board public hearing in order for adequate time for review.

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
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 Revised March, 2017

PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required		COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS	
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051		DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER		RESPONSE <sup>(2)</sup> DATE: August 10, 2021		FINAL DISPOSITION <sup>(3)</sup>	
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE: August 10, 2021	FINAL DISPOSITION <sup>(3)</sup>		
5.01	Proffer 34 b. iii	This proffer must include the option of a roundabout at Wellington Rd/Hornbaker Rd; if the SJR determines that this is the best option instead of a traffic signal.	1	Comment acknowledged. The proffer has been revised.	8-13-21 - The Applicant has addressed the County's comments about modifying the calculations. A note must be added that states that detailed cost estimates will be provided to the County for approval before the Board public hearing. These estimates must be provided 3 weeks before the hearing to ensure adequate review time.	Proffer 30 b. iii has been revised to include the roundabout option. However, Sheet 5D includes a note that reads "May have option of contributing funds to planned roundabout in lieu of constructing signal." This comment must be removed from Sheet 5 D.	

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
 (2) The PWC reviewer is responsible for the final disposition of all comments.

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 Revised March, 2017



PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required		COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS	
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051		DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE: AUGUST 10, 2021	FINAL DISPOSITION (3)		
5.02	Proffer/ Waiver 38 k.	Plan sheets 5 C and D do not show dimensions for any of the criteria listed in this waiver/proffer. PWCDOT cannot approve a waiver for items that are not specifically enumerated.	1	Agreed and any reference to a waiver not included at this time has been removed. Any other dimensions depicted are for reference only and do not intend to demonstrate compliance if a waiver is required at a later date.	8-13-21 - This comment has been satisfied.		
5.03	Proffer/ Waiver 38 l.	Section 603.20 F of the DCSM allows alternative roadway lighting fixtures (decorative streetlights) as approved by PWCDOT/VDOT on internal roadways. Section 602.13 specifies that standard streetlights are required at major intersections and all entrances to the development, cul-de-sacs, stub-outs and dead ends. There is no need for a waiver of this section if	1	Agreed and the proffer has been removed.	All waivers for Section 600 of the DCSM have been removed from the proffers. This comment has been satisfied.		

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
 (2) The PWC reviewer is responsible for the final disposition of all comments.  
 Note: This form is to be used by the PWC Transportation Planning to provide comments or concerns associated with the rezoning applications, site plans, special use permit applications or any other plans when requested by or the applicants.  
 Revised March, 2017

**PRINCE WILLIAM COUNTY  
DEPARTMENT OF TRANSPORTATION  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

TTA Required

**COMMENT CATEGORIES:**  
1. CORRECTIONS  
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PAGE 5 OF 23

COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)

DEVELOPER/ENGINEER: LDC

REVIEWER(S): ELIZABETH SCULLIN  
703-792-4051

DATE: 7-27-21/8-13-21

ITEM No.	DWG. No. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE: August 10, 2021	FINAL DISPOSITION (3)
5.04	Proffer/ Waiver 38 m	Street name signs must follow the required Font Size, Placement, Color and Reflectivity per DCSM 650.52 and 650.53. There is some flexibility in "ornamental" posts and frames, but no other items are waivable.	1	Agreed, and the proffer has been removed.	All waivers for Section 600 of the DCSM have been removed from the proffers. This comment has been satisfied.
5.05	Proffer/ Waiver 38 o	A waiver to allow off street parking in excess of 150' from single family attached lots is not allowable during the rezoning. Each case must be analyzed at subdivision plan review.	1	Agreed and the proffer has been removed. The applicant may pursue a waiver or modification at a later date in conjunction with the site plan.	All waivers for Section 600 of the DCSM have been removed from the proffers. This comment has been satisfied.
4.01	TTA	The Applicant must develop intermediate improvements at the intersection of Prince William Pkwy/Wellington to address the developments contribution to the intersection operation preceding the ultimate improvements to the intersection. (see comment 3.17)	1	As stated in a previous response, The PW Pkwy & Wellington intersection is part of a regional issue that Prince William County is continuing to examine and the large scale solution cannot be put onto one applicant. The applicant is providing significant mitigation to the quadrant area by advancing the county's regional solution. This includes realigning Bethlehem Road to the future location which helps distribute traffic and also provides more space for queuing at	6-25-21- The Applicant's response is a reasonable approach to mitigating impacts of its development on the intersection of Prince William Parkway and Wellington Road. An estimated cost of the extension of the turn lanes must be submitted to PWMC to determine if it is a valid estimate for these

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
(2) The PWMC reviewer is responsible for the final disposition of all comments.

Note: This form is to be used by the PWMC Transportation Planning to provide comments or concerns associated with the rezoning applications, site plans, special use permit applications or any other plans when requested by or the applicants.  
Revised March, 2017

PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required	COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051	DATE: 7-27-21/8-13-21
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER			
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE: AUGUST 10, 2021	FINAL DISPOSITION(3)
				234/Wellington. Additionally, a signal is being proposed at Hornbaker Rd & Wellington which is also an important intersection included in the County's project. Both of these improvements provide additional capacity to roads parallel to Prince William Parkway and advance the County's plan.	Improvements. The cost estimate must be submitted before the Board approves the case.
				6-15-21 - In order to additionally mitigate the impacts at the PW Pkwy & Wellington Rd intersection, the applicant is proffering to provide either a fair share monetary contribution to the County to facilitate future regional improvements at this intersection or to extend the NB and SB left turn lanes, depending on County funding at the time the 500 <sup>th</sup> residential unit.	Applicant Response: The cost estimate has been submitted.
				If County funding exists at that time, there would be no benefit from extending the turn lanes as the County plans to remove the NB and SB left turn movements. If funding does not exist at that time, the extension of the NB and SB left turn lanes would provide additional storage and prevent potential starvation and overflow issues.	A cost estimate of \$400,000 was provided by email for the extended turn lanes. Proffer 28 a fill refers to an estimate of \$383,114.06. The detailed cost estimate must be submitted with the other items that have been requested so that the County has a comprehensive cost estimate for all proffered transportation improvements.
				8/10/21 Response: A revised cost estimate, dated August 2,	8/10/21 Response: A revised cost estimate, dated August 2,

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
 (2) The PWC reviewer is responsible for the final disposition of all comments.

Note: This form is to be used by the PWC Transportation Planning to provide comments or concerns associated with the rezoning applications, site plans, special use permit applications or any other plans when requested by or the applicants.  
 Revised March, 2017

PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required		COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS	
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051		DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE: August 10, 2021	FINAL DISPOSITION (3)		
4.02	Proffer 26	This proffer states that the Applicant would be relieved of its responsibility to construct Hyfton Blvd. West if the approval for the limited access break at Prince William Parkway is delayed or denied. Given that the Applicant shows access on Hyfton Blvd. West, it would still have to construct this roadway whether it connects to P. W. Pkwy. or not. This proffer must include a trigger such as prior to the issuance of the	1	Please see the revised proffer. Now Proffer 27.	6-25-21 -This comment has been satisfied.  8-13-21 - The cost estimates are general in nature. The County requires a detailed cost estimate of the proffered transportation improvements 3 weeks before the BOCS public hearing so that DOT can assure the BOCS that the MuniCap Proffer Analysis is correct.  2021, is provided with this resubmission.		

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**PRINCE WILLIAM COUNTY  
DEPARTMENT OF TRANSPORTATION  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

TIA Required

**COMMENT CATEGORIES:**  
1. CORRECTIONS  
2. RECOMMENDATIONS

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TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
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4.03	Sheets 5A and 5B	first occupancy permit on the Property or within 36 months after CTB approval of the access from Prince William Parkway.  These sheets provide the same information. Please remove the duplicative sheet.	1	Please see the revised Sheets 5A and 5B that are now separate sheets.	6-25-21 – This comment has been satisfied.		
4.04		The Innovation Park Small Area Plan includes a pedestrian/bicycle bridge over Prince William Parkway north of the planned University Quadrant intersection. This property required for a terminus for this bridge should be proffered unless the bridge design could be incorporated into an upper floor of a parking garage.	1	The Innovation Park Small Area Plan included this pedestrian bridge when the area on the other side of Prince William Parkway was planned to be residential. At the Board public hearing, this area was changed to Office Mixed Use, therefore, the pedestrian bridge is not needed.	It is not the Applicant's responsibility to determine whether the pedestrian bridge is needed or not. It is shown on the Town Center Illustrative Plan (p. 96) and is cited in the Proposed Bicycle and Pedestrian Network. It will connect the office uses west of PW Pkwy to the Town Center and provide connectivity to the Innovation Park Small Area Plan.	<b>Applicant response: Please see new Proffer 37, the Applicant has proffered to dedicate right</b>	
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TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER		RESPONSE <sup>(2)</sup> DATE: AUGUST 10, 2021	
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					7-27-21 – It is doubtful that the County will have funding for the construction of the pedestrian bridge at the time a site plan for Land Bay E is filed. Remove this sunset clause from the proffer.  8/10/21 Response: As discussed with Staff, the Applicant has edited Proffer 37 to commit the Applicant to design the future parking garage located in Land Bay E to facilitate a connection to the future pedestrian bridge.  8-13-21 - This comment has been satisfied.


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4.05	Proffers/ Waivers	All waivers for Section 600 in the DCSM must be removed from the Proffers and submitted through the Waiver process with adequate justifications. The majority of these waivers refer to items that would be determined at site plan and/or are too broad to be included within the Proffers. To ask for a waiver of right of way width, design speed, intersection alignments, tangent distance, land width, median width, bike lane width, max. grade, centerline radius, off-street parking requirements, intersection curb returns, intersection design, entrance/crossover spacing, turn lanes, channelization, location and materials without any details is not reasonable nor feasible.	1	As discussed at the meeting with County Transportation, an updated list of waivers will be submitted to County Transportation to be reviewed concurrent with this rezoning application.	6-25-21 – See comments 5.02 - 5.05.
4.06	Narrative	Transportation – This must be updated to reflect the realignment of the Spine Road (Katherine Johnson Blvd.) to align with Bethlehem Road and remove mention of the potential VRE Station.	1	The Narrative was updated to address this comment.	6-25-21 – The VRE station is shown in the Design Guidelines on p. 10. It should either be labeled as Potential (after 2040) or removed from the graphic. <b>Applicant Response: Agreed and the graphic on Page 10 of</b>

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					This comment has been satisfied; however, the graphic on page 10 includes a reference to the Bethlehem Rd. improvements but, there are other improvements shown on the graphic that are not Bethlehem Rd improvements.  8/10/21 Response: The Applicant intends to update the Design Guidelines between the Planning Commission and Board public hearings to address this comment.  8-13-21 - This information

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ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE: August 10, 2021	FINAL DISPOSITION <sup>(3)</sup>		
4.07	Sheet 5F	The typical sections for Wellington and Bethlehem must conform to the DSCM and include pedestrian facilities.	1	The typical sections for Wellington and Bethlehem Roads were added to the plans.	6-25-21 - The typical section for Wellington does not conform to the MA-1 typical section as the 10' asphalt path is shown outside of the right of way. The Sudley Manor/ PW Pkwy Performance Based Planning Pilot Final Report (July 2020) shows a 10' asphalt path on the east side of relocated Bethlehem Road. See revised typical section on sheet 5F with the relocated shared use path along Wellington Road.	Applicant Response: The sections have been revised.  This comment has been	

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3.01		Proposed Street A does not conform to the Innovation SAP as it should be a UB-1 standard rather than a UTB-1. The UTB standard does not allow for parking and/or a bike lane -- features that are desirable in a Town Center.	1	Katherine Johnson Avenue (former Street A) has been revised from a UTB to a UB-1 standard. (4-29-21)	This submission shows an ADT of 9,299 including traffic from University Village at Innovation. This volume requires a UTB typical section which would preclude parallel parking. Given that on-street parking is important to the function of a Town Center, it is recommended that the Applicant submit a waiver request to allow a UB-1 typical section. (5-24-21)		
					6-25-21 - The waiver and justification has not been submitted. <b>Applicant Response: The UB-1 waiver has been submitted.</b>		

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3.02		A roundabout at the intersection of Road A and Road B should be analyzed. On-street parking would not be allowed on Road B until the first-planned right-in/right-out access into the Town Center due to safety concerns.	1	A roundabout section is included in the TIA. No on-street parking will be proposed west of the first access on Road B (now called Hytton Blvd in the TIA). (4-28-21)		The UB-1 waiver has been received but has not yet been approved.  8/10/21 Response: The UB-1 waiver was approved by PWCDOT on 8/10/21 (WAL2022-00003).  8-13-21 - This comment has been satisfied.	
3.03		Wellington Road is a Minor Arterial with 128' right of way along the frontage of this property. Sixty-four feet of right of way must be dedicated to the County.	1	There is no need for additional ROW dedication to accommodate improvements on Wellington Road. (4-28-21)		This comment has been satisfied. (5-24-21) 6-25-21 - The typical section shown on Plan Sheet 5 F indicates that the shared use	

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3.04		It appears as if the intersection spacing on Wellington between PW Pkwy. and Relocated Bethlehem will not	1	The location of the relocated Bethlehem Road has been shifted to meet spacing requirements. All queues at the study intersections are reported in	This comment has been satisfied. (5-24-21)	This comment has been satisfied.	


