

PLANNING COMMISSION RESOLUTION

MOTION: September 21, 2022 Regular Meeting

SECOND: RES. No. 22-xxx

RE: REZONING #REZ2019-00018, Quartz District

NEABSCO MAGISTERIAL DISTRICT

ACTION:

WHEREAS, this is a request to rezone ±145.05 acres from A-1, Agricultural, O(H), Office High-Rise, and R-2, Suburban Residential, to PMR, Planned Mixed Residential, and PMD, Planned Mixed Use District, to allow up to 1,015 residential units and 374,000 square feet of non-residential uses, with associated waivers/modifications; and

WHEREAS, the site is located at the northwest quadrant of the intersection of Prince William Parkway and Minnieville Road. The site is identified on County maps as 8192-74-5435, 8192-83-4108, and 8192-93-1868; and

WHEREAS, the site is designated CMU, Community Mixed-Use, HDN, High-Density Neighborhood, SN, Suburban Neighborhood, PL, Public Land, POSA, Parks and Open Space Active, and POSP, Parks and Open Space Passive TC, Town Center, in the Comprehensive Plan. The site is located in the Dale City Small Area Plan and is partially located in the Prince William Parkway and Minnieville Road Highway Corridor Overlay Districts; and

WHEREAS, the site is zoned A-1, Agricultural, O(H), Office High-Rise, and R-2, Suburban Residential; and

WHEREAS, County staff recommends that the Planning Commission recommend approval of this Rezoning for the reasons stated in the staff report; and

WHEREAS, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on September 21, 2022, at which time the merits of the above-referenced case were considered; and

WHEREAS, the Planning Commission finds that public necessity, convenience, general welfare, and good zoning practice are served by a recommendation of approval of this request;

NOW, THEREFORE, BE IT RESOLVED that the Prince William County Planning Commission does hereby recommend approval of Rezoning # REZ2019-00018, Quartz District, subject to the proffers dated August 19, 2022.

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ATTACHMENT: Proffer Statement, dated August 19, 2022	
Votes: Ayes: Nays: Abstain from Absent from Absent from	Vote:
MOTION CAR	RIED
Attest:	Antoinette Brzyski Acting Clerk to the Planning Commission

PROFFER STATEMENT

RE: REZ 2019-00018, Quartz District

Applicant/Record Owner: SKW VA, Inc. and Southern Knolls, LLC Property: G.P.I.N.s 8192-93-1868, 8192-83-4108, and 8192-74-5435

Neabsco Magisterial District

Existing/Proposed Zoning:

GPIN 8192-74-5435: A-1 to PMR \pm 11.8478 acres GPIN 8192-83-4108: OH to PMR \pm 13.9469 acres OH to PMD \pm 0.0531 acres R-2 to PMR \pm 40.8013 acres GPIN 8192-93-1868: R-2 to PMD \pm 20.1558 acres A-1 to PMR \pm 10.3313 acres A-1 to PMD \pm 14.4646 acres O(H) to PMR ± 14.6929 acres O(H) to PMD ± 16.8216 acres Existing Right-of-way¹ to PMR ±0.0128 acres to PMD ±1.9250 acres

Collectively these parcels are referred to herein as the "Property" unless the context requires otherwise.

Proposed PMR Acres: \pm 91.633 acres Proposed PMD Acres: \pm 53.420 acres

Date: August 19, 2022

The undersigned hereby Proffers that the use and development of the Property shall be in substantial conformance with the following conditions that shall supersede all other Proffers with respect thereto heretofore made. In the event the above-referenced rezoning is not granted as applied for by the Applicant, these Proffers shall be deemed withdrawn and void. The headings of the Proffers set forth below have been prepared for convenience or

¹ This property is public right-of-way that is not used or to be used that will be vacated and incorporated into the Property.

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reference only and shall not control or affect the meaning or be taken as an interpretation of any provision of the Proffers.

Any improvements proffered herein below shall be provided at the time of development of the portion of the site served by the improvement, unless otherwise specified. The terms "Applicant" and "Developer" shall include all future owners and successors in interest.

"Final Rezoning", as the term is used herein, shall be defined as that zoning which is in effect on the day following the last day upon which the Prince William Board of County Supervisors' decision granting the rezoning may be contested in the appropriate court or, if contested, the day following entry of a final court order affirming the decision of the Board of Supervisors which has not been appealed, or if appealed, the day following which the decision has been affirmed on appeal and the mandate issued.

For purposes of reference in this Proffer Statement, relevant plans and exhibits shall include the following:

- A. "Master Zoning Plan Quartz District" prepared by christopher consultants, dated March 2, 2021, revised August 19, 2022 consisting of the following proffered sheets (collectively the "MZP"):
 - Cover Sheet;
 - Land Use & Landscape Buffer Plan (the "Land Use Plan");
 - Public Transportation Plan;
 - Street Network & Signage Plan;
 - Pedestrian Circulation Plan;
 - Bicycle Network Plan;
 - Proposed Street Sections;
 - Proposed Intersection Details;
 - o Infrastructure & Utility Plan; and
 - Phasing Plan
- B. Design Guidelines dated August 19, 2022, entitled "Quartz District Design Guidelines (the "Design Guidelines").

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- C. Potential East MOT Road & Off-Site SWM Facility Grading Exhibit," prepared by christopher consultants, dated July12, 2022, last revised August 19, 2022.
- D. Proposed Easement & Fencing Exhibit, prepared by christopher consultants, dated April 11, 2022 (the "Easement & Fencing Exhibit").
- E. The Quartz District Illustrative Drive Through, and Motor Vehicle Fuel Station Renderings, prepared by christopher consultants, dated August 3, 2022 (the "Illustrative Renderings").

USES AND SITE DEVELOPMENT

- 1. Except as otherwise provided herein, the Property shall be developed in substantial conformance with the MZP. The exact boundaries and acreage of each Land Bay may be increased or decreased at the site plan/subdivision stage by not to exceed ten percent (10%) of the gross area of the larger Land Bay impacted by each such change; provided that nothing contained in this paragraph shall permit a modification of the zoning line between the PMD and PMR zoned properties. Use of the 10% flexibility will not alter the primary network framework and/or intersections shown on the MZP.
- 2. The maximum number of residential units constructed on the Property may not exceed 610 townhouse units and 405 multifamily units.
- 3. For purposes of calculating density on final site/subdivision plans for each Land Bay, the density permitted in each Land Bay shall be capped at the maximum number of housing types, whether townhomes, or multifamily units, as set forth on the MZP, provided that the total number of residential units on the Property as set forth above is not exceeded. Construction of units will be commenced in each Land Bay in accordance with the Phasing Proffers below. With each site/subdivision plan submitted, the Applicant will provide a cumulative tabulation of the number of residential units and types constructed for the entire Property in order to ensure that these requirements will have been satisfied upon full buildout.

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- 4. For purposes of calculating open space and tree canopy coverage, the entire Property shall be used, and not individual Land Bays or parcels within a Land Bay. With each site/subdivision plan submitted, the Applicant will provide a cumulative tabulation of the open space and tree canopy coverage calculations for the entire Property, in order to ensure that Ordinance requirements will have been satisfied upon full buildout.
- 5. The Applicant may use the Property in accordance with the Planned Mixed-Use District (PMD), and the Planned Mixed Residential District (PMR), as their requirements are waived and/or modified in accordance with these Proffers and in those portions of the Property to which those zoning classifications apply pursuant to this Rezoning.
- Pursuant to § 32-400.07.2 of the Zoning Ordinance, the Applicant shall be permitted one drive-in facility use and a motor vehicle fuel station with convenience store/quick service food store use in Land Bay A, by right, as generally depicted on the Illustrative Renderings. In connection with the foregoing, the buildings may be prototypical retail building designs used by national and regional retailers, or similar types of buildings. The buildings may use design elements, features, materials, and colors that are trademark and/or prototype features for specific retail franchises. Buildings shall have four-sided architecture using similar design elements and materials. Building exteriors shall consist of highquality building materials, such as, but not limited to, brick and architectural concrete masonry, stone and manufactured stone products, architectural concrete, cementitious siding material, synthetic stucco, architectural metals, and glass window or storefront fenestration. Roofs may be flat or sloped, with sloped roofing materials consisting of architectural shingles or metal roofing. Building material and color selections shall exhibit high-quality design. Painted or unpainted standard concrete block, painted or unpainted wood siding, aluminum siding and vinyl siding materials shall not be permitted on the fuel station and drive through buildings. The Applicant shall submit to the Planning Director, or his designee, building elevations/exterior design of the proposed building(s) for review to confirm compliance with this Proffer. The Applicant shall be permitted to obtain preliminary site plan approval on properties subject to these Proffers prior to final review and approval of the building elevations/exterior

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design of the proposed building(s) but shall not be permitted to receive final approval of a site plan prior to final review and approval of the building elevations/exterior design of the proposed building(s) by the Planning Director. Other than a veterinary hospital, any uses requiring a special use permit shall be permitted on the property with an approved special use including additional drive-through uses.

PHASING OF DEVELOPMENT

- 7. The Property will be developed in Phases that are coordinated with transportation and other infrastructure improvements as further set out in these Proffers. Notwithstanding the ordering of these phases, the units, improvements, and infrastructure identified herein may be constructed in any sequence consistent with the requirements of the Property and related improvements and infrastructure.
 - a. Phase 1A will consist of the following:
 - i. Construction of the portions of Roads 1A,1B, 3, and 5, as generally shown on the Phasing Plan, which are necessary to access and service the Phase 1A Units identified below, and the Public Park/School site referenced below.
 - ii. Roads 1A and 1B will be initially constructed to meet Maintenance of Traffic ("MOT") requirements as set forth in Proffer 10, and will not be converted to its ultimate condition until PWCDOT provides written confirmation that Roads 1A and 1B are no longer necessary to provide a MOT function.
 - iii. Construction of those improvements to the Prince William Parkway needed to effect the connection of Road 1A to the Prince William Parkway, as further outlined in Proffer 10.g below, including installation of the traffic signal, if warranted.
 - iv. Dedication of the Public Park/School site(s) to the County. Said dedication(s) shall be made prior to the issuance of 300th residential occupancy permit in Land Bay D or Land Bay F or combination thereof.

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- v. The construction of up to 300 residential units located in Land Bays D and F (the "Phase 1A Units"). Occupancy permits for the Phase 1A Units shall not be issued until Roads 1A and 1B are built to the MOT standards, and open to traffic.
- b. Phase 1B will consist of the following:
 - i. Construction of Roads 2A and 2B, as identified on the Phasing Plan, to Minnieville Road.
 - ii. Roads 2A and 2B will be initially constructed to meet MOT requirements as set forth below and will not be converted to its ultimate condition until PWCDOT, provides written confirmation that Roads 2A and 2B are no longer necessary to provide an MOT function.
 - iii. Construction of those improvements to Minnieville Road needed to effect the connection of Road 2A to Minnieville Road, as further outlined in Proffer 10.g. below, including the installation of the traffic signal, if warranted.
 - iv. The construction of up to 150 residential units located in Land Bays B or C, or combination thereof, (the "Phase 1B Units"), and a motor vehicle fuel station and convenience store in Land Bay A. Occupancy permits for the Phase 1B Units, and the motor vehicle fuel station and convenience store may not be issued until the Roads 2A and 2B arebuilt to MOT standards, and open to traffic.
- c. Phase 1C will consist of the following:
 - i. The Applicant shall grant to the County those necessary temporary easements or other permissions for Roads 1A, 1B, 2A, 2B to be used as MOT Roads during the construction of the Single Point Urban Interchange ("SPUI"), as further described in the Transportation Proffers below as the "West MOT Road."

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- ii. Subject to County approval the Applicant will coordinate with PWCDOT to provide for the use of Applicant's off-site property identified as GPIN 8292-04-3728 (the "Off-Site Property") for the construction staging and the placement of suitable spoil material subject to the requirements set forth herein, and on the attached Potential East MOT Exhibit.
- iii. Prepare and provide design plans for the off-site stormwater management facility, and acquire the necessary permits and approvals for construction by the Applicant. This design will also include the Potential East MOT Road and in general conformance with the Potential East MOT Exhibit or as otherwise provided herein.
- iv. If requested by the County, the Applicant shall grant to the County those necessary temporary easements or other permissions for the use of the Off-Site Property owned or controlled by the Applicant to be used as the "Potential East MOT Road" during the construction of the SPUI, as further described in the Transportation Proffers below.
- v. Prepare and provide 30% design for the SPUI, if not already accomplished.
- d. Phase 1D will consist of the following:
 - i. The construction of nonresidential uses (including, but not limited to a grocery store) may commence in Land Bay A. Prior to the issuance of the first nonresidential occupancy permit in Land Bay A, the County must confirm in writing that the intersection of the Prince William Parkway and Minnieville Road is capable of accommodating the additional traffic generated by such uses prior to substantial completion of the SPUI.
 - ii. Construction of a portion of the eight foot (8') Private Shared Use Path on the north side of Minnieville Road. This eight foot (8') Private Shared Use Path will be maintained by a nonresidential association or nonresidential

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associations, as described in Proffer 43. Said eight foot (8') Private Shared Use Path shall be constructed prior to the issuance of the first nonresidential occupancy permit issued for development in the Land Bay A area on the east side of Roads 2A and 2B.

- e. Phase 2 will consist of the following:
 - i. The construction of up to an additional 350 residential units located in Land Bay B and Land Bay C may commence (the "Phase 2 Units"). Prior to the issuance of an occupancy permit for any Phase 2 Unit, a building permit shall be obtained and construction shall have commenced for a minimum of 100,000 gross square feet of nonresidential in Land Bay A. Commencement of construction shall mean the Applicant shall pursue in good faith and with due diligence, construction of the nonresidential in Land Bay A to include: (i) foundation and structure; (ii) the exterior building skin, (iii) roofing; (iv) parking lot; and (v) landscaping associated with the site plan for the nonresidential. Interior tenant improvements shall not be required to be commenced until done so in conjunction with executed leases in the nonresidential building(s).
 - ii. Construction may commence of additional nonresidential uses (including restaurants) located in Land Bay A.
 - iii. Those transportation improvements identified in Phases 1A, 1B, 1C, and 1D shall be provided as proffered, (or such later date approved by PWCDOT).
- f. Phase 3 will consist of the following:
 - i. Completion of Roads 3, 4, 5, and 6.
 - ii. The construction of the remaining units (the "Phase 3 Units"). Prior to the issuance of an occupancy permit for the 801st residential unit, a building permit shall be obtained and construction shall commence for a cumulative total of

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150,000 gross square feet of nonresidential uses in Land Bay A. Commencement of construction shall mean, the Applicant shall pursue in good faith and with due diligence, construction of the nonresidential to include: (i) foundation and structure; (ii) the exterior building skin; (iii) roofing, (iv) parking lot; and (v) landscaping associated with the site plan for the nonresidential. Interior tenant improvements shall not be required to be commenced until done so in conjunction with executed leases in the nonresidential building(s).

- iii. The remaining authorized nonresidential uses will be developed in accordance with market demand and will be considered a part of Phase 3.
- iv. Those transportation improvements identified in Phases 1A, 1B, 1C, and 1D shall be provided as proffered, (or such later date approved by PWCDOT).
- g. Development of Land Bays A and B may be phased such that any building may be built first, and the Applicant may use surface parking in either Land Bay until structured parking is required in order to meet the required parking standards set forth in the Design Construction and Standards Manual (DCSM) and/or the Prince William County Zoning Ordinance.
- 8. The Applicant will be permitted to bond phases of individual site and subdivision plans according to Phase Lines as shown on preliminary plans for each Land Bay.

TRANSPORTATION

- 9. <u>The Parkway/Minnieville Road Single Point Urban Interchange ("SPUI").</u>
 - a. Prior to issuance of the first occupancy permit on the Property, the Applicant will prepare and submit to PWCDOT and VDOT: (i) A "Preliminary Interchange Alternatives Analysis Report"; (ii) Prince William Parkway/Minnieville Road Interchange Justification Report Scoping Study; and (iii) the "Interchange Justification

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Report – Prince William Parkway (Route 294)/Minnieville Road (Route 640) Interchange (the "IJR")."

- b. These studies will be prepared consistently with the Transportation Chapter of the County Comprehensive Plan, and the 2019 Road Bond Referendum. These studies are intended to facilitate the expedited design of and contracting for the Prince William Parkway/Minnieville Road Interchange and to reduce the cost of its design and construction.
- c. Consistently with the Transportation Bond Referendum it is evident that the studies will confirm that the most efficient and cost-effective design for the new Interchange is a Single Point Urban Interchange ("SPUI") and the proffers here submitted are intended to advance the design and construction of that Interchange.
- d. Prior to the issuance of the 301st residential occupancy permit in Land Bays D and F, the Applicant will initiate Proffers 9.a. c. above, and will provide:
 - i. Required survey work for the SPUI necessary for the 30% design plans for the SPUI;
 - ii. Required geotechnical work for the SPUI necessary for the 30% design plans for the SPUI;
 - iii. Design and construction of the off-site stormwater management facility located on the east side of Prince William Parkway sufficient, with respect to capacity, to accept the required drainage from the SPUI project and the Property; and
 - iv. Prepare Utility Relocation Design Plans for the construction of the SPUI.
- e. The Applicant shall prepare 30% Design for the SPUI, and provide to the County at no charge for its use in completing the design and construction of the SPUI project.

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- f. As provided in Phase 1C above and upon request of the County, the Applicant will provide an off-site location for placement and compaction of excess suitable soils from SPUI excavation, in a manner typical of site preparation for development, with typical geotechnical inspections and testing to be further defined in any easement or agreement between the Applicant and the County, and/or a construction staging area for the contractors.
- 10. <u>Maintenance of Traffic Road West ("West MOT Road")</u> Subject to PWCDOT and VDOT approval, and consistent with the Phasing of Development proffers set forth above, the Applicant shall provide the following with respect to Roads 1A, 1B, 2A and 2B (collectively the "West MOT Road) as generally shown on the MZP:
 - a. Prepare and process the design and construction plans for Roads 1A, 1B, 2A and 2B to function initially as the West MOT Road, with associated stormwater management facilities. The Applicant understands that the design for MOT purposes will differ from the ultimate Road 1A, 1B, 2A and 2B condition, and that the Applicant will modify those Roads to their ultimate condition when they cease to serve an MOT function as detailed on the Proposed Street Sections sheet of the MZP. At the Applicant's election, it may separately design Roads 1A, 1B, 2A and 2B to accommodate its development schedule.
 - b. Grant, at no cost to the County, the temporary easements for the West MOT Road upon approval of the final plans therefor. Subsequently, in conjunction with the conversion of Roads 1A, 1B, 2A and 2B to their ultimate condition, dedicate the final right-ofway therefor.
 - c. Substantially complete the approved stormwater management facility(s) for the West MOT Road that the Applicant constructs.
 - d. In conjunction with the construction of the SPUI Project, complete Roads 1A, 1B, 2A and 2B for use as of the West MOT Road as required herein as shown on the MZP.

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- e. Prepare a Signal Justification Report ("SJR"), and process the interim and permanent traffic signal design plans for new traffic signal, and intersection improvements, at the intersection of Prince William Parkway and Road 1A, as generally depicted on the MZP. Said signal design shall include a pedestrian crosswalk and pedestrian signal head.
- f. Prepare a SJR and process the interim and permanent traffic signal design plans for new traffic signal and intersection modification plans at the intersection of Minnieville Road and Road 2A.
- g. Subject to PWCDOT and VDOT approval of final road construction and signal plans, substantially complete the traffic signal in accordance with approved construction and signal plans on Prince William Parkway, as required for access to/from Road 1A, and substantially complete the traffic signal on Minnieville Road, as generally shown on the MZP for access to/from Road 2A.
- 11. Potential Maintenance of Traffic Road East ("Potential East MOT Road")

 The Applicant owns the property (GPIN 8292-04-3728) on the east side of the Prince William Parkway and as mentioned above, will design and construct an off-site SWM facility to serve the Quartz District and the SPUI. The Applicant will include in the design an access road into the property. The access road will be designed to be sufficient to be utilized for maintenance of traffic by the County or its contractor for the SPUI, at their option. The road design will not connect to Elm Farm Road to give the County or its contractor the flexibility to connect as they determine.

Subject to PWCDOT and VDOT approval, and consistent with the Phasing of Development proffers set forth above, the Applicant shall, if requested by PWCDOT or VDOT, provide the following with respect to the off-site Potential East MOT Road, which will allow for the future connection from Prince William Parkway to Elm Farm Road. The proposed connection to the Parkway will be opposite the signalized intersection of Road 1A and the Parkway as depicted on the Potential East MOT Road Exhibit attached hereto and incorporated herein by reference.

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- a. <u>Plans/Processing</u> As provided above the Applicant will:
 - i. Grant, at no cost to the County, temporary easements for the Potential East MOT Road and associated stormwater management as shown on the Potential East MOT Road Exhibit, upon approval of the final plans therefor.
 - ii. Grant, at no cost to the County, temporary easements for the interchange contractor staging area and the interchange spoils area as generally shown on the Potential East MOT Road Exhibit, upon approval of the final plans therefor.
 - iii. Provide reasonable assistance to the County and VDOT in the construction of the Potential East MOT Road that will connect Prince William Parkway to Elm Farm Road to ensure a continuous flow of east-west traffic during construction of the SPUI.

12. Transportation Level of Service Monetary Contributions:

- a. The Applicant will make a monetary contribution to the Prince William Board of County Supervisors in the amount \$4,500 for each residential unit constructed on the Property for area transportation improvements. Said contribution shall be paid prior to and as a condition of occupancy for each said unit constructed on the Property. Each monetary contribution shall be allocated as follows:
 - i. \$2,500 to be used in the construction of the SPUI;
 - ii. \$500 to be used in the improvement of the intersection of Old Bridge Road and the Prince William Parkway;
 - iii. \$500 to be used in the improvement of Elm Farm Road and Minnieville Road;
 - iv. \$500 to be used in the improvement of Dale Boulevard and Minnieville Road; and

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- v. \$500 to be used in the improvement of such other intersection in the vicinity of the SPUI as the Board of County Supervisors determines.
- 13. Notwithstanding the foregoing allocation of monetary contributions to specific areas, the Board may elect to allocate the contributions to any road project in the area encompassed by the Dale City Small Area Plan that it may determine in its sole discretion, including the SPUI.
- 14. Subject to PWCDOT and VDOT approval, Turn lane improvements for the access points 12, 13 and 16, as shown on the Public Transportation Plan, shall be provided at such time, and in accordance with the processing of an associated plan.
- 15. <u>Signal Modification Study</u> Within 180 days of substantial completion of the SPUI, and intersections 11, 13 and 15 being operational, the Applicant shall submit to VDOT and PWCDOT a signal timing modification study for the existing traffic signals located on Prince William Parkway between Old Bridge Road and Smoketown Road, and those existing traffic signals located on Minnieville Road between Caton Hill Road and Dale Boulevard.

16. Interparcel Access:

- a. The Applicant shall reserve a potential interparcel connection to GPINs 8192-85-3351 and 8192-85-5526 as a right-of-way up to 65 feet in width, as shown on the Street Network & Signage Plan, construction thereof to be by others.
- b. The Applicant shall reserve a potential interparcel connection to GPINs 8192-75-6416 as a right-of-way up to sixty-five feet(65') in width, as shown on the Street Network & Signage Plan, construction thereof to be by others.
- c. The Applicant shall reserve two (2) forty foot (40') wide potential interparcel connections along the western property line as shown on the Street Network & Signage Plan. This reservation shall only be converted to an easement in the event the Cedar Run

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Subdivision located along the western Property line is assembled under single ownership, and rezoned,.

- 17. "Substantial completion of a road or street" means that any public road or street is open to traffic, and may not have final paving, but has not yet been accepted into the VDOT System of Highways except as may be otherwise provided herein.
- 18. <u>Bicycle Racks</u> The Applicant shall provide bicycle racks on the Property in accordance with the DCSM.

PUBLIC TRANSPORTATION AND TRANSIT

- 19. Public Transit.
 - Within nine (9) months of the issuance of the first occupancy a. permit in Land Bay A, and subject to OmniRide's approval, the Applicant shall provide up to two covered transit stop enclosures (the "Enclosures") within Land Bay A, in the general locations identified on the Street Network and Signage Plan. The final locations, design details and construction timing for the Enclosures will be determined in consultation with OmniRide. The architecture and building materials for the Enclosures will be complementary and compatible with the adjacent residential and commercial buildings, and the Enclosures will include lighting, electrical outlets for personal electronic devices, and bicycle racks will be located adjacent to each Enclosure. In addition, the Applicant will provide two (2) empty electrical conduits crossing the adjacent private street to be used for future Omni Ride electrical charging station needs. The Applicant reserves the right to relocate the Enclosures within Land Bay A in consultation with OmniRide to accommodate future land development, and such relocation shall not require further amendments to this rezoning.
 - b. For each initial Enclosure constructed, the Applicant shall provide a one-time cash contribution of \$10,000.00 per enclosure to OmniRide, to be used solely for signage and electronic information/display systems within each Enclosure. Said contribution shall be paid prior to issuance of the building permit

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for the Enclosure(s). In the event an Enclosure is relocated, the relocation shall be at the Applicant's sole expense, and said Enclosure shall include all components of the initial Enclosure except for the electrical conduits, which will not be relocated or provided. An additional cash contribution shall not be required for any relocated Enclosure.

- 20. Transportation Demand Management Plan.
 - a. In conjunction with the creation of the transit stop enclosures, the Applicant shall implement a Transportation Demand Management Program (the "Program") in connection with the development of the Property to be managed by an on-site management company and/or a property or homeowners' association. This Program shall be developed to include strategies to educate about and promote within the Property, such programs as telework and similar alternative work arrangements, bike/walk programs, on-site amenities to reduce vehicle trips, transit benefits program, local public transportation options, flexible work schedules to reduce peak hour vehicle trips, and rideshare opportunities, and shall include coordination with the OmniRide Employer Outreach Program.
 - b. It is the intent of this proffer that the Program be flexible over time to respond to the evolving transportation-related circumstances of the Property, the community, and the region as well as to technologies and/or other improvements that may occur, all which may impact travel behavior and the transportation network. Accordingly, the Program should be modified and amended from time to time, subject to consultation with local transportation service providers.
- 21. Electric Vehicle ("EV") Charging Station. The Applicant will provide a minimum of two (2) electric charging station(s) in Land Bay A retail area, and minimum of two (2) charging stations in the Land Bay B multifamily development area. When not in use, EV parking spaces may be utilized for short-term pick up purposes.

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PARKS AND RECREATION

- 22. In Phase 1A the Applicant shall dedicate to the Prince William Board of County Supervisors in fee simple, by special warranty deed, and at no cost to the County, approximately six (6) acres at the southern end of Land Bay E to be used for parks and recreation purposes ("Public Park Site") as set forth below. Said dedication shall include the following covenants:
 - a. Use restrictions on the land to ensure current and future uses conform to the programing described in Proffer 22 below.
 - b. Reversion rights so that the land would transfer back to the Applicant or its designee for no consideration if the County were to transfer or attempt to transfer by deed or lease, directly or indirectly the land to a third party.
 - c. Mutually agreed cooperation language for both parties.
- 23. This proffer is to provide the site for the County to meet its desire to construct the destination urban adventure park as a described in the approved Dale City Small Area Plan and financed by the November 2019 Park Improvement Bond - Outdoor Facilities. Said park may be developed in a single phase or multiple phases, at the discretion of the County. The urban adventure park is intended to provide an amenityrich community park that includes facilities such as a dynamic playground, climbing wall, hill slides, and skating ribbon, and may include natural and/or passive areas that are compatible and complementary to the urban adventure park. In no event shall the Public Park Site be used for other recreational uses that do not align with the design characteristics and functional intentions of the high-quality multifunction urban adventure park as contemplated to be financed by the November 2019 Park Improvement Bond. If the County elects not to construct the aforesaid high quality multifunction urban adventure park prior to the expiration of the November 2019 Park Improvement Bond or has not commenced construction of the urban adventure park within three years from said bond expiration date, the County will return the site to the Applicant and the Applicant will initiate the applicable

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approval process for a private park and will commence construction of said private park after approvals have been obtained. Said private park will be maintained by the HOA or POA.

- 24. The Applicant will provide off-site storm water management, a stub out of sewer and water connections to the property boundary of the Park Site, and, in the general location shown on the MZP, a perimeter eight foot (8') wide Shared Use Path, (collectively the "Park Improvements"). This shared use path shall be maintained by the County. The Applicant shall work collaboratively with the County to determine mutually agreeable design, location and timing of the Park Improvements.
- 25. The Applicant shall work collaboratively with the County to identify locations on the Property for directional signage for the Public Park Site. Said directional signage may be included in the Comprehensive Sign Plan identified in Proffer 46 below or as mutually agreed upon by the Applicant and the County. Signage for the Public Park Site shall be provided by the County.
- 26. The Applicant shall collaborate with the County and attempt to identify potential temporary parking opportunities within those areas of the Property that are developed for office uses. Said parking is intended to accommodate overflow parking needs for weekend events hosted within the Public Park Site.

SCHOOLS

- 27. <u>Monetary Contribution</u> The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$398.07 per single-family attached unit and \$209.51 for each multifamily unit constructed, to be used for school purposes in the area. Said contribution shall be paid prior to and as a condition of the occupancy permit issuance for each said unit constructed on the Property.
- 28. <u>Dedication</u> In Phase 1A, the Applicant shall dedicate to the Prince William Board of County Supervisors in fee simple, by special warranty deed, and at no cost to the County, approximately twelve (12) acres on

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the northern side of Land Bay E to be used for an elementary school and associated recreational purposes ("Elementary School Site").

- 29. The Applicant shall construct off-site storm water management and a stub out of sewer and water connections to the property boundary of the Elementary School Site, and a perimeter Shared Use Path in an easement at the Elementary School Site, according to a timing and design mutually agreeable to the Applicant and the County. This shared use path shall be maintained by the County
- 30. Notwithstanding the acreage outlined in Proffers 22 and 28 above, the Prince William Board of County Supervisors may modify and allocate the acreages between the Park and School uses, but doing so will not affect the monetary contributions set forth in this section of the proffers

COMMUNITY RECREATION AND OPEN SPACE AMENITIES

- 31. The Applicant shall construct a clubhouse/pool facility on the Property for use of the residents thereof, to be located as generally depicted on the Land Use & Landscape Buffer Plan, subject to change at the time of final site plan. The clubhouse/pool facility shall be constructed prior to the issuance of the 500th residential building permit on the Property. These 500 residential units shall not include multifamily rental units in Land Bay B, which will have an independent pool facility. The recreation center shall include at a minimum a 5,000 square foot clubhouse building, and 4,000 square feet of pool(s).
- 32. In addition, the Applicant shall:
 - a. Provide three (3) playgrounds appropriate for ages 2-12 with playground equipment in the Neighborhood District with a minimum of 2,000 square feet for each playground. Each playground shall one primary play structure, and at least three (3) secondary activity components, along with seating options.
 - b. Provide four (4) passive pocket parks in the Neighborhood District with a minimum of 1,200 square feet for each passive park. Each passive pocket park shall have seating, landscaping, and open play lawn area.

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- c. Provide two (2) dog parks in the Neighborhood District with a minimum of 7,500 square feet for each dog park. Each dog park shall be fenced, and include vestibule fencing and seating options, and may incorporate existing trees.
- d. Provide one (1) passive pocket park in the Urban District with a minimum of 1,500 square feet. Each passive pocket park shall have seating, landscaping, and open play lawn area.
- e. Provide a community lawn area in the Urban District with a minimum of 7,500 square feet and a small pavilion/stage structure with power.
- f. Provide a lakefront plaza amenity area in the Urban District with a minimum of 6,000 square feet including hardscape, public seating, landscape planters, signage, and other wayfinding signage.
- g. Construct the eight foot (8') Private Shared Use Path throughout the community as referenced in these proffers.

Said amenities are generally described in the Design Guidelines, and shall be located and constructed prior to the final building permit for the section in which they are located and shall be shown on each respective site plan.

CEDAR RUN HOA

33. The Applicant will construct two stormwater management ponds on the Property, which is adjacent to the Meadows of Minnieville Sections 1 and 2 (the "Cedar Run Subdivision" or "Cedar Run HOA"), and that will drain towards that Subdivision. The Applicant is willing to exceed County requirements for stormwater management outfall, and notwithstanding any Ordinance provision to the contrary, by coordinating with the Cedar Run HOA to explore the viability of providing additional outfall improvements from these ponds to the applicable portions of the existing Cedar Run Subdivision to mitigate certain existing drainage concerns in Cedar Run. This will be accomplished during the design of the applicable stormwater management ponds, and, as such, shall be

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subject to County approval. In the event that it is not feasible to accomplish the foregoing for any reason, the Applicant will satisfy the County's applicable on-site requirements for stormwater management for the Property.

- 34. The Applicant will provide a commercial grade 6-foot-high black aluminum fence along or immediately adjacent to the property line between the Property and the Cedar Run Subdivision. The fence shall be commercial grade fencing by Iron World Maverick Ultra Plus Commercial Grade Aluminum Fence or the equivalent. Final fence location and style shall be mutually agreed in writing between the Applicant and the Cedar Run HOA. The fence will be maintained by an Association as further defined in Proffer 43.a. Areas of the fence will be installed in conjunction with the completion of the adjacent Land Bays on the Property, but in no event earlier than the demolition of the Verizon Building.
- 35. The Applicant will pave the existing gravel shoulder on the west side of Andorra Drive from Pearlbury Court to the existing sidewalk at Minnieville Road, subject to Prince William County approval. This improvement will be constructed in Phase 1A of the development.
- 36. The Applicant shall provide tree preservation within the buffer located on the Property, which is adjacent to the Cedar Run Subdivision, as further described In Proffer 49.a. In addition, within said buffer, the Applicant shall plant two-hundred and fifteen (215) supplemental trees. Prior to planting said trees, the Applicant will meet with the Cedar Run HOA and review the proposed locations of said trees. Once the final location is determined, the Applicant will prepare a Supplemental Tree Location Plan and plant said trees. Said trees shall be eight to ten feet (8' to 10') tall at time of planting, and will have a two (2) year warranty.

CENTRAL BAPTIST CHURCH OF WOODBRIDGE

37. As shown on the Easement and Fence Exhibit, the Applicant will provide a commercial grade six-foot (6') black aluminum fence along or immediately adjacent to the property line between the Property and Central Baptist Church of Woodbridge (the "Church") property, identified as GPIN 8192-82-4446 (the "Church Property"). The fence

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shall be commercial grade fencing by Iron World Maverick Ultra Plus Commercial Grade Aluminum Fence or the equivalent. Final fence location and style shall be mutually agreed to between the Applicant and the Church. The fence will be maintained by an Association as further defined in Proffer 43.a. In addition, the Applicant will provide a total of two (2) gates within the fence. One gate will provide the Church access to the Property, if needed, to recover sport equipment that may inadvertently fall on the Property. A second gate shall be located in a mutually agreeable location to provide access for those Quartz residents who wish to attend Sunday services or other Church events. The Church will control and manage access to/from this gate. In the event the Church Property is no longer owned and/or operated by the Church, control and management of the gates shall revert to an Association as further defined in Proffer 43.a. The fence and associated gates will be installed in conjunction with the completion of the adjacent Land Bays on the Property but, in no event earlier than the demolition of the Verizon Building.