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3.05		meet the spacing requirements in the DCSM. The westbound left turn queue on Wellington at PW Pkwy must be analyzed.		the TIA. (4-28-21)			
3.06		The TIA doesn't address trips generated or parking spaces required by the development that could be mitigated by a shuttle service to the VRE station or other sections of Innovation Park. A TDM must be submitted that addresses transit options.	1	The revised MZP includes trips from Innovation North and University Village. (4-28-21)  A TDM section has been added to the TIA. (4-28-21) 6/15/21: A separate TDM has been submitted with this application.	This comment has been satisfied. (5-24-21)  A TDM that addresses trip and parking space reductions and a VRE shuttle must be included in the submission. Strategies shall be measurable, monitorable, and enforceable, and implementation shall be designated on the plan and proffered. <b>This comment has not been satisfied. (5-24-21)</b> 6-25-21 - The TDM is general in nature and lists a number of alternative strategies to reduce the number of generated trips and number of required parking spaces. The		

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					Applicant must proffer to submit a TDM with implementation strategies with the first site plan submission and proffer to contribute funds to the implementation of the TDM as approved by PW/CDOT. There must be a proffer addressing the TDM.  <b>Applicant Response: Please see new proffers 36 a. and b. regarding the TDM program.</b>  Proffer 36 (a) must address a monetary contribution to use for the implementation of the TDM through a Transportation Management Association (TMA). The contribution should be based on the SF of commercial buildings and

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3.07	DCSM Table 6-8	There isn't a parking table on the SUP plans and parking lots/garages aren't shown. This must be included on the Plan.	1	Parking tabulations will be provided in accordance to the DCSM at the time of site plan. (4-28-21)	number of residential units. 8/10/21 Response: Proffer 36 has been updated to address Staffs comment. 8-13-21 - This comment has been satisfied.		
3.08		The MZP shows a connection from the Innovation Town Center to Wellington across from JD Reading Drive. However, the TIA doesn't analyze this intersection with a connection to the proposed development.	1	Intersection 4: Wellington Rd & JD Reading Dr/Site Entrance 2 is included in the TIA. Please note that our site entrance is planned to be a RRRO only and will not connect across to JD Reading Drive. (4-28-21)	This comment has been satisfied. (5-24-21)		
3.09		The MZP shows three interparcel connections to George Mason University. The Applicant must show that the GMU Master Plan includes these connections.	1	These roads are for potential future connections and meet the intent of the Small Area Plan. The connections are not assumed in the TIA and do not have trips routed on them. (4-28-21)	This comment has been satisfied. (5-24-21)		

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3.10	Sheet 7 - MZP	The Phasing Plan relies on the connection to PW Pkwy - a limited access roadway. CTB approval could take up to 2 years for approval after the rezoning has been approved by the BOCs. Phase 1 - Option 2 refers to a "Trigger on the Property" - a date must be provided for this option. A recorded ingress/egress easement must also be provided across the off-site property that gives the Applicant the authority to construct this off-site roadway and the improvements at the intersections of University and Road A and University and PW Pkwy; required to mitigate the traffic generated by the proposed development.	1	Comment noted. Please see the revised proffers provided with this submission. As noted in the proffers, many of the improvements identified in this comment will be addressed pursuant to the Inter-Parcel Land Transfer Agreement, entered into by the County. Please see the revised application material provided with this submission. (4-28-21)	This comment has been satisfied. (5-24-21)
3.11	Sheet 7 - MZP	The Phasing Plan in the TIA for Phase 1 (Figure 22) utilizes both the PW Pkwy access and Option 2 access. The phasing plan and subsequent traffic distribution must be clarified.	1	Under Phase 1, the site would be accessed via a full movement intersection along University Boulevard and a RIRO along Prince William Parkway. Under Phase 2, an additional RIRO and full movement entrance along Wellington Road would be constructed and utilized by the site.  Please note that the Phasing Plan included in the application submission does not designate a Phase to the RIRO connection to Prince William	This comment has been satisfied. (5-24-21)

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3.12		The internal capture rates are relatively low given the symbiotic relationship between the Town Center, residential units and GMU. It appears that this Town Center is not a regional Town Center but part of a University Village for the campus residents and students.	1	Parkway as the timing is dependent on CTB approval and the timing of that is unknown. As previously stated, this report assumes the RIRO connection will be approved and constructed with Phase 1 of the development. If the CTB does not approve the break, then a supplemental transportation analysis memo will be provided showing the operations of the affected intersections. (4-28-21)		This comment has been satisfied. (5-24-21)	
3.13	TTA Page 144 -	States that "Due to the relatively low number of left turns and the urban nature of the intersections, roundabouts are not proposed at the subject locations." But in the previous section it states "The left turn movements represent ... of the total volume. Based on the traffic forecasts, the left turns	1	Double lane roundabouts are not currently proposed at these locations as signals are anticipated to better fit the nature of Wellington Road and the associated traffic. However, roundabouts at these locations will be further explored in the Signal Justification Reports. (4-28-		This comment has been satisfied. (5-24-21)	

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3.14	TIA	The preliminary signal warrants need to be addressed further. The existing ADT for Wellington and Hornbaker and Wellington and Bethlehem along Wellington per 2019 VDOT ADT was 18,000 and 12,000, respectively. The projected TF ADT for both is listed at 20,200 and 23,440, respectively. The 2019 VDOT ADT for Hornbaker is 6000, and the ADT shown in the report is 3010. Please check these inconsistencies. Also, analyze moving the proposed signal at Wellington/Hornbaker to Phase 1. It appears to be already warranted based on existing AADT and should satisfy the warrant with Phase 1.	1	(21) The volumes used for the preliminary signal warrants are based on the TF 2030 - With University Village at Innovation volumes. Therefore, the numbers differ from existing conditions. Further, the volumes used for the preliminary signal warrants represent the approach volumes at the intersection, not the ADT of the road link (both directions). Signal Justification Reports (SJR) will be required for the proposed signals and will provide detailed analysis for all signal warrants.	This comment has been satisfied. (5-24-21)
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TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE: August 10, 2021	FINAL DISPOSITION (3)		
3.15	TTA	For intersection 5 (234 at University), the EB and WB left turns are coded as "Perm" and should be "P+P" in the EX AM and PM Synchro files.	1	This has been corrected. (4-28-21)	This comment has been satisfied. (5-24-21)		
3.16	TTA	Since the reconstructed signal at Wellington and Bethlehem will have flashing yellow arrow for the left turn modes, the left turns should be coded as "DP+P" instead of "pm+pr" in Synchro.	1	This has been corrected. (4-28-21)	This comment has been satisfied. (5-24-21)		
3.17	TTA	The overall intersection delay at the Rt. 234/Wellington intersection practically doubles from existing to 2030 w/University Village (51 vs 95.5 in the AM, 55.9 vs 109.9 in the PM), and gets particularly worse in the NB and SB approaches that triple in the PM. Propose mitigation measures other than signal timing.	1	This intersection operates unacceptable under existing conditions and will continue to do so in the future with or without the proposed development. This intersection is part of a regional issue that Prince William County is continuing to examine and the large-scale solution cannot be put onto one applicant.  The applicant is providing significant mitigation to the quadrant area by advancing the county's regional solution. This includes realigning Bethlehem Road to the future location which helps distribute traffic and also provides more space for	This comment has not been satisfied. See comment 4.01. (5-24-21)  8/10/21 Response: See response to comment 4.01.  8-13-21 - This comment has been satisfied.		

(1) To be filled out by Applicant/Engineer. Date of Response is required.  
(2) The PWC reviewer is responsible for the final disposition of all comments.

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Revised March, 2017

PRINCE WILLIAM COUNTY DEPARTMENT OF TRANSPORTATION PROJECT REVIEW COMMENT AND RESOLUTION SHEET				TIA Required		COMMENT CATEGORIES: 1. CORRECTIONS 2. RECOMMENDATIONS	
COUNTY PROJECT NUMBER: REZ2016-00030 AND SUP2016-00031 (TOWN CENTER)		DEVELOPER/ENGINEER: LDC		REVIEWER(S): ELIZABETH SCULLIN 703-792-4051		DATE: 7-27-21/8-13-21	
TYPE & SUBMITTAL #7		PROJECT NAME: INNOVATION TOWN CENTER					
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE: AUGUST 10, 2021	FINAL DISPOSITION(3)		
3.18		The information included in the TIA is complex given the number of scenarios and phases. It is recommended that the Applicant's Traffic Engineer schedule a meeting with VDOT and the County to discuss the phases and mitigation measures.	2	Acknowledged. (4-28-21)  (4-28-21)  queuing at 234/Wellington. Additionally, a signal is being proposed at Hornbaker Rd & Wellington which is also an important intersection included in the County's project. Both of these improvements provide additional capacity to roads parallel to Prince William Parkway and advance the County's plan. These improvements represent our mitigation for the quadrant.	This comment has been satisfied. (5-24-21)		

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 Revised March, 2017



VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET			870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION	
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.	REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21	
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE		
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
5.01	GDP	Provide distances from intersection/entrance to adjacent intersection/entrance on Road B and C. (Between outer edge of inscribed circle or roundabout to Road I and to Road G; outside edge of inscribed circle of roundabout and Road B)	1		
5.02	GDP	Update the radius of the Centerline of Bethlehem Road to be readable as it overwritten and cannot be read.	1		
5.03	GDP	Update the RIRO along Katherine Johnson Ave. North to be a minimum of 200' from the intersection of Road F.	1		

(1) Indicate drawing no./page no. or use "G" for general comment.  
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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER; P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAK & MOHSIN ZAIDI - TRAFFIC ENGINEERING CINA S. DABESTANI - TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
5.04	G	Since it is the intent to submit the required design waivers at time of site plan, please note that the VDOT no objection to approval of the rezoning does not guarantee approval of the design waivers. If the rezoning is approved and if the design waivers are not approved, then these plans will need to be revised to meet VDOT standards. The county will then need to determine if the revised plans are in substantial conformance with the rezoning.	1		
4.01	5	Provide classification for Wellington Road, Prince William Parkway and University Parkway		Wellington Road was identified on sheet 3 as a major collector as well as University Boulevard. The Prince William Parkway was identified as a Freeway. All classifications taken from VDOT 2014 Approved Functional Classification Map.	Comment Closed.

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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIC SPENCER, P. E. <a href="mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV">ERIK.SPENCER@VDOT.VIRGINIA.GOV</a> GEOFFREY SARMAAC & MOHSIN ZAIDI - TRAFFIC ENGINEERING CINA S. DABESTANI - TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER			REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
4.02	5	Provide VPD, Classification, ROW, design speed, posted speed, etc. for Bethlehem Road.	1	Agreed and provided.	Comment Closed
4.03	5	Provide distances from intersection/entrance to adjacent intersection/entrance on Road B and C.	1	Agreed and these dimensions were provided on sheet 5E previously and are 554' and 366' measured to the inscribed circle of the roundabouts. The total distance is 920'.	Repeat.
4.04	5	Update the label for Katherine Johnson Avenue South to UB-1, not UTB-1	1	Agreed and revised.	Comment Closed
4.05	5	Update Hylton Blvd. East to be private.	1	Agreed and revised on all applicable sheets as private maintenance.	Comment Closed

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 Revised September, 2014

**VIRGINIA DEPARTMENT OF TRANSPORTATION  
PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

870 TIA: ACCEPTED

- COMMENT CATEGORIES:**
1. REQUIREMENT
  2. RECOMMENDATION
  3. CLARIFICATION

**COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031**

**DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.**

**REVIEWER(S): ERIK SPENCER, P.E.  
ERIK.SPENCER@VDOT.VIRGINIA.GOV  
GEOFFREY SARMA & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING**

**DATE: 06/28/21**

**PROJECT NAME: INNOVATION TOWN CENTER**

**REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.**

**DISCIPLINE: PWC LAND USE**

**FINAL DISPOSITION<sup>(3)</sup>**

ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
4.06	5	Katherine Johnson Avenue should be classified a VDOT Collector Street for access management and funding reasons and Road B and C should be classified as a VDOT local Streets. Provide a Label stating that the VDOT functional Classification for Katherine Johnson Avenue as a VDOT classified Collection and Roads A and B as a VDOT classified Local Street.		As agreed during our meeting, Katherine Johnson Avenue shall be designated a local street and reference has been provided as such. Roads B and C are also referenced as local streets as per your request.	Comment Closed
4.07	5A & 5B	Per Table A(1)-1 Bicycle facility Design for Curb and Gutter, Bike lanes are not allowed on streets with parking over 6,000 ADT. Please submit a design waiver to allow bike lanes on streets with over 6,000 ADT.	1	Table A(1)-1 states roads designed in excess of 6,000 ADT, with parking may provide a bike facility with either an SUP or SBL. A design waiver will be submitted prior to site plan.	Comment Closed.
4.08	5B	It appears that sheet 5A and 5B are the same, please clarify if this is the case and update accordingly.	3	Agreed and revised.	Comment Closed

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Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAAC & MOHSIN ZAIDI - TRAFFIC ENGINEERING CINA S. DABESTANI - TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER			REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE	
ITEM No.	DWG. No. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
4.09	5C & 5D	The notes state that design waivers may be required and will be applied for at time of final site plan, what charges will be needed and how will this plan change if the said waivers are not granted at time of final site plan?	3	No additional waivers are anticipated beyond those mentioned herein at this time. The applicant simply reserves the right to request a waiver or exception, if deemed appropriate, at a later date.	Comment Closed
4.10	5D	Provide radius of the proposed Bethlehem Road centerline.	1	Agreed and provided.	See Comment Above.
4.11	5D	Update Thong Pan Road to tie into Bethlehem Road at 90 degrees.	1	Agreed and provided.	Comment Closed.
4.12	5E	Label which entrances are to be right in / right out.	1	Agreed and these references were previously provided on sheet 5.	Comment Closed.
4.13	5E	Update the intersection spacing along Katherine Johnson Avenue to meet spacing requirements of Table 2-2 of Appendix F of the RDM for collector roads with design speed of 30 mph.	1	As previously mentioned, this street shall be designated local thus, no changes to comply with Appendix F are required. The application utilized and satisfies the standards set forth in Table B(2)-2.	See Comments above.

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 Revised September, 2014



**VIRGINIA DEPARTMENT OF TRANSPORTATION  
PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031

DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.

REVIEWER(S): ERIC SPENCER, P.E.  
ERIK.SPENCER@VDOT.VIRGINIA.GOV  
GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING

870 TIA- ACCEPTED

COMMENT CATEGORIES:  
1. REQUIREMENT  
2. RECOMMENDATION  
3. CLARIFICATION

DATE: 06/28/21

PROJECT NAME: INNOVATION TOWN CENTER

REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.

DISCIPLINE: PWC LAND USE

FINAL DISPOSITION<sup>(3)</sup>

ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
4.14	SF	Update the typical sections for Wellington Road and Bethlehem Road to show all aspects of the section, ie: shoulder, ditch, ROW, pedestrian facilities, etc.	1	Agreed and revised.	Comment Closed.
4.15	SF	Label the minimum and maximum distances for any dimensions that will vary on the typical section.	1	Agreed and revised.	Comment Closed.

Name: **Geoffrey Sarmaac & Mohsin Zaidi**  
Discipline: **TE Review**

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Revised September, 2014

4.16	G	<p><b>Repeat Comment: 1.</b> It is obvious that the intersection of PW Pkwy and Wellington does not meet the PW county LOS requirements. It is recommended to suggest solutions to the LOS problems. We disagree with the argument that The PW Pkwy &amp; Wellington intersection operates unacceptably under existing conditions and will continue to do so in the future with or without the proposed development. It is also obvious that the development traffic will make the bad situation worse by adding more traffic to the system.</p> <p>2. We also disagree with any proposed changes to the signal timing as mitigation to improve the LOS of the intersection. Re-timing of individual signals can have a system-wide impact on the network. Therefore, the impact of such an action should be analyzed for the entire corridor or network. Also, other mitigation measures should be considered in the event the signal optimization cannot be implemented.</p>	1	<p>The signal timing modifications are a recommendation on an improvement that could be implemented to improve operations at the signal. Results are presented with and without the timing changes and the adjustments are at VDOT's discretion to implement.</p> <p>As stated in a previous response, The PW Pkwy &amp; Wellington intersection is part of a regional issue that Prince William County is continuing to examine and the large scale solution cannot be put onto one applicant. The applicant is providing significant mitigation to the quadrant area by advancing the county's regional solution. This includes realigning Bethlehem Road to the future location which helps distribute traffic and also provides more space for queuing at 234/Wellington.</p> <p>Additionally, a signal is being proposed at Hornbaker Rd &amp; Wellington which is also an important intersection included in the County's project. Both of these improvements provide additional capacity to roads parallel to Prince William Parkway and advance the County's plan.</p> <p>In order to additionally mitigate the impacts at the PW Pkwy &amp; Wellington Rd intersection, the applicant is proffering to provide either a fair share monetary contribution to the County to facilitate future regional improvements at this intersection or to extend the NB and SB left turn lanes, depending on County funding at the time the 500<sup>th</sup> residential unit.</p>	
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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARRIAC & MOHSIN ZAIDI - TRAFFIC ENGINEERING CIMA S. DABESTANI - TRAFFIC PLANNING	
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
4.17	TIA	Table 11: Internal Trip Capture-Office to Retail for the PM Peak Hour-Out trips for the Shopping Center should be 0 instead of -6.	1	The table has been corrected and the TIA has been revised.	
4.18	TIA	Extend the NB/SB left turn lanes along PW/ Pkwy at Wellington Road to mitigate both starvation and overflow problems evident in all the scenarios.	2	Please see response to 4.16 above.	

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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIC SPENCER, P.E. <a href="mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV">ERIK.SPENCER@VDOT.VIRGINIA.GOV</a> GEOFFREY SARMAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE:	FINAL DISPOSITION (3)
4.19	T1A	Check the lane utilization factors and/or other related parameters as the simulation shows significant under-utilization of the median left turn lane of the NB/SB dual left turn lanes along P W Pkwy at Wellington Road.	3	Based on Simtraffic observations, both dual left turn lanes for the northbound and southbound approaches appear to be utilized efficiently. Therefore, the lane utilization factors and other parameters where not changed. Further, please note that the TIA provides Synchro results and the Simtraffic changes will not affect the results of the TIA.	
<b>3<sup>rd</sup> Submission Comments on REZ 2016-00030</b>					
3.01	G	Waiver 13: Update to state that the waiver is for private streets	1	This waiver has been removed from the application.	Comment Closed
3.02	G	Waiver 15: Note that the minimum curb return along VDOT roads for intersections is 15' with a minimum effective turning radii of 25'	1		Comment Closed

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 Revised September, 2014

**VIRGINIA DEPARTMENT OF TRANSPORTATION  
PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

870 TIA: ACCEPTED

**COMMENT CATEGORIES:**  
1. REQUIREMENT  
2. RECOMMENDATION  
3. CLARIFICATION

COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031

DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.

REVIEWER(S): ERIK SPENCER; P.E.  
ERIK.SPENCER@VDOT.VIRGINIA.GOV  
GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING

DATE: 06/28/21

PROJECT NAME: INNOVATION TOWN CENTER

REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.

DISCIPLINE: PWC LAND USE

FINAL DISPOSITION<sup>(3)</sup>

ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.03	G	Waiver 16: Provide plan view or update the plans to identify what pavement sections will be modified. Are any going to be in VDOT ROW?	1	This waiver refers to potential decorative paving on privately maintained travelways	Comment Closed
3.04	G	Waiver 17: Identify where the modified guardrail will be located. Please note that VDOT will not accept modified guardrail inside VDOT ROW or for VDOT streets.	1	This waiver has been removed from the application.	Comment Closed.
3.05	G	Proffer 25.a.iv.: Update to state that the left turn lane will shall be constructed.	1	The proffer typo has been revised to reference the left turn lane.	Comment Closed

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Revised September, 2014



VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): Erik SPENCER, P.E. <a href="mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV">ERIK.SPENCER@VDOT.VIRGINIA.GOV</a> GEOFFREY SARMAC & MOHSIN ZAIDI - TRAFFIC ENGINEERING CINA S. DABESTANI - TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER			REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.06	G	Provide typical sections for University Boulevard, Bethlehem Road, Wellington Road, and Prince William Parkway showing the proposed improvements.	1	Typical sections for Bethlehem Road and Wellington Road have been included. Please reference rez-2021-00005 for improvements to University Boulevard and Prince William Parkway. In the event that rezoning rez-2021-00005 is not approved or the improvements contemplated there under have not been site plan approved and bonded prior to the trigger on the property, subject to PWC/DOT and VDOT approval (and waivers and modifications granted at final site plan) the applicant shall design and construct the road segment highlighted.	Missing details, see comments above.
3.07	G	Provide typical sections for the proposed public streets.	1	Typical Sections for the proposed public streets have been provided. See MZP Sheets 5A and Sheet 5B.	See Comments above.

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 Revised September, 2014

**VIRGINIA DEPARTMENT OF TRANSPORTATION  
PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

870 TIA: ACCEPTED

**COMMENT CATEGORIES:**  
1. REQUIREMENT  
2. RECOMMENDATION  
3. CLARIFICATION

COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031

DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.

REVIEWER(S): ERIK SPENCER, P.E.  
ERIK.SPENCER@VDOT.VIRGINIA.GOV  
GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING

DATE: 06/28/21

PROJECT NAME: INNOVATION TOWN CENTER

REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.