- 38. The Applicant shall vacate the existing access easement across the Church Property that provides access to the existing Verizon Building. The vacated easement area will be filled and graded with topsoil and stabilized as green space for the Church in accordance with a site plan prepared by the Applicant, in collaboration with the Church, and subject to County approval. Vacation of the easement shall be in conjunction with the completion of the adjacent Land Bays on the Property but, in no event earlier than the demolition of the Verizon Building.
- 39. The Applicant shall grant a perpetual open space easement to the Church over a portion of the Property as shown on the Easement & Fencing Exhibit, (the "Church Open Space Easement"). The Church shall be responsible for the maintenance of the Church Open Space Easement, and the Church shall be subject to the terms of the deed granting the Church Open Space Easement. Granting of said easement shall be in conjunction with the completion of the adjacent Land Bays on the Property but, in no event earlier than the demolition of the Verizon Building.
- 40. The Applicant shall provide tree preservation within the buffer located on the Property which is adjacent to the Church, and as further

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described in Proffer 49.b. In addition, within said buffer, the Applicant shall plant seventy-five (75) supplemental trees. Prior to planting said trees, the Applicant will meet with the Church and review the proposed locations of said trees. Once the final location is determined, the Applicant will prepare a Supplemental Tree Location Plan and plant said trees. Said trees shall be eight to ten feet (8' to 10') tall at time of planting, and will have a two (2) year warranty.

THE EXISTING VERIZON BUILDING

- 41. The existing Verizon Building on the Property will be demolished during the development of Land Bay D.
- 42. The existing asphalt road accessing the Verizon Building will be abandoned in place, covered with topsoil, and with berming where feasible, and stabilized.

COMMUNITY DESIGN

43. HOAs and POAs.

- a. The Applicant shall establish an association or multiple associations (residential and/or nonresidential) for the Property (collectively and individually the "Associations") subject to such covenants, conditions, and restrictions ("CC&Rs") that will include such matters as architectural controls, signage, building materials, and lighting requirements, and that shall assign the responsibility to oversee the on-going management and maintenance of the common areas of the Property, including, without limitation, landscaping and maintenance of community use space and private streets. All such CC&Rs shall be consistent with these Proffers, applicable County Ordinances and Standards, and the Design Guidelines.
- b. The Associations shall be responsible for the review and approval of all plans for development on the Property to ensure that such plans are in substantial conformance with the Design Guidelines.
- c. Modifications to the Design Guidelines:

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- i. Minor modifications to the Design Guidelines may be made at the time of final site/subdivision plan. More substantial modifications to the Design Guidelines may be approved by the Prince William County Planning Director, or its designee, who shall notify the Applicant what has been determined in regard to the modification's consistency with the Design Guidelines. The Planning Director's written determination shall include specific references to those portions of the Design Guidelines or conditions of the zoning which are the basis for such determination. The Applicant shall not approve any such substantive amendment found to be inconsistent by the Planning Director.
- ii. Changes to allow additional building materials shall be approved by the Planning Director or his designee prior to the issuance of the building permit. Compliance with this proffer shall be evidenced with the submission of building elevations to the Development Services Land Development Division two weeks prior to the request for a building permit release letter.
- iii. In the event there is a conflict between the Design Guidelines and the Proffer Statement, the Proffer Statement shall control.
- 44. <u>Graffiti Removal –</u> The Applicant shall remove any graffiti from nonresidential development on the Property. Graffiti shall be deemed any inscription or marking on walls, buildings or structures not permitted by the sign regulations in § 32-250.21 <u>et seq.</u> of the Zoning Ordinance. Any graffiti is to be reported to the Prince William County Police Department before removal.
- 45. <u>Height</u> Notwithstanding the building height identified in the Design Guidelines or MZP, pursuant to Section 32-300.05.04 of the Zoning Ordinance, the maximum building height for single-family attached and multifamily units shall be fifty-five feet (55'), (or as otherwise approved by the Planning Director), in order to accommodate rooftop terraces.

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SIGNAGE

- 46. A comprehensive sign package shall be developed for the Property. If signage requires a special use permit for a sign modification the Applicant shall submit for approval a comprehensive signage special use permit.
- 47. Temporary signage shall be allowed only as set forth in the Zoning Ordinance.
- 48. Monument Signs
 - a. Entry monument signs shall be permitted on the Property in locations depicted on the Street Network & Signage Plan. In addition to the monument signs shown on the Street Network & Signage Plan, additional monument signs shall be permitted with any subsequent comprehensive sign packages or special use permits on the Property.
 - b. The Street Network & Signage Plan identifies the general locations of the monument signs at key entrances to the community, and at entrances to Land Bays. As the Land Bay layout is finalized the locations of the monument signs may be modified accordingly.

BUFFERS, LANDSCAPED AREAS, TREE CANOPY, TREE PRESERVATION, AND OPEN SPACE

- 49. Notwithstanding any other requirement for tree canopy coverage, the Applicant will:
 - a. Create a one hundred-foot (100') setback line, of which seventy feet (70') will be tree preservation area as a buffer, along the common property line with the Cedar Run HOA, as generally shown on the Land Use & Landscape Buffer Plan, and thirty feet (30') will be open space. Stormwater management facilities may be installed within the additional thirty feet (30') setback area when this 30' area is on the Quartz side of the 70' preservation

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area. Removal of invasive vines shall be allowed in any tree preservation area.

- b. Create a seventy-foot (70') setback line consisting of tree preservation area along the common property line with the Central Baptist Church, as generally shown on the Land Use & Landscape Buffer Plan.
- c. Create additional interior tree preservation areas in locations as generally depicted on the Land Use & Landscape Buffer Plan. Minor adjustments to the perimeter of the tree preservation areas are permissible based on final engineering as depicted on the final site plan. Removal of diseased, noxious, and/or invasive vegetation, and invasive vines shall be allowed in the interior tree preservation areas.
- d. The Applicant may perform minimal clearing for, and place utilities in the foregoing areas, as required for development. Additionally, within the buffers against Cedar Run Subdivision and the Church Property, the Applicant may provide a natural surface trail and minimal utility crossings as required.
- e. In the event tree planting standards per Section 802.47.A and Table 8-6 of the DCSM cannot be met within the Urban District and East Village areas, the Applicant shall, instead, plant within the five-foot (5') front yard setback sufficient shrubs, bushes or groundcover, that provide permanent environmental and aesthetic benefit to the development.
- f. Excess open space and tree canopy coverage in the PMR District may be allocated to the PMD zoned area.
- 50. With each site plan that contains a tree preservation area, the Applicant shall commit to providing physical protections in accordance with the DCSM.
- 51. <u>Invasive Species Management Program Document</u> Tree preservation areas containing plant species that are known to be invasive in quantities that threaten the long-term health and survival of the existing

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vegetation present will be the subject of an invasive plant species management program. At the time of the first site plan submission the Applicant shall provide an invasive species management program (the "Management Program") that will specify the common and scientific name of invasive species proposed for management, the target area for management efforts, methods of control and disposal of invasive plants, timing of treatments and monitoring. The Management Program shall begin not later than the first Spring after construction commences for an area that contains the tree preservation area. The tree preservation areas will be monitored by the Applicant's project arborist to document conformance with the Management Program for a period of 2 years after the initial implementation of the Management Program.

- 52. The Applicant shall provide buffers on the Property as shown on the Land Use & Landscape Buffer Plan and shall be subject to the following:
 - a. Planted buffers shall be planted in accordance with the DCSM planting standards or as otherwise provided herein.
 - b. For those buffers shown on the Land Use & Landscape Buffer Plan as "70' Perimeter Tree Preservation Area," the Applicant shall retain existing trees and vegetation to the extent feasible at the time of final engineering, and existing vegetation/trees shall satisfy planting requirements and shall be in lieu of the typical required buffer between uses, subject to the Management Plan.
 - c. Within the fifty-foot (50') buffer located in Land Bay A adjacent to Prince William Parkway as shown on the MZP, plantings within the buffer will be in accordance with the DCSM except evergreen trees shall be eight feet (8') to ten feet (10') in height at planting, and deciduous trees will be a minimum of 3" caliper at planting.
 - d. Within the fifty-foot (50') buffer located in Land Bay F, adjacent to Prince William Parkway, as shown on the MZP, plantings within the buffer will be in accordance with the DCSM. To the extent practical, the Applicant shall use retaining walls to preserve existing trees within said buffer, and will supplement to achieve 50' buffer plant unit density where trees cannot be preserved.

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- 53. Buffers shall be provided at the time adjacent portions of the Property are developed and shall be shown on each respective final site or subdivision plan. The Applicant shall plant street trees in accordance with the Design Guidelines, and may be measured in the aggregate (i.e., on average), or otherwise clustered where needed so as to provide adequate sight distance and to avoid conflicts with utilities, driveways, etc. Street trees shall be provided at the time the adjacent portions of the Property are developed and shall be shown on each respective final site or subdivision plan.
- 54. In the event the GPIN 8192-75-6416 is developed or zoned for residential use, the thirty foot (30') planted buffer shown on the Land Use & Landscape Buffer Plan abutting that parcel shall not be required.
- 55. To facilitate adequate expansion of tree and shrub roots to support healthy plants, all landscape areas, parking lot islands and buffers, which have previously been paved shall have, prior to planting: a) all foreign materials (asphalt, concrete, rock, gravel, debris, etc.) removed and the soil loosened to a depth of a minimum of 3", and b) a top dressing of 4" to 6" of clean topsoil provided. This topsoil shall be a loam, sandy loam, clay loam, silt loam, or sandy clay loam. The topsoil shall not be a mixture of, or contain contrasting textured, subsoils. The topsoil shall contain less than 5% by volume of cinders, stones, slag, coarse fragments, sticks, roots, trash, or other materials larger than 1" in diameter and shall not contain gravel. The topsoil shall contain a minimum of 5% natural fine organic matter, such as leaf mold, peat moss, etc. This proffer shall not, however, apply to the Verizon access road, which shall remain in place, and it will be stabilized and covered with a berm.

CULTURAL RESOURCES

56. With the submission of the first site plan approval in Land Bay A, the Applicant shall offer to the County in writing the option to salvage any or all of the structural material from the barn as the County determines appropriate. In the event the County does not complete its salvage operation within ninety (90) days following such written notice the Applicant may remove the barn as part of the construction process. Alternatively, the County may request access to the Property for the

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purpose of such salvage at any time after the Property is rezoned, provided that no such access will be permitted after the aforesaid ninety (90) days.

ENVIRONMENTAL

- 57. The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$75.00 per acre for water quality monitoring. This contribution shall be paid prior to and as a condition of the issuance of the first occupancy permit issued in connection with each site or subdivision plan for residential uses and the first building permit for nonresidential uses, and shall be based on the acreage reflected on each such approved plan.
- 58. The Applicant shall install at least two Low Impact Development ("LID") Best Management Practice measure(s) on the PMD portion of the Property. Such LID measure(s) may include, but shall not be limited to, bioretention facilities/rain gardens, water quality swales, sheet flow to vegetative filter strips, infiltration trenches, or any alternative LID practice proposed by the Applicant and deemed to be acceptable to the County. Final location and design of the LID measure(s) shall be determined at final site plan.

PUBLIC SAFETY

- 59. Residential Monetary Contribution: The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$235.35 per single-family attached unit, and \$172.83 for each multifamily unit constructed on the Property, to be used for police, fire and rescue services in the area. Said contribution shall be paid prior to and as a condition of the issuance of an occupancy permit for each said residential unit constructed on the Property.
- 60. <u>Non-Residential Monetary Contribution</u>: The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$0.61 per square foot of gross floor area of nonresidential space, excluding parking structures, constructed on the Property to be used for fire and rescue service and facilities in the area. Said contribution shall be paid prior to and as a condition of the issuance

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of the first building permit for each approved plan for nonresidential uses.

AFFORDABLE/WORKFORCE HOUSING

- 61. The Applicant shall provide a total of ninety (90) units as Affordable Dwelling Units ("ADUs"), and/or Workforce Dwelling Units ("WDUs") on the Property, subject to the following:
 - a. Provided such actions would not violate applicable laws or regulations, ninety (90) of the residential units (townhouse or multifamily units) shall be offered exclusively for sale or rental to those persons who qualify and as follows:
 - b. Thirty (30) ADUs shall be available for rent to households earning up to 60% Area Median Income ("AMI") as determined for the Prince William area by the United States Department of Housing and Urban Development.
 - c. Thirty (30) WDUs shall be available for sale or rent at the Applicant's option to households earning up to 80% AMI.
 - d. Thirty (30) WDUs shall be available for sale or rent at the Applicant's option to households earning up to 100% AMI.
 - e. The Applicant shall provide the foregoing subject to the following residential phasing schedule:
 - i. Fifteen (15) units of the first three hundred (300) residential occupancy permits will be either ADUs or WDUs. The ADU, WDU, or mix thereof, shall be determined at the Applicant's sole discretion and shall be determined at site plan.
 - ii. Thirty (30) units of the second three hundred (300) residential occupancy permits will be either ADUs or WDUs. The ADU, WDU, or mix thereof, shall be determined at the Applicant's sole discretion and shall be determined at site plan.

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- iii. Forty-five (45) units of the third three hundred (300) residential occupancy permits will be either ADUs or WDUs. The ADU, WDU, or mix thereof, shall be determined at the Applicant's sole discretion and shall be determined at site plan.
- f. The housing cost to be paid by the foregoing income tiers shall be such that the monthly rent or the monthly mortgage payment does not exceed 30% of gross annual family income.
- g. For sale units will be sold subject to a restrictive covenant in the deed of conveyance that restricts the owner(s) thereof from selling the unit at fair market value for a period of ten (10) years following the date of Closing.
- h. The Applicant will manage the process of identifying qualified tenant and purchaser applicants and will report annually to the Director of Planning as to the number of such tenants and purchasers and the rental rates and purchase prices paid for each such unit.
- i. Notwithstanding the foregoing, the Applicant may enter into a separate written agreement with the appropriate Prince William County agency as to terms and conditions of the administration of the ADUS and WDUs either by such agency or in coordination with the Applicant. Such an agreement shall be on terms mutually acceptable to both the Applicant and the County and may occur after the approval of this rezoning and approved by the County Attorney. If such an agreement is executed by all relevant parties, then the ADUs and WDUs shall thereafter be administered solely in accordance with such agreement and the provisions of these Proffers as they apply to ADUs and WDUs shall be of no further force and effect. Such an agreement and any modifications thereto shall be recorded in the land records of Prince William County.

In lieu of Proffer 61.c above, the Applicant reserves the right to provide twenty (20) of the thirty (30) residential units to be made available for purchase by purchasers qualified and approved

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through the Prince William County Office of Housing and Community Development's First Time Homebuyers Program ("FTHB"), and shall be fully administered by the County's Office of Housing and Community Development. Said units shall not be subject to the deed restriction identified in Proffer 61.g. above. These units will be identified by the Applicant in consultation with the County's Office of Housing and Community Development and will be sold at 75% of the base price of the current market price at the time of sale. These units shall be listed for 6 months after the issuance of a building permit. If these units are not identified/purchased within said 6 months, the Applicant may revert these units to market rate units. These identified units will be subject to the eligibility and lien guidelines and requirements of the FTHB Program.

WATER AND SEWER

62. The Property shall be served by public sanitary sewer and water and the Applicant shall be responsible for the costs and construction of those on and off-site improvements required in order to provide such service for the demand generated by the development on the Property.

COMPREHENSIVE PLAN CONFORMITY

63. Acceptance and approval of this rezoning application by the Board of County Supervisors authorizes the location and provision of those public uses and facilities specifically referenced on the MZP in this Proffer Statement, and the extension and construction of water and sewer lines and facilities, Public Shared Use Paths, and roads necessary to serve this property pursuant to the Virginia Code Ann. § 15.2-2232 (D) and Prince William County Code § 32-201.12(a)(2) as being in substantial accord with Prince William County's adopted Comprehensive Plan. The general location of these uses and facilities are as shown on the MZP and referenced in the Proffers, with the exact locations to be determined based on final engineering and as approved by Prince William County at the time of final site plan. Acceptance of this Proffer constitutes approval of the public uses and facilities and their general locations and thereby

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accepts said uses and facilities from further Comprehensive Plan conformity review.

MISCELLANEOUS

- 64. For Phases 1A and 1B, in the event the monetary contributions set forth in the Proffer Statement are paid to the Prince William County Board of County Supervisors ("Board") within eighteen (18) months of the approval of this rezoning, as applied for by the Applicant, said contributions shall be in the amounts as stated herein. Any monetary contributions set forth in this Proffer Statement for Phases 1A and 1B which are paid to the Board after eighteen (18) months following the approval of this rezoning shall be adjusted in accordance with the Urban Consumer Price Index ("CPI-U") published by the United States Department of Labor, such that at the time contributions are paid they shall be adjusted by the percentage change in the CPI-U from that date eighteen (18) months after the approval of this rezoning to the most recently available CPI-U to the date the contributions are paid, subject to a cap of 6 percent (6%) per year, noncompounded.
- 65. For Phases 2 and 3, in the event the monetary contributions set forth in the Proffer Statement are paid to the Prince William County Board of County Supervisors ("Board") prior to substantial completion of the SPUI, as applied for by the Applicant, said contributions shall be in the amounts as stated herein. Any monetary contributions set forth in this Proffer Statement for Phases 2 and 3 which are paid to the Board after substantial completion of the SPUI shall be adjusted in accordance with the Urban Consumer Price Index ("CPI-U") published by the United States Department of Labor, such that at the time contributions are paid they shall be adjusted by the percentage change in the CPI-U from that date on which substantial completion of the SPUI has occurred to the most recently available CPI-U to the date the contributions are paid, subject to a cap of 6 percent (6%) per year, noncompounded.
- 66. The Applicant reserves the right to retain (or obtain at no cost in the future) necessary temporary and permanent grading, slope, construction, utility, signage, drainage, stormwater management and access easements on all public use parcels which are dedicated to Prince

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William County, provided said easements do not preclude development of the property for the intended public purpose.

67. The Applicant submits these Proffers pursuant to the provisions of Va. Code Ann. § 15.2-2303.4(D)(1).

WAIVERS/MODIFICATIONS

- 68. The following waivers and modifications to the requirements of the Zoning Ordinance and DCSM shall be deemed granted and approved as provided by § 32-700.25 of the Zoning Ordinance.
 - a. Modification of Sections 32-201.18, 32-250.31.6 of the Zoning Ordinance and Section 802.47.B of the DCSM to allow a ten foot (10') foot wide landscaped area around public use and community recreation sites
 - b. Waiver of Sections 32-210.11 and 32-210.12 of the Zoning Ordinance to allow mobile or modular offices for construction or sales offices to be located on a parcel for which there may not be an approved site plan or building permit.
 - c. Modification Section 32-280.12.3 of the Zoning Ordinance requiring a thirty-foot (30')- setback requirement from all street rights-of-way for all principal structures and allow for a minimum ten foot (10') setback from all street right-of ways in the non-residential areas or as shown in the MZP for the residential areas.
 - d. Waiver of Sections 32-250.30, 32-250.31 and 32.250.32 of the Zoning Ordinance; and Sections 802.11 and 802.12 of the DCSM requiring internal buffers between different uses.
 - e. Modification of Section 32-250.40 of the Zoning Ordinance and Sections 802.20, 21, 30 and 31 of the DCSM for tree canopy and open space requirements in the PMD District to allow calculations based upon the entire PMD area instead of each individual site PMD land bays.
 - f. Modification of Sections 32-280.41.1, and 32-405.03.2 of the Zoning Ordinance to permit the following uses on the Property by-

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right: drive-in restaurant, motor vehicle fuel station, and veterinary hospital.

- g. Modification of Sections 1003.01 and 1003.02 of the DCSM to allow a 20' buffer against Prince William Parkway and Minnieville Road along the Land Bay A frontage, as shown on the MZP.
- h. Modification of Section 32-306.12.3 of the Zoning Ordinance to reduce the side setback, requirement for residential units as shown in the Design Guidelines.
- i. Modification of Sections 32-306.12.6. F, G, H, and I to allow residential units to be developed in accordance with standards set forth in the Design Guidelines, and to increase the building height for single-family attached and multifamily units in accordance with Proffer 45 above.
- j. Waiver of Section 32-405.04.3 of the Zoning Ordinance to allow non-residential areas to be based on the entire PMD area and the max lot coverage shall not exceed 80%, with a minimum of 20% open space.
- k. Modification of Section 32-405.04.4 of the Zoning Ordinance and Section 802.47.B of the DCSM to allow the perimeter buffer as shown on the MZP.
- I. Waiver of Sections 32-401.14.5, 32-401.24.7, 32-402.14.5,32-402.34.5 and 32-402.44.5 of the Zoning Ordinance to allow a 75' maximum building height in the B-1, B-2, O(M), O(L) and O(F) underlying zoning districts.
- m. Modification of Section 32-250.28 of the Zoning Ordinance to allow the location and number of Project Identification signs and Multitenant sings as shown on the Street Network & Signage Plan.
- n. Modification of Section 601.04 and Details UB-1, and UAS-1 of the DCSM to allow standard street section and details as shown on the MZP which allow for the eight foot (8') Private Shared Use Path to be located outside of the ROW.

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- o. Modification of Sections 602.04, 602.07, 610.01, 602.14, 602.22, 650.00 and 1003.04 of the DCSM requirements for roadway sections with respect to right-of-way width, design speed, intersection alignments, tangent distance, lane width, median width, bike lane width, maximum grade, centerline radius, off-street parking requirements, intersection curb returns, intersection design, entrance/crossover spacing, turn lanes, channelization, location, and materials.
- p. Waiver of Section 602.18 of the DCSM to the general sidewalk/trail requirements; specifically, the requirement to provide sidewalks or trails within the street right-of-way of all proposed streets within urban section streets.
- q. Waiver of Section 32-250.31 of the Zoning Ordinance and Sections 702.06(F), 702.06(I), and 802.42(A)(3) of the DSCM to modify the standard to allow for minor permanent structures like mailboxes or signage, unauthorized obstructions, and encroachments within easements as well as to allow for easements to be located within buffer areas.
- r. Waiver of Sections 802.42 and 802.43 of the DCSM of the 10 foot (10') wide landscape strips along right-of-way and perimeter parking lots.
- s. Waiver of Section 802.47.A and Table 8-6 of the DCSM of the DCSM to allow, within the Urban District and East Village areas, in lieu of the on-lot tree requirement the Applicant shall plant, and landscape in accordance with Proffer #49.e above.

[Signature Pages to Follow]

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SIGNATURE PAGE

SKW VA, 1	Inc			
BY:				
NAME:				_
TITLE:				

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SIGNATURE PAGE

SOUTHERN KNOLLS, LLC				
BY:				
NAME:				
TITLE:				

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STAFF REPORT

PC Meeting Date: September 21, 2022

Agenda Title: Rezoning #REZ2019-00018, Quartz District

District Impact: Neabsco Magisterial District

Requested Action: Recommend Approval of Rezoning #REZ2019-00018, Quartz District,

subject to proffers dated August 19, 2022

Department: Planning Office **Case Planner:** Stephen Gardner

EXECUTIVE SUMMARY

This is a proposal to rezone ±145.05 acres from A-1, Agricultural, O(H), Office High-Rise, and R-2, Suburban Residential, to PMR, Planned Mixed Residential, and PMD, Planned Mixed Use District, to allow up to 1,015 residential units and 374,000 square feet of non-residential uses, with associated waivers/modifications, including signage and height modifications. The site is generally located at the northwest quadrant of the intersection of Prince William Parkway and Minnieville Road.

It is the recommendation of staff that the Planning Commission recommend approval of Rezoning #REZ2019-00018, Quartz District, subject to the proffers dated August 19, 2022.

BACKGROUND

A. Request: This is a request to rezone ±145.05 acres from A-1, Agricultural, O(H), Office High-Rise, and R-2, Suburban Residential, to PMR, Planned Mixed Residential, and PMD, Planned Mixed Use District, to allow up to 1,015 residential units and 374,000 square feet of non-residential uses, with associated waivers/modifications, including signage and height modifications. More specifically, this application will rezone ±91.633 acres to the PMR District and ±53.420 acres to the PMD District.

This application proposes development within two distinct districts, a Neighborhood District and an Urban District. The Neighborhood District correlates to the land area proposed as PMR, also identified as Land Bays C, D, E, F, and G, and is comprised predominantly of single-family attached townhouse units, open space, and a public use site. The Urban District correlates to the land area proposed as PMD and includes a mix of commercial retail, office, multi-family residential, and open space. A public use site (Land Bay E) is proposed for dedication to Prince William County in order to accommodate a possible school site and a public adventure park.

Uses/Features	Existing	Proposed
Zoning	A-1, Agricultural	PMR, Planned Mixed
	O(H), Office High-Rise	Residential (91.633 acres)
	R-2, Suburban Residential	PMD, Planned Mixed Use
		District (53.420 acres)
Use(s)	8192-74-5435, Office	PMR: Townhouse,
	8192-83-4108, Undeveloped	Multifamily
	8192-93-1868, Undeveloped	PMD: Mixed-Use
Uses/Features	Required in PMR/PMD	Proposed with
	zoning districts	Development
REZ area	Minimum 10 acres for PMR	PMR, 91.633 acres
	Minimum 25 acres for PMD	PMD, 53.420 acres
Residential Unit	PMR: 3 unit types/styles	2 unit types proposed in both
Type / #	PMD: 2 unit types/styles	PMR/PMD: Townhouse and
		Multi-family
Uses/Features	Required in The Dale City	Proposed with
	Small Area Plan	Development
Target Density	HDN: T-5; 12 – 30 d.u./acre;	HDN: 17 d.u./acre
	1.2 – 3.0 FAR	
	CMU: T-4; = 8 – 24 d.u./ac.	CMU: 7 d.u./acre

	0.57 – 1.38 FAR	0.23 FAR
	SN: T-3; 1-10 d.u./acre 0.23 – 0.57 FAR	SN: 9 d.u./acre
Target Land Use	HDN: Residential 90 – 100%	HDN/CMU
Mix	Non-Residential 0 – 10%	PMD: 410 d.u.
	Civic: 0%	374,000 square feet non- residential
	CMU: Residential 30 – 60% Non-Residential 30 – 60% Civic 10%	
	SN: Residential 85 – 100% Non-Residential 0 – 10% Civic: 5%	SN PMR: 100% Residential
Target Building	T-5: 6 – 12 stories	Residential: 55 feet
Height	T-4: 4 - 8 stories	per Proffer 45
	T-3: 3 – 5 stories	Non-Residential: Modified to
		75 feet
Open Space	HDN: 20%	PMD: 20%
	CMU: 20%	PMR: 30%
	SN: 30%	MZP Sheet 3, Note 3

- B. <u>Site Location</u>: The property is located at the northwest quadrant of the intersection of Prince William Parkway and Minnieville Road, and is identified on County maps as GPINs 8192-74-5435, 8192-83-4108, and 8192-93-1868.
- C. <u>Comprehensive Plan</u>: The site is designated CMU, Community Mixed-Use, HDN, High-Density Neighborhood, SN, Suburban Neighborhood, PL, Public Land, POSA, Parks and Open Space Active, and POSP, Parks and Open Space Passive, in the Comprehensive Plan. The site is located within the Dale City Small Area Plan and is partially located within the Prince William Parkway and Minnieville Road Highway Corridor Overlay Districts.
- D. <u>Zoning</u>: The site is currently zoned A-1, Agricultural, O(H), Office High Rise, and R-2, Suburban Residential.
- E. <u>Surrounding Land Uses</u>: Existing land uses located immediately adjacent to the site to the west consist predominantly of single-family detached dwelling units. Properties to the north of the site include vacant land and an electrical substation. Properties to the east across Prince William Parkway include vacant land. The Central Baptist Church abuts the property to the south. Multi-family residential and commercial office condos are also located to the south across Minnieville Road.

F. <u>Background and Context</u>:

a. Dale City Small Area Plan: On December 10, 2019, the Board of County Supervisors adopted the Dale City Small Area Plan. The primary focus of the Dale City Small Area Plan is to create a sustainable transit oriented and pedestrian friendly community anchored around five (5) nodes consisting of one (1) new community mixed-use center and four (4) revitalized commercial/civic nodes that offer a mix of arts, public space, retail and transit-oriented opportunities while also preserving existing natural resources. The specific planning areas of focus are denoted as follow: Parkway Node, East Gateway, Minnieville Node, Mapledale Node, and West Gateway.

STAFF RECOMMENDATION

Staff recommends approval of Rezoning #REZ2019-00018, Quartz District, subject to the proffers dated August 19, 2022, for the following reasons:

- The proposed PMD, Planned Mixed Use District, directly implements the CMU, Community Mixed-Use, and HDN, High-Density Neighborhood, land use designations, and the proposed PMR, Planned Mixed Residential, zoning district directly implements the SN, Suburban Neighborhood, land use designation.
- The proposal includes a development pattern and mix of uses that is consistent with the intent of the Dale City Small Area Plan.
- The application includes the development of a 20.26 acre public use site intended to facilitate a public park and future elementary school, consistent with the POSA land use designation.

Comprehensive Plan Consistency Analysis

<u>Long-Range Land Use</u>: This site is located within the Development Area of the County and is subject to the Parkway Node of the Dale City Small Area Plan which designates the property as follows: CMU, Community Mixed-Use, HDN, High-Density Neighborhood, SN, Suburban Neighborhood, PL, Public Land, POSA, Parks and Open Space Active, and POSP, Parks and Open Space Passive.

The proposed PMD, Planned Mixed Use District, directly implements both the CMU, Community Mixed-Use, and HDN, High-Density Neighborhood, land use designations, and the proposed PMR, Planned Mixed Residential, directly implements the SN, Suburban Neighborhood, land use designation. The proposed Urban District includes an urban street grid with a mix of both residential and non-residential uses that is consistent with the intent of the CMU and HDN designations as further described within the Dale City Small Area Plan. Although the densities within the CMU portion of the property are below the thresholds anticipated by the Small Area Plan, the application includes a framework through which the greater vision of the Plan can be achieved over time. Further, key components of the Dale City Small Area Plan, including a main street design, the

interchange at Prince William Parkway/Minnieville Road, a public use site to accommodate an Adventure Park, and transit facilities, are incorporated into the design. Finally, the proposed uses and density within the proposed PMR portion of the property are consistent with that anticipated by the Small Area Plan.

Level of Service (LOS): This rezoning proposal is subject to the proffer legislation, Virginia State Code Section 15.2-2303.4(D)(1). Although the application was submitted on or after July 1, 2016, but before July 1, 2019, the Applicant has elected to proceed under the current version of 15.2-2303.4. Pursuant to Virginia State Code Section 15.2-2303.4.(D)(1), an applicant or owner may, at the time of filing an application pursuant to this section or during the development review process, submit any onsite or offsite proffer that the owner and applicant deem reasonable and appropriate, as conclusively evidenced by the signed proffers. A Proffer Justification Narrative, which was prepared by Municap, Inc. and dated May 13, 2021, has been provided.

The LOS impacts related to this subject rezoning request would be mitigated by the monetary proffers provided by the Applicant and in accordance with policy guidelines, as per the Proffer Statement, dated August 19, 2021, as follows:

Transportation	\$4,500 per residential unit for area	\$4,567,500
_	transportation improvements as	
	stipulated by Proffer 12.a. & 13	
	(1,015 total units)	
	10,000 per covered transit stop for	\$20,000
	signage/electronic displays (2 total)	
Environmental	\$75.00 per acre	\$10,878.75
	145.05 total acres)	
Parks & Recreation	In-Kind donation of land	NA
	(see details below)	
Public Safety	\$235.35 per single-family attached	SFA: \$143,563.5
	unit (610 total)	MF: \$69,996.15
	\$172.83 per multi-family unit (405	Non-Res: \$228,140
	total units)	Total: \$441,699.65
	\$0.61 per square foot of non-	
	residential (374,000 s.f. total)	
Schools	\$398.07 per single-family attached	SFA: \$242,822.7
	(610 total units)	MF: \$84,851.55
	\$209.51 per multi-family unit (405	Total: \$327,674.25
	total units)	
	In-Kind donation of land	NA
	(see details below)	
TOTAL LOS \$		\$5,367,752.65
CONTRIBUTION		

In addition to the above, the Applicant will dedicate six acres to the Prince William County Board of Supervisors (BOCS) to be used for parks and recreation purposes (Proffer 22), and the Applicant will dedicate twelve acres to the BOCS to be used for a future elementary school (Proffer 28). Collectively, these two public use sites constitute the totality of Land Bay E on the MZP. Proffer 30 provides the option for the BOCS to allocate the acreage between the school and park uses at their discretion.

The Applicant has also committed as part of Proffer 9 to provide 30 percent design plans for the Single Point Urban Interchange (SPUI), survey work and geotechnical work for the 30% design plans, and utility relocation design plans. The Applicant has also committed to the design/construction of a stormwater management facility off-site on Applicant controlled property on the east side of Prince William Parkway, identified as GPIN 8292-04-3728, to accommodate drainage from the SPUI and the proposed development.

Community Input

Notice of the rezoning application has been transmitted to property owners within 1,320 feet of the site. As of the date of this staff report, the Planning Office has received one letter from J. Chapman Petersen, representing Atlantic Funding, Ltd., owners of adjacent property located at the intersection of Elm Farm Road and Prince William Parkway. This letter has been previously transmitted to the Planning Commission and notes concerns about the access to the Quartz District as well as the proposal for additional signalized intersections along Prince William Parkway.

Other Jurisdiction Comments

The subject site is located outside of the required notification area for adjacent jurisdictions.

Legal Issues

If the rezoning is approved, the ±145.05 acre project site could be developed pursuant to the PMR, Planned Mixed Residential, and PMD, Planned Mixed Use, districts as qualified and as stipulated by the MZP and proffers. Should this rezoning not be approved, the property could be developed pursuant to the existing A-1, Agricultural, O(H), Office High-Rise, and R-2, Suburban Residential, zoning districts, as applicable. Legal issues resulting from the Planning Commission's action are appropriately addressed by the County Attorney's Office.

<u>Timing</u>

The Planning Commission has until December 21, 2022, which is 90 days from the first public hearing date, to take action on the rezoning proposal. A recommendation to approve or deny the request would meet the 90-day requirement.

STAFF CONTACT INFORMATION

Stephen Gardner | (703) 792-7615 sgardner@pwcgov.org

ATTACHMENTS

Attachment A – Area Maps

Attachment B – Staff Analysis

Attachment C – Historical Commission Resolution

Attachment D - Proffer Justification Narrative (by MuniCap, Inc.)

Attachment E – Master Zoning Plan (MZP)

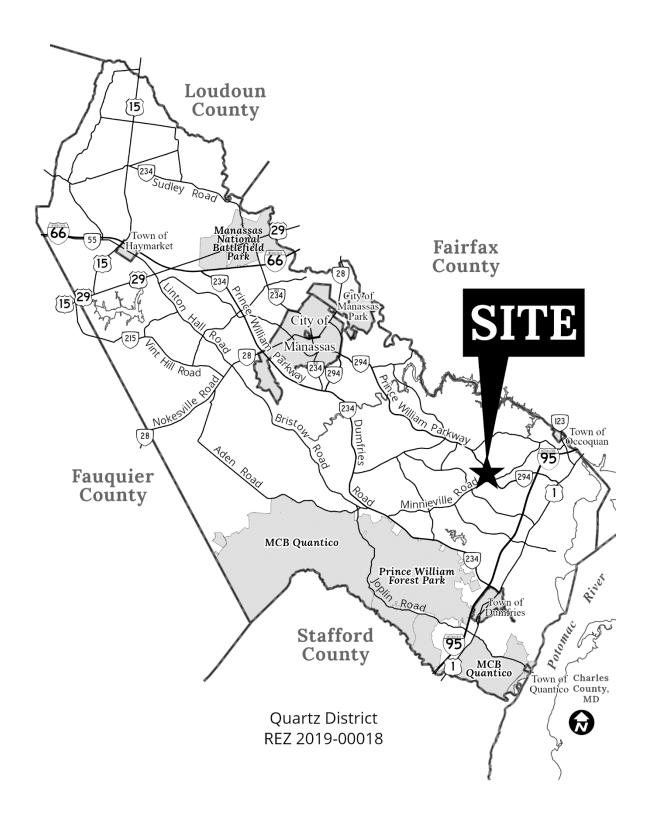
Attachment F – Environmental Constraints Analysis (ECA)

Attachment G – Design Guidelines

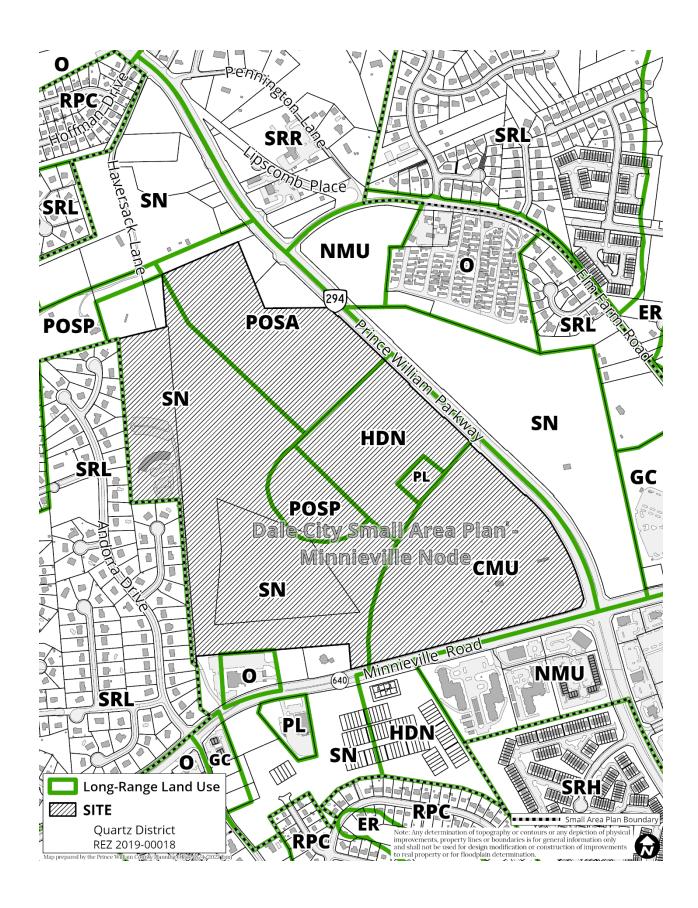
Attachment H - Illustrative Drive Through & Motor Vehicle Fuel Station Renderings

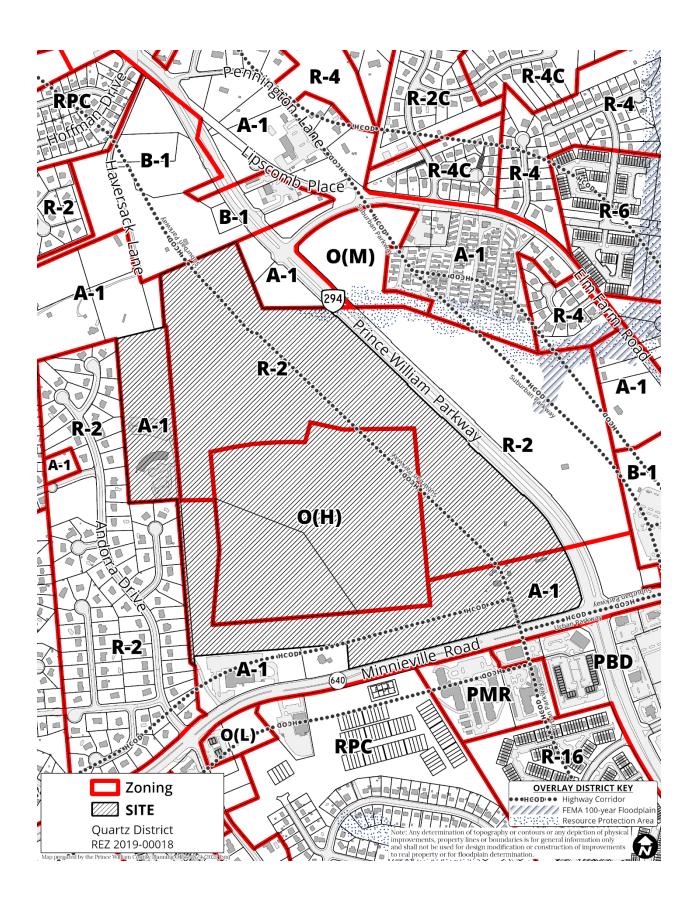
Attachment I - Potential East MOT Road & Off-Site SWM Facility Grading Exhibit

Attachment J - Sight Distance Profiles









Part I. Summary of Comprehensive Plan Consistency

Staff Recommendation: APPROVAL

This summary analysis is based on the relevant Comprehensive Plan action strategies, goals, and policies. A complete analysis is provided in Part II of this report.

Comprehensive Plan Sections	Plan Consistency		
Long-Range Land Use	Yes		
Community Design	Yes		
Cultural Resources	Yes		
Environment	No		
Fire and Rescue	Yes		
Housing	Yes		
Parks, Open Space and Trails	Yes		
Police	Yes		
Potable Water	Yes		
Sanitary Sewer	Yes		
Schools	Yes		
Transportation	Yes		

Part II. Comprehensive Plan Consistency Analysis

The following table summarizes the area characteristics (see attached maps):

Direction	Land Use	Long-Range Future Land Use Map Designation	Zoning
North	Vacant Land; Utility Substation	SN	A-1; B-1
South	Multi-family; office; religious institution	NMU, HDN, SN, PL	RPC; PMR; PBD

East	Vacant Land	SN, NMU, SRR	A-1; R-2; O(M)
West	Single-Family Detached	SRL; SN	A-1; R-2

Long-Range Land Use Plan Analysis

Through wise land use planning, the County ensures that landowners are provided a reasonable use of their land while the County is able to judiciously use its resources to provide the services for residents and employers' needs. The Long-Range Land Use Plan sets out policies and action strategies that further the County's goal of concentrating on population, jobs, and infrastructure within vibrant, walkable, mixed-use centers serviced by transit. In addition to delineating land uses on the Long Range Land Use Map, the Plan includes smart growth principles that promote a countywide pattern of land use that encourages fiscally sound development and achieves a high-quality living environment; promotes distinct centers of commerce and centers of community; complements and respects our cultural and natural resources, and preserves historic landscapes and site-specific cultural resources; provides adequate recreational, park, open space and trail amenities that contribute to a high quality of life for county residents; and revitalizes, protects, and preserves existing neighborhoods.

This site is located within the Development Area of the County and is subject to the Parkway Node of the Dale City Small Area Plan which designates the property as follows: CMU, Community Mixed-Use, HDN, High-Density Neighborhood, SN, Suburban Neighborhood, PL, Public Land, POSA, Parks and Open Space Active, and POSP, Parks and Open Space Passive. The following table summarizes the uses and densities intended within the TC, OMU, and POSP designations as they relate to this project:

Long-Range Land Use Map Designation	Intended Uses and Densities
Community Mixed-Use (CMU)	Mixed-Use live work centers include both residential and commercial uses arranged in a pedestrian-friendly form. These centers are locations for community commercial, entertainment destinations, and public facilities directly accessible to surrounding neighborhoods. Streets are interconnected and serve cars, cyclists, and pedestrians. Mixed-Use Centers should be connected by bus transit to nearby destinations and to nearest rail transit. Development is arranged in short blocks with shallow setbacks and both on-street parking or parking lots are appropriate.

High-Density Neighborhood (HDN)	High-Density Neighborhoods provide opportunities to develop compact neighborhoods that can act as transitions between mixed use centers and lower density residential areas. These are high-density, walkable neighborhoods focusing on connections to mixed-use centers and transit. These areas can accommodate attached and multifamily housing types and may include small scale retail or office uses integrated into the neighborhood. Neighborhood design focuses on interconnected streets with short blocks and shallow setbacks. Parking is accommodated on-street or behind buildings. Development's visibility from Dale Boulevard is limited by substantial buffers, berms, and landscaping.
Suburban Neighborhood (SN)	Suburban Neighborhoods primarily accommodate single-family homes arranged in small to medium lots. Connections and pedestrian amenities should still be a priority for development design. These areas should also include parks, trails and open space integrated into the development in appropriate locations. Small office or service uses may be appropriate. Neighborhood design includes longer block with homes setback from the street and parking accommodated in private driveways and garages.
Public Land (PL)	The purpose of identifying public lands in the Comprehensive Plan is to provide an indication of existing and planned public facilities, institutions, or other government installations such as but not limited to government centers, judicial centers, and related facilities.
Parks and Open Space Active (POSA)	The purpose of this classification is to designate existing and projected parks and recreational areas of the County. Active uses involving development of parkland to provide facilities including the construction of buildings, fields, courses and other related infrastructure to support active recreational activities.
Parks and Open Space Passive (POSP)	The purpose of this classification is to designate existing and projected parks and recreational areas of the County. Passive uses generally require or result in little or no alteration of the landscape and produce little or no light, noise or visual intrusion on their surroundings.

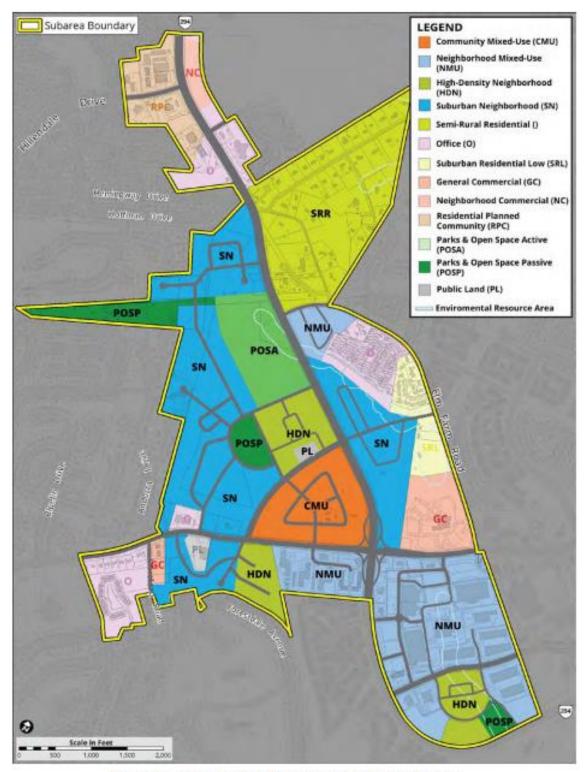
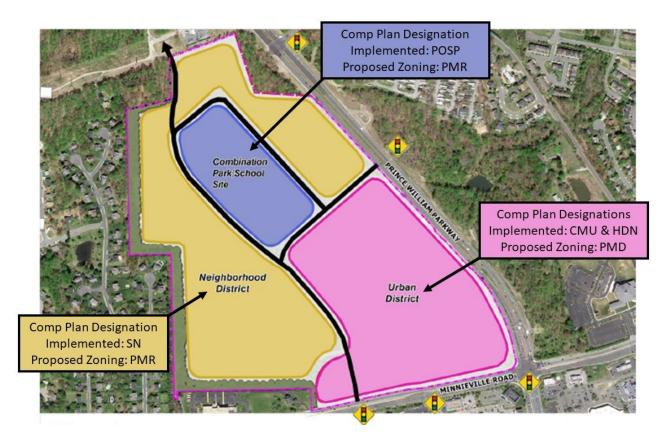


Figure 58: Proposed Long-Range Land Use Classifications (Parkway Node)

In addition, the Dale City Small Area Plan correlates transect density with the land use designation as follows:

- Community Mixed-Use areas are designated a T-4 transect which anticipates residential density at 8 to 24 dwelling units per acre and non-residential intensity at 0.57 to 1.38 FAR.
- High Density Neighborhood areas are designated a T-5 transect which anticipates residential density at 12 to 30 dwelling units per acre and non-residential intensity at 1.2 to 3.0 FAR.
- Suburban Neighborhood areas are designated a T-3 transect which anticipates residential density at 1 to 10 dwelling units per acre and non-residential intensity at .23 to 0.57 FAR.
- Public Open Space Active and Public Open Space Passive areas are designated a T-1 transect which anticipates non-residential intensity up to 0.02 FAR.

Conformance with the Comprehensive Plan – Residential Density and Non-Residential Intensity



The Quartz District rezoning application proposes three distinct districts which are intended to implement the various Comprehensive Plan land use designation. These include: an Urban District designated as Land Bays A and B, proposed to be rezoned to PMD, and intended to implement the Community Mixed-Use and High Density Neighborhoods designations; a Neighborhood District designed as Land Bays C, D, F, and G, proposed to be rezoned to PMR, and intended to implement the Suburban Neighborhood designation; and a Combined Park/School site, proposed to be rezoned to PMR, and intended to implement the Public Open Space Active designation. Proposed density, primary land uses, and implementing zoning districts are summarized in the table below.

Proposed District	Comp Plan Target Residential Density	Proposed Residential Density	Comp Plan Target Non- Residential Intensity	Proposed Non- Residential Intensity	Proposed Implementing Zoning Districts	Proposed Primary Uses
Urban District (Designated CMU/HDN)	CMU Area 8 – 24 d.u. per acre	CMU Area 7 du per acre	CMU Area 0.57 – 1.38 FAR	CMU Area 0.23 FAR	B-1, B-2, O(L), O(M), O(H), O(F), HDR, OS,	Commercial, Office, Multi-Family,
	HDN Area 12 – 30 d.u. per acre	HDN Area 17 du per acre	HDN Area 1.2 – 3.0 FAR	HDN Area NA	UDR	Townhomes
Neighborhood District (Designated SN)	1 – 10 d.u. per acre	9 du per acre	.23 to .57 FAR	NA	LDR, MDR, HDR, OS	Multi-Family, Townhomes
Combination Park/School Site (Designated POSA)	NA	NA	Up to 0.02 FAR	Unknown	LDR, OS	Public Park, School

Proposal's Strength

- <u>Land Use & Zoning Compatibility</u>: The proposed PMD, Planned Mixed Use District, directly implements both the CMU, Community Mixed-Use, and HDN, High-Density Neighborhood, land use designations. The proposed PMR, Planned Mixed Residential, directly implements the SN, Suburban Neighborhood, land use designation.
- <u>Suburban Neighborhood (SN) Density:</u> The proposed residential density of 9 dwelling units per acre is consistent with the 1 to 10 dwelling units per acre anticipated within SN designated areas.
- <u>High-Density Neighborhood (HDN) Density:</u> The proposed residential density of 17 dwelling units per acre is consistent with the 12 to 30 dwelling units per acre anticipated within HDN designated areas.
- <u>Public Open Space Active (POSA):</u> The dedication of Land Bay E as stipulated by Proffers 22 and 28 as public use sites for a public park and future elementary school site are consistent with the intent of the POSA land use designation.

Proposal's Weaknesses

• <u>Community Mixed-Use Residential Density and Non-Residential Intensity:</u> The residential density of 7 dwelling units per acre is below the anticipated density of 8 to 24 dwelling units

per acre for areas designated CMU, and the non-residential intensity of 0.23 FAR is below the anticipated intensity of 0.57 to 1.38 FAR for areas designated as a T-4 transect.

O(F) as PMD Implementing District: The Proposed Land Bay and Density Tabulations Table
on Sheet 3 of the MZP notes Office/Flex O(F) as an implementing district zoning for Land Bay
A. Certain uses permitted within the O(F) zoning district are not consistent with those
anticipated within either the CMU or HDN land use designations and do not further the
Small Area Plan's vision of the site as a synergistic, mixed-use development.

On balance, this application is found to be consistent with the relevant components of the Long-Range Land Use Plan.

Community Design Plan Analysis

An attractive, well-designed County will attract quality development, instill civic pride, improve the visual character of the community, and create a strong, positive image of Prince William County. The Community Design Plan sets out policies and action strategies that further the County's goals of providing quality development and a quality living environment for residents, businesses, and visitors, and creating livable and attractive communities. The Plan includes recommendations relating to building design, site layout, circulation, signage, access to transit, landscaping and streetscaping, community open spaces, natural and cultural amenities, stormwater management, and the preservation of environmental features.

Urban Design and Site Layout

The Dale City Small Area Plan, Illustrative Plan, Parkway Node, Page 123, states the following:

The Parkway Node Illustrative Plan (inserted below) demonstrates the following development and redevelopment opportunities:

- A new commercial community center with vertical mixed-use, ground floor retail, and walkable streets. A main street would provide a pedestrian spine through the commercial center with access to a new transit center.
- The greatest intensity of mixed-use development takes place across from the proposed new transit center.
- Mixed-use development along the northwest and southwest portions of the Commercial Center provides a transition zone from the areas of greatest intensity to the lower density, single-family homes just outside the study area.
- Focus building entrances along walkable pedestrian focused streets.
- A new transit center providing access to various points of destination throughout the region.
- A new Parkway and Minnieville interchange to provide continuity of traffic flow that meets VDOT Requirements.
- Creation of a central park to provide civic and green space to all residents of the district.

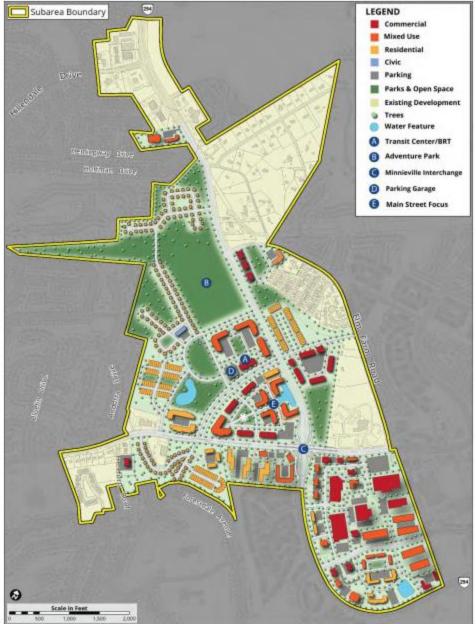


Figure 68: Illustrative Plan (Parkway Node)

The Applicant's proposed development program is illustrated by conceptual renderings included as part of the Design Guidelines. The mixed-use component is realized by the Urban District, which includes a Main Street design with buildings oriented toward private streets. The proposed development within the Urban District is largely composed of low-rise, single-use structures supported by surface parking. Although the Design Guidelines illustrate the concept of a Main Street, specific building orientation, design, and uses are not reflected on the MZP. The allocation of non-residential uses is largely segregated to Land Bay A, and Land Bay B is almost entirely composed of residential multi-family and townhomes.

Although the Urban District lacks the vertical integration of uses and intensity envisioned within the

Dale City Small Area Plan, the Applicant has committed to a strong internal street grid which is clearly delineated on the MZP. In addition, the Applicant has included geometric street sections and street cross sections both on the MZP and within the Design Guidelines. Staff recognizes that full implementation of the Dale City Small Area Plan is one that will occur over time, and a well-defined street grid with accompanying streetscape establishes a framework through which future development/intensification can occur. As such, the proposed development establishes a baseline of development that is consistent with the intent of the Dale City Small Area Plan.

Drive-Through & Motor Vehicle Fuel Station Uses

Proffer 6 permits the establishment of one drive-in/drive-through facility and one motor vehicle fuel station with convenience store/quick service food store to be located in Land Bay A. The only detail on these two uses are illustrative diagrams that depict a site layout and building orientation. Per these diagrams, the drive-in/drive-through will be located at the northwest quadrant of the intersection of Minnieville Road and Private Road 7A/B, and the motor vehicle fuel station will be located at the northwest quadrant of Minnieville Road and Public Road 2A. Building elevations have not been provided. However, the proffers provide for high quality building materials and four-sided architecture. Specific building elevations shall be subject to review by the Planning Director, or designee, prior to site plan approval. The orientation of both uses as depicted on the illustrative diagrams was deliberate to ensure the primary façade of a building faced Minnieville Road as opposed to a vehicular drive-through or fuel pumps. Staff recommends Proffer 6 be revised to more clearly state this objective.

Sign Modification Request

As noted within Waivers/Modifications Proffer 68.m., the Applicant is requesting, "Modification of Section 32-250.28 of the Zoning Ordinance to allow the location and number of Project Identification signs and Multi-Tenant signs as shown on the Street Network and Signage Plan." The Street Network and Signage Plan is included as Sheet 5 of the MZP and illustrates four "project ID/monument signs" and three "multi-tenant signs." Neither the proffers nor Design Guidelines provide detail on the specific dimensions or design of these signs. Size and height of these signs will be pursuant to the Zoning Ordinance. This modification is simply to increase the number of signs, and the sign locations in question are solely located within the PMD portion of the property where the predominance of the nonresidential uses are proposed.

Section 32-250.23.6. of the Zoning Ordinance allows the Board of County Supervisors to approve signage that is not consistent with the sign standards within the Zoning Ordinance as part of a rezoning or SUP request. Pursuant to Section 32-250.23.6. of the Zoning Ordinance and in the context of this subject SUP proposal, the following criteria must be considered to grant modifications for signage.

a) Compatibility of the proposed sign(s)
 The compatibility of the proposed sign(s) with the existing and/or approved buildings,
 landscaping, onsite amenities, overall design character of the on-site development, and design character of development adjacent to the subject property.

This modification is solely to increase the number of free-standing signs. Design details are

unknown at this time, and specific dimensions (area, height, etc.) will be pursuant to the Zoning Ordinance unless modified subsequently through a SUP. Not-withstanding, Proffer 46 stipulates that a comprehensive sign package will be developed for the Property.

b) Improve the Scenic Quality along Highways

The ability of the proposed sign(s), particularly when accompanied by landscaped treatments and lighting, to improve the scenic quality of highly visible areas along interstate highways, regional highways, and major County thoroughfares, with particular emphasis for signs proposed in proximity to the County's major gateways as identified in the Comprehensive Plan.

The modification proposes a total of seven free-standing signs, including five signs along the frontage of Prince William Parkway and Minnieville Road. The other two sign are located within the interior of the site along future public roads 2B and 1B. Generally speaking, signage is located at proposed/existing intersections or access points into the property. Information about lighting and landscaping is unknown at this time.

c) Consistency of Signage in a Mixed-Use Development In the case where the proposed sign or signs would be included in a mixed-use development, the consistency of the design for the proposed sign(s) with a comprehensive sign program.

This application proposes signage exclusively within the PMD portions of the property, which is proposed to include a mix of both residential and nonresidential uses. As noted above, Proffer 46 does include a commitment to provide a Comprehensive Sign Plan for this development.

d) Degree of Deviation

The degree of deviation from the sign regulations, considering whether the proposed sign design represents the minimum amount of modification necessary to provide adequate identification of the proposed use while still remaining consistent with the purposes and intent of County Code Section 32-250.21.

This modification increases the number of signs. Sign area and height will be pursuant to the Zoning Ordinance unless subsequently modified by a future SUP. Given the size of the property, the linear road frontage, and the location of signage at access points and/or intersections, the proposed modification is reasonable.

e) Existence of Special Visual Obstruction

The existence of a special visual obstruction or difficulty in locating the use, due to unique challenges associated with the location, topography, size, or configuration of the lot, including access to the lot, which makes the customary application of the sign regulations unreasonably restrictive.

There are no known visual obstructions. However, the request is reasonable given the size of the development combined with the number of proposed access points.

f) Highway Corridor Overlay District (HCOD) Whether the proposed sign would be located within a Highway Corridor Overlay District (HCOD). Both Prince William Parkway and Minnieville Road are located within the Highway Corridor Overlay District (HCOD), and signage is proposed within the HCOD.

Based on the above, staff can support the proposed modification.

Proposal's Strengths

- <u>Provision for Design Guidelines</u>: Design Guidelines have been included which provide standards for street design, building design, public spaces, amenities, among others.
- Internal Street Grid & Street Sections: The MZP depicts a strong grid of streets consisting of both public and private roadways. Geometric street sections for both the public and private streets on the property have been included on the MZP and within the Design Guidelines. The Design Guidelines also include cross sections which provide details on the public streetscapes.
- Shared-Use Paths: Eight-foot wide shared-use paths will be provided along one side of Roadways 2A, 2B, 1A, and 1B as well as along the frontage of Minnieville Road and the perimeter of the Land Bay E, which is proposed as a public use site for a school and public park. Shared-use paths along all new roadways will be constructed in conjunction with those roadways.

Proposal's Weaknesses

- Lack of Plan Details: The MZP does not include specific details on site layout or building placement. General guidance is included within the Site Planning Principles section of the Design Guidelines, but this language is vague and ambiguous, including phrases such as "building setbacks shall be tailored to the desired roadway...," "...building shall be located closer to internal streets...," and "...front yard setbacks shall be set between the building and roadway curb..." These phrases, which are very general in nature, do not provide the specificity that is needed in an urbanizing environment.
 - Staff recognizes that a specific site layout on a 145 acres property that will build out over many years is not practical. In such an instance, it is the burden of the Applicant to provide a framework through which development will occur. The Applicant has provided this framework through the street grid depicted on the MZP, and the cross sections contained within the Community Infrastructure section of the Design Guidelines, which visually depicts a building relationship to the street. Staff recommends language be included within the Design Guidelines that cross references the concepts contained within the Site Principles Section with the cross sections contained within the Community Infrastructure section.
- Highway Corridor Overlay District (HCOD) Buffer: The Applicant has proposed modifications
 that would reduce the required fifty foot wide HCOD buffer to twenty feet along the entirety
 of Minnieville Road and along large portions of Prince William Parkway, particularly in Land
 Bay A. The only tree save proposed within the HCOD buffer is along an approximately six
 hundred foot portion of the site located within Land Bay F. No tree save is proposed along

any of the Prince William County Parkway frontage of Land Bay A. It is staff's understanding that topographical and grade differentials, including steep slopes located adjacent to Prince William Parkway, may limit the practicality of tree save area in a large portion of this area. Notwithstanding, staff recommends the full fifty foot HCOD width be provided, even if the width is populated with new plantings.

• Residential Building Heights: Proffer 45 specifies that both single-family attached dwelling units AND multi-family dwelling units may have a residential building height up to 55 feet, or as otherwise approved by the Planning Director. The Applicant has stated this proffer is necessary for the purpose of allowing rooftop terraces. Although staff does not object to this in concept, a 55 foot building height is not appropriate for single-family attached dwelling units and may result in compatibility concerns in areas adjacent to existing single-family detached neighborhoods. The provision within the proffer for additional height increases approved by the Planning Director is not appropriate as that latitude already exists within the minor modification provisions of the Zoning Ordinance, which limits any increase in height to ten feet. Finally, the inclusion of this language in the proffers contradicts the development standards tables within the Design Guidelines and may result in confusion upon implementation.

On balance, this application is consistent with the relevant components of the Community Design Plan.

Cultural Resources Plan Analysis

Prince William County promotes the identification, evaluation, and protection of cultural resource sites throughout the County, as well as the tourism opportunities these sites present. The Cultural Resources Plan recommends identifying, preserving, and protecting Prince William County's significant historical, archaeological, architectural, and other cultural resources – including those significant to the County's minority communities – for the benefit of all of the County's citizens and visitors. To facilitate the identification and protection of known significant properties that have cultural resource values worthy of preservation, the land use classification County Registered Historic Site (CRHS) is used in the Comprehensive Plan. The Plan includes areas of potentially significant known but ill-defined or suspected pre-historic sites, Civil War sites, historic viewsheds, landscapes or areas of potential impact to important historic sites, and encourages the identification, preservation, protection, and maintenance of all cemeteries and/or gravesites located within the County.

A Phase I cultural resources study was prepared and submitted to the County for this application. The cultural resources report is titled "Phase I Cultural Resources Survey of the ±59.8 Hectare (±148 Acre) Quartz District Project Area (Dutton and Hope 2018). Four archaeology sites and one architectural site were identified in the report. The Historical Commission recommended no additional studies (no Phase II evaluation/No Further Work) during their first review of this application. The Historical Commission recommended Phase II evaluation on one archaeology site, 44PW2033, during their review on August 10, 2021. This most recent review was executed by a different Commissioner than the previous reviews.

The County Archeologist has reviewed the description of archaeology site 44PW2033 in the report titled "Phase I Cultural Resources Survey of the ±59.8 Hectare (±148 Acre) Quartz District Project Area (Dutton and Hope 2018). A total of eight artifacts were recovered from nine shovel tests. This site measures approximately 150 feet by 125 feet, or 18,750 square feet. In this case, there are a low number of artifacts found across a relatively large area. This site is likely a short-term occupation where stone tool maintenance was performed. The likelihood of additional studies finding new or significant information is low and further studies are not recommended.

Proposal's Strengths

- No Further Work: The Historical Commission reviewed this proposal at its August 10, 2021 meeting and recommended a Phase II evaluation on one archaeology site, 44PW2033. Upon further review by the County Archaeologist, no additional studies are recommended as the likelihood of finding new or significant information is low.
- <u>Barn Material Salvage</u>: Per Proffer 56, the Applicant shall offer the County the opportunity to salvage any or all of the structural material from the large barn located on the property.

Proposal's Weaknesses

None identified.

On balance, this application is found to be consistent with the relevant components of the Cultural Resources Plan.

Environment Plan Analysis

Prince William County has a diverse natural environment, extending from sea level to mountain crest. Sound environmental protection strategies will allow the natural environment to co-exist with a vibrant, growing economy. The Environment Plan sets out policies and action strategies that further the County's goal of preserving, protecting, and enhancing significant environmental resources and features. The Plan includes recommendations relating to the incorporation of environmentally sensitive development techniques, improvement of air quality, identification of problematic soil issues, preservation of native vegetation, enhancement of surface and groundwater quality, limitations on impervious surfaces, and the protection of significant viewsheds.

With the exception of an existing office building located at the northwestern portion of the property, the 145.05 acres subject to this rezoning is generally vacant and undisturbed. The vast majority of the site is covered with heavy tree cover, consisting of understory and overstory growth. This forested tree cover includes fifty-one specimen trees, located sporadically throughout the property with higher concentrations along the north and northeast property boundaries. A small section of Resource Protection Area (RPA) is located at the northeast corner of the site adjacent to Prince William Parkway. The site includes steep slopes, 15 – 25 percent and greater than 25 percent, located throughout the site, with larger concentrations located along the north, northeast, and northwest property boundaries.

The MZP includes a defined Limits of Disturbance, and it designates portions of the property as "areas undisturbed by Applicant." Tree Preservation Areas are designated within the buffer areas along the western property boundary, a portion of the southern property located within Land Bay C, adjacent to the RPA at the northeastern property boundary, and within the HCOD buffer along Prince William County Park way within Land Bay F. Additional tree save areas are located outside of the buffer in Land C, D, and G, which have been located to coincide with areas containing concentrations of specimen trees. Approximately sixteen specimen trees are located within undisturbed tree save areas. The majority of Land Bay E, the proposed public use site, is also designated as "undisturbed," although it is anticipated that a large portion of this land bay will be disturbed to facilitate the construction of the park and school sites.

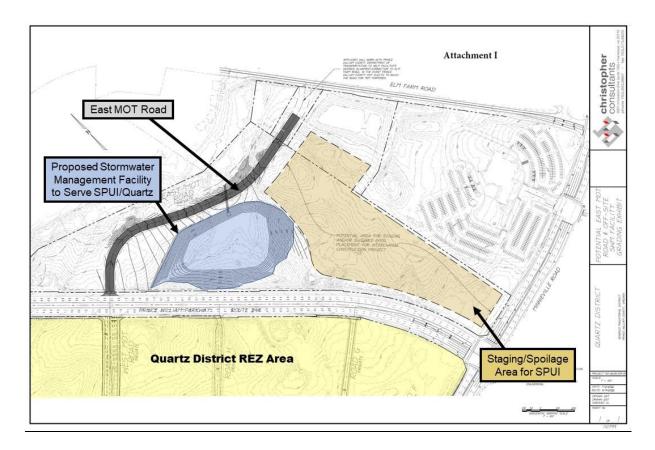
Approximately seven percent of the site is currently designated as undisturbed open space and/or tree save, a large percentage of which is located within perimeter buffers. Undisturbed tree save areas located outside of the buffers are fragmented and disconnected. In addition, the proffers include allowances for utilities and other land disturbance within open space areas/tree save areas, and the Design Guidelines note that significant portions of the tree save areas located outside of the buffers will be used for recreational amenities, including a dog park and playground. These possible encroachments diminish the intent of providing undisturbed naturalized areas and may, in fact, jeopardize specimen trees intended for preservation.

The Watershed Management staff recommends a minimum of twenty percent of the total site area be preserved as natural open space, with an emphasis being at the northern end of the site where forests are of good quality. Preservation should be focused to create and connect large blocks of forest contiguous to the RPA, stream headwaters, and off-site open space.

Notwithstanding the above, Staff is cognizant that the Applicant has agreed to provide twenty percent of the PMB land area and thirty percent of the PMR land area as open space, and staff is cognizant that a balance must be maintained between realizing the more urban intent of the Small Area Plan and the preservation of naturalized resources. That being said, Staff recommends the Applicant target additional areas at the northern portion of the site for additional tree save, and staff recommends the proffers be revised to explicitly preserve specimen trees. These recommendations are further enumerated in the weaknesses below.

<u>Impacts of Proposed Off-Site Improvements on Environmental Resources:</u>

Proffer 11 provides for the potential construction of a Maintenance of Traffic (MOT) Road and stormwater management facility off-site on property identified as GPIN 8292-04-3728. The MOT Road is intended to support the construction of the Prince William Parkway/Minnieville Road Single Point Urban Interchange (SPUI), and the stormwater management facility is intended to support both the Quartz development as well as the SPUI. A graphic has been included as part of the rezoning materials (inserted below) that depicts these improvements as well as identifies a large portion of the site as "potential area for staging and/or suitable spoil placement for interchange construction project." This off-site parcel contains extensive woodland resources as well as potential RPA and floodplain, the majority of which will be impact by these off-site facilities.



Water Quality

The Applicant has proffered to make a monetary contribution to the Board of County Supervisors in the amount of \$75.00 per acre for water quality monitoring, drainage improvements, and/or stream restoration projects. Said contribution shall be made prior to and as a condition of site plan approval with the amount to be based on the site area acreage.

Proposal's Strengths

- <u>Limits of Disturbance (LOD):</u> The MZP clearly depicts a LOD, which has been located to preserve existing specimen trees. There are fifty-one specimen trees located on the property, and sixteen specimen trees are noted for preservation.
- Tree Save/Buffers Adjacent to Cedar Run Homeowners Association (HOA): The MZP depicts a
 100-foot setback concurrent with the western property boundary along and adjacent to the
 existing Cedar Run HOA. The MZP denotes this area as tree save, additional setback, and
 existing access easement to be abandoned. Specific expectations for this one hundred foot
 wide buffer area are addressed by Proffers 36, 42, and 49.
 - Proffer 36 states tree preservation will be provided within the buffer, and the proffer further states 215 supplemental plantings will be planted in an area to be determined by the Applicant in conjunction with the Cedar Run HOA.
 - o Proffer 42 states the existing asphalt road will be abandoned in place, covered with topsoil, and will include berming where feasible.

 Proffer 49 states a one hundred foot wide setback line will be established and will include a seventy foot wide tree preservation area and thirty foot wide area of open space. The thirty foot wide open space area may include stormwater management facilities.