DISCIPLINE: PWC LAND USE

ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
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3.08	G	Label all proposed streets on the plans as either public or private.	1	Please see the legend on the On-site Multi-Modal Transportation Plan which classifies each road as private or public. A table has also been added on Sheet 5 of the MZP.	Comment Closed.
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3.09	G	Provide turn lane warrant analysis for all intersections / entrances for the spine roads A, B, C and D.	1	The internal intersections are part of the urban grid section and will distribute traffic throughout the site and are not anticipated to have a large concentration of turning vehicles at any of the streets. However, as discussed with VDOT and the County, turn lane warrants have been provided for the worst-case intersection in the TIA. Please note that the two main internal intersections are planned to be roundabouts and therefore do not require turn lane warrants.	Comment Closed.
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PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

<p>COUNTY PROJECT NUMBER: REZ 2016-00030 &amp; SUP 2016-00031</p>		<p>DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.</p>		<p>REVIEWER(S): ERIK SPENCER, P.E. <a href="mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV">ERIK.SPENCER@VDOT.VIRGINIA.GOV</a> GEOFFREY SARMAK &amp; MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING</p>		<p>DATE: 06/28/21</p>	
<p>PROJECT NAME: INNOVATION TOWN CENTER</p>				<p>REVIEW PHASE &amp; TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.</p>		<p>DISCIPLINE: PWC LAND USE</p>	
<b>COMMENTS</b>		<b>COMMENT CATEGORY</b>		<b>RESPONSE<sup>(2)</sup> DATE:</b>		<b>FINAL DISPOSITION<sup>(3)</sup></b>	
3.10	G	Provide the design speed and functional classification for Bethlehem Road, Wellington Road, Prince William Parkway and University Boulevard and all proposed streets. If the intent is to use the posted speed as the design speed for the existing streets and not the design speed based on the PWC DCSM then a waiver from PWC may be required.	1	The design speed, VDOT functional classification, and Prince William County functional classification have been included in the TIA.  As discussed at the transportation meeting on March 10, 2021 Prince William County has agreed that the posted speed may be used as the design speed.		See Comments Above.	
3.11	G	Provide centerline stationing for all existing and proposed streets (University, Wellington, PWC Pkwy, etc.)	1	Centerline stationing has been provided for Prince William Parkway and Wellington Road -See MZP – Sheet 5. Please reference rez-2021-00005 for stationing for University Boulevard. Stationing for the proposed Katherine Johnson Avenue is included in MZP – Sheet 5E.		Comment Closed.	
3.12	3	Provide distances between all proposed intersections and the adjacent intersections.	1	Distances between all proposed intersection have been provided.		See Comments above.	

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 REVISED SEPTEMBER, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET			870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION	
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM No.	DWG. No. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.13	3	Provide centerline radii for all proposed streets to verify they meet VDOT Standards.	1	Centerline radii for all proposed public streets have been provided – see MZP, Sheet 5.	Comment Closed
3.14	5A-5B	Sheets 5A and 5B need to be at a scale.	1	The plans have been added at a scale.	Comment Closed
3.15	5A-5B	Label the missing turn lane lengths and taper lengths for all turn lanes (existing and proposed) and the acceleration lane.	1	This is a preliminary plan and the turn lane/taper lengths could change slightly during the application process. Therefore, the plan commits to meeting the requirements and if they don't, waivers will be submitted at the site plan/PIP stage.	Comment Closed
3.16	5A	Will this application be removing Thong Pan Road? If not, show how this application will connect Thon Pan Road.	1	The plan has been updated to show a connection from Thong Pan Road to the Relocated Bethlehem Road.	Comment Closed

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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET			870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION	
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.	REVIEWER(S): ERIC SPENCER, P.E. ERIC.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAK & MOHSIN ZAIDI - TRAFFIC ENGINEERING CMA S. DABESTANI - TRAFFIC PLANNING	DATE: 08/28/21	
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 6 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE		
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.17	5A	There is a current rezoning adjacent to this property that is proposing to revise the intersection of JD Reading Drive and Wellington Road. How will this work with those proposed improvements?	1	The road improvements associated to the adjacent rezoning will be included in the proposed plan.	Comment Closed
3.18	5A	The TIA calls for the site entrance at JD Reading to be right-in / out. Verify if this is truly the case and if so, label as right-in / out and show entrance channelization.	1	Correct, the site entrance is a RIRO. The label has been added.	Comment Closed
3.19	5A	Provide the distance between the proposed signal at Hornbaker Road and PWC Pkwy and Sudley Manor Drive.	1	This has been added.	Comment Closed

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PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE		
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.20	5A-5B	On both sheets 5A and 5B the applicant is stating that w waiver is likely but is not providing enough to determine if a waiver is required. Provide additional information to verify if a waiver is required or not.	1	This is a preliminary plan and the dimensions could change slightly during the application process. Therefore, the plan commits to meeting the requirements and if they don't, waivers will be submitted at the site plan/PIP stage.	See Comments above.
3.21	5A	Shift the proposed Bethlehem Road / Road A intersection 15' so that an AME is not required.	1	The intersection has been shifted so that an AME is no longer required.	Comment Closed
3.22	5A	Provide distance between JD Reading and Bethlehem Road.	1	This has been added.	Comment Closed
3.23	5A-5B	Show and provide dimensions for the existing and proposed ROW for Bethlehem Road, Wellington Road, Prince William Parkway, University Boulevard and all proposed streets.	1	This has been added.	Comment Closed

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PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE		
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.24	5A-5B	Update the design of University and Wellington to match the PWC MA-1 standards. Show any proposed sidewalk along each road in accordance with the MA-1 standards.	1	Wellington Road improvement are in accordance to the PWC MA-1 standards. A 10' multi-use trail is proposed within the property in lieu of the shared use path.	Comment Closed.
3.25	5A-5B	Provide preliminary sight distance and profiles to verify that the entrances / intersections will work where proposed.	1	Preliminary sight distance and profiles have been provided. See Sheet 5F of the MZP.	Comment Closed.
3.26	5B	Provide distance to the north from the proposed Road B intersection to the adjacent intersection / entrance.	1	This has been added.	Repeat Comment.
3.27	5A-5B	Show the limits of the limited access ROW.	1	The limits of the limited access ROW is labeled on the plan. See MZP-Sheet 5.	Comment Closed.
3.28	5B	Update the view or provide view showing the extents of the acceleration lane.	1	This has been added.	Comment Closed
3.29	5B	Provide the distance between Cannon Creek and Road A.	1	This has been added.	Comment Closed

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COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031

DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.

REVIEWER(S): ERIC SPENCER, P.E.  
[ERIK.SPENCER@VDOT.VIRGINIA.GOV](mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV)  
GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING

DATE: 06/28/21

PROJECT NAME: INNOVATION TOWN CENTER

REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.

DISCIPLINE: PWC LAND USE

FINAL DISPOSITION<sup>(3)</sup>

ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.30	5B	Provide the distance between Road A and the Comcast entrance	1	This has been added.	Comment Closed
3.31	5B	Label the Comcast entrance as a Right in/out and provide the entrance channelization.	1	The label has been added. The channelized right turn is being removed as part of the quadrant improvement.	Comment Closed
3.32	5B	Is the right turn lane onto Road A a through right or right turn only?	1	The right turn onto Road A (now called Katherine Johnson Ave in the TIA ) is a right turn only.	Comment Closed
3.33	5B	Remove the statement "In the event those standards cannot be met, a waiver of the condition shall be furnished for approval" from the grey box on the upper right corner of the page. Identify any waivers / exceptions that will be required. These need to be applied for an approved prior to the rezoning being approved to verify that the proposed design will work. If any waivers / exceptions are not approved then the design will need to be updated accordingly.	1	This is a preliminary plan and the dimensions could change slightly during the application process. Therefore, the plan commits to meeting the requirements and if they don't, waivers will be submitted at the site plan/PIP stage.	See Comment Above.

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COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIC SPENCER, P.E. <a href="mailto:ERIK.SPENCER@VDOT.VIRGINIA.GOV">ERIK.SPENCER@VDOT.VIRGINIA.GOV</a> GEOFFREY SARMAC & MOHSIN ZAIDI - TRAFFIC ENGINEERING CINA S. DABESTANI - TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER			REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE (2) DATE:	FINAL DISPOSITION (3)
3.34	5B	Reconstruct the median on University west of PWC Pkwy not just re-stripe.	1	The median improvements west of PWC Pkwy are part of the quadrant improvements and are not being done as part of this project. Comment noted	Comment Closed.
3.35	G	Please note that the CTB will need to approve the Limited Access Break along PWC Pkwy. If the CTB does not approve the break, then the access to PWC Pkwy will need to be removed.	1		Comment Closed.
		<b>Name: Geoffrey Sarmac &amp; Mohsin Zaidi</b> <b>Discipline: TE Review</b>			

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<b>VIRGINIA DEPARTMENT OF TRANSPORTATION                  PRINCE WILLIAM LAND USE                  PROJECT REVIEW                  COMMENT AND RESOLUTION SHEET</b>		870 TIA: ACCEPTED	<b>COMMENT CATEGORIES:</b> 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
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COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031	DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.	REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARIMAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21
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PROJECT NAME: INNOVATION TOWN CENTER	REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.	DISCIPLINE: PWC LAND USE	
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ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.36	Pg 25 & 28	PW Pkwy at Wellington Rd & University Blvd, there are a number of rear-end type crashes, it may be beneficial to investigate further to see if there is something that can be done to correct/improve this. Is there a sight distance issue, are there existing warning signs (signal ahead, stopped vehicles ahead, etc.)?	2	The intersection of Prince William Parkway & University Village has a high crash rate (0.98 crashes per MEV) and is near the rate of 1.0 crashes per MEV which can indicate that further evaluation is needed. This intersection is undergoing the Quadrant intersection improvement which will remove left turns from the intersection and is anticipated to improve safety.  The intersection of Prince William Parkway & Wellington Rd has a lower crash rate of 0.64 crashes per MEV which indicates that this is not a high crash location. A majority of the crashes (40%) are due to distracted drivers. The relocation of Bethlehem Drive is also expected to enhance safety as it will shift the existing signal further away and provide more room for the intersection.  There are not any known sight distance issues.	Closed

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<p>3.37</p> <p>G</p>	<p>There is considerable coordination with other developments and future improvements, notably the QI and PW Pkwy &amp; Wellington ultimate improvements. What is the expected completion date of the QI is, is it reasonable to assume it will be in operation in 2025?</p> <p>For PW Pkwy and Wellington, no mitigations beyond signal timings are proposed due to ultimate improvements being discussed. Since some movements at the intersection will deteriorate significantly (delays and queuing) even with the modified signal timings, has any smaller scale improvements been considered that may keep overall operations closer to FB conditions? Possibly extending turn lanes, modify phasing and/or lane usage? Should phase 2 be closer coordinated with the development of the long term improvements at Wellington to avoid having the intersection operate unacceptably with no certain plans for improvements/mitigation?</p> <p><b>Repeat Comment: 1.</b> It is obvious that the intersection of PW Pkwy and Wellington does not meet the PW county LOS requirements. It is recommended to suggest solutions to the LOS problems. We disagree with the argument that The PW Pkwy &amp; Wellington intersection operates unacceptably under existing conditions and will continue to do so in the future with or without the proposed development. It is also obvious that the development traffic will make the bad situation worse by adding more traffic to the system.</p>	<p>3</p> <p>The Quadrant Improvement is anticipated to be complete in 2022 and therefore it is reasonable to assume that it will be complete by 2025.</p> <p>The PW Pkwy &amp; Wellington intersection operates unacceptable under existing conditions and will continue to do so in the future with or without the proposed development. This intersection is part of a regional issue that Prince William County is continuing to examine and the large scale solution cannot be put onto one applicant.</p> <p>The applicant is providing significant mitigation to the quadrant area by advancing the county's regional solution. This includes realigning Bethlehem Road to the future location which helps distribute traffic and also provides more space for queuing at 234/Wellington. Additionally, a signal is being proposed at Hornbaker Rd &amp; Wellington which is also an important intersection included in the County's project. Both of these improvements provide additional capacity to roads parallel to Prince William Parkway and advance the County's plan. These improvements represent our mitigation for the quadrant.</p>	<p>Open</p>
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COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): Erik SPENCER; P.E. ERIC.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAK & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
		2. We also disagree with any proposed changes to the signal timing as mitigation to improve the LOS of the intersection. Re-timing of individual signals can have a system-wide impact on the network. Therefore, the impact of such an action should be analyzed for the entire corridor or network. Also, other mitigation measures should be considered in the event the signal optimization cannot be implemented.			
3.38	Pg 78 & 127	For comparison tables, where delay is shown, delays 10% or more from FB should be identified, shown in red or highlighted. Footnote should be provided to define this.	2	This has been added.	Closed
3.39	Pg 131	Include delay comparison for intersections 3 thru 5, the queue comparison is shown by mistake.	1	This has been corrected.	Closed

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ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.40	G	A comparison table (LOS, delay, queues) should be provided for 2036  <b>Name: Cina Dabestani</b> <b>Discipline: TP Review</b>	2	This has been added.	Closed
3.41	G	Transportation Planning Section at NoVa District office of VDOT has evaluated this traffic impact analysis for compliance with the above Virginia code and has found it to be in conformance therefore no comments being offered (minor arterial and above) with traffic volumes above its capacity per CH536 requirements.	1	Comment Acknowledged	Comment Closed.
3.01	G	<b>3<sup>rd</sup> Submission Comments on SUP 2016-00031</b> Provide typical sections for the proposed public streets.	1	VDOT REZ comment 3.07	Comment Closed

(1) Indicate drawing no./page no. or use "G" for general comment.  
 (2) To be filled out by Applicant/Engineer. Date of Response is required.  
 (3) The VDOT reviewer is responsible for the final disposition of all comments.