Per the DCSM, the location of single-family attached dwelling units adjacent to existing single-family detached units would require a thirty-foot wide Type B buffer, and as such, the proposed one hundred foot wide buffer, which exceeds the DCSM requirement, is clearly a strength of the application. That being said, the inclusion of commitments for this buffer within three separate proffers creates possible confusion, and there are certain aspects of the proposal which should be clarified by the Applicant. Notably, Proffer 36 provides for supplemental plantings, which could theoretically be placed within the open space or within the existing asphalt roadbed proposed to be abandoned and located directly behind existing residences. Proffer 42 states the roadbed will be retained and covered with topsoil. The placement of any new plantings on top of existing asphalt will jeopardize survivability. The Applicant should commit within the proffers to remove the existing asphalt.

In addition, Proffer 49.a. states the open space area as a possible location for stormwater management. However, Proffer 49.d. states minimal clearing will be permitted within tree preservation areas within the buffer to allow for utilities and a trail. The Applicant should define minimal and should qualify to what extent utilities can be placed within the tree save area.

Proposal's Weaknesses

- Additional Tree Save Area/Protection of Specimen Trees: In order to provide additional
 protections for the existing high quality forest cover and to fully realize the protection of
 specimen trees, staff recommends the following:
 - Proffer 49.d. be revised to explicitly state that any such minimal clearing necessary to accommodate utilities be located outside of the critical root zone areas of any specimen tree noted on the MZP as to be preserved. It may be further necessary that an additional proffer be included specifying a critical root zone area of specimen trees noted for preservation will be identified at site plan and clearly marked during construction.
 - Additional tree save area be identified within Land Bay F and/or G to facilitate the
 preservation of the high quality forest resources located at the north, northeastern,
 and northwestern portions of the property.
 - o In furtherance of the above, the LOD and corresponding undisturbed areas located at the northwestern corner of Land Bay D be expanded to the northern property boundary within Land Bay G and include specimen tree 9063. This area represents an approximately 70-foot wide area located between the currently designated LOD and the 30-foot wide perimeter buffer. This area includes extensive tree cover of high quality as well as 15 to 25 percent steep slopes. Specimen tree 9063 is an existing high quality 46-inch northern red oak.
- Off-site Impacts to Environmental Resources: As noted above, environmental resources located off-site on GPIN 8292-04-3728 may potentially be impacted by proffered

improvements proposed as part of this rezoning. Documentation detailing these existing environmental resources has not been provided, and the impacts have not been evaluated. The Applicant should clarify the intent for this off-site parcel, and additional information should be submitted regarding the extent to which the stormwater needs of the Quartz District will be achieved off-site on acreage not subject to the rezoning. If it is the case that these facilities are intended to directly support the development proposed on the Quartz District, the Applicant should provide some expectation and commitments regarding how impacts to environmental resources will be mitigated.

On balance, this application is found to be inconsistent with the relevant components of the Environment Plan.

Fire and Rescue Plan Analysis

Quality fire and rescue services provide a measure of security and safety that both residents and businesses have come to expect from the County. The Fire and Rescue Plan sets out policies and action strategies that further the County's goal of protecting lives, property, and the environment through timely, professional, humanitarian services essential to the health, safety, and well-being of the community. The Plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility for fire and rescue facilities. The Plan also includes recommendations to supplement response time and reduce risk of injury or death to County residents, establishment of educational programs, such as cardio-pulmonary resuscitation (CPR) training, automatic external defibrillators (AED), and encourage installation of additional fire protection systems – such as sprinklers, smoke detectors, and other architectural modifications.

Fire/Rescue Station #20 (Prince William Commons) is the first due fire/rescue resource for the subject property. The site is inside the 4.0-minute travel time for Basic Life Support and Fire and inside the 8.0-minute travel time for Advanced Life Support. In FY 2021, Fire/Rescue Station #20 responded to 2,889 incidents, and with a workload capacity of 4,000.

Proposal's Strengths

- Monetary Contribution: Per Proffer 59, the Applicant will make a monetary contribution to the Board of County Supervisors in the amount of \$235.35 per single-family attached residential unit and \$172.83 per multifamily residential unit constructed on the Property to be used for police, fire, and rescue services in the area. Said contribution shall be made prior to and as a condition of issuance of an occupancy for each residential unit.
 - Proffer 60 provides for a monetary contribution in the amount of \$0.61 per square foot of nonresidential space to be paid prior to and as a condition of the issuance of the first building permit for nonresidential uses.
- <u>Inside of 4.0-Minute Travel Time</u>: The site is located within the required 4.0-minute travel time for basic life support and fire suppression.

- <u>Inside of 8.0-Minute Travel Time</u>: The site is located within the required 8.0-minute travel time for advanced life support services.
- <u>Station Workload</u>: Fiscal Year 2021 figures indicate that Fire and Rescue Station #20 responded to 2,889 incidents, while the workload capacity for Station 20 is 4,000 incidents per year. As such, it is operating within capacity.

Proposal's Weaknesses

None identified.

On balance, this application is found to be consistent with the relevant components of the Fire and Rescue Plan.

Housing Plan Analysis

Prince William County is committed to clean, safe, and attractive neighborhoods for all its residents, and the elimination of neighborhood blight and substandard housing. The Housing Plan sets out policies and action strategies that further the County's goal of identifying locations and criteria for the provision of diverse housing opportunities for all segments of our population and to promote economic development. The Plan includes recommendations relating to neighborhood preservation and improvement, affordable housing, special needs housing, and public/private partnerships to address housing needs.

The Housing chapter of the Comprehensive Plan encourages the provision of diverse and affordable housing for all segments of the County's population, and encourages developers to contribute to the Housing Preservation and Development Fund during the rezoning process for any residential project. The Applicant has proffered to provide 90 units in lieu of a contribution.

Proposal's Strengths

Workforce/Affordable Housing Units: Per Proffer 61, the Applicant will provide for 90 residential dwelling units as either Affordable Dwelling Units (ADUs) and/or Workforce Dwelling Units (WDUs) pursuant to the following income tiers: 30 ADUs shall be made available for rent to households earning up to 60% of the Area Median Income (AMI); 30 WDUs shall be available for sale or rent to housing earning up to 80% AMI; and 30 WDUs shall be made available for rent to household earning up to 100% AMI.

The County does not have either an adopted ADU or WDU ordinance, and as such, there is no baseline by which to evaluate this proposal. General policy guidance is included as part of the Pathway to 2040 Comprehensive Plan update, which is currently scheduled for a Planning Commission Public Hearing on September 28, 2022. Notwithstanding, staff does offer the following commentary on Proffer 61:

 The terms WDU and ADU are currently not defined as it relates to Prince William County.

- Proffer 61.b. states the AMI income tiers will be determined for the Prince William area of the United States Department of Housing and Urban Development. Typically, AMI is calculated for the Washington Metropolitan Statistical Area.
- The proffer provides no specificity regarding how many of the WDUs and/or ADUs will be realized as townhouse units versus multifamily units. Ideally, the proportion would be relative to the proportion of units proposed. Proffer 61.e. does, however, provide phasing of the units relative to construction of the first 900 residential units on the property. As such, a combination of townhouse and multifamily can be anticipated, although the specific mix is unknown.
- Similar to the above, the proffer provides no baseline regarding how the various income tiers will be distributed between the townhouse versus multifamily units. Again, each unit type would ideally include a mix of income tiers. As written, it is possible that the lower income tiers could be realized solely as multi-family units, and the townhouse could be realized at the 100% AMI income tier.
- Ideally, the proffer would contain language that states ADUS/WDUs would be indistinguishable from market rate units as such units are viewed from a public street.
- Finally, the Applicant should clarify Proffer 61.i. which states 20 of the 30 units intended to for households up to 80% AMI would be made available through the Prince William County Office of Housing and Community Development's First Time Homebuyers Program. Although Staff does not object to this in concept, the proffer states that the units will revert to market rate if they are not identified/purchased within six months after issuance of a building permit. It is highly unlikely construction will be completed in six months.

Proposal's Weaknesses

None identified.

On balance, this application is found to be consistent with the relevant components of the Housing Plan.

Parks, Open Space and Trails Plan Analysis

The quality of life for residents of Prince William County is linked closely to the development and management of a well-maintained system of parks, trails, and open space. Prince William County contains a diversity of park, open space, and trail resources. These parklands, open spaces, and recreational facilities play a key role in shaping both the landscape and the quality of life of Prince William County residents through the conservation of natural and cultural resources, protection of environmental quality, and provision of recreational facilities. The Parks, Open Space and Trails Plan sets out policies and action strategies that further the County's goal of providing park lands and recreational facilities of a quantity, variety, and quality appropriate to meet the needs of the current and future residents of Prince William County. The Plan includes recommendations to preserve existing protected open space, maintain high quality open space, expand the amount of protected

open space within the County, and to plan and implement a comprehensive countywide network of trails.

The Prince William County Department of Parks, Recreation & Tourism (DPRT) has reviewed a copy of the subject application and offers the following comments, relevant to the level of service (LOS) standards contained in the Parks, Open Space and Trails Chapter of the Prince William County Comprehensive Plan (adopted March 10, 2020), and park goals identified in the DRPT Parks, Recreation and Open Space Master Plan (adopted October 6, 2020), as well as the Dale City Small Area Plan (adopted December 10, 2019).

PWC PARKS AND RECREATION FACILITIES WITHIN THE SERVICE AREA THE PROJECT

<u>Park Type</u> <u>Park/Facility</u>

Neighborhood None

Community Sharron Baucom-Dale City Recreation Center

Lake Ridge Marina & Golf Course

PWC Stadium Complex (Pfitzner Stadium)

Regional Chinn Aquatics & Fitness Center

Andrew Leitch Regional Park Veterans Memorial Park

Linear/Resource None Natural/Cultural Res. None

School/Community Use Community use fields at Kerrydale ES, Minnieville ES, and Bel Air ES; K9

Gunner Memorial Dog Park

Trails Existing shared use path along primary roadway

Adventure Park

The Dale City Small Area Plan, Parkway Node Illustrative Plan, designated an "Adventure Park" in an area that correlates with the Parks and Open Space Active (POSA) land use designation. The Parks and Recreation section of the Dale City Small Area Plan states the following, "...the development of a new mixed use community center in the Parkway node will create a new walkable urban adventure park and open space resources, such as pocket parks and linear promenades..." Per the Small Area Plan, this Adventure Park is to be located adjacent to and oriented parallel to Prince William Parkway.

Per Proffers 22 and 23, the Applicant will dedicate 6 acres within Land Bay E as a Public Park Site to be used as the Adventure Park referenced in the Small Area Plan. The Applicant will provide off-site stormwater management, a stub out of sewer and water connections to the park boundary, and will construct an eight-foot wide shared use trail around the perimeter of Land Bay E. Construction of the park facilities will be the responsibility of the County. The proffers include a reversion clause that the acreage would be conveyed back to the Applicant if the County elects not to construct the Adventure Park prior to the expiration of the November 2019 Park Improvement Bond or if construction has not commenced within three years following the expiration of the bond.

Proposal Strengths

- <u>Community Recreational Amenities</u>: Proffers 31 and 32 detail community recreation and open space amenities to provided through the property. These include:
 - Construction of a clubhouse/pool facility within the Neighborhood District prior to the issuance of the 500th residential building permit (excluding multifamily residential units in Land Bay B). The pool will include a minimum 5,000 square foot clubhouse building and a minimum 4,000 square feet of pools.
 - Three playgrounds located within the Neighborhood District, each to include a minimum of 2,000 square feet.
 - Five passive pocket parks, including four 1,200 square foot parks located in the Neighborhood District and one 1,500 square foot park located within the Urban District. Each park will include seating, landscaping, and open play lawn area.
 - Two dog parks located in the Neighborhood District, each with a minimum area of 7,500 square feet.
 - A community lawn area in the Urban District with a minimum area of 7,500 square feet and which will include a small pavilion/stage structure.
 - A lakefront plaza amenity area in the Urban District with a minimum area of 6,000 square feet to include hardscape, public seating, and signage.

The locations of each of these facilities is generally illustrated on Page 38 of the Design Guidelines. The Design Guidelines include an illustration of the layout for amenity spaces to be located within the nonresidential portion of the Urban District. Aside from the description noted in the proffers, there is no design or illustration for most of the facilities located in the Neighborhood District.

In addition, the Design Guidelines depict two pocket parks serving the Urban District townhomes whereas the proffer only identifies one pocket park. This discrepancy should be reconciled.

Proposal Weaknesses

- <u>Public Use Site Area:</u> DPRT staff have noted that six acres may not be sufficient size to fully accommodate the Adventure Park and recommends a minimum of fifteen acres be provided. Staff notes that the total area of Land Bay E is 20.26 acres and includes two public use sites, one for a park and one for a school. Per Proffer 30, the specific allocation of land within Land Bay E for the two uses is at the discretion of the Board, who will make the final decision regarding the size of the Adventure Park.
- <u>Public Use Site Reversion Clause:</u> Staff cannot support the Applicant's proposed language that the six-acre public use site would be conveyed back to the Applicant should construction not commence within the specified three year timeframe. Design and construction timing are unknown and often beyond the County's control. A similar reversion clause has not been included as part of the dedication of the school public use site, the timing of which is also unknown, and the inclusion of a reversion clause for one site and not the other site may actually limit the Board's flexibility in how the property is used.

On balance, this application is found to be consistent with the relevant components of the Parks, Recreation, and Tourism component of the Comprehensive Plan.

Police Plan Analysis

Residents and businesses expect a high level of police service for their community. This service increases the sense of safety and protects community investments. The Police Plan is designed to promote Prince William County's public safety strategic goal to continue to be a safe community, reduce criminal activity, and prevent personal injury and loss of life and property, as well as to ensure effective and timely responses throughout the County. This Plan encourages funding and locating future police facilities to maximize public accessibility and police visibility as well as to permit effective, timely response to citizen needs and concerns. The Plan recommends educational initiatives, such as Neighborhood and Business Watch, and Crime Prevention through Environmental Design (CPTED), which encourages new development to be designed in a way that enhances crime prevention. The Plan also encourages effective and reliable public safety communications linking emergency responders in the field with the Public Safety Communications Center.

Comments from the Police Department included recommendations for minimum illumination in parking and pedestrian areas of 2.0 foot candles and a preference for 3.0 to 5.0 foot candles as well as adequate maintenance of shrubs and trees to ensure visibility. Further, the Applicant should coordinate with the Police Department as the site develops, and apply the various Crime Prevention Through Environmental Design (CPTED) principles, which can be found at the following: https://www.pwcva.gov/assets/documents/police/002035.pdf.

Proposal's Strengths

• <u>Impacts to Levels of Service</u>: The Police Department does not believe this application will create significant impact on calls for service.

Proposal's Weaknesses

None identified.

On balance, this application is found to be consistent with the relevant components of the Police Plan.

Potable Water Plan Analysis

A safe, dependable drinking water source is a reasonable expectation of County residents and businesses. The Potable Water Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound drinking water system. The Plan includes recommendations relating to system expansion, required connections to public water in the development area, and the use of private wells or public water in the Rural Area.

Public water is not available from the Service Authority. The subject parcels are located within the Virginia American Water Company (VAWC) service area. These applications have been forwarded to the VAWC for comment, who subsequently contacted staff requesting additional information. As of this writing, no comments have been provided.

Proposal's Strengths

 Water Connection & Service: As proffered, the Property shall be served by public water, and the Applicant shall be responsible for the costs and construction of those on and off-site improvements needed to provide service for the demand generated by the Property.

Proposal's Weaknesses

• None identified.

On balance, this application is found to be consistent with the relevant components of the Potable Water Plan.

Sanitary Sewer Plan Analysis

Appropriate wastewater and sanitary facilities provide needed public health and environmental protections. The Sanitary Sewer Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound sanitary and stormwater sewer system. The Plan includes recommendations relating to system expansion, required connections to public sewer in the development area, and the use of either private or public sewer systems in locations classified as Semi-Rural Residential (SRR), as well as the Rural Area.

The referenced rezoning application is for parcels located within the Dale City area of the County which are not within the service area of the Service Authority. However, in accordance with a service agreement dated November 6, 1984, the Service Authority is to provide sanitary sewer service to the eastern portion of the site.

The Service Authority has an existing 8-inch gravity sewer main located on the east side of Prince William Parkway, approximately 650 feet south of Elm Farm Road. The developer will be required to provide a sewer study to determine if there is adequate capacity in the existing collection system to accommodate the projected peak flows of the proposed development. If the existing system is inadequate, the developer will be required to design and construct all new offsite improvements necessary for their development, except for Service Authority responsibility to right size existing assets in accordance with the Development Review Process and System Improvement Policy as described in the USM. The Service Authority may enter into a MPUA Agreement with the applicant to right size existing assets. All connections to the public water/sewer system shall be in accordance with the Service Authority's USM requirements and restrictions.

Proposal's Strengths

• <u>Sewer Connection & Service</u>: As proffered, the Property shall be served by public sanitary sewer, and the Applicant shall be responsible for the costs and construction of those on and off-site improvements needed to provide service for the demand generated by the Property.

Proposal's Weaknesses

None identified.

On balance, this application is found to be consistent with the relevant components of the Sanitary Sewer Plan.

<u>Schools Plan Analysis</u>

A high-quality education system serves not only the students and their families, but the entire community by attracting employers who value educational opportunities for their employees. The Schools Plan sets out policies and action strategies that further the County's goal of providing quality public education to our school-aged population. The Plan includes recommendations relating to facility size and location, sitting criteria, compatible uses, and community use of school facilities.

Based on the most recent submission, the Schools Division provided a School Board Impact Statement dated June 13, 2021. For reference purposes, student generation, enrollment, capacity, Capital Improvements Program (CIP) projects information is provided below, as applicable.

Current Countywide Capacity and Projected Student Enrollment

Countywide Current and Projected Student Enrollment & Capacity Utilization

Available Space		e Space	2021–22			2026–27			2031–32		
		Portable		Space Available			Space Available			Space Available	
School Level	Capacity	Classrooms	Students	(+/-)	Util. (%)	Students	(+/-)	Util. (%)	Students	(+/-)	Util. (%)
Elementary School	43,249	74	38,734	4,515	89.6%	40,586	2,663	93.8%	39,847	3,402	92.1%
Middle School	22,822 1	46	20,625	1,658	92.6%	20,741	1,898	91.6%	22,477	162	99.3%
	22,639 ²		,,	2,222		,	-,		,		
High School	28,754	67	28,772	-18	100.1%	29,918	-1,164	104.0%	31,177	-2,423	108.4%
	22,639 ² 28,754			-,		,	-,		,		L

¹ Capacity on which available space is calculated for the 2021-22 school year.

² Capacity on which available space is calculated for the 2022-23 through 2031-32 school year. It reflects the 11-classroom addition opening at Gainesville MS and the six-classroom addition opening at Reagan MS in the 2022-23 school year.

Quartz District Student Generation

Proposed Residenti (number of u	• * *	Student Gener Re	ation for P zoning	roposed
Housing Units Pro	posed	Student	s Generated	
Single-Family	0	Elemen	tary 230	
Townhouse	610	Midd	le 112	
Multi-family	405	Hig	n 149	
Total	1,015	Tota	d 491	

Schools Analysis

Current and Projected Student Enrollment & Capacity Utilization - Schools in same attendance area as Proposed Rezoning

Under the School Division's 2021-22 school attendance area assignments, students generated from the Proposed Rezoning will attend the following schools:

	Av	Available Space		2021-22		2026-27			2031-32			
					Space			Space			Space	
	Program	Planning	Portable		Available			Available			Available	
School	Capacity	Capacity	Classrooms	Students	(+/-)	Util. (%)	Students	(+/-)	Util. (%)	Students	(+/-)	Util (%)
Jenkins ES		752	0	575	177	76.5%	561	191	74.5%	560	192	74.5%
Belville MS		1,256	0	1,095	161	87.2%	1,056	200	84.1%	1,146	110	91.2%
Gar-Field HS	2,839		0	2,430	409	85.6%	2,400	439	84.5%	2,612	227	92.0%

Current and Projected Student Enrollment

- Schools in same attendance area as Proposed Rezoning, including the effect of students generated from Proposed Rezoning

	Available Space		2021-22			2026-27			2031-32			
	Planning	Program	Portable		Space Available			Space Available			space Available	
School	Capacity	Capacity	Classrooms	Students	(+/-)	Util. (%)	Students	(+/-)	Util. (%)	Students	(+/-)	Util. (%)
Jenkins ES		752	0	575	177	76.5%	791	-39	105.1%	790	-38	105.0%
Belville MS		1,256	0	1,095	161	87.2%	1,168	88	92.9%	1,258	-2	100.1%
Gar-Field HS	2,839		0	2,430	409	85.6%	2,549	290	89.7%	2,761	78	97.2%

Developer Proposed Mitigation:

- The applicant shall make a monetary contribution in the amount of \$398.07 per townhouse unit and \$209.51 per multi-family unit, to be used for school purposes in the area, for a total of \$327,674.24.
- The applicant's monetary proffer amounts were calculated from the Proffer Justification Narrative dated May 13, 2021
- The applicant shall dedicate to the Board of County Supervisors approximately 12 acres to be used for an elementary school and associated recreational uses.

- The Overview Park/School Site identifies area D as 230+/- space parking for School and Park. Shared parking is a concern whereby the school needs dedicated parking during school hours and after school hour events.
- The applicant shall construct off-site storm water management and stub out sewer and water connections to the property boundary of the elementary school site, and a perimeter shared use path in an easement at the elementary school site.

Comments from Prince William County Public Schools include the following:

- The School Board is opposed to any rezoning application that causes student enrollment by school level or by student enrollment at any assigned school, to exceed 100 % of capacity.
- Current enrollment at the assigned elementary, middle, and high schools has existing capacity.
- As indicated above, projected enrollment at the assigned elementary (Jenkins) is expected to
 exceed capacity by 5% with the additional students under this application.
- The applicant has proffered approximately \$327,674, as well as 12 acres for a school site, to help mitigate the students generated by this application.
 - The approved 2023-32 10-Year CIP does not presently identify the need for an additional elementary school. However, an elementary school site could be useful in addressing longer-term needs of the School Division.
 - School staff will need additional information to conduct further analysis of the feasibility of the proposed site to determine suitability.
- For these reasons, the School Board is not opposed to the subject application.

Proposal's Strengths

- Adequate Capacity at Middle/High Schools: The additional students generated by this
 application will not result in either Bellville Middle School or Garfield High School exceeding
 capacity.
- <u>Monetary Contribution</u>: Per Proffer 27, the Applicant has proffered a monetary contribution for schools of \$398.07 per townhouse unit and \$209.51 per multi-family unit, to be used for school purposes in the area.
- <u>School Public Use Site:</u> Per Proffer 28, the Applicant shall dedicate 12 acres on the northern side of Land Bay E to be used for the future construction of an elementary school and associated recreational facilities.

Proposal's Weaknesses

- <u>Elementary School Capacity:</u> The residential units associated with this application will result in Jenkins Elementary School exceeding capacity by five percent.
- Public Facility Review of Land Bay E (school and park public use site): Proffer 63 states that acceptance and approval of this rezoning by the BOCS authorizes the location and provision of those public uses and facilities referenced on the MZP and in the proffers. This proffer essentially negates the need for a subsequent public facility review by the Planning

Commission of any public use sites contained herein. The allocation of acreage within Land Bay E between the school and park site is unknown, and the specific design of both facilities is unknown. Further, Land Bay E is noted by this MZP as undisturbed acreage. Although it is highly unlikely that the entire site will remain undisturbed to facilitate the construction of a possible elementary school, it may be the case that one or both sites include additional open space/tree save once a final design is determined. As such, Staff recommends that the public use sites within Land Bay E be exempt from Proffer 63 and subject to a Public Facility Review (PFR) review by staff and the Planning Commission.

On balance, this application is found to be consistent with the relevant components of the Schools Plan.

<u>Transportation Plan Analysis</u>

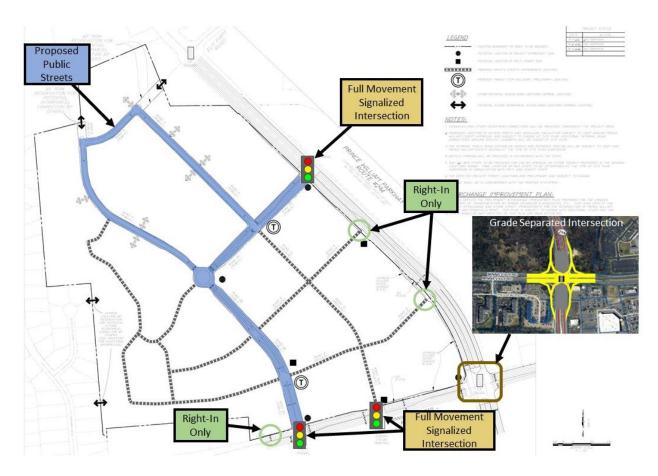
Prince William County promotes the safe and efficient movement of goods and people throughout the County and surrounding jurisdictions by providing a multi-modal approach to traffic circulation. The Transportation Plan establishes policies and action strategies that further the County's goal of creating and sustaining an environmentally friendly, multi-modal transportation system that meets the demands for intra- and inter-county trips, is integrated with existing and planned development, and provides a network of safe, efficient, and accessible modes of travel. The Plan includes recommendations addressing safety, minimizing conflicts with environmental and cultural resources, maximizing cost effectiveness, increasing accessibility of all travel modes, minimizing projected trip demand, and providing sufficient network capacity. Projects should include strategies that result in a level of service (LOS) of "D" or better on all roadway corridors and intersections, reduce traffic demand through transportation demand management strategies, dedicate planned rights-of-way, provide and/or fund transit infrastructure, pedestrian and bicycle pathways, and improved and coordinated access to transit facilities.

A Traffic Impact Study from Gorove Slade was submitted, dated October 2, 2018, with revisions dated November 29, 2021, and March 18, 2022. The Traffic Study assumed 1,015 residential units (including townhomes, 2-over-2's, and multifamily units), 232,000 square feet of office uses, 136,000 square feet of retail uses, 6,000 square feet of drive-through fast-food uses, and a gas station with 16 fueling stations. These uses are anticipated to generate 912 net new trips during the AM peak hour, 1,221 net new trips during the PM peak hour, 15,279 net new daily trips on a typical weekday.

The following summary table provides the latest Virginia Department of Transportation (VDOT) annual average daily traffic counts and Prince William County Travel Demand model levels of service (LOS) information in the vicinity of the site.

Roadway Name	Number of Lanes	2021 VDOT Annual Average Daily Traffic Count; Vehicles Per Day (VPD)	Travel Demand Model 2019 Daily LOS
Prince William Parkway	6	39,000	С
Minnieville Road	6	40,000	С

Proposed Street Grid and Site Access



The proposed internal street grid and proposed site access points are summarized in the graphic above. Access to the site along Prince William Parkway will include a new full movement signalized intersection and two right-in only points of ingress. Access to the site along Minnieville Road will include two full movement signalized intersections, one new traffic signal, and the modification of an existing traffic signal, as well as one right-in only point of ingress. New public streets will include Roads 1A, 2A, 2B, 1B, 3, 4, 5. The remaining streets in the development will be private. The proposed new public and private streets are generally consistent with the grid anticipated as part of the Dale City Small Area Plan and create an optimum grid for the movement of vehicles and people within the development.

Single Point Urban Interchange (SPUI)

The Dale City Small Area Plan identifies the replacement of the existing traffic signal at the Prince William Parkway and Minnieville Road intersection with a grade separated interchange as a needed infrastructure improvement. This intersection is currently operating at Levels of Service (LOS) E in the AM peak and LOS F in the PM peak, a condition that will improve following construction of the interchange. This interchange was included in the 2019 Bond Referendum that was approved by 73% of the public. The County has applied for funding for the design and construction of this interchange. If complete funding is appropriated in fiscal year 25-26, the project could be completed by fiscal year 29. There is currently partial funding allocated to the project to begin environmental studies.

Per Proffer #9, the Applicant has committed to provide 30 percent design plans for the SPUI, survey work and geotechnical work for the 30% design plans, utility relocation design plans, and the design/construction of an off-site stormwater management facility off-site on Applicant controlled property on the east side of Prince William Parkway to accommodate drainage from the SPUI and the proposed development. Proffer # 12 provides for monetary contributions of \$4,500 per residential unit, \$2,500 of which is specifically allocated toward construction of the interchange. In addition, the Applicant's phasing program facilitates the construction of on-site roadways 1A, 1B, 2A, and 2B that will serve as a Maintenance of Traffic (MOT) Road on the west side of Prince William Parkway. The Applicant has further agreed to provide an off-site access road on Property identified as GPIN 8292-04-3728 located on the east side of Prince William Parkway that will serve as a second MOT Road, should it be necessary. These two MOT roads will effectively function as alternative routes should traffic need to be rerouted through any part of the Prince William Parkway/Minnieville intersection during construction of the interchange.

Transit Center

The Dale City Small Area Plan envisions a new transit center in a location that corresponds to the Public Land (PL) designation on the land use map. This transit center is noted as a "multimodal hub" on the Future Transit Network Map. The Dale City Small Area Plan, Parkway Node, states, "The greatest intensity of mixed-use development takes place across from the proposed new transit center," and the development will include "a new transit center providing access to various points of destination throughout the region." The mobility section states, "The incorporation of a new transit center along with a new Interchange at the intersection of the Prince William Parkway and Minnieville Road is an important element to improve mobility in the study area." The Proposed Transit Network text states, "The Parkway and Minnieville Nodes are planned for OmniRide transit centers which will provide access to various locations throughout the Washington Metropolitan area."

Staff has coordinated with OmniRide, and at this time, ridership and service levels do not justify the construction of the full transit center as noted in the Dale City Small Area Plan. In lieu of providing the full transit center, Proffer 19 states the Applicant will provide up to two covered transit stop enclosures within Land Bay A. One of the enclosures is located internal to the site generally in the area noted as on the land use map as Public Land, and the other enclosure is located near the intersection of Road 2A with Minnieville Road. Page 49 of the Design Guidelines details the proposed enclosures, which are anticipated to be more substantive structures than a typical bus shelter, and

which are anticipated to include information on transit options.

Proposal's Strengths

- Level of Service Monetary Contributions: Per Proffer 12, the Applicant will make a monetary contribution in the amount of \$4,500 for each residential unit constructed on the property to be allocated to specific transportation projects. However, Proffer 13 states that the BOCS may redirect any of the above funds to other projects within the Dale City Small Area Plan at its sole discretion. The County would prefer the flexibility to use the proffered funds for any project within the Neabsco Magisterial District, and therefore recommends that the specific funding for projects be removed.
- <u>Single Point Urban Interchange (SPUI)</u>: Per Proffer 9, the Applicant agrees to provide support to the County in the planning and design of the SPUI, including the preparation of the justification report prior to the first occupancy permit on the property and preparation of 30 percent design prior to the 301st residential occupancy report.
- Phasing of Development Based on Infrastructure: Proffer 7 provides a phasing plan of the construction of specific roadways, public facilities, residential units and non-residential uses that is tied to the capacity of the roadway network/infrastructure to provide adequate capacity at each phase of the development. It provides sufficient flexibility to allow the developer with the County's consent to change the sequence of the phases if circumstances change without having to process a proffer amendment.
- <u>Electric Vehicle Charging Stations:</u> Per Proffer 21, two electric vehicle charging stations will be provided in Land Bay A, and two charging stations will be provided in Land Bay B.
- <u>Transportation Demand Management Program:</u> Per Proffer 20, a Transportation Demand Management Program will be implemented in conjunction with the proffered transit stops to promote alternative transportation modes in lieu of single occupancy vehicles.

Proposal's Weaknesses

- Conflicts of Proposed Site Access with SPUI: Some proposed intersections and access points on Prince William Parkway and Minnieville Road are within the functional area of the planned Prince William Parkway/Minnieville Road interchange or require an Access Management Exception from VDOT. The proffers must include the statement that intersections 3, 11, 12, 13 and 15 will be analyzed with the design of the interchange through the VDOT review process and are subject to modification. The MZP should also include this note.
- Waiver Requests: Proffers 68.n., 68.o., and 68.p. include various waivers/modifications of
 the Zoning Ordinance and DCSM related to roadway sections and shared use path/sidewalk
 locations relative to the public right-of-way. These waivers/modifications should be formally
 submitted to the Prince William County Department of Transportation through Land
 Development Services for review and approval. Review of these waivers/modifications
 during rezoning is necessary to determine whether they can be supported. Should the

waivers/modifications not be approved at site plan, modifications to the MZP may be necessary.

• Enforceability of Off-Site Improvements for the SPUI: Per Proffer 11, the Applicant has agreed to provide improvements off-site on property identified as GPIN 8292-04-3728, located on the east side of Prince William County Parkway. These improvements include an East Maintenance of Traffic (MOT) Road, a stormwater management facility, and a location for staging/spoilage. Since this property has not been included within the rezoning, Staff is concerned that should these issues not be clarified, the County may be required to acquire the site to use during the construction of the SPUI. Finally, the County Attorney's Office is still reviewing the issue, but has identified initial concerns regarding enforceability. At this time, Staff recommends that the acreage on which these facilities are proposed should be dedicated or reserved at no cost to the County.

On balance, this application is found to be consistent with the relevant components of the Transportation Plan.

Strategic Plan

This section of the report is intended to address the project's alignment with the outcomes provided within the County's Strategic Plan. The Strategic Plan posits that individuals, families and businesses prefer communities with a robust economy; easy access to jobs, services and activities; that support even the most vulnerable in the community; are safe and secure; and provide a quality education that assures lifelong learning and steady employment. From this analysis, the Strategic Plan Team developed seven strategic goal areas to guide Board actions: "Health, Wellbeing, and Human Services", "Safe and Secure Community", "Resilient Economy", "Quality Education and workforce development", "Environmental Conservation", "Sustainable Growth", and "Transportation and Mobility". It is important to note that no single area is viewed as more critical than another. Rather, each are interrelated and have direct impact on each other. Collectively, these goal areas impact the quality of life in all facets of the community issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The aspects of the proposal relative to the Strategic Plan are as follows:

Resilient Economy – A resilient economy for Prince William County improves the economic well-being of the community, creates and maintains jobs, expands the commercial tax base, and enhances the quality of life for residents. The proposed mixed use residential development will add to the mixture of uses onsite and will support the existing housing stock in the immediate vicinity. Therefore, the project aligns with the Strategic Plan goal to creates jobs and expand the commercial tax base. Therefore, the project aligns with the Strategic Plan goal to provide a resilient economy.

Increase commercial tax base

• The proposed rezoning is favorably aligned with the County's goal to increase the commercial tax base.

Increase at-place employment

• Increase growth in at-place employment.

Robust Economy Strategy

The development increases new investments in the area and provides increased job growth.

Materially Relevant Issues

This section of the report is intended to identify issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The materially relevant issues in this case are as follows:

None identified.

Modifications / Waivers

The following waivers and/or modifications to the requirements of the Zoning Ordinance and the DCSM are being requested and are incorporated into the proffers for this rezoning request. Waivers/modifications are as noted in Proffer 68 as followings:

Modification of Sections 32-201.18, 32-250.31.6 of the Zoning Ordinance and Section 802.47.B of the DCSM to allow a ten foot (10') foot wide landscaped area around public use and community recreation sites.

Staff can support this modification as the proposed uses are complimentary. Additional buffering around the public use site, if necessary, would be identified during a subsequent public facility review (PFR).

Waiver of Sections 32-210.11 and 32-210.12 of the Zoning Ordinance to allow mobile or modular offices for construction or sales offices to be located on a parcel for which there may not be an approved site plan or building permit.

Staff can support this waiver, which is intended to allow for temporary structures during construction and initial occupancy.

Modification Section 32-280.12.3 of the Zoning Ordinance requiring a thirty-foot (30')- setback requirement from all street rights-of-way for all principal structures and allow for a minimum ten foot (10') setback from all street right-of ways in the non-residential areas or as shown in the MZP for the residential areas.

Staff can support this modification. Zoning Ordinance Section 32-280.12.3 states, "Except in town centers, all buildings and other principal structures shall be set back at least 30 feet from all street

rights-of-ways." The proposed PMD non-residential portion of the property will function as a defacto town center, in which reduced setbacks along internal streets is encouraged.

Waiver of Sections 32-250.30, 32-250.31 and 32.250.32 of the Zoning Ordinance; and Sections 802.11 and 802.12 of the DCSM requiring internal buffers between different uses.

Staff can support this waiver as it supports the intent of an urban, mixed-use community as envisioned by the Dale City Small Area Plan.

Modification of Section 32-250.40 of the Zoning Ordinance and Sections 802.20, 21, 30 and 31 of the DCSM for tree canopy and open space requirements in the PMD District to allow calculations based upon the entire PMD area instead of each individual site PMD land bays.

Staff can support this modification, provided all other standards related to tree canopy and open space are met.

Modification of Sections 32-280.41.1, and 32-405.03.2 of the Zoning Ordinance to permit the following uses on the Property by-right: drive-in restaurant, motor vehicle fuel station, and veterinary hospital.

Staff can support this modification. Additional discussion is included in the Community Design section of this report.

Modification of Sections 1003.01 and 1003.02 of the DCSM to allow a 20' buffer against Prince William Parkway and Minnieville Road along the Land Bay A frontage, as shown on the MZP.

Staff cannot support this modification as this constitutes a reduction in the required fifty foot HCOD buffer requirements. Additional discussion is included in the Community Design section of this report.

Modification of Section 32-306.12.3 of the Zoning Ordinance to reduce the side setback, requirement for residential units as shown in the Design Guidelines.

Staff can support this modification. The side yard setbacks are generally noted in the development standards of the Design Guidelines as eight feet, which is greater than the five-foot minimum stated in the Zoning Ordinance. Staff is not aware of any objections from the Fire Marshall's office.

Modification of Sections 32-306.12.6. F, G, H, and I to allow residential units to be developed in accordance with standards set forth in the Design Guidelines, and to increase the building height for single-family attached and multifamily units in accordance with Proffer 45 above.

Staff can support this modification in part. Effectively, the Applicant is proposing to modify the PMR, Planned Mixed Residential, housing types to allow two unit types, single family attached townhouse units of varying configurations and multi-family. The modified PMR housing type development standards are noted on Pages 62 through 65 of the Design Guidelines. Staff could support the height modification for single-family attached and multifamily residential units, provided the Applicant addresses the concerns/clarifications as noted in the Community Design section of this report.

Waiver of Section 32-405.04.3 of the Zoning Ordinance to allow non-residential areas to be based on the entire PMD area and the max lot coverage shall not exceed 80%, with a minimum of 20% open space.

Staff can support this modification as it does not change the overall intent of the standard.

Modification of Section 32-405.04.4 of the Zoning Ordinance and Section 802.47.B of the DCSM to allow the perimeter buffer as shown on the MZP.

Staff cannot support this modification. The PMD, Planned Mixed-Use District, Section 32-405.04.4 requires a 50-foot Type C perimeter buffer, which would be applicable to Land Bays A and B. Although staff can support the elimination of the perimeter buffer along interior property boundaries and roadways, the MZP depicts a perimeter of twenty feet along much of Prince William Parkway and Minnieville Road, areas which are located within the Highway Corridor Overlay District (HCOD). Further, staff is uncertain why DCSM Section 802.47.B is being included, as this section refers to buffering along Community Facilities, which are largely located within the PMR portion of the property. Consolidation of these modification requests is not appropriate.

Waiver of Sections 32-401.14.5, 32-401.24.7, 32-402.14.5,32-402.34.5 and 32-402.44.5 of the Zoning Ordinance to allow a 75' maximum building height in the B-1, B-2, O(M), O(L) and O(F) underlying zoning districts.

Staff can support this waiver. This waiver is applicable to Land Bays A and B, which are proposed to be rezoned to the PMD, Planned Mixed-Use District, and which are referred to as the Urban District. These areas are noted by the Dale City Small Area Plan as transects T-4 and T-5, which would anticipate building heights 4 to 8 stories and 6 to 12 stories, respectively. A height of 75-feet would be consistent with these transects.

Modification of Section 32-250.28 of the Zoning Ordinance to allow the location and number of Project Identification signs and Multitenant sings as shown on the Street Network & Signage Plan.

Staff can support this waiver. Specific analysis is included in the Community Design section of this report.

Modification of Section 601.04 and Details UB-1, and UAS-1 of the DCSM to allow standard street section and details as shown on the MZP which allow for the eight foot (8') Private Shared Use Path to be located outside of the ROW.

Staff defers to transportation on this modification.

Modification of Sections 602.04, 602.07, 610.01, 602.14, 602.22, 650.00 and 1003.04 of the DCSM requirements for roadway sections with respect to right-of-way width, design speed, intersection alignments, tangent distance, lane width, median width, bike lane width, maximum grade, centerline radius, off- street parking requirements, intersection curb returns, intersection design, entrance/crossover spacing, turn lanes, channelization, location, and materials.

Staff defers to transportation on this modification.

Waiver of Section 602.18 of the DCSM to the general sidewalk/trail requirements; specifically, the requirement to provide sidewalks or trails within the street right-of-way of all proposed streets within urban section streets.

Staff defers to transportation on this modification.

Waiver of Section 32-250.31 of the Zoning Ordinance and Sections 702.06(F), 702.06(I), and 802.42(A)(3) of the DSCM to modify the standard to allow for minor permanent structures like mailboxes or signage, unauthorized obstructions, and encroachments within easements as well as to allow for easements to be located within buffer areas.

Staff cannot support this modification, as the specific applicable of this waiver is unknown, and it appears to facilitate unnecessary encroachments into the buffers that would otherwise be prohibited. In addition, it appears this waiver would further compromise the already reduced HCOD buffers along Prince William Parkway and Minnieville Road.

Waiver of Sections 802.42 and 802.43 of the DCSM of the 10 foot (10') wide landscape strips along right-of-way and perimeter parking lots.

Staff cannot support this modification. Surface parking is an inevitable characteristic in early stages of mixed-use development, and as such, staff does not object to its inclusion. That being said, the impacts of surface parking should be mitigated and should be subject to the same perimeter parking lot requirements as would be expected outside of a mixed-use setting.

Waiver of Section 802.47.A and Table 8-6 of the DCSM of the DCSM to allow, within the Urban District and East Village areas, in lieu of the on-lot tree requirement the Applicant shall plant, and landscape in accordance with Proffer #49.e above.

Staff can support this modification, provided there is a clear commitment that tree canopy requirements of the DCSM would otherwise be met, AND there is a clear commitment that street trees will be provided as part of the streetscape along public and private streets within the affected areas.

Proffer Issues / Deficiencies

There are a multitude of comments from the County Attorney's Office specific to the proffers. Although many of these comments are for clarification purposes, there are many significant issues that have been identified, including the public use site dedication proffer, the off-site improvements in support of the SPUI, the use restrictions on the public use site, commitments made to benefit the Cedar Run HOA and the Central Baptist Church of Woodbridge, the ADU/WDU proffer, among others. Staff is not enumerating all of these revisions herein but will do so prior to the BOCS if the concerns are not adequately addressed by the Applicant.

Agency Comments

The following agencies have reviewed the proposal and their comments have been summarized in relevant comprehensive plan chapters of this report. Individual comments are in the case file in the Planning Office:

- PWC Archaeologist
- PWC Building Official
- PWC Economic Development
- PWC Fire Marshal Office
- PWC Historical Commission
- PWC Housing & Community Development
- PWC Libraries
- PWC Planning Office Case Manager / Long-Range Planning / Proffer Administration
- PWC Police / Crime Prevention
- PWC Public Works Environmental Services / Watershed Management
- PWC Schools
- PWC Service Authority
- PWC Transportation
- Virginia Department of Transportation (VDOT)

Attachment C

HISTORICAL COMMISSION RESOLUTION

MOTION: HENSON August 10, 2021

Regular Meeting

SECOND: JOHNSON Res. No. 21-040

RE: LAND DEVELOPMENT RECOMMENDATIONS

ACTION: APPROVED

WHEREAS, the Prince William County Historical Commission seeks to identify, preserve and protect historic sites and structures in Prince William County; and

WHEREAS, the Prince William County Historical Commission's review of pending land development applications assists in determining the necessity for cultural resource surveys and other research and evaluations; and

WHEREAS, the Prince William County Historical Commission believes that the identification, preservation and protection of historic sites and structures throughout Prince William County is well served by this action;

NOW, THEREFORE, BE IT RESOLVED, that the Prince William County Historical Commission does hereby recommend to the Prince William County Planning Commission the action(s) noted for the following properties:

<u>Case Number</u>	<u>Name</u>	Recommendation
REZ2019-00018	Quartz District	Phase II evaluation on archaeology site 44PW2033, and if warranted, a Phase III study. All artifacts from all archaeology sites to be donated to and curated with the County.
REZ2021-00003	John Marshall Commons Tech Park – 4 th Submission	Request Applicant revise The Phase I study (as requested previously) and submit revised Phase I for review as soon as possible.
SUP2021-00004	John Marshall Commons Tech Park – 4 th Submission	No Further Work
REZ2017-00008	Purcell Road Rezoning – 3 rd Submission	No Further Work

Case Number	<u>Name</u>	<u>Recommendation</u>
REZ2016-00026	Marumsco Assemblage – 2 nd Submission	No Further Work
REZ2021-00019	Compton Property	Request project area history, additional information about the headstones found in the creek and, in the absence of an architectural study, preservation of the agricultural farm complex.
SUP2021-00030	Compton Property	No Further Work
REZ2021-00021	Richmond Station Landbay C Proffer Amendment	No Further Work
REZ2021-00022	KH Data Capital Development	No Further Work
PFR2021-00026	NOVEC Substation	No Further Work
REZ2021-00023	Raising Cane's Drive-Through Proffer Amendment	No Further Work
SUP2021-00033	Raising Cane's Restaurant Drive-Through	No Further Work
REZ2021-00020	John Marshall Village Addition	Request applicant fabricate and install an historical marker at the property, on the subject of horse racing in Haymarket, with content developed by the Historical Commission and the Planning Office.
SUP2022-00003	John Marshall Village Addition	No Further Work
REZ2021-00024	USA Self-Storage at Old Dominion Drive	No Further Work
SUP2021-00034	USA Self-Storage at Old Dominion Drive	No Further Work

August 10, 2021 Regular Meeting Res. No. 21-040 Page 3

<u>Case Number</u>	<u>Name</u>	<u>Recommendation</u>
PFR2021-00013	Wakeman Substation	No Further Work
SUP2022-00004	Chick-Fil-A on Pine Bluff Drive	No Further Work

Votes:

Ayes: by acclamation

Nays: None

Absent from Vote: None **Absent from Meeting:** Duley

MOTION CARRIED

ATTEST: _____

Secretary to the Commission

Attachment D

QUARTZ DISTRICT RESIDENTIAL DEVELOPMENT BUCHANAN PARTNERS PRINCE WILLIAM COUNTY, VA

PROFFER JUSTIFICATION NARRATIVE

MAY 13, 2021

PREPARED BY:



QUARTZ DISTRICT RESIDENTIAL DEVELOPMENT BUCHANAN PARTNERS PRINCE WILLIAM COUNTY, VA

PROFFER JUSTIFICATION NARRATIVE

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Introduction

The purpose of this document is to update the previously submitted SB 549 Proffer Justification Narrative dated October 15, 2018 by MuniCap, Inc. in order to respond to questions and comments received January 16, 2019 from Prince William County, Virginia (the "County") staff.

The purpose of this *Proffer Impact Analysis* is to satisfy portions of the County's requirements as they relate to the 2016 legislation (as subsequently described, and as subsequently amended) for the proposed residential component of the Quartz District Residential Development (the "Residential Development"). More specifically, this document addresses legislative requirements and County policy related to "proffers" that the applicant has elected to propose with the Residential Development rezoning.

LEGISLATION PERTAINING TO RESIDENTIAL PROFFERS

Section 15.2-2303.4 of the Code of Virginia (the "Residential Proffer Legislation"), as it was amended effective July 1, 2019, places certain limitations on proffers for residential rezoning cases filed after July 1, 2016, or July 1, 2019. As stipulated by the Residential Proffer Legislation, and unless an applicant elects to apply for a rezoning pursuant to Subsection D of that statute, a local government may only request or accept a proffer if it addresses an impact that is specifically attributable to a proposed new residential development, and, if it is an offsite proffer, it addresses an impact to an offsite public facility, such that (a) (i) the new residential development creates a need, or an identifiable portion of a need, for one or more public facility improvements in excess of existing public facility capacity at the time of the rezoning, and (b) (ii) each such new residential development applied for receives a direct and material benefit from a proffer made with respect to any such public facility improvements. For the purposes of the statute, a locality may base its assessment of public facility capacity on the projected impacts specifically attributable to the new residential development.

The Residential Proffer Legislation designates four categories of public improvements and facilities, which are as follows:

- Public school facility improvements: construction of new primary and secondary public schools or expansion of existing primary and secondary schools, to include all buildings, structures, parking, and other costs directly related thereto;
- Public safety facility improvements: construction of new law enforcement, fire, emergency, medical, and rescue facilities or expansion of existing public facilities, to include all buildings, structures, parking and other costs directly related thereto;
- Public park facility improvements: construction of public parks or improvements and/or
 expansion of existing public parks, with "public parks" including playgrounds and other
 recreational facilities; and
- Public transportation facility improvements: construction of new roads; improvement or expansion of existing roads and related appurtenances as required by applicable standards of

the Virginia Department of Transportation, or the applicable standards of a locality; and construction, improvement, or expansion of buildings, structures, parking, and other costs directly related to transit.

According to the statute, expenses of an existing public facility, such as ordinary maintenance or repair, or any capital improvement to an existing public facility, such as a renovation or technology upgrade, that does not expand the capacity of such facility shall be excluded. In addition, all proffers will be deemed unreasonable unless the proffer addresses an impact to public facilities that is specifically attributable to the proposed residential development and for which there will not be adequate existing capacity for the proposed residential development.

This document addresses the projected impact of the Residential Development on three of the foregoing infrastructure categories to which residential proffers may be directed. A separate traffic impact analysis (the "Traffic Impact Analysis") will be prepared that addresses impacts to public transportation facility improvements.

PROFFER JUSTIFICATION NARRATIVE REQUIREMENT

In response to the Residential Proffer Legislation, the County adopted policies to ensure any proffer requested or accepted meets the standards mandated by it. Among them is the requirement that any residential rezoning or proffer amendment application subject to the residential proffer legislation include a justification narrative identifying impacts to public facility improvements. The requirement further states that the justification narrative must, in detail:

- Identify all of the impacts of the proposed rezoning/proffer amendment;
- Propose specific and detailed mitigation strategies and measures to address all of the impacts of the proposed rezoning/proffer amendment;
- Address whether all of the mitigation strategies and measures are consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation; and
- Demonstrate the sufficiency and validity of those mitigation strategies using professional best accepted practices and criteria, including all data, records, and information used by the applicant or its employees or agents in identifying any impacts and developing any proposed mitigation strategies and measures.

This document focuses on the identification of potential impacts to public facility improvements resulting from the proposed Residential Development.

Subsequent sections of this document provide a detailed description of the Residential Development and its potential impacts on public facility improvements. This document also provides a detailed explanation of the methodology employed in calculating these impacts.

II. The Quartz District Residential Development

THE RESIDENTIAL DEVELOPMENT

As proposed by Buchanan Partners (the "Developer," or the "Applicant"), the Residential Development consists of 610 for-sale single-family attached townhome units, 180 condominiums units, and 225 multifamily units on an approximately 145.14-acre site within the Neabsco Magisterial District in the County. The Residential Development is part of a larger, mixed-use development, the plans for which also include approximately 285,000 square feet of office, commercial and retail space. The County defines condominium units as multi-family homes; therefore, for purposes of this analysis 610 single family attached townhomes and 405 multi-family units are analyzed.

The site of the proposed Residential Development is located on the west side of Prince William Parkway and north of Minnieville Road. The site includes the following GPINs:

8192-93-1868

8192-74-5435

8192-83-4108

The Residential Development site currently includes three different zoning classifications. The maximum residential development allowed "by-right" under current zoning is estimated to be 125.47 single-family detached units. Table II below shows the how the total number of by-right units were estimated.

TABLE II
By-right residential unit calculation

Parcels	Zoning	Acres	Dwelling Units Per Acre	Units Allowed
8192-93-1868				
	A-1	26.48	0.10	2.65
	О-Н	32.00	0.00	0.00
	R-2	60.82	2.00	121.64
8192-74-5435	A-1	11.85	0.10	1.18
8192-83-4108	О-Н	14.00	0.00	0.00
Total		145.14		125.47

Accordingly, impacts herein are estimated for those units net of the 125.47 "by-right" single-family detached units.

As shown in Table II, the site is currently zoned as A-1 (Agricultural, which allows one detached single-family unit per ten acres), O-H (Office High-Rise, which allows for high-rise offices and institutional uses), and R-2 (Suburban Residential, which allows two detached single-family dwellings per acre). According to County Assessor records, existing improvements to the site include an office building, a parking lot, and two vacant incidental structures. All existing improvements will be demolished to facilitate the proposed development.

The Applicant is requesting a rezoning of 91.63 acres to PMR (Planned Mixed Residential), which, in accordance with the County Comprehensive Plan, is designed to permit and encourage the establishment of communities of varied housing types in planned developments. The planned single-family attached and multi-family units will be located within the area designated as PMR. The Applicant requests the remaining acreage to be rezoned as PMD (Planned Mixed Development), which allows a mix of nonresidential and residential development. Single-family attached and multi-family units will be located in the area designated as PMD.

The site (see Exhibit A) is primarily designated as Parks and Open Space Active (POSA), Suburban Neighborhood (SN), High-Density Neighborhood (HDN), Public Land (PL), and Community Mixed-Use (CMU) on the County Comprehensive Plan Long-Range Land Use Map. The property is also part of the Dale City Small Area Plan.

EXHIBIT A: QUARTZ DISTRICT RESIDENTIAL DEVELOPMENT SITE PLAN



III. Public Facility Improvement Impacts

OVERVIEW

As mentioned, this document includes a calculation of public facility impacts, which are detailed in the subsequent sub-sections. Included in each section is a discussion of the methodology employed in estimating impacts. The included subsections are as follows:

- Public school facility improvements In keeping with County practices, separate impacts are calculated for elementary, middle, and high schools, and are based on projected incremental additional students that will result from the Residential Development.
- Public safety facility improvements In keeping with County practices, impacts are calculated for both police services and fire and rescue services; impacts are based on projected incremental additional residents that will result from the Residential Development.
- Public park facility improvements Impacts are based on projected incremental additional residents that will result from the Residential Development.

Public transportation facility improvements will be addressed in the Traffic Impact Analysis.

It should be noted that level of service ("LOS") standards shown herein represent the County standards as described in the County Comprehensive Plan and the Dale City Small Area Plan. In some cases, the current LOS actually provided in the County does not meet the stated LOS standard. Any calculation of proffers will take into account the LOS standard as set out in the Comprehensive Plan and the Dale City Small Area Plan, the current County LOS, and the amount pledged in the County's Capital Improvement Plan to raise the current County LOS to meet the planned LOS standard.

III-A. Public School Facility Improvement Impacts

METHODOLOGY

To project impacts to public school facility improvements, MuniCap first researched the student generation factors used by Prince William County Public Schools. These factors are calculated separately by school type (elementary, middle, and high school) and by unit type (single-family, townhouse, and multi-family). Current 2021 and historical student generation factors are shown below in Table III-A.1.

TABLE III-A.1
Current and Historical Student Generation Factors

					Historic	cal Data					
	School Type	Single Family	Unit Type Townhouse	Multi-Family	Total		School Type	Single Family	Unit Type Townhouse	Multi-Family	Total
2020-21	Elementary	0.273	0.278	0.160	0.251	2014-15	Elementary	0.294	0.285	0.176	0.272
2020-21	Middle	0.159	0.143	0.073	0.138	2014-10	Middle	0.156	0.128	0.070	0.134
	High	0.228	0.187	0.087	0.189		High	0.206	0.155	0.085	0.172
	Total	0.660	0.608	0.320	0.578		Total	0.656	0.569	0.331	0.578
	School Type	Single Family	Unit Type Townhouse	Multi-Family	Total		School Type	Single Family	Unit Type Townhouse	Multi-Family	Total
2019-20	Elementary	0.292	0.289	0.172	0.267		Elementary	0.302	0.287	0.184	0.279
2019-20	Middle	0.164	0.145	0.076	0.142	2012-13	Middle	0.156	0.120	0.075	0.133
	High	0.230	0.185	0.088	0.189		High	0.205	0.147	0.083	0.169
	Total	0.686	0.619	0.336	0.598	l i	Total	0.662	0.554	0.342	0.582
	School Type	Single Family	Unit Type Townhouse	Multi-Family	Total		School Type	Single Family	Unit Type Townhouse	Multi-Family	Total
	Elementary	0.292	0.288	0.175	0.268		Elementary	0.301	0.258	0.167	0.268
2018-19	Middle	0.163	0.144	0.075	0.140	2010-11	Middle	0.152	0.111	0.067	0.127
	High	0.224	0.179	0.085	0.185		High	0.202	0.139	0.072	0.164
	Total	0.680	0.611	0.335	0.592		Total	0.655	0.509	0.306	0.560
	School Type	Single Fee 7	Unit Type	Made For 2	Total		School Type	Single Fee 7	Unit Type	Made Force	Total
	Elementary	Single Family 0.300	Townhouse 0.313	Multi-Family 0.163	0.273		Elementary	Single Family 0.298	Townhouse 0.245	Multi-Family 0.142	0.258
2016-17	Middle	0.300	0.145	0.163	0.273	2008-09	Middle	0.290	0.107	0.055	0.122
	High	0.220	0.184	0.082	0.181		High	0.206	0.139	0.069	0.166
	Total	0.679	0.642 ¹	0.309 ¹	0.590		Total	0.652	0.491	0.265	0.546

Source: Prince William County Public Schools 2020-21 Student Generation Factors

MuniCap then applied these student generation factors to the proposed units within the Residential Development that are in excess of the development that would be allowed under the current zoning designation. For purposes of this exercise, it is assumed that all of the projected students are new to the County, rather than relocated from elsewhere within the Prince William County Public Schools system.

Finally, MuniCap identified the schools that will be impacted by the Residential Development based on school boundaries and researched the current capacity at each applicable school. MuniCap then determined whether the projected net student impacts represented a burden beyond current school capacity.

PROJECTED NET STUDENT IMPACTS

As previously described, the Residential Development includes 610 single-family attached ("townhome") units and 405 multi-family units. Based on projected development and the student generation factors identified in Table III-A.1, the proposed development will generate an estimated 501 total students, 83 of whom are estimated to be generated by-right. As shown in Table III-A.2 on the following page, the Residential development is estimated to create 418 new students, which is the total estimated number of students generated less the estimated number of by-right students.

¹ For purposes of this analysis, for-sale condominium units are treated as multi-family units.

TABLE III-A,2
Projected Student Generation – Quartz District Residential Development

School Type	Units(a)	Unit Type	Generation Factor(b)	Total Projected Students
Proposed Resider	ntial Developr	nent		
Elementary	610	Townhome	0.278	169.58
Middle	610	Townhome	0.143	87.23
High	610	Townhome	0.187	114.07
Subtotal - Townhor	me:			370.88
Elementary	405	Multi-family	0.160	64.80
Middle	405	Multi-family	0.073	29.57
High	405	Multi-family	0.087	35.24
Subtotal - Multi-fa	mily:			129.60
Elementary	1,015			234.38
Middle	1,015			116.80
High	1,015			149.31
Total propose	d			500.48
By-right units				
Elementary	125	Single-family detached	0.273	(34.13)
Middle	125	Single-family detached	0.159	(19.88)
High	125	Single-family detached	0.228	(28.50)
Less: Total-by	-right			(82.50)
Elementary	1,015			200.26
Middle	1,015			96.92
High	1,015			120.81
Net students				417.98
(a) Source: Buchanan				
(b) See Table III-A.1.				

CURRENT CAPACITY OF PUBLIC SCHOOL FACILITIES

The public school facilities potentially impacted by the Residential Development are: Jenkins Elementary School, Beville Middle School, and Gar-Field High School. Table III-A.3 on the following page shows the current capacity and enrollment at each school.

TABLE III-A.3
County School Facilities – Current Capacity and Enrollment

School	Capacity ^(a)	Current Enrollment(a)	Excess Capacity
Jenkins Elementary School	662	582	80
Beville Middle School	1,191	1,072	119
Gar-Field High School	2,839	2,280	559
Total	4,692	3,934	758
(a)Source: Prince William County		ools 2020-21 Of	ficial Student
Enrollment as of September 30, 2020)		

Elementary School Facilities

The Residential Development site is located within the John D. Jenkins Elementary School boundaries (see Exhibit C). According to Prince William County Public Schools, the school has a planning capacity of 662 students and a current enrollment of 582 students, meaning that the school has unused capacity for 80 additional students. Therefore, 120 of the 200 projected elementary school students that will be created by the Residential Development are in excess of existing capacity and represent an additional need for Prince William County Public Schools facilities.

Middle School Facilities

The Residential Development site is located within the Beville Middle School boundaries (see Exhibit D). According to Prince William County Public Schools, the school has a planning capacity of 1,191 students and a current enrollment of 1,072 students, meaning that the school has unused capacity for 119 additional students. Therefore, the 97 projected middle school students that will be created by the Residential Development do not exceed projected capacity and do not represent an additional need for Prince William County Public School facilities.

High School Facilities

The Residential Development site is located within the Gar-Field High School boundaries (see Exhibit E). According to Prince William County Public Schools, the school has a planning capacity of 2,839 students and a current enrollment of 2,280 students, meaning that the school has unused capacity for 559 additional students. Therefore, the 121 projected high school students that will be created by the Residential Development do not exceed projected capacity and do not represent an additional need for Prince William County Public Schools facilities.

EXHIBIT B: AREA MAP (DEVELOPMENT SITE & SCHOOL FACILITIES)

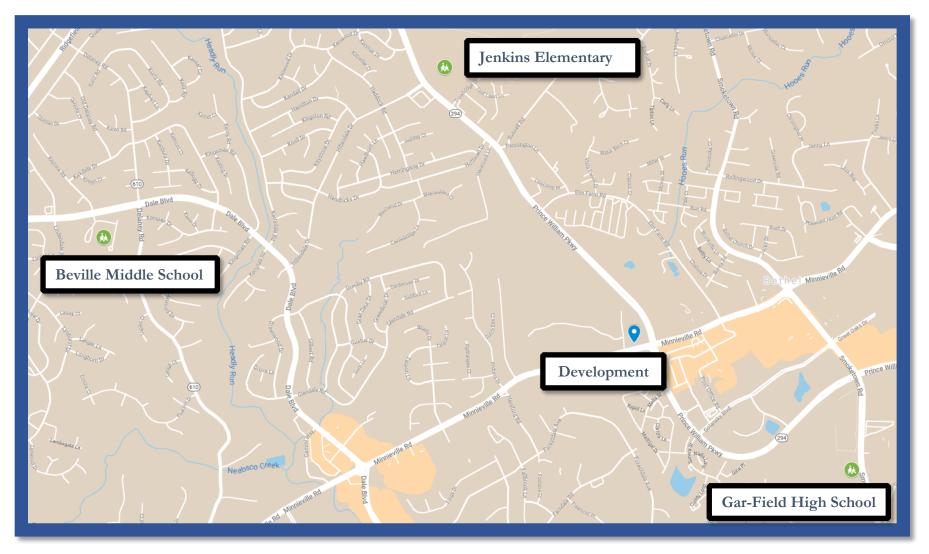


EXHIBIT C: AREA MAP (DEVELOPMENT SITE, JOHN D. JENKINS ELEMENTARY SCHOOL)

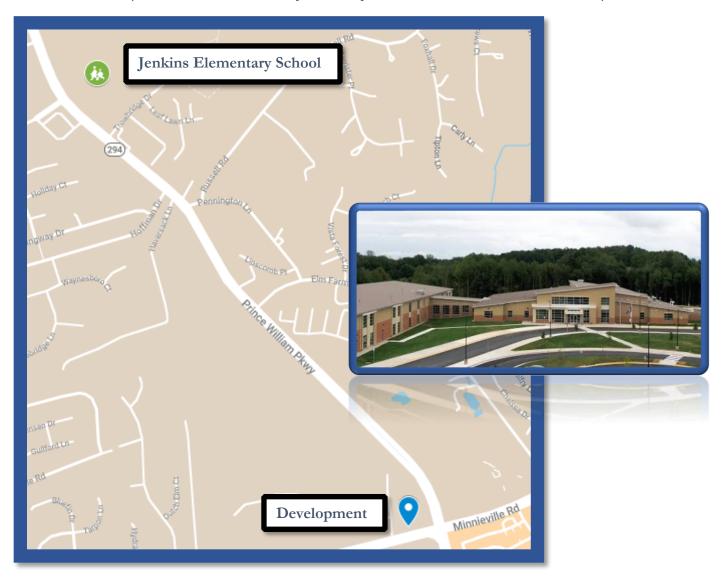


EXHIBIT D: AREA MAP (DEVELOPMENT SITE, BEVILLE MIDDLE SCHOOL)

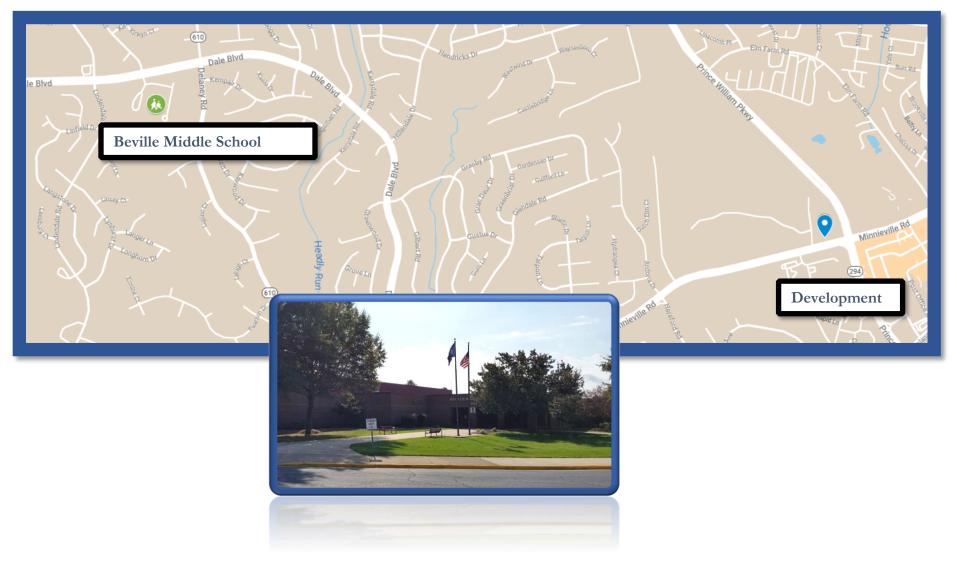
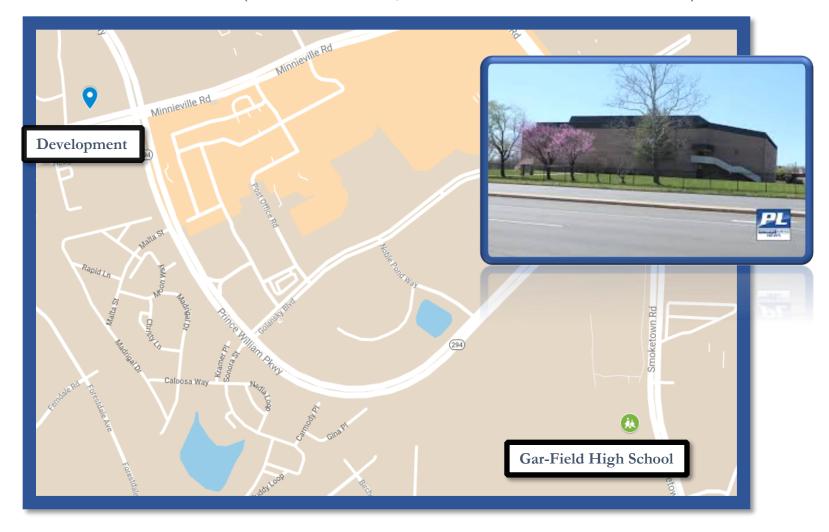


EXHIBIT E: AREA MAP (DEVELOPMENT SITE, GAR-FIELD HIGH SCHOOLHIGH SCHOOL)



MITIGATION STRATEGIES

The Residential Proffer Legislation stipulates that proffers can only address needs exceeding existing capacity. Therefor, any monetary proffer for public school facility improvements must be calculated on a per student basis for the projected students that will exceed the current capacity.

As previously mentioned, the projected elementary school students resulting from the Residential Development are in excess of existing capacity for the relevant elementary facility. The Residential Development includes the dedication of twelve acres for the development of a school site. Accordingly, the estimated cost of public school facilities resulting from an additional 120 elementary students and after taking into consideration dedication of land is shown in Table III-A.4 on the following page.

The projected middle school and high school students resulting from the Residential Development are within existing capacity. As a result, no proffers are necessary.

TABLE III-A.4 Projected School District Impact

School Impact for Proposed Zoning Reclassification	
Elementary School	
(a) Total student capacity at new school ^(a)	657
(b) Approximate construction cost (per school) ^(a)	\$38,832,000
(c) Facility cost per capita (b ÷ a)	\$59,105
(d) Projected students at Residential Development after by-right allocation(b)	200.26
(e) Elementary school excess capacity ^(c)	80.00
(f) Projected students above school capacity (d - e)	120.26
(g) Sub-total: elementary school proffer contribution for Residential Development (c × f)	\$7,107,675
(h) Developer site contribution ^(d) (i) Net proffer contribution	(\$6,780,000) \$327,675
(j) Total Residential Development proffer contribution: public school facilities (i)	\$327,675
(k) Percentage of total students generated: townhome units	74%
(l) Percentage of total students generated: multi-family units	26%
Proffer contribution: per townhome unit (j \times k \div 610)	\$398
Proffer contribution: per multi-family unit (j × 1 ÷ 405)	\$210

⁽a) Source: Prince William County Public Schools Capital Improvements Program Fiscal Years 2021-30. Based on Occoquan/Woodbridge Area Elementary School.

⁽b) See Table III-A.2.

⁽c) See Table III-A.3.

⁽d) Source: Assessed value of land provided by Buchanan Partners.

III-B. Public Safety Facility Improvement Impacts

METHODOLOGY

To estimate impacts to public safety facilities, MuniCap first estimated the total population that will reside within the proposed Residential Development based on residential unit generation factor from the Prince William County Geographic Information Systems Demographics Quarterly Estimates as of December 31, 2020.

MuniCap then applied the LOS standards for various public safety services as identified in the County Comprehensive Plan, approved June 18, 2019, to calculate the impact of the Residential Development on public safety services. MuniCap then compared the existing capacity at the relevant public safety facilities to the forecasted increase in required services resulting from the proposed development and determined whether the projected demand exceeded current capacity.