Note: This form is to be used by the VDOT land use team to provide comments or concerns associated with the rezoning applications, site plans or any other plans when requested by the county or the applicants.  
 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER; P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARMAK & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	DATE: 06/28/21
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM No.	DWG. No. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.02	G	Label all proposed streets on the plans as either public or private.	1	VDOT REZ comment 3.08	Comment Closed
3.03	G	Provide the design speed and functional classification for Prince William Parkway and all proposed streets.	1	VDOT REZ comment 3.10	See Comments above
3.04	G	Provide centerline stationing for all existing and proposed streets	1	VDOT REZ comment 3.11	Comment Closed
3.05	G	Provide distances between all proposed intersections and the adjacent intersections.	1	VDOT REZ comment 3.12	See Comments above.

(1) Indicate drawing no./page no. or use "G" for general comment.  
 (2) To be filled out by Applicant/Engineer. Date of Response is required.  
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 Revised September, 2014

VIRGINIA DEPARTMENT OF TRANSPORTATION PRINCE WILLIAM LAND USE PROJECT REVIEW COMMENT AND RESOLUTION SHEET				870 TIA: ACCEPTED	COMMENT CATEGORIES: 1. REQUIREMENT 2. RECOMMENDATION 3. CLARIFICATION
COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031		DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.		REVIEWER(S): ERIK SPENCER, P.E. ERIK.SPENCER@VDOT.VIRGINIA.GOV GEOFFREY SARIMAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING CINA S. DABESTANI – TRAFFIC PLANNING	
PROJECT NAME: INNOVATION TOWN CENTER		REVIEW PHASE & TYPE: 5 <sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.		DISCIPLINE: PWC LAND USE	
ITEM NO.	DWG. NO. (1)	COMMENTS	COMMENT CATEGORY	RESPONSE(2) DATE:	FINAL DISPOSITION(3)
3.06	G	Provide turn lane warrants for the intersections along the main spine road A and Hylton Blvd.	11	Similar to VDOT REZ comment 3.09  Response: The internal intersections are part of the urban grid section and will distribute traffic throughout the site and are not anticipated to have a large concentration of turning vehicles at any of the streets. However, as discussed with VDOT and the County, turn lane warrants will be provided for the worst-case intersection in the TIA.  Please note that the two main internal intersections along Katherine Johnson Ave are planned to be roundabouts and therefore do not require turn lane warrants.	Comment Closed.

(1) Indicate drawing no./page no. or use "G" for general comment.  
 (2) To be filled out by Applicant/Engineer. Date of Response is required.  
 (3) The VDOT reviewer is responsible for the final disposition of all comments.

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 Revised September, 2014



**VIRGINIA DEPARTMENT OF TRANSPORTATION  
PRINCE WILLIAM LAND USE  
PROJECT REVIEW  
COMMENT AND RESOLUTION SHEET**

870 TIA: ACCEPTED

**COMMENT CATEGORIES:**  
1. REQUIREMENT  
2. RECOMMENDATION  
3. CLARIFICATION

COUNTY PROJECT NUMBER: REZ 2016-00030 & SUP 2016-00031

DEVELOPER/ENGINEER: STANLEY MARTIN COMPANIES, LLC / LAND DESIGN CONSULTANTS, INC.

REVIEWER(S): ERIK SPENCER, P.E.  
ERIK.SPENCER@VDOT.VIRGINIA.GOV  
GEOFFREY SARMAAC & MOHSIN ZAIDI – TRAFFIC ENGINEERING  
CINA S. DABESTANI – TRAFFIC PLANNING

DATE: 06/28/21

PROJECT NAME: INNOVATION TOWN CENTER

REVIEW PHASE & TYPE: 5<sup>TH</sup> SUBMISSION, REZONING AND SPECIAL USE PERMIT.

DISCIPLINE: PWC LAND USE

FINAL DISPOSITION<sup>(3)</sup>

ITEM NO.	DWG. NO. <sup>(1)</sup>	COMMENTS	COMMENT CATEGORY	RESPONSE <sup>(2)</sup> DATE:	FINAL DISPOSITION <sup>(3)</sup>
3.07	G	Show the proposed improvements for Prince William County Parkway.	1	Please reference rez-2021-00005 for improvements to Prince William Parkway. In the event that rezoning rez-2021-00005 is not approved or the improvements contemplated there under have not been site plan approved and bonded prior to the trigger on the property, subject to PWC/VDOT and VDOT approval (and waivers and modifications granted at final site plan) the applicant shall design and construct improvements to Prince William Parkway.	Comment Closed.
3.08	G	Show the street layout with number of lanes, parking, intersections, median breaks, etc.	1	The street layout for the proposed public Katherine Johnson Avenue has been provided in sheet 6. The typical street sections are provided in sheet 6A. Street layout for travelways and private streets will be provided at site plan.	Comment Closed.

(1) Indicate drawing no./page no. or use "G" for general comment.  
(2) To be filled out by Applicant/Engineer. Date of Response is required.  
(3) The VDOT reviewer is responsible for the final disposition of all comments.

Note: This form is to be used by the VDOT land use team to provide comments or concerns associated with the rezoning applications, site plans or any other plans when requested by the county or the applicants.  
Revised September, 2014

# Planning Commission Resolution



Planning Commission

## PLANNING COMMISSION RESOLUTION

**MOTION:** MCKAY **September 8, 2021**  
**SECOND:** TAYLOR **Regular Meeting**  
**RES. No. 21-088**

**RE:** REZONING AND RELEASE DECLARATION #REZ2016-00030,  
INNOVATION TOWN CENTER  
BRENTSVILLE MAGISTERIAL DISTRICT

**ACTION:** RECOMMEND APPROVAL

**WHEREAS**, this is a request to rezone ±24.19 acres from PBD, Planned Business District, and A-1, Agricultural, to PMD, Planned Mixed Use District, to allow a mixed-use development consisting of 1,630 multifamily units (including 1,480 student housing units) and ±1,625,000 sq. ft. of nonresidential uses (office, retail, and hotel) and associated waivers and modifications, including building height and floor area ratio increases; and

**WHEREAS**, this includes a request to release the subject site from the Declaration for the property of the Prince William Board of County Supervisors at Innovation at Prince William County; and

**WHEREAS**, the subject site is located on the north side of University Blvd., between Prince William Parkway (Route 234 By-Pass) and Cannon Creek Ln; and

**WHEREAS**, the site is currently PBD, Planned Business District and located within the Technology Overlay District (TeOD), and the Airport Safety Overlay District; and

**WHEREAS**, the site is designated TC, Town Center, in the Comprehensive Plan, and is located within the Innovation Park Small Area Plan; and

**WHEREAS**, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on September 8, 2021; and

**WHEREAS**, the Prince William County Planning Commission believes that public general welfare as well as good planning practices are served by the approval of this request;

# Planning Commission Resolution

September 8, 2021  
Regular Meeting  
RES. No. 21-088  
Page 2

**NOW, THEREFORE, BE IT RESOLVED** that the Prince William County Planning Commission does hereby recommend approval of Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center subject to the proffers dated August 26, 2021, on the regular agenda.

**Votes:**

**Ayes:** McKay, McPhail, Milne, Moses-Nedd, Perry, Taylor

**Nays:** Berry

**Abstain from Vote:** None

**Absent from Vote:** Fontanella

**Absent from Meeting:** Fontanella

**MOTION CARRIED**

**Attest:**

  
\_\_\_\_\_  
Robbyn L. Smith  
Clerk to the Planning Commission

# Planning Commission Resolution



**PRINCE WILLIAM  
COUNTY**

Planning Commission

## PLANNING COMMISSION RESOLUTION

**MOTION: MCKAY**

**September 8, 2021**

**SECOND: TAYLOR**

**Regular Meeting**

**RES. No. 21-089**

**RE: SPECIAL USE PERMIT #SUP2016-00031, INNOVATION TOWN CENTER  
BRENTSVILLE MAGISTERIAL DISTRICT**

**ACTION: RECOMMEND APPROVAL**

**WHEREAS**, this is a request for a special use permit (SUP) for a modified "Town Center" for a ±25.1-acre area in the PMD, Planned Mixed Use District, to include up to 364 residential units and up to 331,100 square feet of nonresidential uses and associated waivers and modifications; and

**WHEREAS**, the property is located at the southeast quadrant of the intersection of Prince William Parkway/Route 234 By-Pass and Wellington Road; and

**WHEREAS**, the site is currently zoned A-1, Agricultural, and PBD, Planned Business District. The site is also located within the Technology Overlay District (TeOD), and the Airport Safety Overlay District; and

**WHEREAS**, the site is designated OMU, Office Mixed Use, CMU, Commercial Mixed-Use, and TC, Town Center, in the Comprehensive Plan, and is located within the Innovation Park Small Area Plan, which was adopted by the Board of County Supervisors on December 15, 2020; and

**WHEREAS**, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on September 8, 2021; and

**WHEREAS**, the Prince William County Planning Commission believes that public general welfare as well as good planning practices are served by the approval of this request;

# Planning Commission Resolution

September 8, 2021  
Regular Meeting  
RES. No. 21-089  
Page 2

**NOW, THEREFORE, BE IT RESOLVED** that the Prince William County Planning Commission does hereby recommend approval of Special Use Permit #SUP2016-00031, Innovation Town Center subject to the conditions dated June 15, 2021, on the regular agenda.