PROJECTED NET RESIDENT IMPACTS

As previously described, the Residential Development includes 610 for-sale single-family attached townhome units and 405 multi-family units. Based on projected development and the average occupancy of residential units in the County, the proposed development will house an estimated 2,447 residents above by-right, as shown below in Table III-B.1.

TABLE III-B.1

Projected Residents – Quartz District Residential Development

Unit Type	Units ^(a)	Residents Per Unit ^(b)	Total Projected Residents
(a) Townhome units ^(a)	610	3.16	1,928
(b) Multi-family units	405	2.32	940
(b) Less single-family detached units allowed by-right ^(b)	125	3.37	(421)
Total: Above by-right (a-b)	890	2.75	2,447

⁽a) Source: Buchanan Partners

CURRENT CAPACITY OF PUBLIC SAFETY FACILITIES

Police Facilities

The County LOS standards for police work suggest two sworn officers per 1,000 residents. In addition, the facility requirements for the Prince William County Police Department are 250 square feet per sworn officer with a building minimum size of 50,000 square feet. Therefore, the projected

⁽b) Residential unit generation factors. Source: Prince William County Geographic Information Systems Demographics Quarterly Estimates (as of 12/31/2020).

impact created by the additional 2,447 residents estimated for the Residential Development is 1,223 square feet, as shown below in Table III-B.2.

TABLE III-B.2
Projected Police Station Facility Impacts

Projected Resident Impact ^(a)	Officers Per 1,000 Residents ^(b)	Officer Requirement ^(b)	Facility Sq. Ft. Per Officer ^(b)	Additional Facility Sq. Ft. Requirement
2,447	2	4.8935	250	1,223
(a) See Table III-B.1. (b) Source: Prince William County Comprehensive Plan Safety and Secure Community.				

The project site is within Neabsco Magisterial District, which is serviced by the Central District Police Station (see Exhibit G). According to the Prince William County Police Department, there are currently 121 sworn officers employed at the Central District Police Station. Based on Prince William County Assessor records, the station is 33,651 square feet, implying a capacity to accommodate 135 officers (33,651 total square feet ÷ 250 feet per officer). This means that the station currently has the excess capacity for 14 additional sworn officers, representing 3,500 square feet of facility space (14 officers × 250 square feet). Therefore, the projected residents associated with the Residential Development are not anticipated to place demands on police station facilities in excess of current capacity.

County LOS standards for police facilities also include requirements for animal control, training, and administrative support facilities. The projected demand created by the Residential Development is shown below in Table III-B.3.

TABLE III-B.3
Other Projected Police Facility Impacts

Facility Type	Projected Resident Impact ^(a)	Sq. Ft. Required per 1,000 Residents ^(b)	Additional Facility Sq. Ft. Requirement
Animal control	2,447	67	164
Training	2,447	324	793
Administrative support	2,447	274	670

⁽a) See Table III-B.1.

The County LOS standard for animal control facilities is 67 square feet per 1,000 residents. According to the Prince William County Geographic Information Systems Demographics Quarterly Estimates, the total population of Prince William County is estimated at 469,236 people as of December 31, 2020. This translates to a need for 31,439 square feet of animal control facility space (67 square feet per thousand residents × 469.24 thousand residents). Based on County Assessor data, the existing Prince William County Animal Shelter includes 8,032 square feet of animal control facility space, implying

⁽b) Source: Prince William County Comprehensive Plan Safety and Secure Community.

that the shelter is already over capacity and cannot accommodate any additional demand. Therefore, the projected impact of 164 square feet in necessary animal control facility space that will be generated by the Residential Development represents a requirement in excess of current capacity. According to the County Capital Improvement Program for Fiscal Years 2021 – 26, there is currently a project in place to renovate the animal control facility, expanding it to 25,760 square feet of space. The estimated costs of this expansion were used as the basis of cost for impacts as described in Table III-B.6.

The County LOS standard for police training facilities is 324 square feet per 1,000 residents. Therefore, the County's current population of 469,236 creates a need for 152,032 square feet of police training facility space (324 square feet per thousand residents × 469.24 thousand residents). Based on County Assessor data, the existing County Public Safety Training Center includes 54,651 square feet of space. This suggests that existing police training facility space is inadequate and cannot accommodate any additional demand. Therefore, the projected impact of 793 square feet in necessary police training facility space that will be generated by the Residential Development represents a requirement in excess of current capacity. According to the Capital Improvement Program for Fiscal Years 2021 – 26, there is currently a project in place to create a Master Plan for the Training Center expansion. Impact costs were estimated based on a shared portion of the cost of this project as described in Table III-B.6.

The County LOS standard for police administrative support facilities is 274 square feet per 1,000 residents. Therefore, the County's current population of 469,236 creates a need for 128,571 square feet of police administrative facility space (274 square feet per thousand residents × 469.24 thousand residents). At the time of this writing, total existing administrative support facility space was unavailable, as much of this space is leased and not consolidated with other Police Department operations. The Applicant will coordinate with appropriate County staff to determine whether the projected impact of 670 square feet in necessary police administrative support facility space that will be generated by the Residential Development represents a requirement that exceeds current capacity.

EXHIBIT G: AREA MAP (DEVELOPMENT SITE & POLICE DEPARTMENT FACILITY)



Fire and Rescue Facilities

The County LOS standards for fire and rescue facilities servicing residential development is broken down into incident capacity and travel times. Table III-B.4 summarizes the LOS standards according to the County Comprehensive Plan.

TABLE III-B.4
Prince William County Fire and Rescue Level of Service Standards

A. Travel Times

Area	First Unit Travel Time in Minutes
Fire Suppression Emergency Standard - (Countywide)	4.0
Basic Life Support (BLS) Emergency Standard - (Countywide)	4.0
Advanced Life Support (ALS) Emergency Standard (Countywide)	8.0
(a) Source: Prince William County Comprehensive Plan Safety and Secure Comm	unity Table 4.

B. Workload

Factor	Standard
Responses per Tactical Unit	2,000 per year
(a) Source: Prince William County Comp Community Table 5.	rehensive Plan Safety and Secure

According to the County Fire Department, the fire and rescue facility that primarily serves the Residential Development's location is Station 20, located less than two miles away and estimated to be a travel time of less than four minutes.

TABLE III-B.5
Projected Fire and Rescue Facility Impacts

Projected Resident Impact (a)	Average Annual Incident Rate (b)	Projected Annual Increase
2,447	0.23	555
(a) See Table III-B.1.		
(b) See Table III-C.3.		

According to the County Assessor, Station 20, constructed in 2002, consists of 9.5 acres and 23,181 building square feet and supports five tactical units. Based on County LOS standards, this implies that the station can service 10,000 incidents per year. According to the County department of Fire and Rescue, in calendar year 2018, Station 20 served 10,572 residents and answered 2,398 calls, which

suggests a call volume per resident of 0.23 (calculated as 2,398 calls \div 10,572 residents) . This implies that Station 20 has the capacity to accommodate an additional 7,602 calls per year (10,000 – 2,398 = 7,602). Therefore, the projected impact of 555 calls per year that will be generated by the Residential Development can be accommodated by existing excess capacity. Table III-B.5 on the previous page shows the projected annual increase of call volume.

EXHIBIT H: AREA MAP (DEVELOPMENT SITE & DALE CITY FIRE STATION #20 FACILITY)



MITIGATION STRATEGIES

The excess capacities at the Central District Police Station exceed the projected increase in service demand by the Residential Development, any proffer related to police station facility costs would be inappropriate under the Residential Proffer Legislation. In addition, the County Capital Improvement Plan does not include any capital improvements to Police Administrative facilities that increase capacity in the Residential Development's service area. Since the County Capital Improvement Plan does not include any capital improvements to increase capacity, a proffer related to Police Administrative facilities would be inappropriate under the Residential Proffer Legislation.

The excess capacities at the Dale City Fire Station 20 exceed the projected increase in service demand by the Residential Development, any proffer related to fire and rescue facility costs would be inappropriate under the Residential Proffer Legislation.

Proffers for eligible public safety facilities are calculated in accordance with County LOS standards and shown on the following page in Table III-B.6. The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable law, including, but not limited to, the Residential Proffer Legislation.

TABLE III-B.6 Proffer Estimates - Projected Public Safety Facility Impacts

Public Safety Impact for the Proposed Zoning Reclassification	
Police - Animal Control	
(a) Current population of Prince William County ^(a)	469,236
(b) Projected residents above by-right. ^(b)	2,447
(c) Total projected County population (a + b)	471,683
(d) Total cost of Animal Shelter Expansion/Renovation(c)	\$5,370,000
(e) Building cost per capita (d÷ c)	\$11.38
(f) Animal Control Proffer required for Residential Development (e × b)	\$27,855.69
Police - Training	
(g) Current population of Prince William County ^(a)	469,236
(h) Projected residents above by-right.(b)	2,447
(i) Total projected County population above by-right (g + h)	471,683
(j) Total cost of Public Safety Training Center Master Plan(c)	\$35,800,000
(k) Project cost per capita (j ÷ i)	\$75.90
(l) Training Proffer required for Residential Development (k × h)	\$185,704.59
Estimated Cost Per Home	
(m) Gross cost per capita (e + k)	\$87.28
(n) Residents in development above by-right (b)	2,447
(o) Total proffer for development in units in excess of by-right (m × n)	\$213,560.27
(p) Percentage of total residents generated: townhome units	67%
(q) Percentage of total residents generated: multi-family units	33%
Proffer contribution: Per townhome unit (u × v ÷ 610)	\$235.35
Proffer contribution: Per multi-family unit (u × w ÷ 405)	\$172.83

⁽a) Source: Prince William County Geographic Information Systems Demographics Quarterly Estimates (as of 12/31/2020).

⁽b) See Table III-B.1.

⁽c) Source: Prince William County Capital Improvements Program Fiscal Years 2021-26.

III-C. Public Parks Facility Improvement Impacts

METHODOLOGY

MuniCap applied the LOS standards for public parks as identified in the County Comprehensive Plan – (Parks Recreation and Tourism approved March 10, 2020) to calculate the impact of the Residential Development on public parks services. MuniCap then compared the existing capacity at the relevant public parks facilities to the forecasted increase in required services resulting from the proposed development and determined whether the projected demand exceeded current capacity.

PROJECTED NET RESIDENT IMPACTS

As previously described, the Residential Development includes 610 for-sale single-family attached townhome units and 405 multi-family units. Based on projected development and the average occupancy of residential units in the County, the proposed development will house an estimated 2,447 residents above by-right, as shown in Table III-B.1 of the preceding section.

CURRENT CAPACITY OF PUBLIC PARKS FACILITIES

Based on the County's established Park Planning Districts, the Residential Development falls within Park Planning District 9. In order to show that the Residential Development's impacts on the parks system, service area and LOS quality were taken into account. Table III-C.1 below shows the LOS standard for parks and recreation service area requirements.

TABLE III-C.1
Prince William County Parks and Recreation Service Area Standards

PARK TYPE	WALK/BIKE SERVICE AREA	DRIVE TIME SERVICE AREA
Neighborhood	5 to 10-minute walk/bike time; bus stop within ¼-mile, preferred	Less than 10 minutes
Community	10 to 15-minute walk/bike time	10 to 20-minute drive time
Regional	Greater than 15-minute walk/bike time	20 to 30-minute drive time
School/Community-Use	5 to 10-minute walk/bike time	Less than 10 minutes

Source: Prince William County Comprehensive Plan Parks Recreation & Tourism.

Based on the location of the Residential Development and available parks in Park Planning District 9, the Residential Development will have access to a park from each classification within the specified service area travel times as shown below in Table III-C.2.

TABLE III-C.2

<u>Development Service Area Parks</u>

Park ^(a)	Classification	Distance from Development ^(b)	Drive - Time Estimate
John D. Jenkins Park	Neighborhood	2.8 miles	8 minutes
Sharron Baucom-Dale Recreation Center	Community	2.2 miles	7 minutes
Andrew Leitch Park	Regional	5.0 miles	10 minutes
Bel Air Elementary School	School-use	1.9 miles	7 minutes
(a) Source: Prince William County Comprehensive Pr	lan Parks Recreation & T	ourism.	
(b) Source: Google Maps.			

The County further evaluates parks and facilities using a quality rating to assess overall LOS. According to the County Comprehensive Plan – (Parks Recreation and Tourism approved March 10, 2020), the County goal is to have all parks and facilities at or above a "B" LOS letter grade. The current quality ratings of the abovementioned parks are shown below in Table III-C.3. As of this writing, quality letter grades were not assigned to School-use parks.

TABLE III-C.3

Development Service Area Parks- Quality Score

Park	Classification	Quality Score	LOS Letter Grade
John D. Jenkins Park	Neighborhood	0.70	B-
Sharron Baucom-Dale Recreation Center	Community	0.64	B-
Andrew Leitch Park	Regional	0.60	С
(a) Source: Prince William County Comprehensive P	lan Parks Recreation & To	ourism.	

Based on the LOS letter grades shown in Table III-C.3, the parks currently identified in the Service Area of the Residential Development are below the County's stated goal for quality. Thus, the projected impact on public park facilities that will be generated by the Residential Development represents a requirement beyond existing capacity.

MITIGATION STRATEGIES

Any proffer related to public parks must only mitigate the costs reasonably attributable to an increase in population. Moreover, the County Capital Improvement Plan does not include any capital improvements to Park Planning District 9 that increase capacity in the Residential Development's service area. As such, any proffer related to such facilities would be inappropriate under the Residential Proffer Legislation.

In addition, the Residential Development is planned to include numerous amenities for residents, including a clubhouse, trails, and open space which will include six acres of dedicated park space to reasonably offset any increase in demand on existing facilities.

The Applicant will undertake efforts necessary to ensure that the proposed mitigation strategy is consistent with all applicable laws, including, but not limited to, the Residential Proffer Legislation.

IV. Conclusions, Assumptions, and Limitations

The preceding narrative provides projections of impacts to public facility improvements as mandated by the Prince William County Justification Narrative Requirement and has been updated based on comments received by County staff. This narrative is being resubmitted for review. Upon receipt of such review and any additional commentary, the Applicant will further augment this submission with specific mitigation strategies as appropriate.

SUMMARY OF ANALYSIS

Based on MuniCap's analysis, the estimated cash proffer that may be collected from the Residential Development is as shown in Table IV-A.

TABLE IV-A Summary of Analysis

Public Facilities	Estimated Proffer per Dwelling Unit
a) Public school facilities total proffer cost ^(a)	
i) Townhome unit	\$398.07
ii) Multi-family unit	\$209.51
b) Public safety facilities ^(b)	
i) Townhome unit	\$235.35
ii) Multi-family unit	\$172.83
d) Public parks facilities cost per unit	N.A.
e) Public transportation facilities	N.A.
f) Total estimated proffer per unit	
i) Townhome unit	\$633.42
ii) Multi-family unit	\$382.34
g) Proposed residential units ^(c)	
i) Townhome unit	610
ii) Multi-family unit	405
Total Development Proffer Contribution ((e(i) × f(i))) + (e(ii) × f(ii)))	\$541,234.79
(a) See Table III-A.4.	

⁽b) See Table III-B.6.

⁽c) See Table III-B.1.

ASSUMPTIONS AND LIMITATIONS

This narrative does not contemplate public transportation facility improvement impacts, which are addressed in the Traffic Impact Analysis.

MuniCap obtained the information presented and used in this narrative from multiple sources. While these sources are believed to be reliable, MuniCap has not undertaken any efforts to independently verify the veracity of any such information.

While the methodology employed, and the content provided herein, are believed to be consistent with applicable law, including the Residential Proffer Legislation, none of the statements in this document should be construed as legal advice.

PROJECT STATUS		
DATE	ACTION	
05-3-2021	2ND SUBMISSION	
04-21-2022	3RD SUBMISSION	
08-19-2022	4TH SUBMISSION	

MASTER ZONING PLAN REZ2019-00018

QUARTZ DISTRICT

NEABSCO MAGISTERIAL DISTRICT PRINCE WILLIAM COUNTY

PROJECT TEAM

APPLICANT/DEVELOPER BUCHANAN PARTNERS

9841 WASHINGTONIAN BOULEVARD SUITE 300 GAITHERSBURG, MD 20878

LAND ATTORNEY

WALSH COLUCCI LUBELEY & WALSH PC 4310 PRINCE WILLIAM PARKWAY SUITE 300 PRINCE WILLIAM, VA 22192

<u>CIVIL ENGINEER</u>

christopher consultants, Itd 9900 MAIN STREET FOURTH FLOOR FAIRFAX, VA 22031

TRANSPORTATION

GOROVE / SLADE, INC. 3914 CENTREVILLE ROAD, SUITE 330 CHANTILLY, VA 20151

ENVIRONMENTAL CONSULTANTS

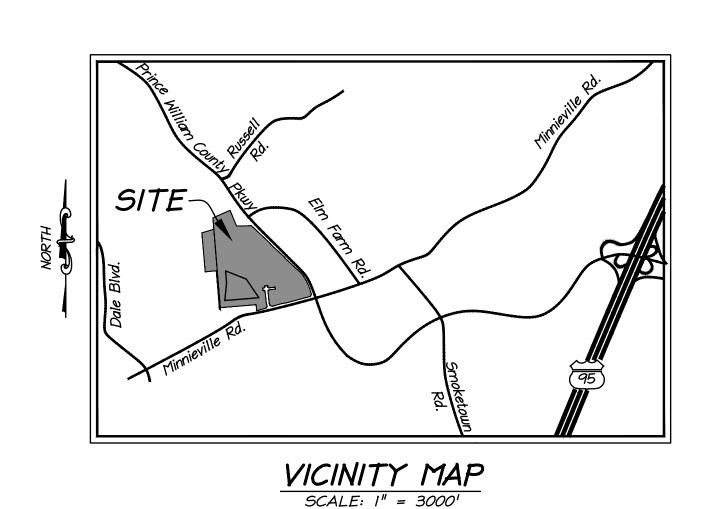
ACORN ENVIRONMENTAL, INC. 708 LAUREL LANE SEVERNA PARK, MD 21146

WETLAND STUDIES AND SOLUTIONS 5300 WELLINGTON BRANCH DRIVE, SUITE 100 GAINESVILLE, VA 20155

CULTURAL RESOURCES

DUTTON # ASSOCIATES

III5 CROWDER DRIVE MIDLOTHIAN, VA 22113



SHEET INDEX:

- OI COVER SHEET
- 02 EXISTING CONDITIONS PLAN

Attachment E

- 03 LAND USE & LANDSCAPE BUFFER PLAN
- 04 PUBLIC TRANSPORTATION PLAN
- 05 STREET NETWORK & SIGNAGE PLAN
- 06 PEDESTRIAN CIRCULATION PLAN 07 - BICYCLE NETWORK PLAN
- 08 PROPOSED STREET SECTIONS
- 09 PROPOSED INTERSECTION DETAILS
- 10 INFRASTRUCTURE & UTILITY PLAN
- II PHASING PLAN

GENERAL NOTES:

THE SUBJECT PROPERTY SHOWN TO BE REZONED IS COMPRISED OF THREE PARCELS OF APPROXIMATELY ±143.1169 ACRES AND IS RECORDED IN THE LAND RECORDS OF PRINCE WILLIAM COUNTY, VIRGINIA AS

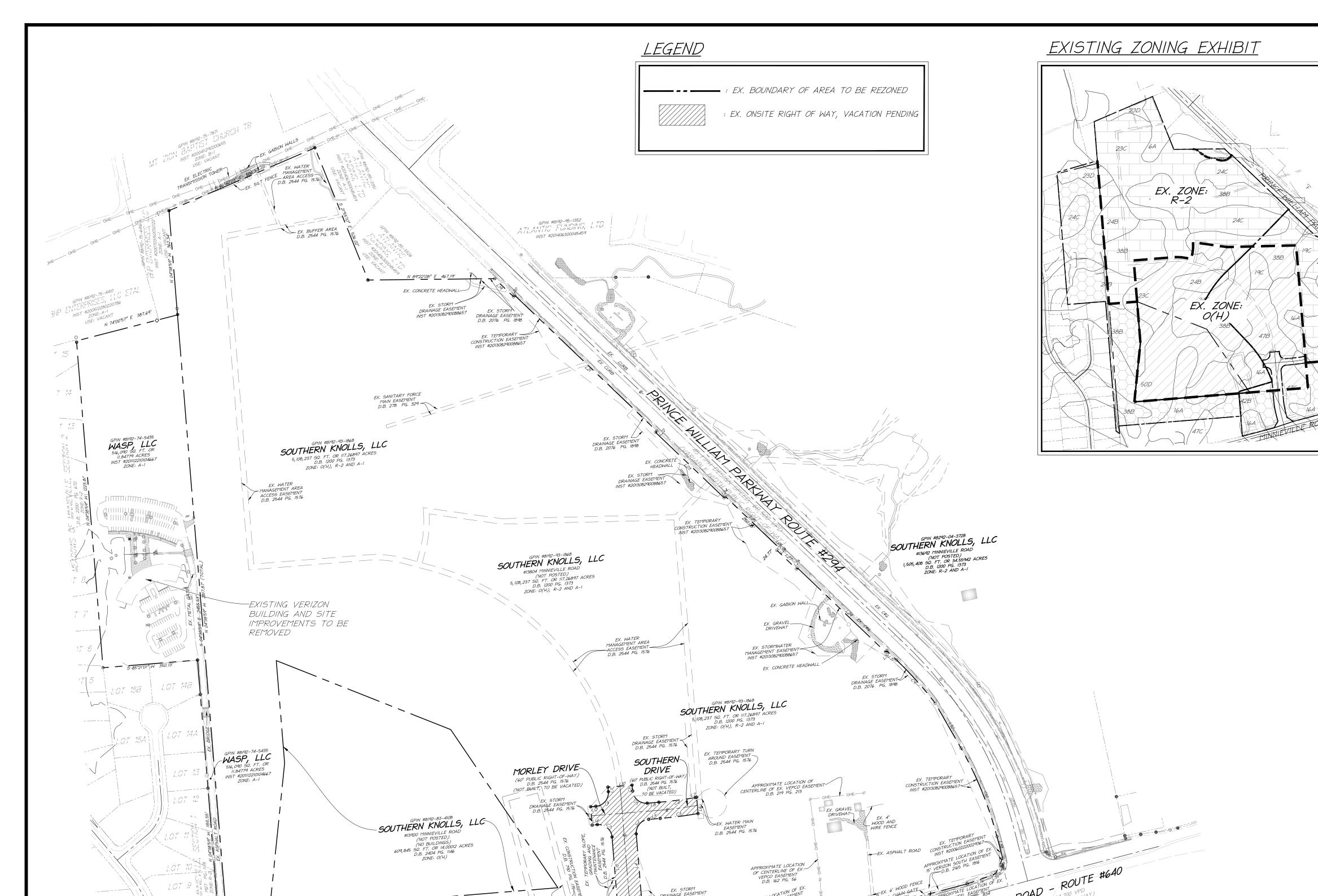
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8192-93-1868	13804 MINNIEVILLE ROAD	117.2690	A-1, R-2, O(H)	SOUTHERN KNOLLS, LLC
8192-83-4108	13920 MINNIEVILLE ROAD	14.001	O(H)	SOUTHERN KNOLLS, LLC
8192-74-5435	13930 MINNIEVILLE ROAD	11.8478	A-I	SKW VA, INC.

2. THE AREAS FOR REZONING, AS SHOWN HEREON, ARE BASED ON A FIELD RUN SURVEY COMPLETED BY THIS

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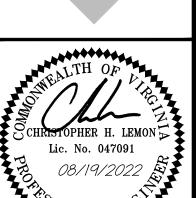
SHEET No.

01 OF 11



	PROJECT STATUS
DATE	ACTION
05-3-2021	2ND SUBMISSION
04-21-2022	3RD SUBMISSION
08-19-2022	4TH SUBMISSION





DISTRICT

QUART

ZONING

CURVE TABLE

LINE TABLE

5 82°00'09" W

N 59°47′36" W

N 14°44'48" W

N 04°41′04" W

N 85°18'56" E

N 04°41′04" W

S 79°57'39" W

S 85°18'56" W

S 85°18'56" W

S 04°41'04" E

S 85°18'56" W

S 04°41'04" E

N 85°18'56" E

S 14°44'48" E

5 30°12'24" W

S 75°09'35" W

S 46°18'03" E

N 43°45'42" E

S 04°43′54″ E

<u>LEGEND</u>

R-2 ZONING DISTRICT

O(H) ZONING DISTRICT

A-I ZONING

DISTRICT

100.7.

70.77

44.26

256.50

70.96'

17.00

68.00

56.5

70.65

302.40

96.64

89.20

5 46°18'03" E

S 46°18'03" E

N 43°45'42" E

S 46°18'03" E

S 43°45'42" W

S 46°18'03" E

N 43°45'42" E

5 46°18'03" E

N 43°45'42" E

5 46°18'03" E

S 29°52'05" .

S 14°43′53″ 1

L34 S 23°59'15" W

L37 N 53°51'39" W

L38 N 04°43′54" W

L39 5 71°32'46" W

S 43°45'42" W

349.81

274.85

99.95

449.76

49.5

82.66

39.67

45.02

190.40'

GRAPHIC SCALE 1" = 500'

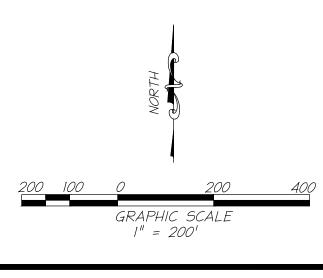
		<u> </u>				
CURVE	RADIUS	LENGTH	DELTA	TANGENT	BEARING	CHORD
Cl	755.00'	132.59'	10°03'44"	66.47'	N 09°42′56″ W	132.42'
C2	50.00'	78.54′	90°00'00"	50.00'	N 40°18′56″ E	70.71'
C3	1506.27'	140.77'	5°21′17″	70.44'	N 82°38'17" E	140.72'
C4	1446.27'	135.16'	5°21′17″	67.63'	5 82°38′17″ W	135.12'
C5	50.00'	77.74'	89°04'40"	49.20'	N 50°08'44" W	70.14'
C6	50.00'	78.54′	90°00'00"	50.00'	S 40°18'56" W	70.71'
<i>C7</i>	50.00'	78.54′	90°00'00"	50.00'	S 49°41'04" E	70.71'
C8	845.00'	148.40'	10°03'44"	74.39'	N 09°42′56″ W	148.21'
C9	1384.65′	214.42'	8°52'21"	107.42'	S 41°51′52" E	214.20'
CIO	1372.20'	353.11'	14°44'38"	177.54′	S 22°06'36" E	352.14'
CII	883.76′	21.81'	1°24′51″	10.91'	N 06°18'49" W	21.81'

TES:							
SUR IFCT	PPAPERTY	SHOWN	TO	RF	PEZONED	15	CO

I. THE SUBJECT PROPERTY SHOWN TO BE REZONED IS COMPRISED OF THREE PARCELS OF APPROXIMATELY ±143.1169 ACRES AND IS RECORDED IN THE LAND RECORDS OF PRINCE WILLIAM COUNTY, VIRGINIA AS FOLLOWS:

GPIN	<i>ADDRESS</i>	ACREAGE	CURRENT ZONING	OWNER
8192-93-1868	13804 MINNIEVILLE ROAD	117.2690	A-1, R-2, O(H)	SOUTHERN KNOLLS, LLC
8192-83-4108	13920 MINNIEVILLE ROAD	14.001	O(H)	SOUTHERN KNOLLS, LLC
8192-74-5435	13930 MINNIEVILLE ROAD	11.8478	A-/	SKW VA, LLC

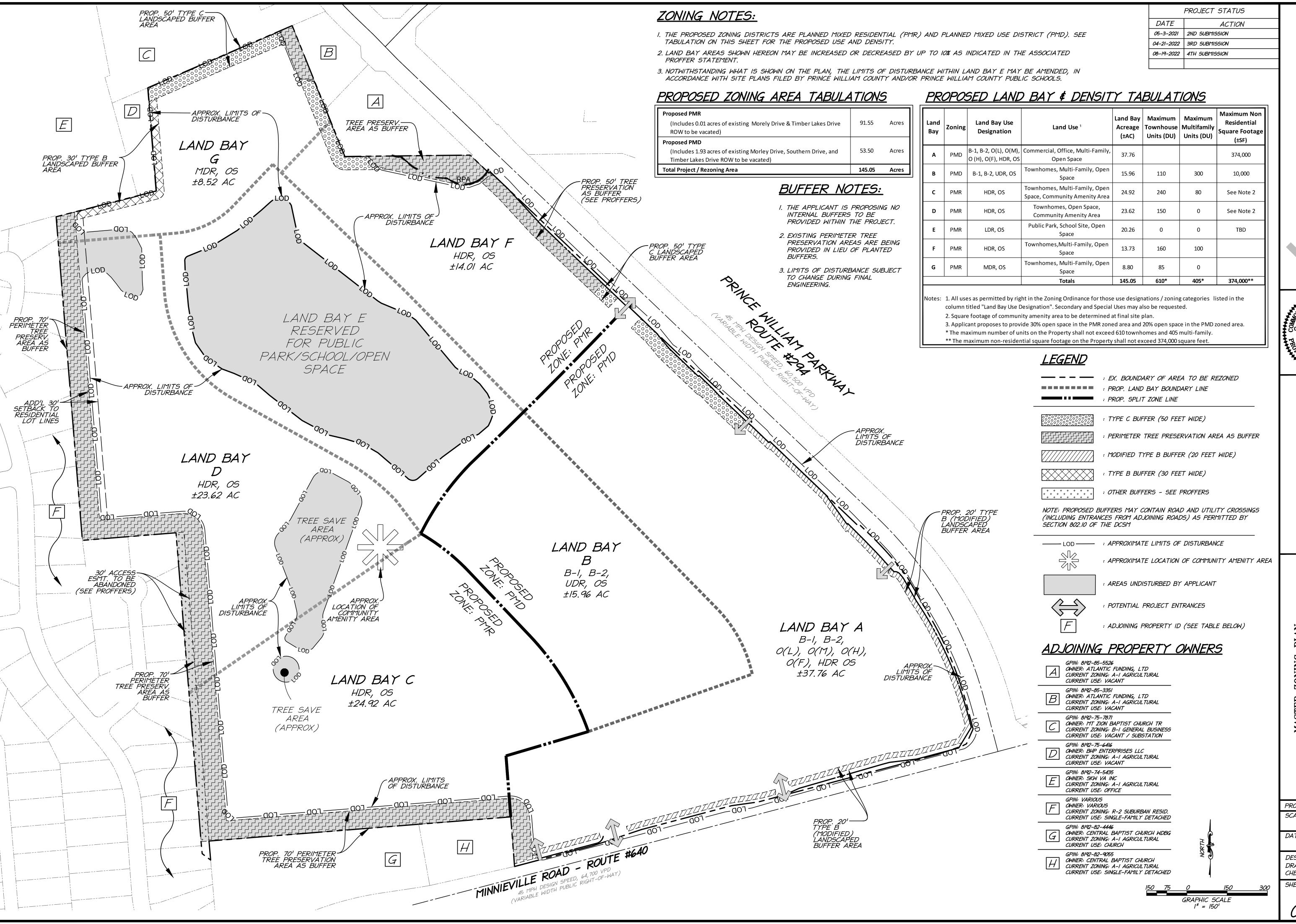
- 2. THERE EXISTS ON SITE A PUBLIC RIGHT-OF-WAY THAT IS APPROXIMATELY ±1.94 ACRES IN SIZE. IT IS TO BE VACATED/ABANDONED BY SEPARATE INSTRUMENT.
- 3. BOUNDARY INFORMATION IS BASED ON A ZONING PLAT BY THIS FIRM DATED FEBRUARY
- 4. TOPOGRAPHY IS BASED ON AERIAL SURVEY.
- 5. WETLANDS SHOWN HEREON ARE BASED ON A DELINEATION OF THE SUBJECT PROPERTY BY ACORN ENVIRONMENTAL, INC DATED JANUARY 23, 2018.