**Votes:**

**Ayes:** McKay, McPhail, Milne, Moses-Nedd, Perry, Taylor

**Nays:** Berry

**Abstain from Vote:** None

**Absent from Vote:** Fontanella

**Absent from Meeting:** Fontanella

**MOTION CARRIED**

**Attest:**

  
\_\_\_\_\_  
Robbyn L. Smith  
Clerk to the Planning Commission





**PRINCE WILLIAM**  
— COUNTY —

# Innovation Town Center

## #REZ2016-00030

## #SUP2016-00031

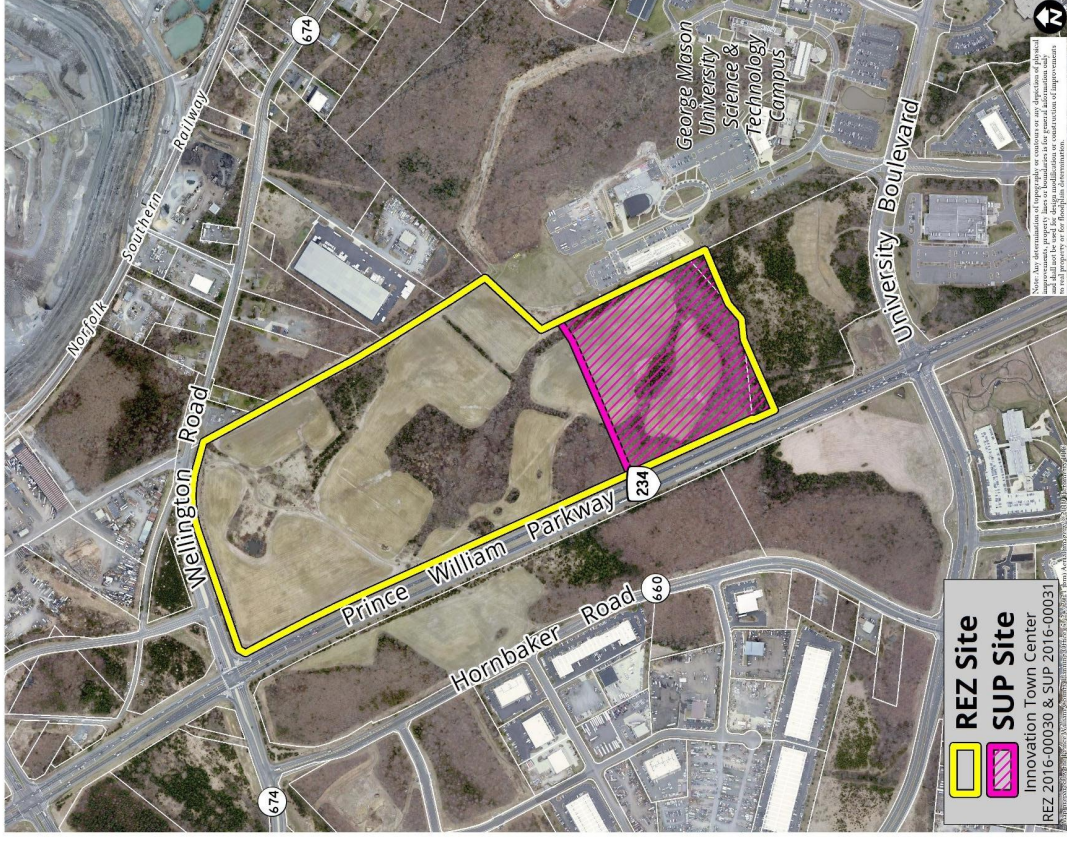
Brentsville Magisterial District

Meika Daus, AICP  
Planning Office

# Innovation Town Center

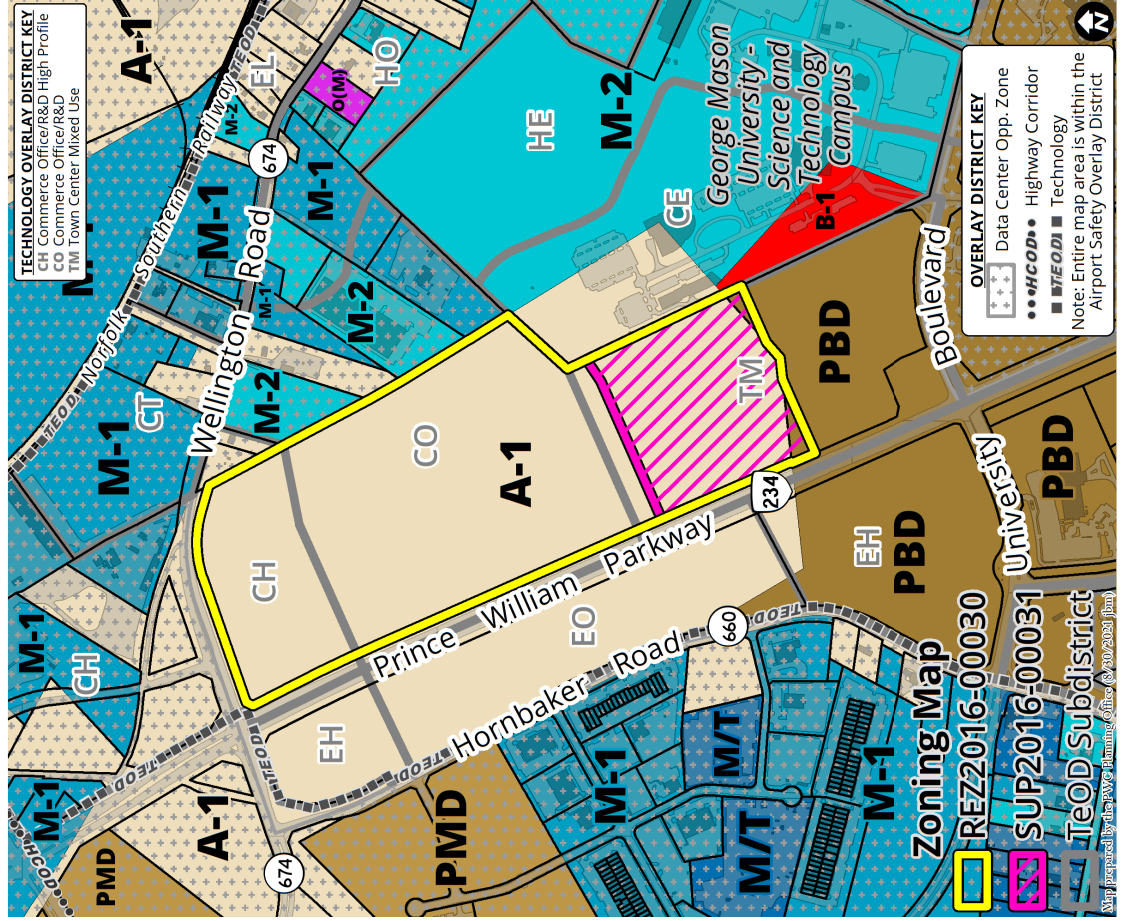
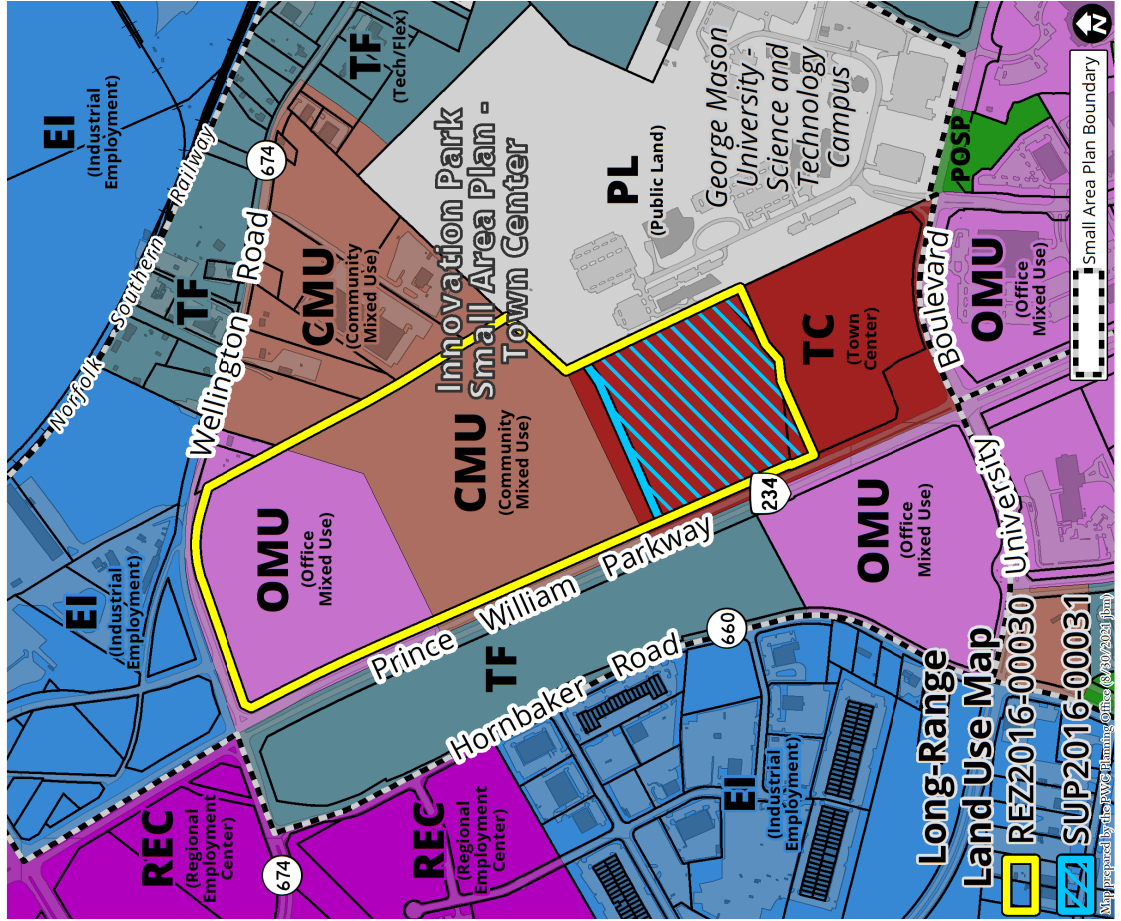
## #REZ2016-00030 and #SUP2016-00031

- **Request:** To rezone  $\pm 107.4$  acres from A-1, Agricultural, and PBD, Planning Business District, to PMD, Planned Mixed Use District, to allow a mixed-use development that will include up to 1,032 residential units and  $\pm 447,100$  square feet of nonresidential uses. The companion Special Use Permit is for a modified “Town Center”.
- **Location:** Southeast quadrant of the intersection of Prince William Parkway/Route 234 By-Pass and Wellington Road.
- **Recommendation: Approval.**





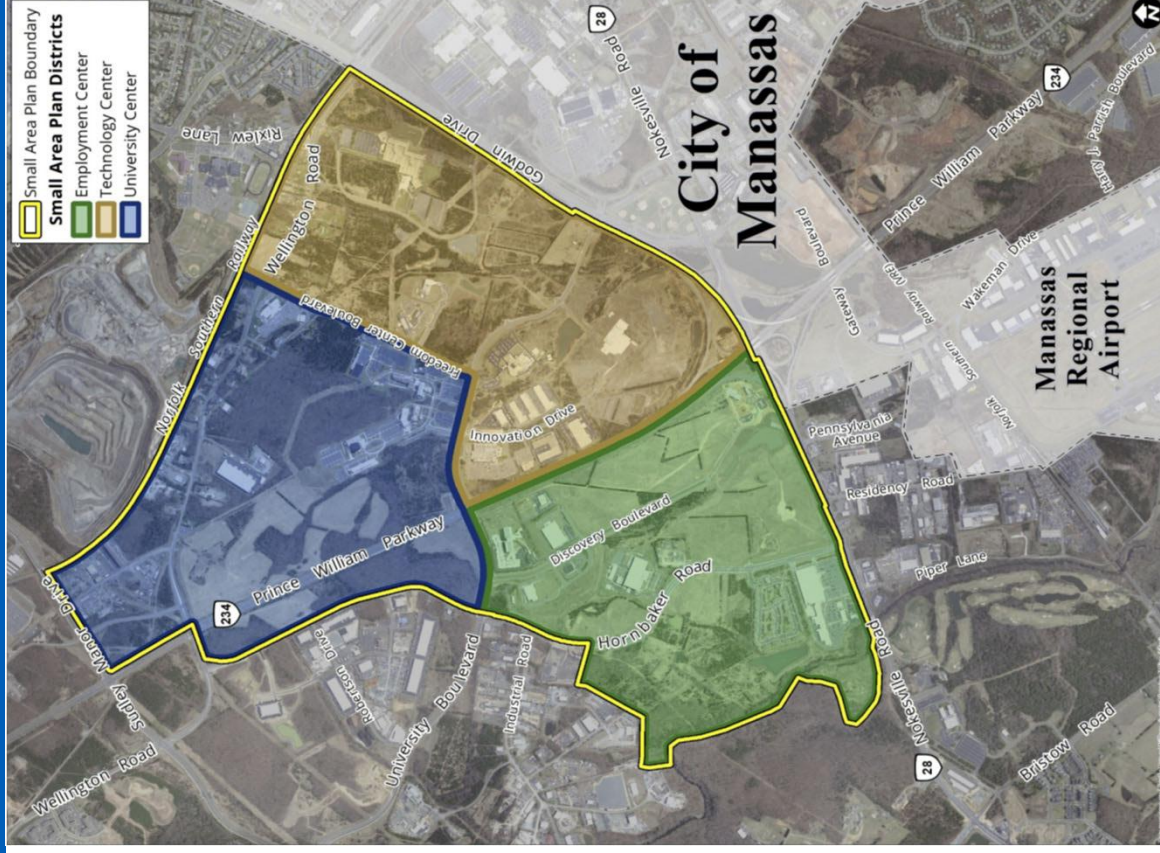
# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