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IDE 115 400) - fairfax, va 220

character (suite 400)



CHRISTOPHER H. LEMON Dic. No. 047091

08/19/2022

LAND USE & LANDSCA, BUFFER PLAN

ZONING PLAN

Z DISTRICT

M COUNTY, VIRGINIA

QUARTER ZONIN PRINCE WILLIAM COL

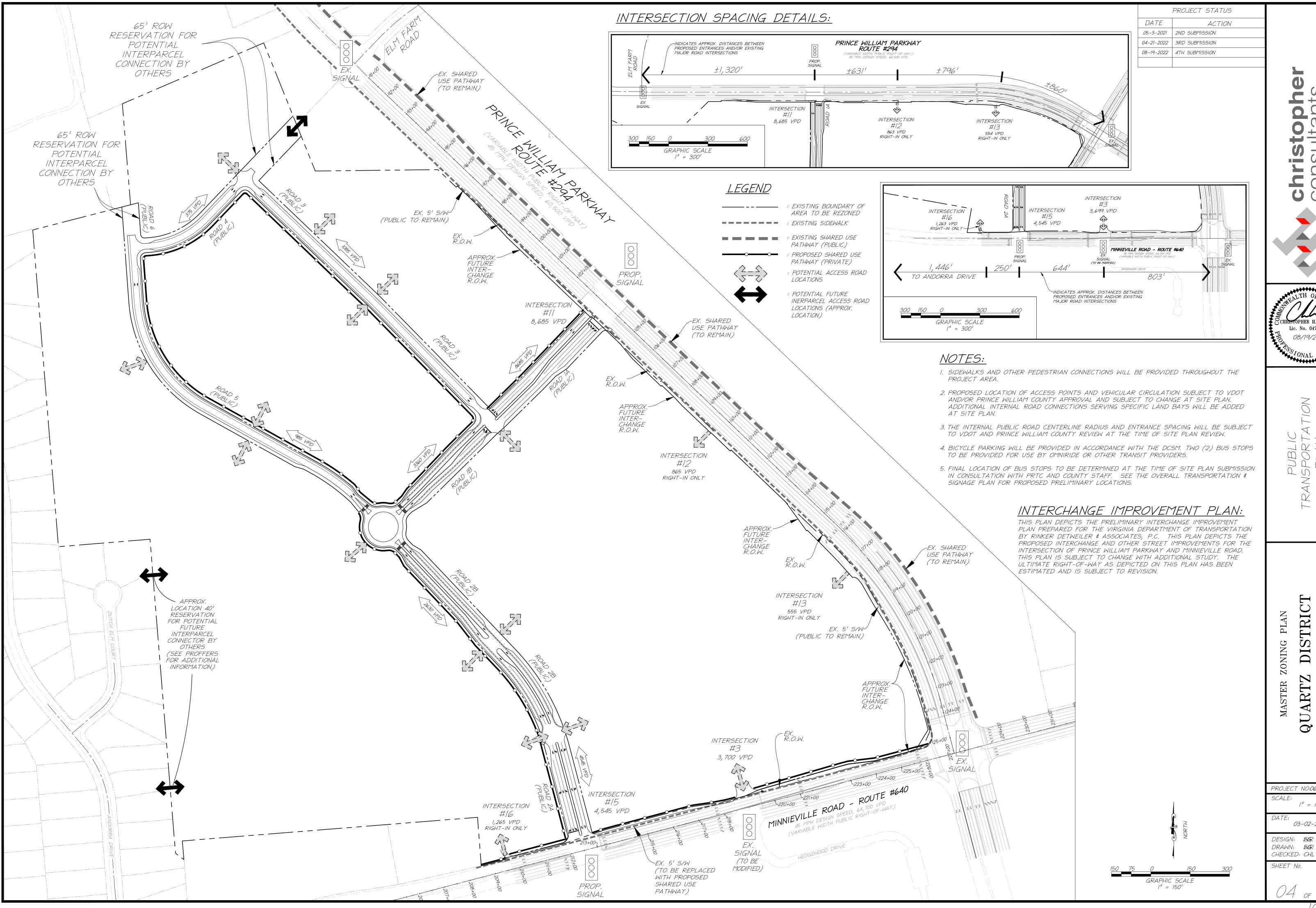
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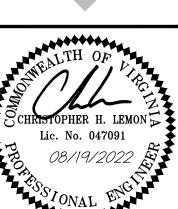
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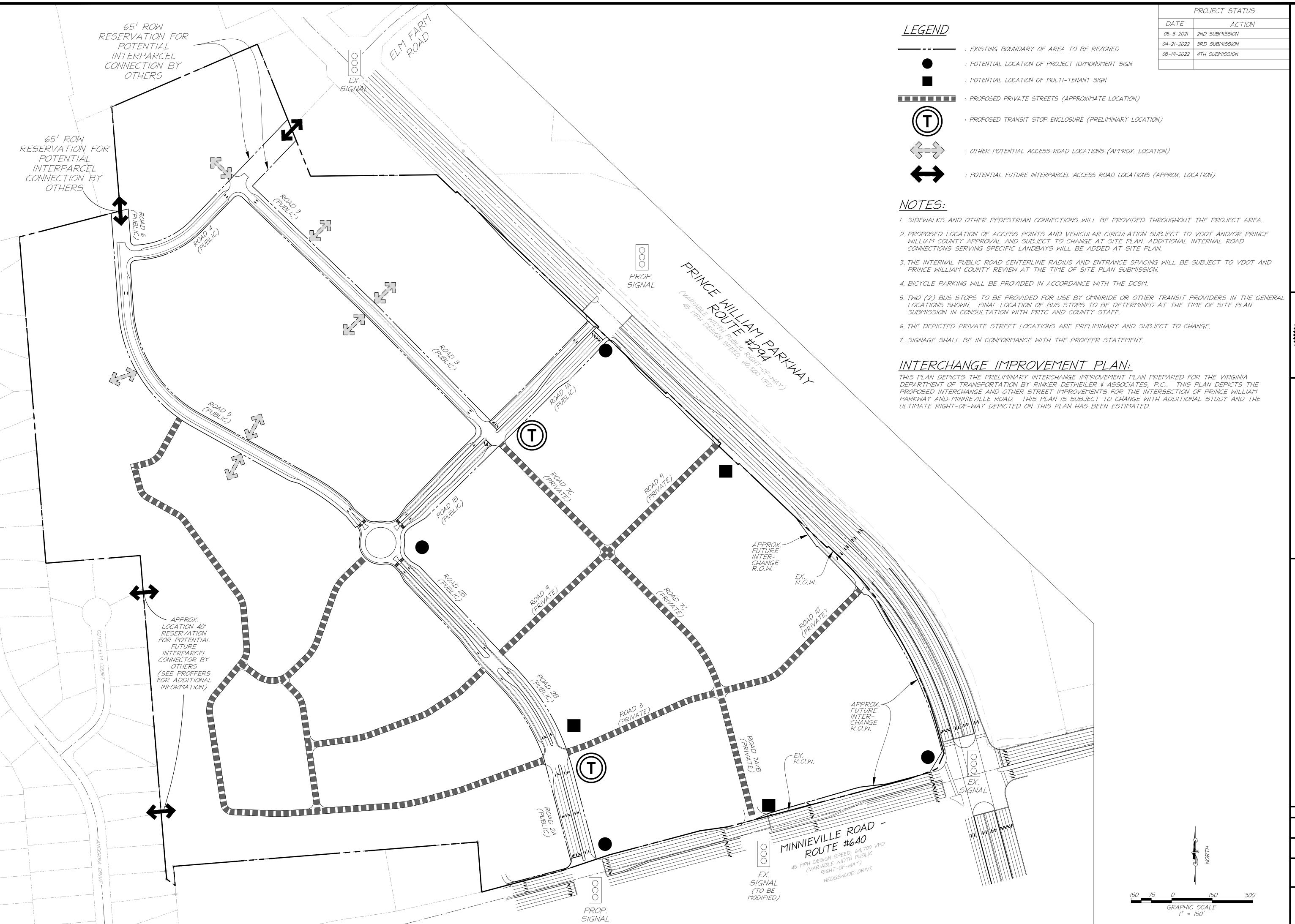


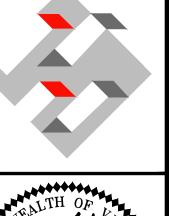
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DISTRICT

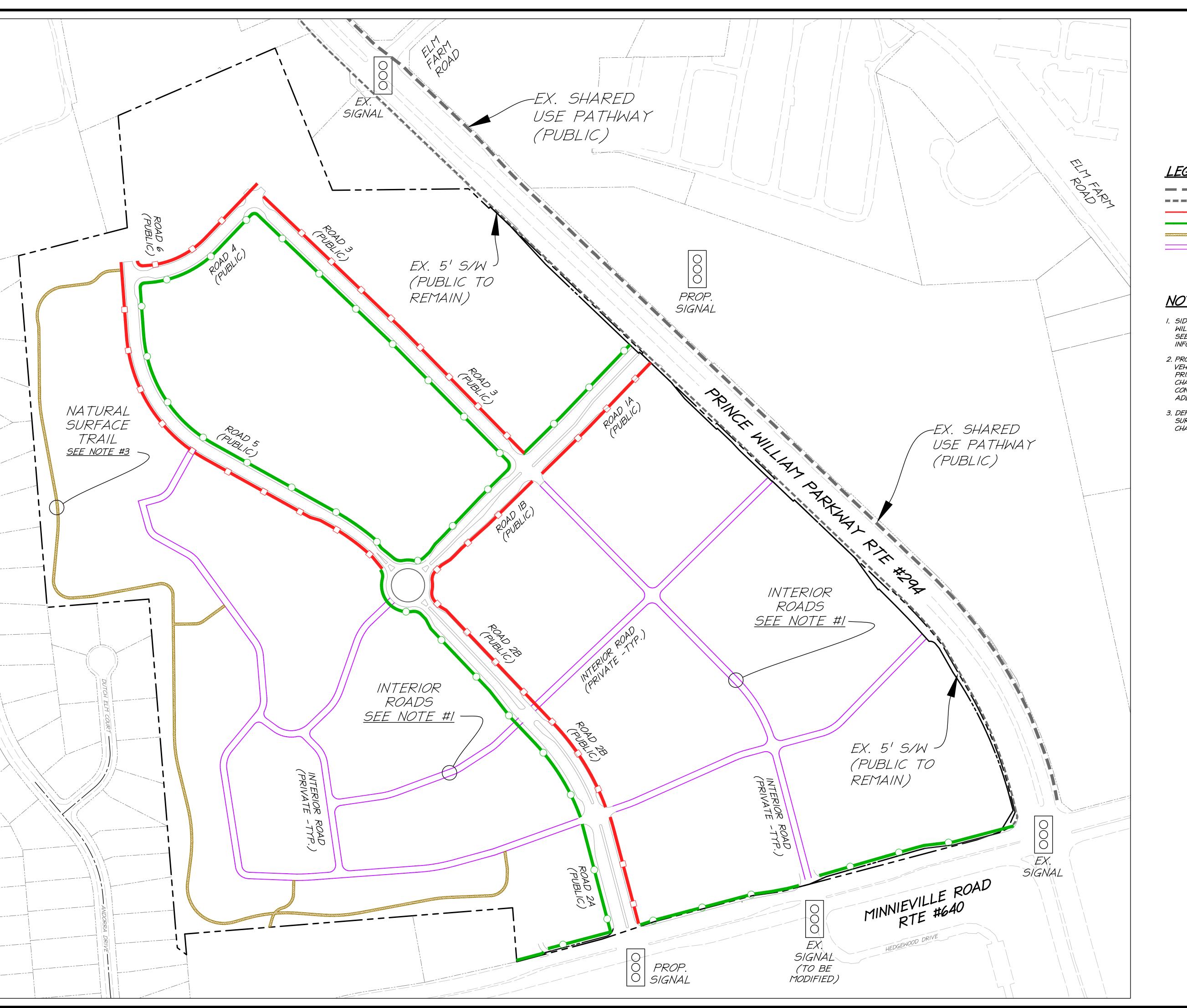
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PROJECT STATUS		
DATE	ACTION	
05-3-2021	2ND SUBMISSION	
04-21-2022	3RD SUBMISSION	
08-19-2022	4TH SUBMISSION	

<u>LEGEND</u>

: EXISTING SHARED USE PATHWAY (PUBLIC)

: PROPOSED SHARED USE PATHWAY (PRIVATE)

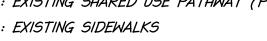
: PROPOSED NATURAL SURFACE TRAIL

NOTES:

- I. SIDEWALKS AND OTHER PEDESTRIAN CONNECTIONS
 WILL BE PROVIDED THROUGHOUT THE PROJECT AREA.
 SEE THE DESIGN GUIDELINES FOR ADDITIONAL
 INFORMATION.
- 2. PROPOSED LOCATION OF ACCESS POINTS AND VEHICULAR CIRCULATION SUBJECT TO VDOT AND/OR PRINCE WILLIAM COUNTY APPROVAL AND SUBJECT TO CHANGE AT SITE PLAN. ADDITIONAL INTERNAL ROAD CONNECTIONS SERVING SPECIFIC LANDBAYS MAY BE ADDED AT SITE PLAN.
- 3. DEPICTED LOCATION OF PROPOSED NATURAL SURFACE TRAIL IS PRELIMINARY AND SUBJECT TO CHANGE AT THE TIME OF SITE PLAN SUBMISSION.

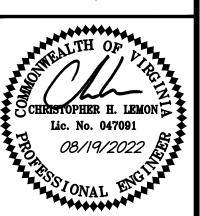
GRAPHIC SCALE I" = 200'





: PROPOSED SIDEWALKS

: PROPOSED PRIVATE STREET SIDEWALKS / PEDESTRIAN FACILITIES (SEE NOTE #I)



DISTRICT

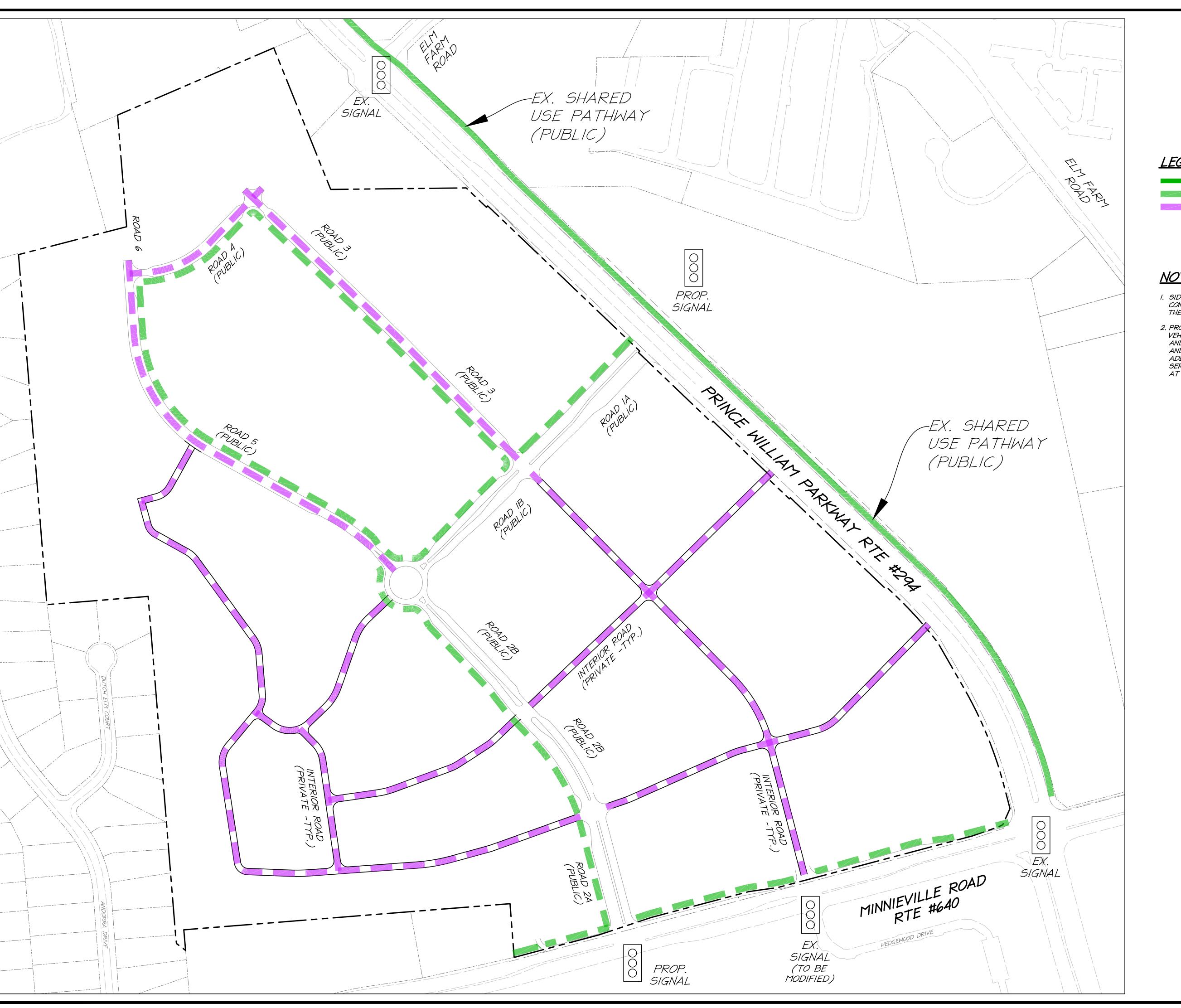
PLAN QUARTZ
PRINCE WILLIAM MASTER

PROJECT NO:06108.009.00 I" = 200'

03-02-2021 DESIGN: ISUR DRAWN: ISUR CHECKED: CHL

SHEET No.

06 of 11



PROJECT STATUS DATE **ACTION** 05-3-2021 | 2ND SUBMISSION 04-21-2022 3RD SUBMISSION 08-19-2022 4TH SUBMISSION

<u>LEGEND</u>



EXISTING SHARED USE PATHWAY (PUBLIC) : PROPOSED SHARED USE PATHWAY (PRIVATE)

: PROPOSED SHARROWS

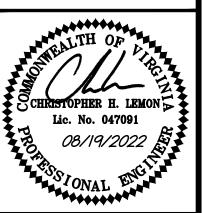
NOTES:

- I. SIDEWALKS AND OTHER PEDESTRIAN CONNECTIONS WILL BE PROVIDED THROUGHOUT THE PROJECT AREA.
- 2. PROPOSED LOCATION OF ACCESS POINTS AND VEHICULAR CIRCULATION SUBJECT TO VDOT AND/OR PRINCE WILLIAM COUNTY APPROVAL AND SUBJECT TO CHANGE AT SITE PLAN. ADDITIONAL INTERNAL ROAD CONNECTIONS SERVING SPECIFIC LANDBAYS MAY BE ADDED AT SITE PLAN.

GRAPHIC SCALE I" = 200'







DISTRICT

PLAN ZONING QUARTZ
PRINCE WILLIAM MASTER

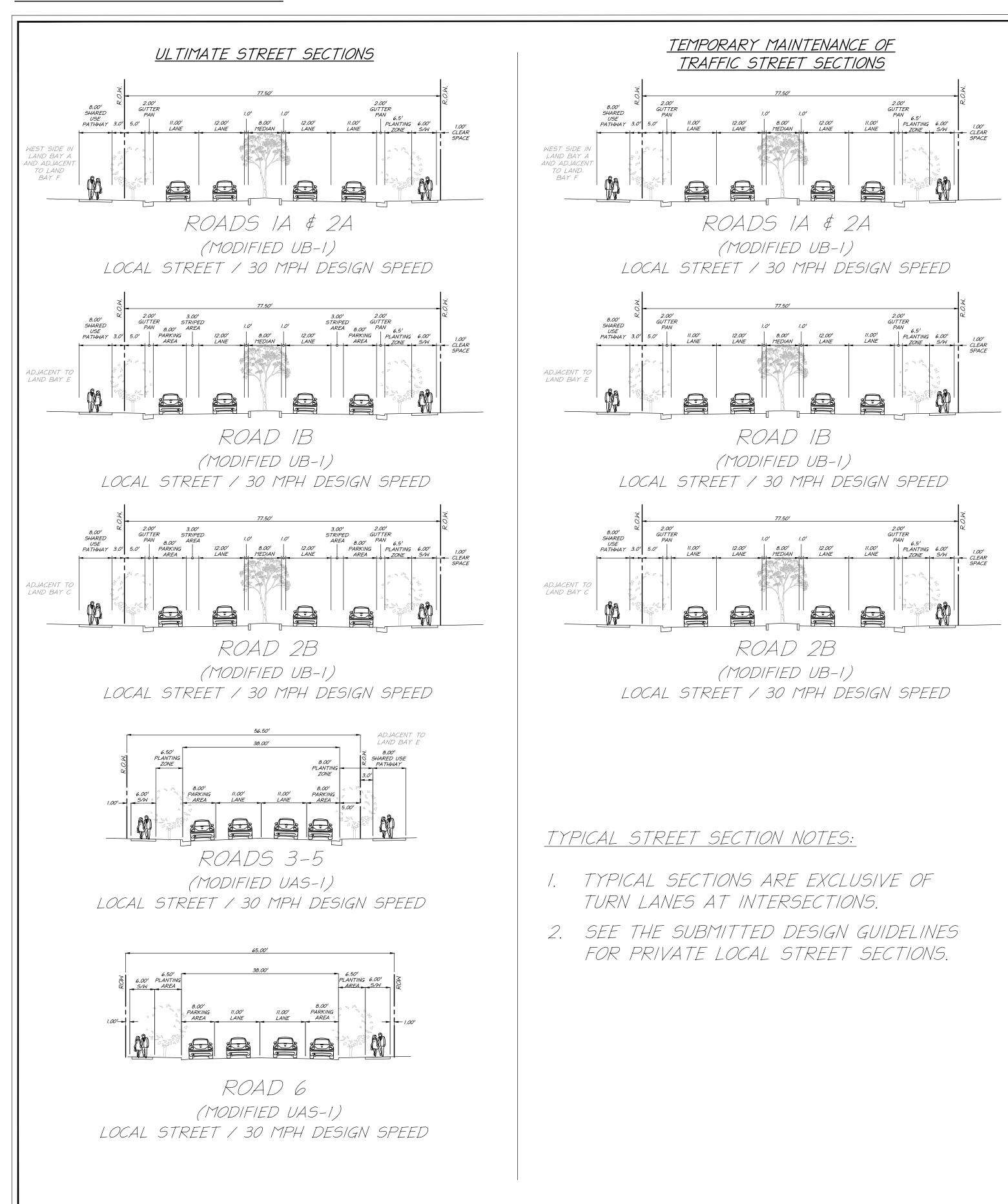
PROJECT NO:06108.009.00 I" = 200'

03-02-2021

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SHEET No. 07 OF 11

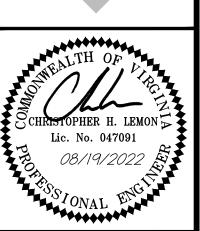
PROPOSED STREET SECTIONS



PROJECT STATUS DATE 05-3-2021 2ND SUBMISSION 04-21-2022 3RD SUBMISSION

08-19-2022 | 4TH SUBMISSION





DISTRICT

PROJECT NO:06108.009.0 N. T. S.

QUARTZ

03-02-2021

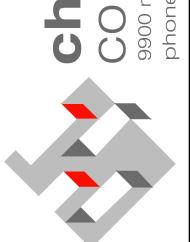
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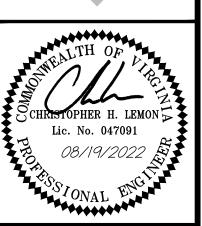
SHEET No.

I. SIDEWALKS AND OTHER PEDESTRIAN CONNECTIONS WILL BE PROVIDED THROUGHOUT THE PROJECT AREA.

PROJECT STATUS			
DATE	ACTION		
05-3-2021	2ND SUBMISSION		
04-21-2022	3RD SUBMISSION		
08-19-2022	4TH SUBMISSION		

istopher Sultants street (suite 400) - fairfax





PROPOSED VTERSECTION DETAILS

MASTER ZONING PLAN
QUARTZ DISTRICT
INCE WILLIAM COUNTY, VIRGINIA

PROJECT NO:06108.009.0

SCALE:

|" = 200'

DATE: 03-02-2021

DESIGN: ISUR

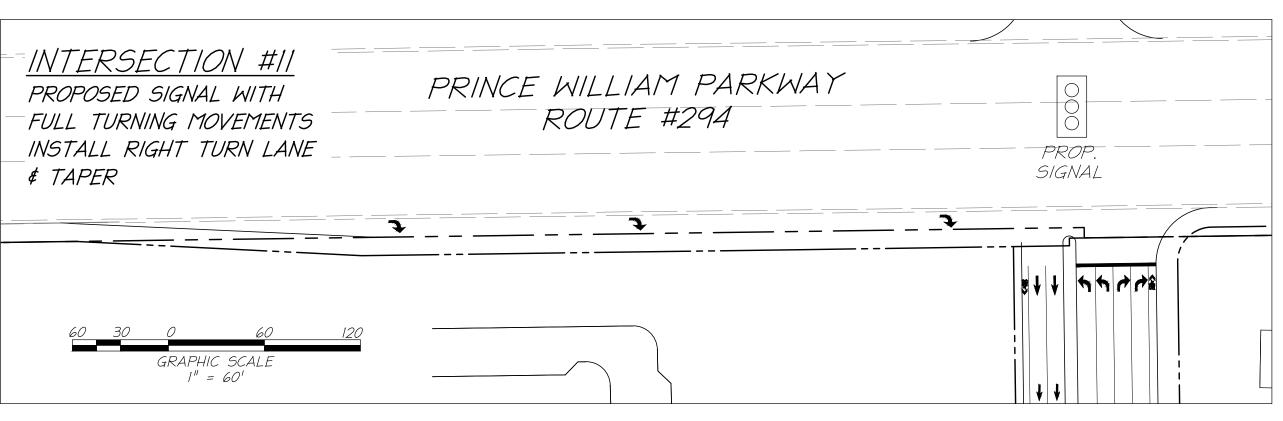
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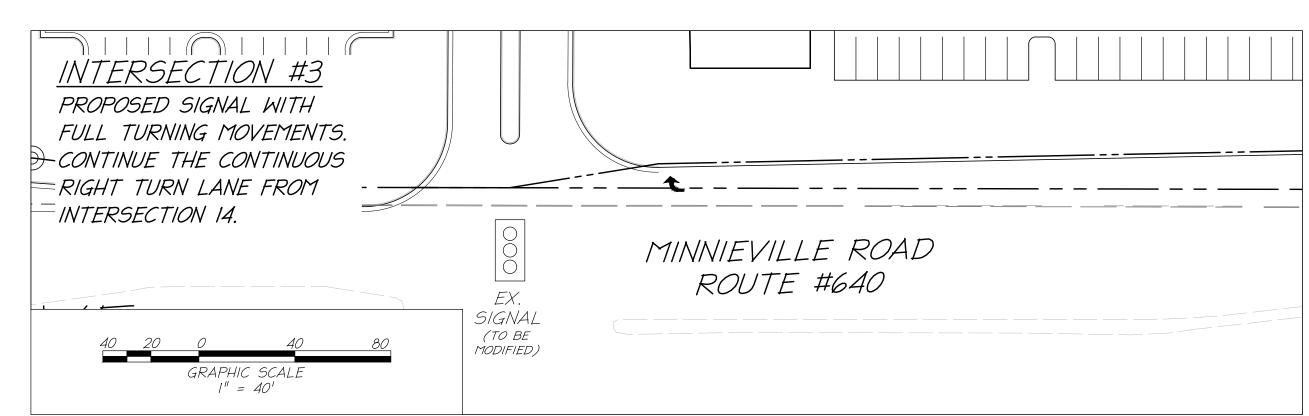
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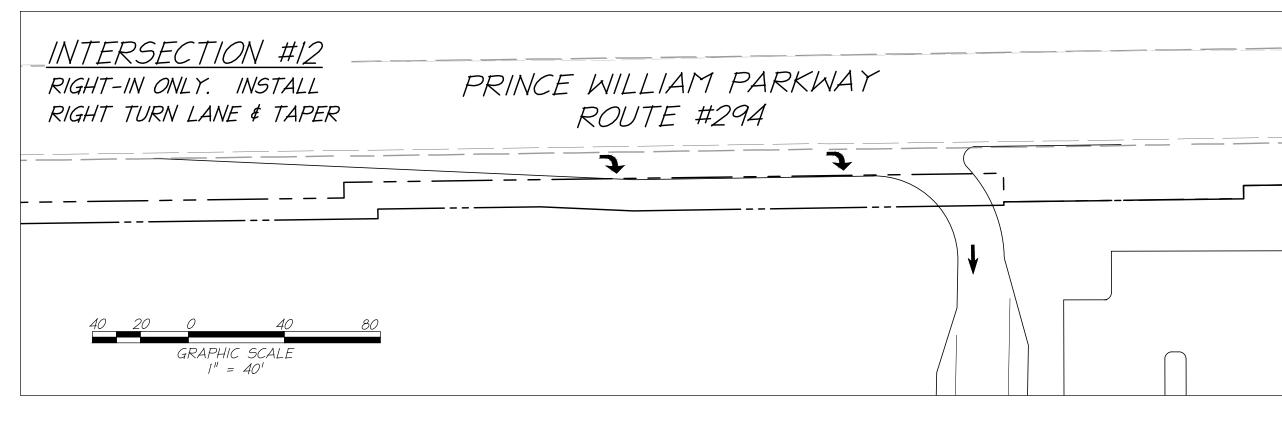
09 _{of} 11

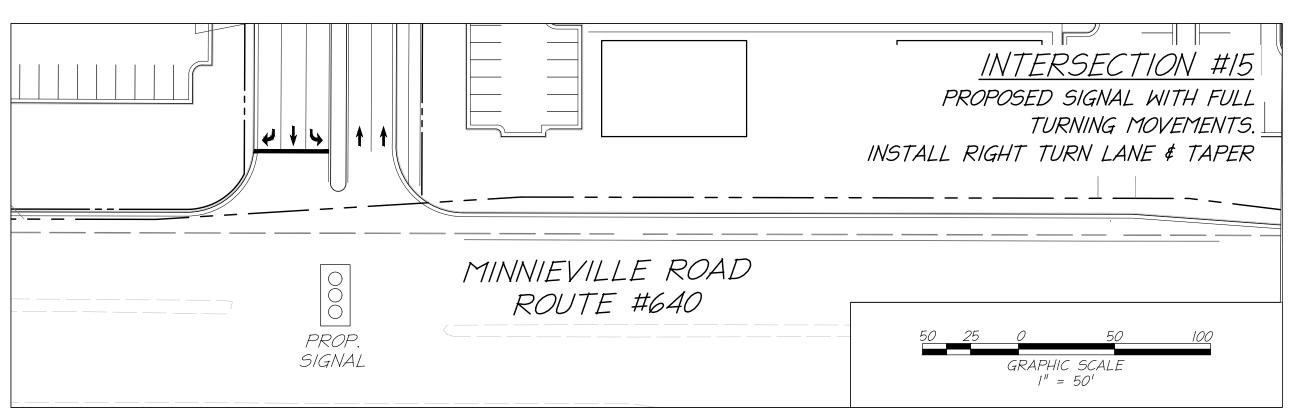
MINNIEVILLE ROAD INTERSECTIONS

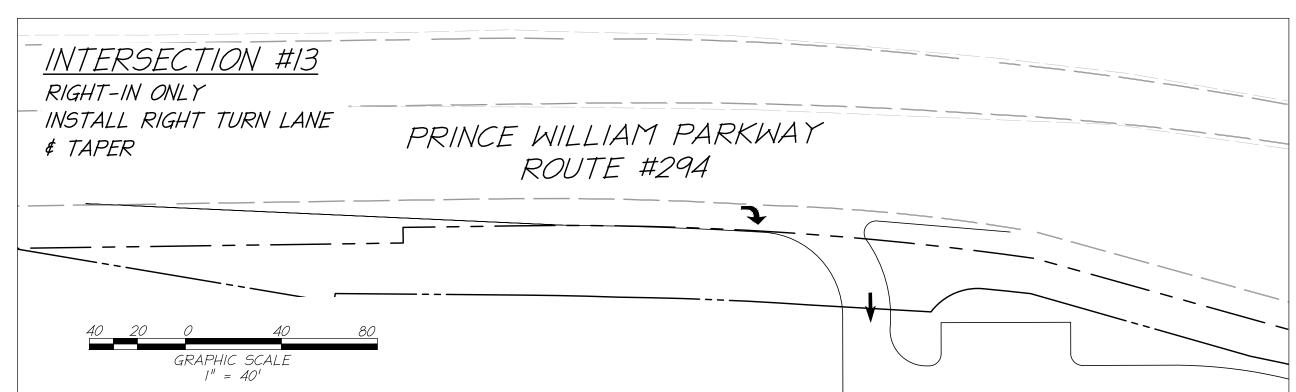


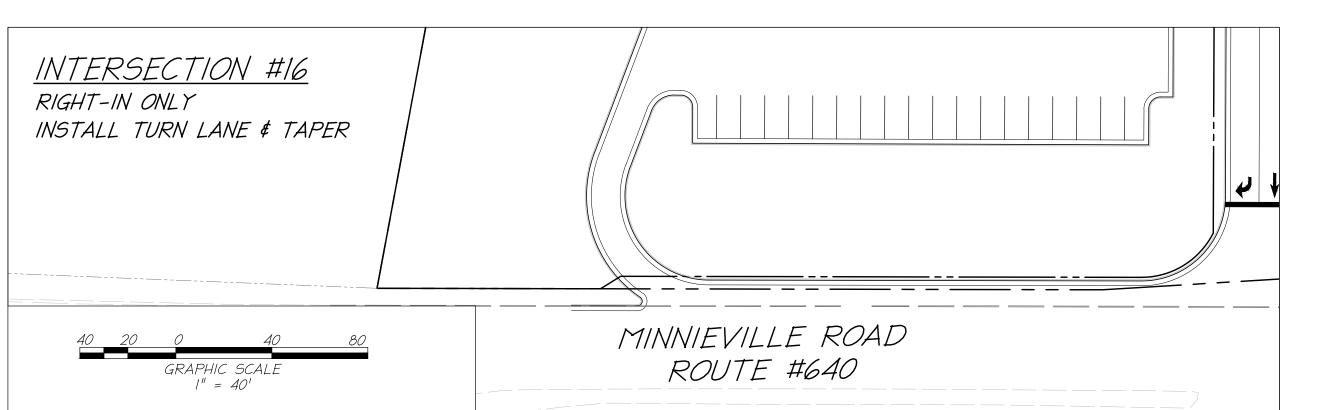
PRINCE WILLIAM PARKWAY INTERSECTIONS







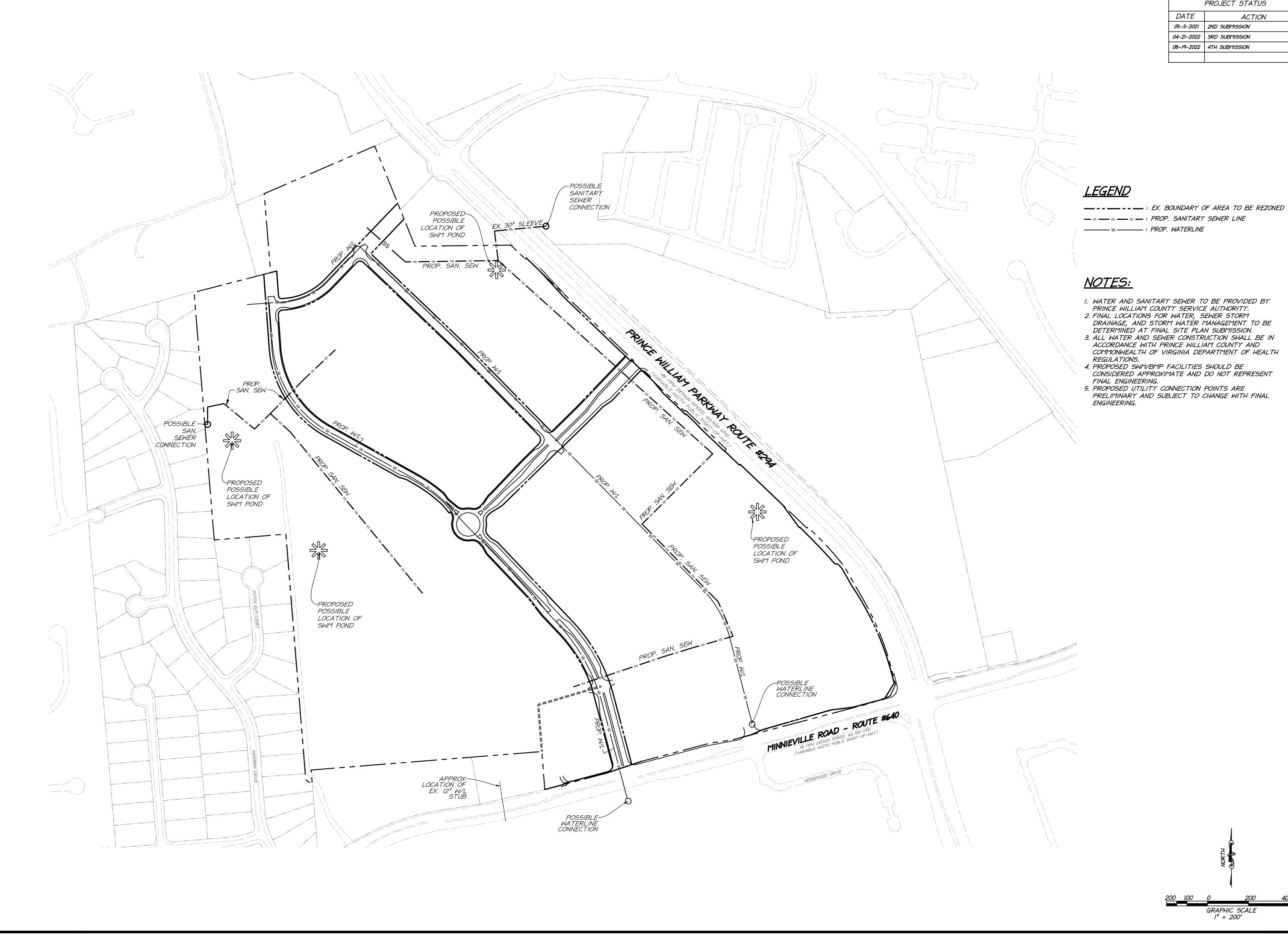




NOTE: DEPICTED BUILDINGS AND PARKING LOTS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND ARE SUBJECT TO CHANGE.