# INNOVATION PARK SMALL AREA PLAN DISTRICTS



- Study Area covers 1,760 acres.
- Subdivided into three districts:
  - Employment Center,
  - Technology Center,
  - University Center





# INNOVATION PARK / LAND USE PLAN

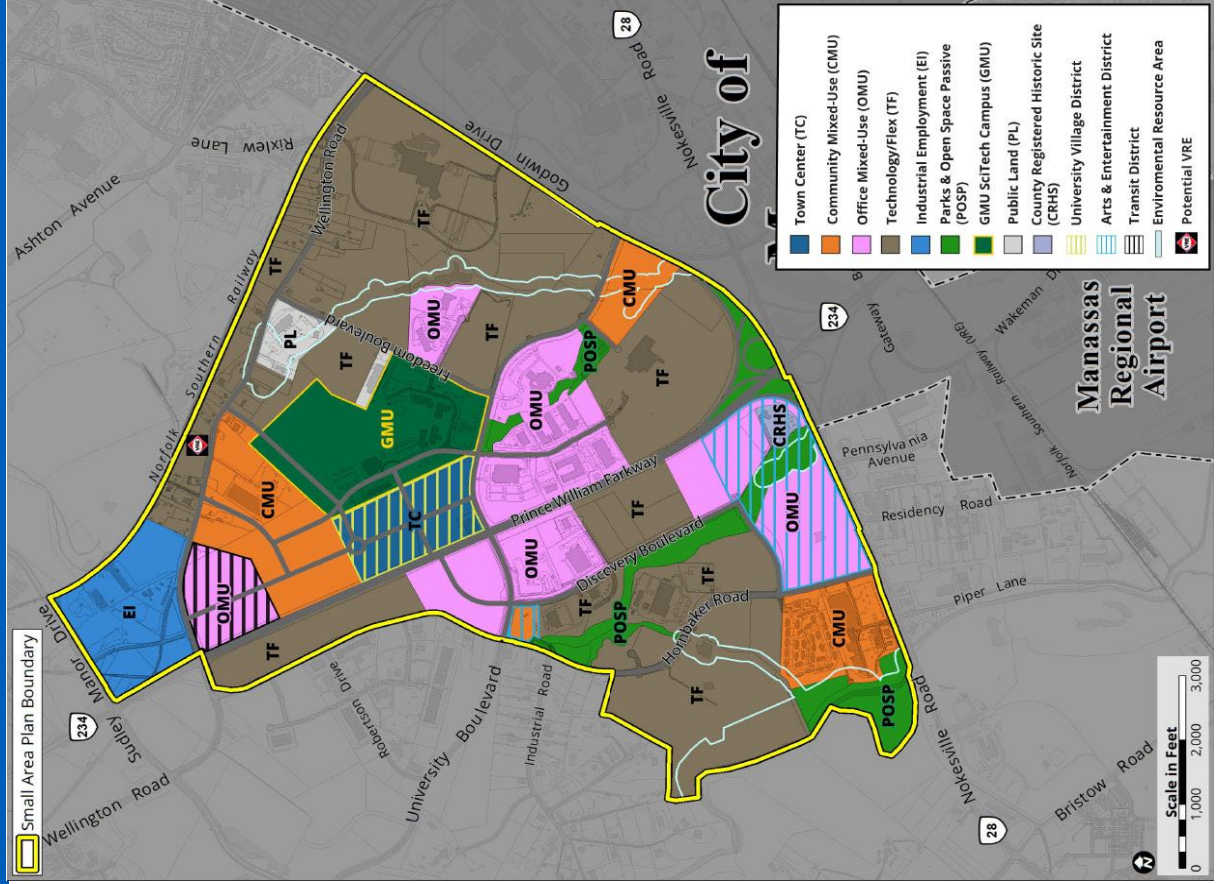


## 8 Land Use Designations

- Town Center
- Community Mixed Use
- Office Mixed Use
- Tech/Flex
- Industrial Employment
- Public Land
- Parks & Open Space
- County Registered Historic Site

## 3 Overlay Districts

- University Village
- Arts & Entertainment
- Transit District





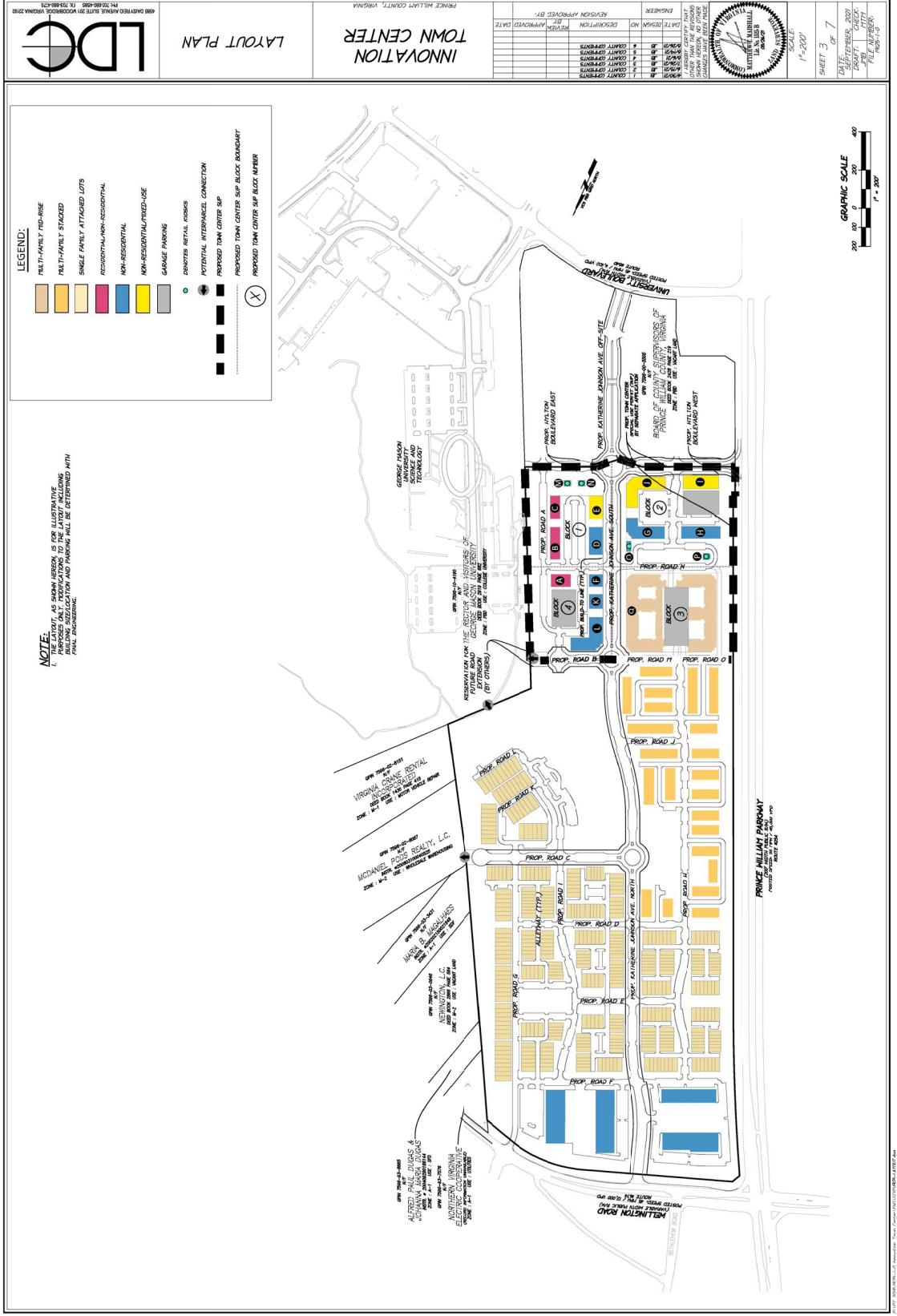
# INNOVATION PARK TOWN CENTER ILLUSTRATIVE PLAN



- The Town Center represents the geographic core.
- The main street will be the primary east-west spine.
- Designed to integrate closely with the GMU campus.



# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



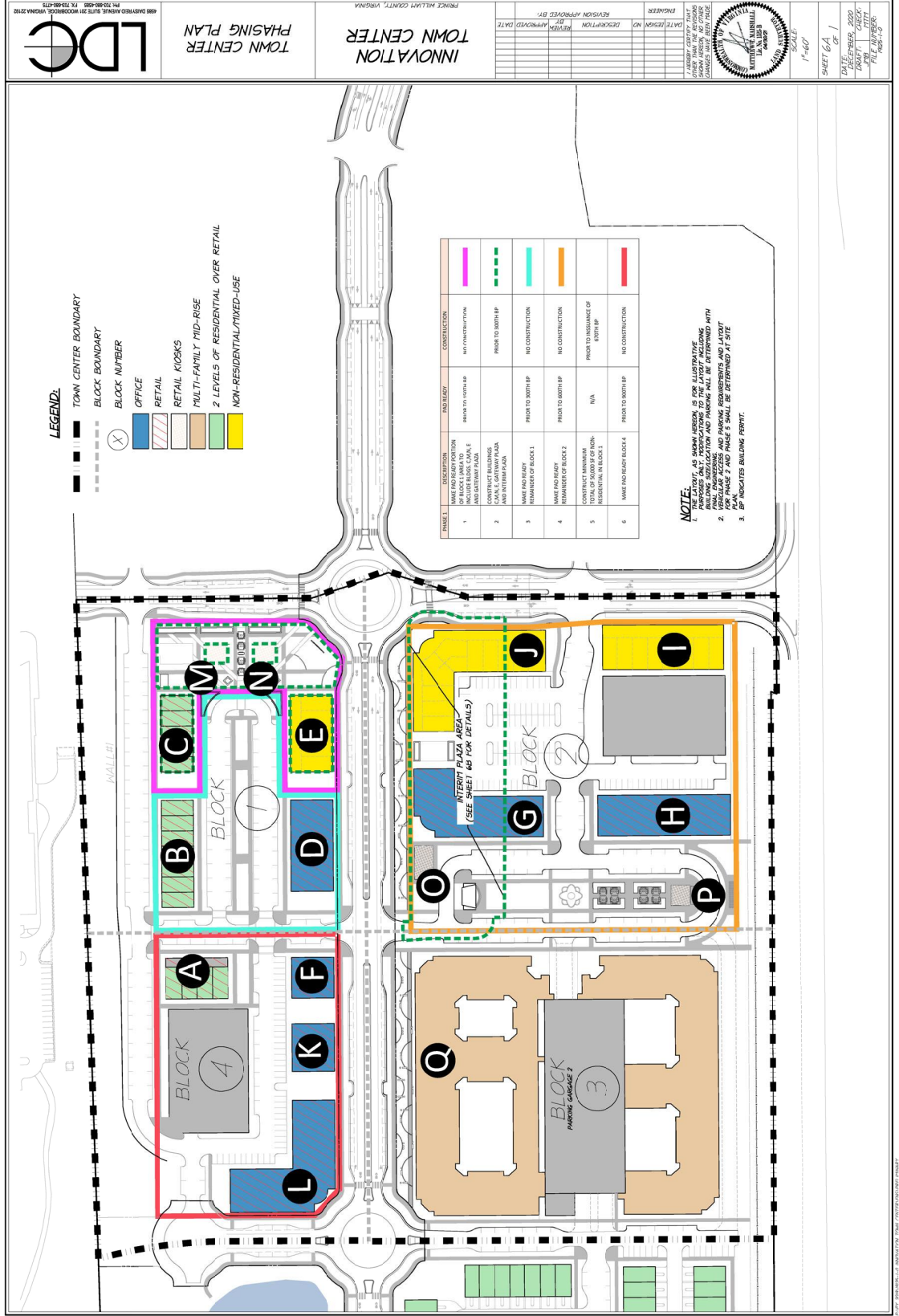




# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



PRINCE WILLIAM COUNTY



TOWN CENTER PHASING PLAN

INNOVATION TOWN CENTER

REVISION APPROVED BY:	DATE:	DESCRIPTION:

DATE: 12/15/2016  
 SHEET: 6A OF 1  
 PROJECT: INNOVATION TOWN CENTER PHASING PLAN  
 DRAWN BY: J. W. WILSON  
 CHECKED BY: J. W. WILSON  
 FILE NUMBER: 16-00031





# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



## 2. THE PLAN Open Space & Amenities

### OVERALL AMENITY PLAN

The design concept of Innovation Town Center incorporates a well composed mix of outdoor spaces woven together through a network of pedestrian and vehicular corridors. Many green spaces serve to bring people together and create community. These spaces, which vary in scale from a large nature park & trail to small neighborhood greens or pocket parks, provide a venue for the types of activities essential for a thriving and interactive community.

Potential recreation areas include:

- \* 1. Community Gathering Area & Innovation Playground (± 36,000 SF)
- \* 2. Neighborhood Promenade (± 29,200 SF)
- 3. Pocket Park 1 (± 3,500 SF)
- 4. Pocket Park 2 (± 15,800 SF)
- \* 5. Club House and Community Pool (± 155,200 SF)
- 6. Nature Playground (± 52,800 SF)
- \* 7. Nature Park (± 18,700 SF)
- 8. Courtyard Green (± 13,200 SF)
- 9. Linear Courtyard (± 12,600)
- 10. Pocket Park 3 (± 8,800 SF)
- 11. Pavilion Green & Urban Park (± 29,200 SF)
- 12. Bike Rest Area (± 7,000 SF)
- \* 13. Urban Promenade (± 20,500 SF)
- \* 14. Gateway Plaza (± 25,600 SF)
- 15. Stream Preservation Area with Trail (± 193,000 SF)

Total Area: (± 621,100 SF)\*    \* Denotes major recreation areas

### INNOVATION DESIGN GUIDELINES



\*These areas are approximate and subject to change at site plan.

# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



## 2. THE PLAN Open Space & Amenities

TOWN CENTER

**12 BIKE REST AREA**  
The Bike Rest Area will feature a fix-it station, water station, bike racks and a seating area. This area creates an opportunity to implement a bike rental system within the community.

**14 GATEWAY PLAZA**  
The Gateway Plaza is an urban plaza located along Hynton Boulevard providing ample and inviting siting areas to support the commercial uses. It will feature some small retail kiosks, public art, and different types of seating areas. During special events the adjacent streets will be closed to vehicular traffic allowing vendors to setup along the parallel parking areas.

**13 URBAN PROMENADE**  
The Urban Promenade provides an ample attractive path connecting the development to the adjacent University Village project providing a strong visual and pedestrian connection between the two activity nodes.

**14 PAVILION GREEN**  
The Pavilion Green will include movable lounging chairs, a lawn area, and a pavilion for concerts and movies.

**URBAN PARK**  
The Urban Park features a series of pocket park spaces that function as urban living rooms with coffee tables and lounging chairs.





### 3.1 NON-RESIDENTIAL DETAILS Town Center Amenity Details

#### TOWN CENTER

##### AMENITY AREAS

The Town Center proposes several amenity areas that provide opportunities for social interactions such as, but not limited to:

- Pavilion for entertainment,
- Flex-community lawn space,
- Trail,
- Bicycle rest area,
- Lounging style seating areas,
- Bistro style seating areas,
- Movable and fixed seating,
- Retail kiosk with plaza area and seating
- Public art, and
- Fire feature

##### PLACEMAKING ELEMENTS

The amenities should provide a placemaking formula to include, but not limited to:

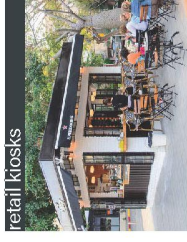
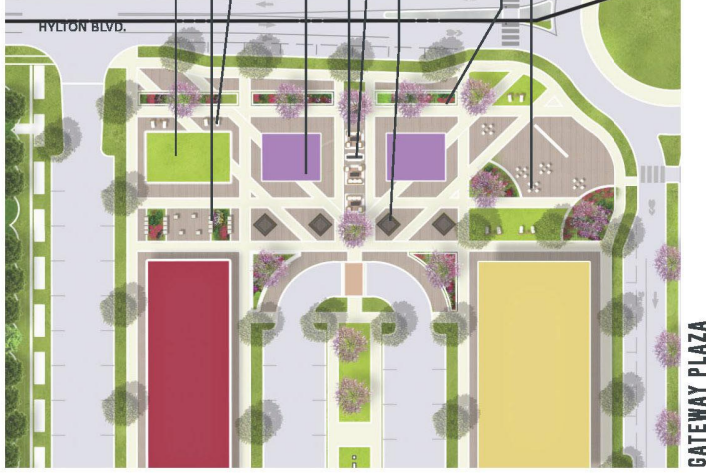
- Proper physical form & human scale;
- Proper mix of land uses and functions;
- Proper mix of social opportunity, and
- Programmed special events

They should include elements such as, but limited to:

- Quality public spaces;
- Attractive urban streetscape;
- Arts, culture and creativity;
- Green Spaces, and
- Recreation

##### AMENITY LANDSCAPING

Landscaping throughout the Town Center amenities shall provide year-round seasonal interest, as well as help to define and frame community areas. A mix of evergreen and deciduous, predominately native and drought resistant plant material will be used to promote a sustainable landscape. Thoughtfully selected planters, benches, lighting poles with hanging planters and tree lined streets will visually enhance connections between spaces and provide unifying design elements.



retail kiosks

LAWN AREA  
CONCRETE SEATING CUBES  
LOUNGE CHAIRS

RETAIL KIOSK

URBAN LIVING ROOM SEATING  
DOUBLE SIDED FIRE PLACE

STRUCTURAL MODULAR SEAT

RAISED PLANTER

BISTRO STYLE SEATING



retail kiosks

FOR ILLUSTRATIVE PURPOSES ONLY

# Innovation Town Center

## #REZ2016-00030 and #SUP2016-00031



### 4. RESIDENTIAL PLAN DETAILS Residential Overview

The Innovation Town Center residential component offers a community with a varied selection of home types. The residential housing mix is composed of mid-rise multi-family units within the Town Center. Then it transitions north to stacked townhouses also arranged in a neo-traditional urban pattern. An environmental amenity corridor which includes a clubhouse, a pool, a nature park and playground, as well as a trail, separates this neighborhood with the other area that offers single family detached townhouse. The development proposes an array of lot sizes and home choices catering to the different needs of potential home buyers.

Each unit type within the community is strategically located to front a street or a public space. The alleyways have been located in a manner in which they are not directly visible from another street.

Residential areas also provide a significant amount of active and passive recreational areas strategically located within walking distance from the community.



FOR ILLUSTRATIVE PURPOSES ONLY

# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



**Planning Commission Recommendation:** The Planning Commission recommends approval of Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center, subject to proffers dated August 26, 2021, and Special Use Permit #SUP2016-00031, Innovation Town Center, subject to conditions dated June 15, 2021.



# Innovation Town Center #REZ2016-00030 and #SUP2016-00031

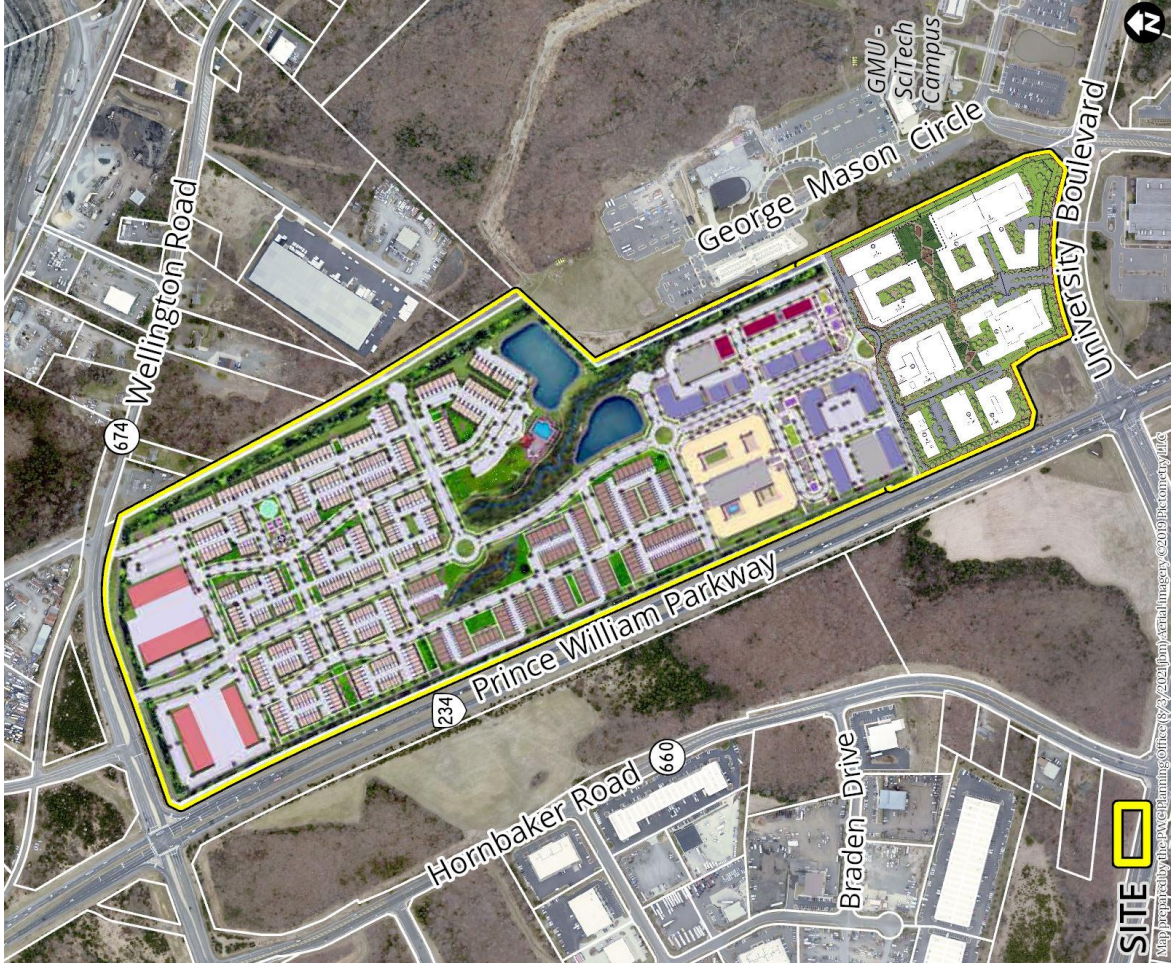


PRINCE WILLIAM  
COUNTY

**Staff Recommendation:** Staff recommends approval of Rezoning and Release of Declaration #REZ2016-00030, Innovation Town Center, subject to revised proffers dated September 9, 2021, and Special Use Permit #SUP2016-00031, Innovation Town Center, subject to revised conditions dated September 9, 2021, for the following reasons:

- The proposal will reasonably implement the Innovation Park Small Area Plan.
- The Applicant has committed to multistory construction within the Town Center (TC) designation.
- One-story standalone retail or retail service uses have been restricted.
- Triggers for the construction of nonresidential uses have been provided.
- The proposed design guidelines further the recommendations in the Innovation Park Small Area Plan.
- Level of service impacts will be mitigated.

# Innovation Town Center #REZ2016-00030 and #SUP2016-00031



Map prepared by the Planning Office, 4/2/2016. Aerial images © 2016 Panoramio, LLC