NOTES:

- I. SIDEWALKS AND OTHER PEDESTRIAN CONNECTIONS WILL BE PROVIDED THROUGHOUT THE PROJECT AREA.
- 2. PROPOSED LOCATION OF ACCESS POINTS AND VEHICULAR CIRCULATION SUBJECT TO VDOT AND/OR PRINCE WILLIAM COUNTY APPROVAL AND SUBJECT TO CHANGE AT SITE PLAN. ADDITIONAL INTERNAL ROAD CONNECTIONS SERVING SPECIFIC LANDBAYS MAY BE ADDED AT SITE PLAN.



PROJECT STATUS ACTION 05-3-2021 | 2ND SUBMISSION 04-21-2022 | 3RD SUBMISSION 08-19-2022 | 4TH SUBMISSION



08/19/2022 ONAL

#

INFRAST UTILI

DISTRICT

PLAN

ZONING QUARTZ
INCE WILLIAM MASTER

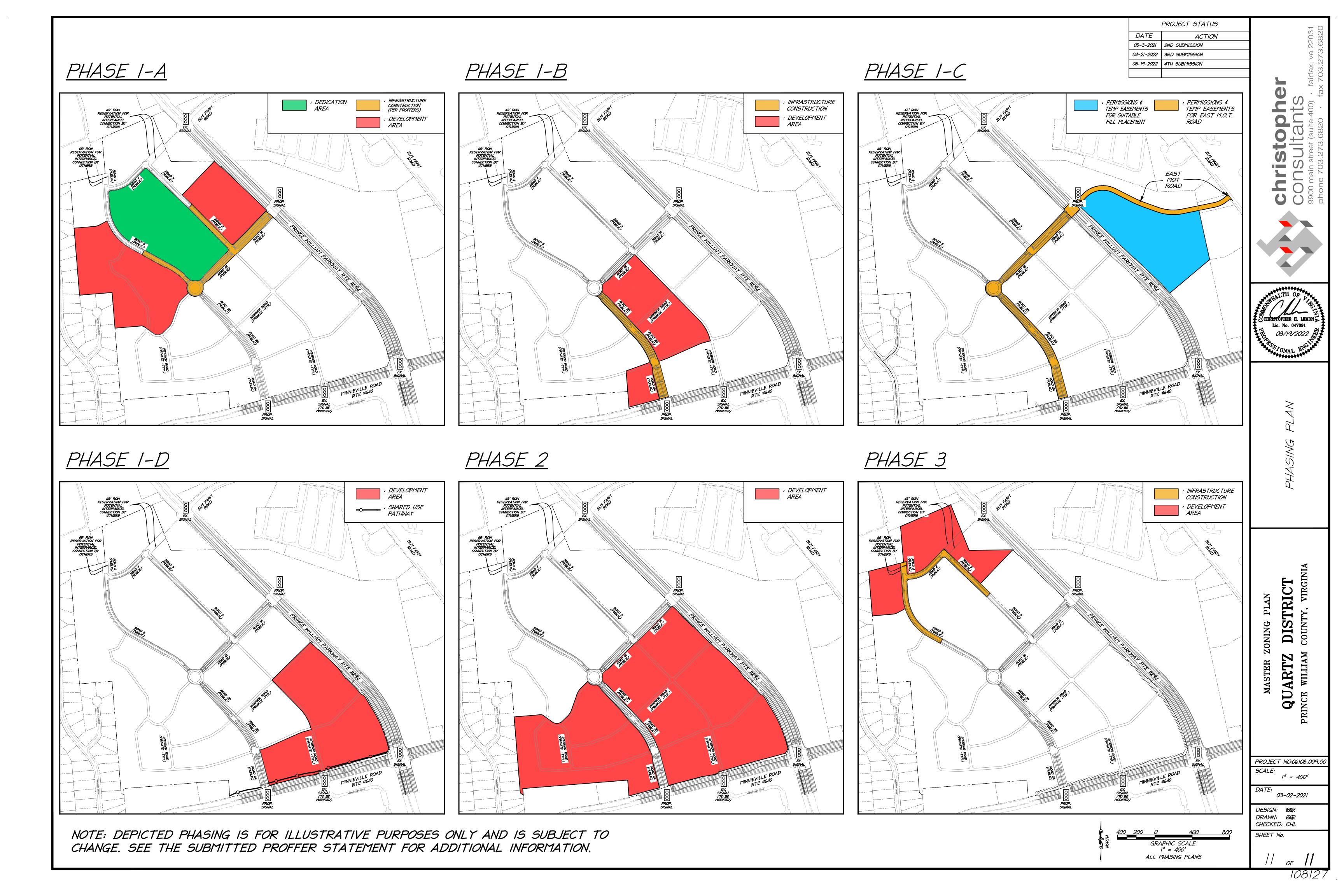
PROJECT NO:06108.009.00 I" = 200'

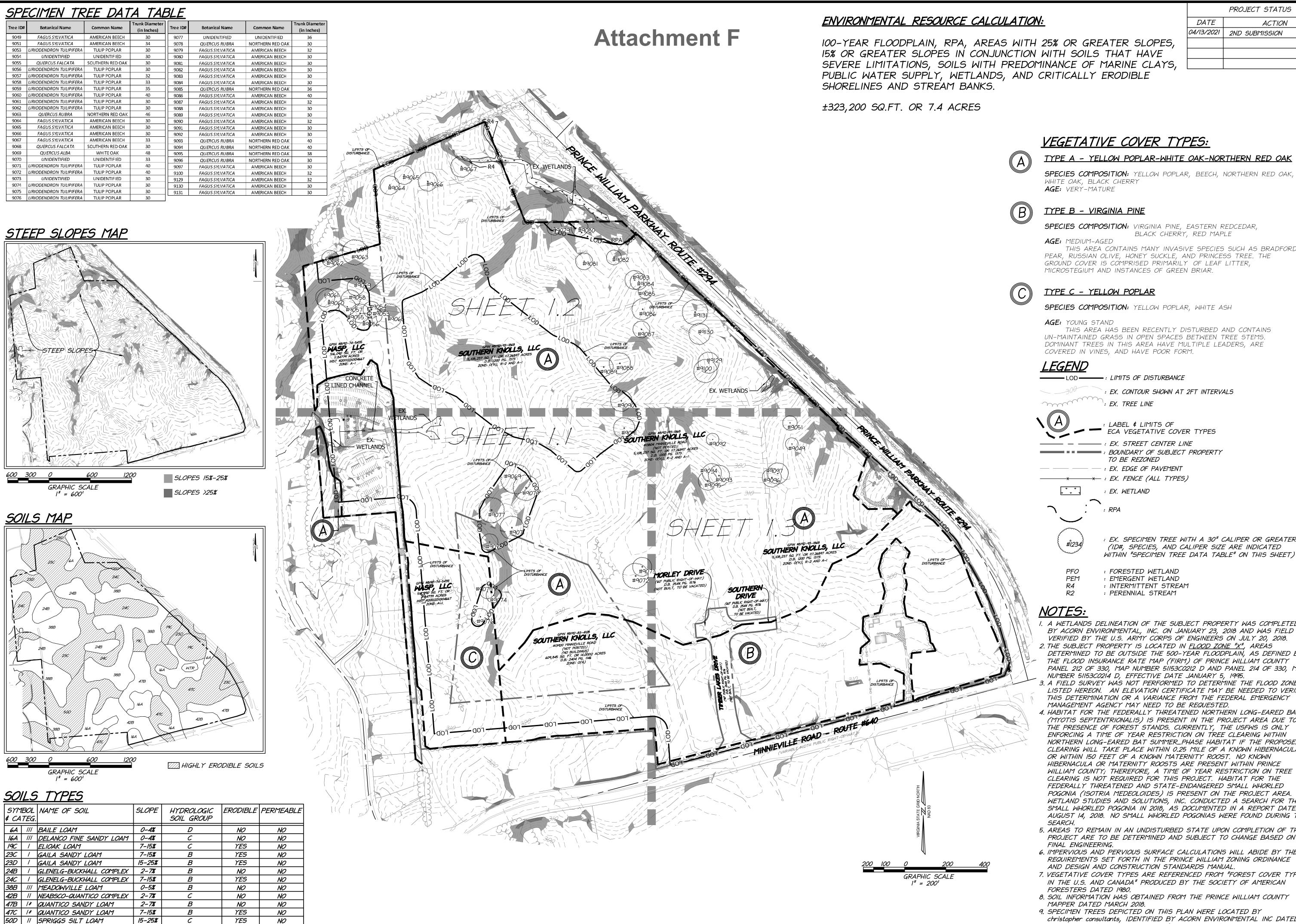
03-02-2021

DESIGN: ISICR DRAWN: ISICR CHECKED: CHL

SHEET No.

OF |





* WHEN THESE SOILS ARE LOCATED IN THE EASTERN AND WESTERN COASTAL PLAINS,

THEY WILL BE CONSIDERED AS CATEGORY III AND II SOILS, RESPECTIVELY.

DATE **ACTION** 04/13/2021 | 2ND SUBMISSION

PROJECT STATUS



LABEL & LIMITS OF ECA VEGETATIVE COVER TYPES

: EX. CONTOUR SHOWN AT 2FT INTERVALS

THIS AREA HAS BEEN RECENTLY DISTURBED AND CONTAINS

BLACK CHERRY, RED MAPLE

THIS AREA CONTAINS MANY INVASIVE SPECIES SUCH AS BRADFORD

— : EX. STREET CENTER LINE · BOUNDARY OF SUBJECT PROPERTY TO BE REZONED : EX. EDGE OF PAVEMENT

ullet : EX. FENCE (ALL TYPES) : EX. WETLAND

- : LIMITS OF DISTURBANCE

: EX. TREE LINE

EX. SPECIMEN TREE WITH A 30" CALIPER OR GREATER (ID#, SPECIES, AND CALIPER SIZE ARE INDICATED WITHIN "SPECIMEN TREE DATA TABLE" ON THIS SHEET)

: FORESTED WETLAND : EMERGENT WETLAND : INTERMITTENT STREAM : PERENNIAL STREAM

NOTES:

I. A WETLANDS DELINEATION OF THE SUBJECT PROPERTY WAS COMPLETED BY ACORN ENVIRONMENTAL, INC. ON JANUARY 23, 2018 AND WAS FIELD VERIFIED BY THE U.S. ARMY CORPS OF ENGINEERS ON JULY 20, 2018.

2. THE SUBJECT PROPERTY IS LOCATED IN FLOOD ZONE "X", AREAS DETERMINED TO BE OUTSIDE THE 500-YEAR FLOODPLAIN, AS DEFINED BY THE FLOOD INSURANCE RATE MAP (FIRM) OF PRINCE WILLIAM COUNTY PANEL 212 OF 330, MAP NUMBER 51153C0212 D AND PANEL 214 OF 330, MAP NUMBER 51153C0214 D, EFFECTIVE DATE JANUARY 5, 1995.

. A FIELD SURVEY WAS NOT PERFORMED TO DETERMINE THE FLOOD ZONES LISTED HEREON. AN ELEVATION CERTIFICATE MAY BE NEEDED TO VERIFY THIS DETERMINATION OR A VARIANCE FROM THE FEDERAL EMERGENCY MANAGEMENT AGENCY MAY NEED TO BE REQUESTED.

4. HABITAT FOR THE FEDERALLY THREATENED NORTHERN LONG-EARED BAT (MYOTIS SEPTENTRIONALIS) IS PRESENT IN THE PROJECT AREA DUE TO THE PRESENCE OF FOREST STANDS. CURRENTLY, THE USFWS IS ONLY ENFORCING A TIME OF YEAR RESTRICTION ON TREE CLEARING WITHIN NORTHERN LONG-EARED BAT SUMMER_PHASE HABITAT IF THE PROPOSED CLEARING WILL TAKE PLACE WITHIN 0.25 MILE OF A KNOWN HIBERNACULA OR WITHIN 150 FEET OF A KNOWN MATERNITY ROOST. NO KNOWN HIBERNACULA OR MATERNITY ROOSTS ARE PRESENT WITHIN PRINCE WILLIAM COUNTY; THEREFORE, A TIME OF YEAR RESTRICTION ON TREE CLEARING IS NOT REQUIRED FOR THIS PROJECT. HABITAT FOR THE FEDERALLY THREATENED AND STATE-ENDANGERED SMALL WHORLED POGONIA (ISOTRIA MEDEOLOIDES) IS PRESENT ON THE PROJECT AREA. WETLAND STUDIES AND SOLUTIONS, INC. CONDUCTED A SEARCH FOR THE SMALL WHORLED POGONIA IN 2018, AS DOCUMENTED IN A REPORT DATED AUGUST 14, 2018. NO SMALL WHORLED POGONIAS WERE FOUND DURING THE

5. AREAS TO REMAIN IN AN UNDISTURBED STATE UPON COMPLETION OF THE PROJECT ARE TO BE DETERMINED AND SUBJECT TO CHANGE BASED ON FINAL ENGINEERING.

6. IMPERVIOUS AND PERVIOUS SURFACE CALCULATIONS WILL ABIDE BY THE REQUIREMENTS SET FORTH IN THE PRINCE WILLIAM ZONING ORDINANCE AND DESIGN AND CONSTRUCTION STANDARDS MANUAL

7. VEGETATIVE COVER TYPES ARE REFERENCED FROM "FOREST COVER TYPES IN THE U.S. AND CANADA" PRODUCED BY THE SOCIETY OF AMERICAN FORESTERS DATED 1980.

8. SOIL INFORMATION WAS OBTAINED FROM THE PRINCE WILLIAM COUNTY

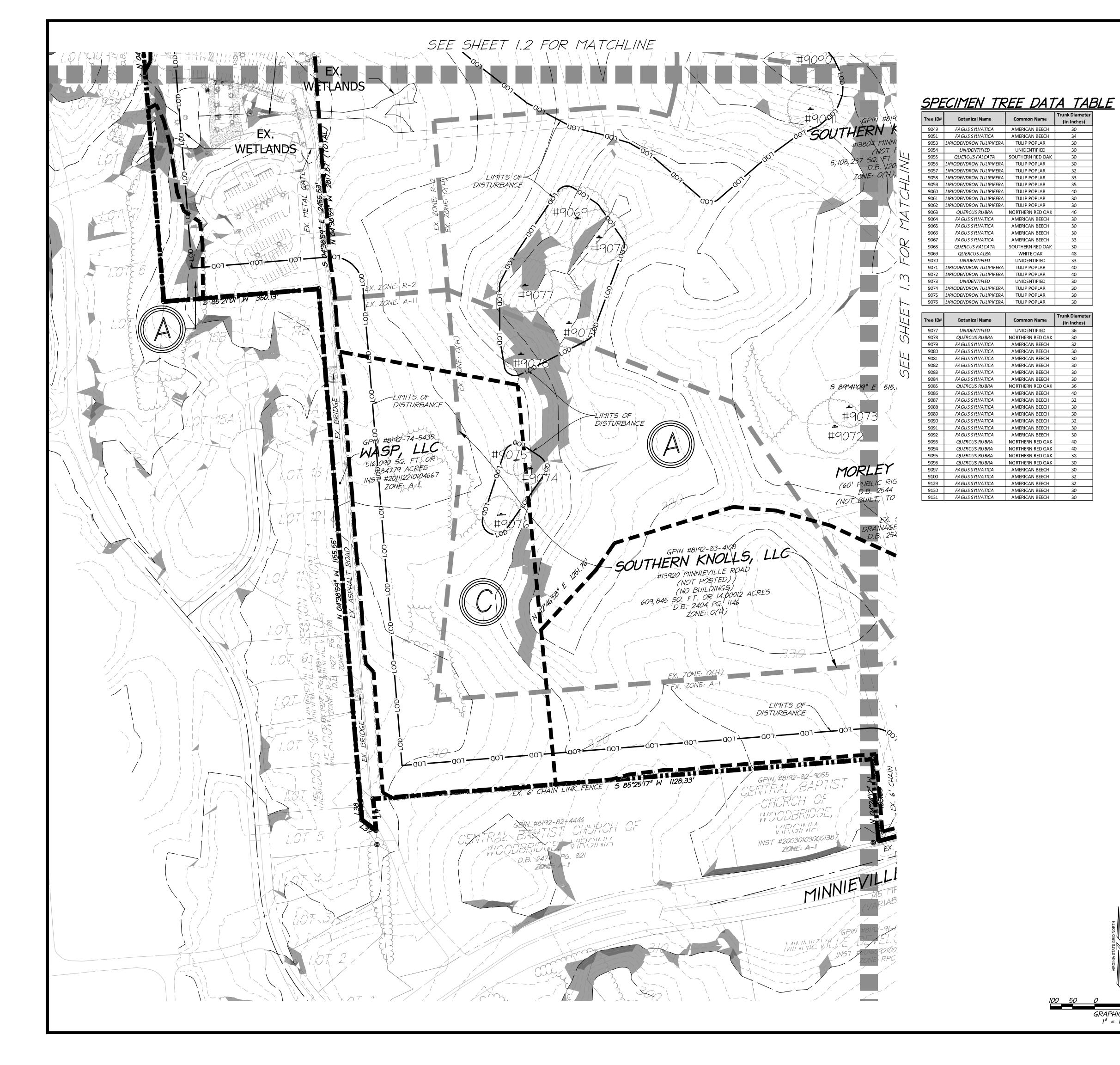
MAPPER DATED MARCH 2018. 9. SPECIMEN TREES DEPICTED ON THIS PLAN WERE LOCATED BY christopher consultants, IDENTIFIED BY ACORN ENVIRONMENTAL INC DATED

01-23-2018, AND MEASURED AT 4.5' ABOVE GROUND. IO. ALL AREAS CONTAINING SLOPES OF 15% OR GREATER ARE LABELED ON DISTRIC ART QU

PROJECT NO:06108.009.00 *|" = 200'*

04-13-2021 DESIGN: WR

DRAWN: WR CHECKED: SHEET No.



	PROJECT STATUS
DATE	ACTION
04/13/2021	2ND SUBMISSION

VEGETATIVE COVER TYPES:

SPECIES COMPOSITION: YELLOW POPLAR, BEECH, NORTHERN RED OAK, WHITE OAK, BLACK CHERRY

THIS AREA HAS BEEN RECENTLY DISTURBED AND CONTAINS UN-MAINTAINED GRASS IN OPEN SPACES BETWEEN TREE STEMS. DOMINANT TREES IN THIS AREA HAVE MULTIPLE LEADERS, ARE COVERED IN VINES, AND HAVE POOR FORM.

LOD : LIMITS OF DISTURBANCE

: EX. CONTOUR SHOWN AT 2FT INTERVALS

: EX. TREE LINE

ECA VEGETATIVE COVER TYPES - : EX. STREET CENTER LINE

BOUNDARY OF SUBJECT PROPERTY TO BE REZONED

-×-----×- : EX. FENCE (ALL TYPES) : EX. WETLAND

GRAPHIC SCALE I" = 100'

(ID#, SPECIES, AND CALIPER SIZE ARE INDICATED WITHIN "SPECIMEN TREE DATA TABLE" ON THIS SHEET)

PEM

:SLOPES >25%

TYPE A - YELLOW POPLAR-WHITE OAK-NORTHERN RED OAK

AGE: VERY-MATURE

TYPE C - YELLOW POPLAR

SPECIES COMPOSITION: YELLOW POPLAR, WHITE ASH

AGE: YOUNG STAND

<u>LEGEND</u>

: LABEL # LIMITS OF

---- : EX. EDGE OF PAVEMENT

: EX. SPECIMEN TREE WITH A 30" CALIPER OR GREATER

: FORESTED WETLAND : EMERGENT WETLAND : INTERMITTENT STREAM : PERENNIAL STREAM

:SLOPES 15%-25%

DISTRICT CONSTRAINTS

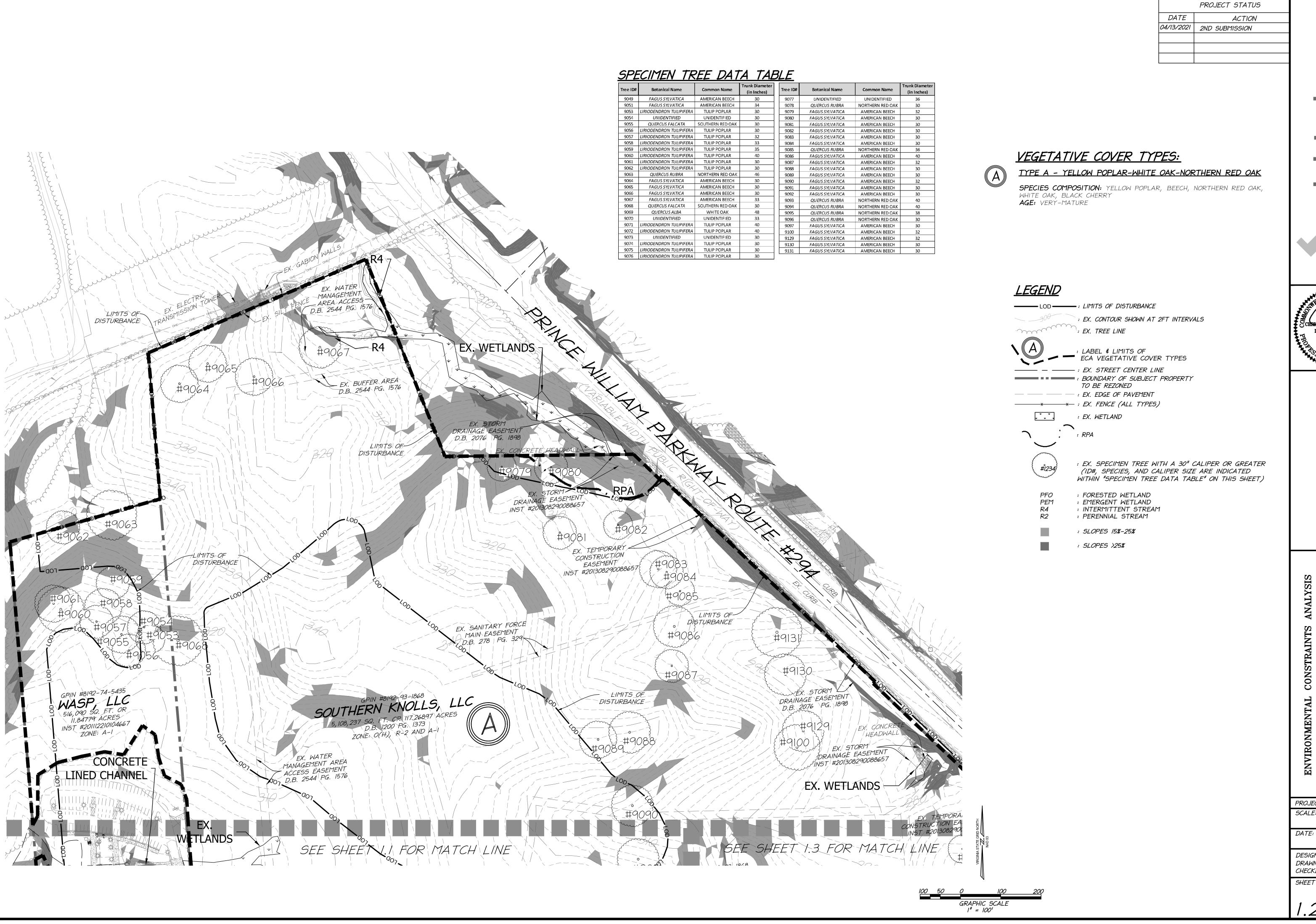
QUART ENVIRONMENTAL

PROJECT NO:06108.009.00 *|" = 100'*

04-13-2021 DESIGN: WR

DRAWN: WR CHECKED: SHEET No.

0F



DISTRICT

PROJECT NO:06108.009.00

QUART

/" = 100'

04-13-2021

DESIGN: WR DRAWN: WR CHECKED:

SHEET No.

PROJECT STATUS DATE **ACTION** 04/13/2021 | 2ND SUBMISSION

VEGETATIVE COVER TYPES:

AGE: VERY-MATURE

AGE: MEDIUM-AGED

PEAR, RUSSIAN OLIVE, HONEY SUCKLE, AND PRINCESS TREE. THE GROUND COVER IS COMPRISED PRIMARILY OF LEAF LITTER, MICROSTEGIUM AND INSTANCES OF GREEN BRIAR.

: EX. CONTOUR SHOWN AT 2FT INTERVALS

---- : EX. EDGE OF PAVEMENT

: EX. SPECIMEN TREE WITH A 30" CALIPER OR GREATER (ID#, SPECIES, AND CALIPER SIZE ARE INDICATED

: PERENNIAL STREAM

TYPE A - YELLOW POPLAR-WHITE OAK-NORTHERN RED OAK

SPECIES COMPOSITION: YELLOW POPLAR, BEECH, NORTHERN RED OAK, WHITE OAK, BLACK CHERRY

TYPE B - VIRGINIA PINE

SPECIES COMPOSITION: VIRGINIA PINE, EASTERN REDCEDAR, BLACK

THIS AREA CONTAINS MANY INVASIVE SPECIES SUCH AS BRADFORD



: EX. TREE LINE

: LABEL # LIMITS OF ECA VEGETATIVE COVER TYPES ----- : EX. STREET CENTER LINE BOUNDARY OF SUBJECT PROPERTY TO BE REZONED

: EX. WETLAND



WITHIN "SPECIMEN TREE DATA TABLE" ON THIS SHEET)

: FORESTED WETLAND : EMERGENT WETLAND : INTERMITTENT STREAM

: SLOPES 15%-25%

: SLOPES >25%

IRONMENTAL AINTS ANAL

04/27/2021

DISTRICT CONSTRAINTS COUNTY,

QUART

ENVIRONMENTAL

PROJECT NO:06108.009.00 *|" = 100'*

04-13-2021

DESIGN: WR DRAWN: WR CHECKED:

SHEET No.

0F



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	* Note: This document was prepared to describe the design approach for Quartz District and is subject to final design decisions and final engineering.				





OVERVIEW – TEAM

DEVELOPER

- Buchanan Partners
- Mitchell L. Phelps, Inc.

BRANDING & RETAIL CONSULTING

- CBRE
- StreetSense

LAND PLANNING

- Christopher Consultants
- Lessard Design
- LD7 Design
- MV&A
- StreetSense

CIVIL ENGINEERING

- Christopher Consultants
- RDA

LANDSCAPE DESIGN

- Christopher Consultants
- LD7 Design

ENVIRONMENTAL CONSULTING

- Acorn Environmental
- ECS Mid-Atlantic, LLC
- WSSI

LEGAL LAND USE

• Walsh, Colucci, Lubeley & Walsh P.C.

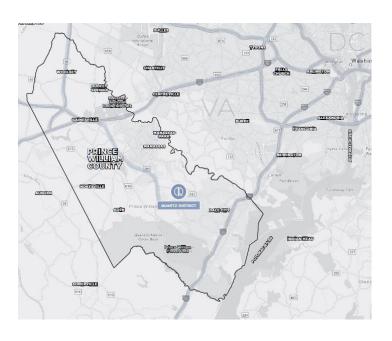
TRAFFIC CONSULTING

• Gorove/Slade Associates, Inc.



OVERVIEW - LOCATION

Quartz District is strategically located on the northwest corner of Minnieville Road and Prince William County Parkway in Woodbridge, VA.







OVERVIEW - DALE CITY SMALL AREA PLAN

Key Quotes from the Dale City Small Area Plan:

- "The Dale City Small Area Plan intends to revitalize and capitalize on one of most prosperous unincorporated communities in the Commonwealth of Virginia ripe for increased economic growth in the eastern portion of Prince William County."
- "The small area plan has been prepared to further the economic development goals of the County to provide opportunities for highquality employment integrated within a pedestrian friendly, mixed-use center that will foster local and regional economic opportunities."
- "The largest transformation of the Parkway Node will be a new community mixed-use center consisting of commercial and residential development along with a transit center on an approximately 143-acre parcel north of the intersection of Prince William Parkway and Minnieville Road."



Dale City Small Area Plan





OVERVIEW – QUARTZ DISTRICT VISION

Quartz District will bring a high-quality, pedestrian friendly mixed-use development to an area of Prince William County that lacks this type of vibrant and distinct placemaking environment. The project will contain a mix of commercial, retail and residential uses using coordinated design, landscaping, pedestrian connectivity and compatible building materials. The residential uses will encompass a variety of single family attached and multifamily residential housing opportunities to complement the existing housing in the area.

Quartz District will incorporate a range of features such as streetscapes, plazas, walkways, open spaces and buffers, along with pedestrian and bicycle networks that will provide connectivity between areas within the project and adjacent areas. The built environment in Quartz District will convey a unified development of complementary scale, material and form. Buildings will be located close to roadways to facilitate views and reinforce pedestrian friendly streetscapes, walkways, and connections.



With a mix of commercial, retail and residential uses integrated using coordinated design, landscaping, pedestrian connectivity, and compatible building materials, Quartz District will be a cohesive community that provides benefits to the greater area and affords residents, businesses, employees, guests and visitors an attractive and desirable environment for living, working, playing and visiting.



OVERVIEW - QUARTZ DISTRICT PROGRAM

Quartz District will be a new 145-acre complete mixed-use community that comprises the majority of the Parkway Node identified in the Dale City Small Area Plan.

The Quartz District development program will address numerous identified goals and desired needs that are highlighted in the Small Area Plan.

Quartz District has three large distinct programmed land areas: the Urban District, the Neighborhood District and a dedicated Public Park/School site.





OVERVIEW – URBAN & NEIGHBORHOOD DISTRICTS

The Urban District will contain the retail and commercial core of the community and will incorporate urban type amenities and architectural character, along with inviting and approachable public spaces, that will support both a lively mix of retail offerings and public events that will provide a unique and memorable experience in this market area. The Urban District will also contain a residential component with urban character and density.

The Neighborhood District will be located on the west, north and east sides of the project and will be a transition area between the surrounding properties and the more active Urban District and Park/School Site.





OVERVIEW - PARK/SCHOOL SITE

The 19-acre Park/School Site, to be dedicated by Quartz District to Prince William County, is envisioned as a combination elementary school and park site, with the final disposition of areas and design between the two uses to be determined solely by the County.

The park component will be the destination urban adventure type of park envisioned by the November 2019 Park Improvement Bond and referenced in the Dale City Small Area Plan. The park is an important planned amenity component that will complement the other planned placemaking and retail amenities that will contribute to the overall vision for Quartz District.

Any illustrative graphics shown in the design guidelines for the Park/School Site plan have been prepared by Quartz District, not the County. The County has sole discretion for how the 19 acres are divided and utilized.







COMMUNITY INFRASTRUCTURE – STREET NETWORK

The roadway system within Quartz District will organize and link all the development areas and be designed in accordance with the following principles:

- The road network will include Private Streets serving internal on-site traffic within residential and commercial development areas and Public Roadways serving off-site and thru traffic demands.
- Internal streets shall have slower travel speeds and, where appropriate, on-street parking.
- All internal streets and public roadways shall have street trees, sidewalks, lighting, and landscape areas to encourage pedestrian activity.
- Internal streets shall incorporate pedestrian crossings. Primary crossings shall have textured surfaces.
- The project will have an overall coordinated lighting plan and site lighting system incorporating parking lot lighting, street lighting, pedestrian scale street lighting, building lighting, and accent lighting.



Public & Primary Private Street Framework



COMMUNITY INFRASTRUCTURE – PUBLIC & PRIMARY PRIVATE ROAD NETWORK

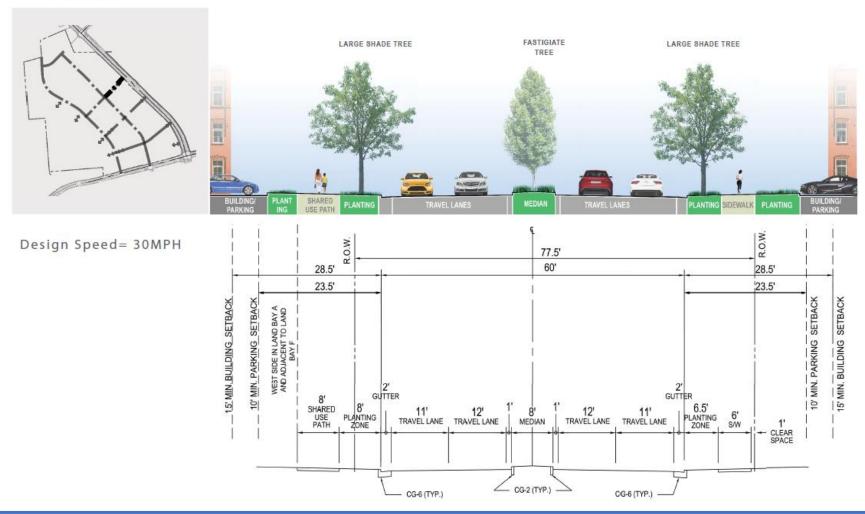
The following detailed, illustrative exhibits for Quartz District's public roads and private streets establish the applicable cross section dimensions for road and lane widths, sidewalk widths, landscaping areas and building setbacks.

In the event the dimensions shown in the cross sections provided in these Design Guidelines conflict with dimensions provided in the Master Zoning Plan, the dimensions shown on the Master Zoning Plan shall prevail.

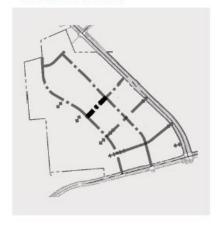
- PUBLIC ROAD
- PRIVATE ROAD
- PEDESTRIAN FOCUSED
- SERVICE FOCUSED



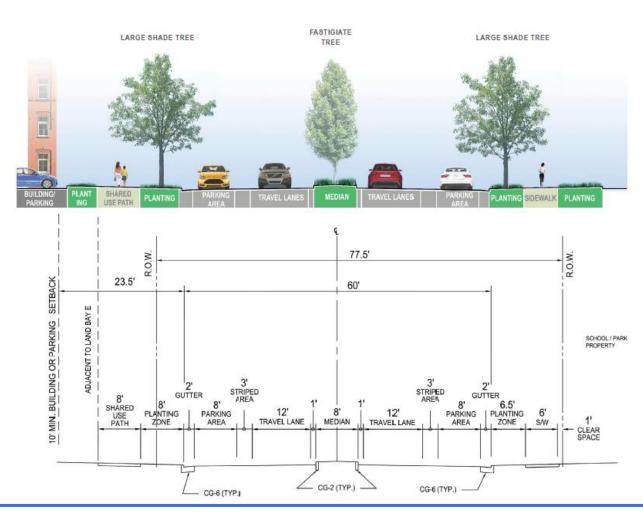








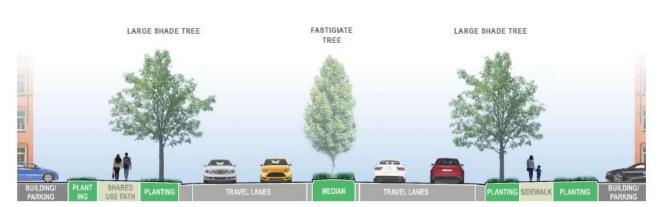
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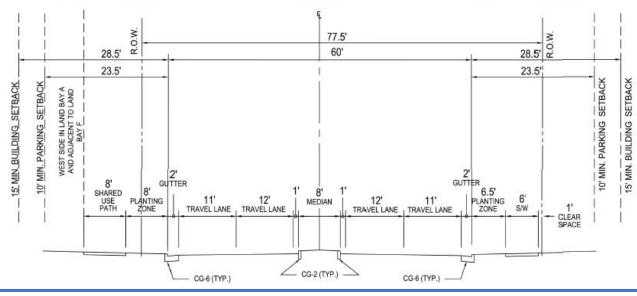




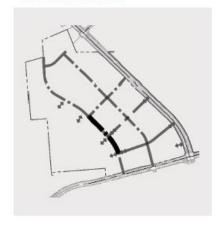


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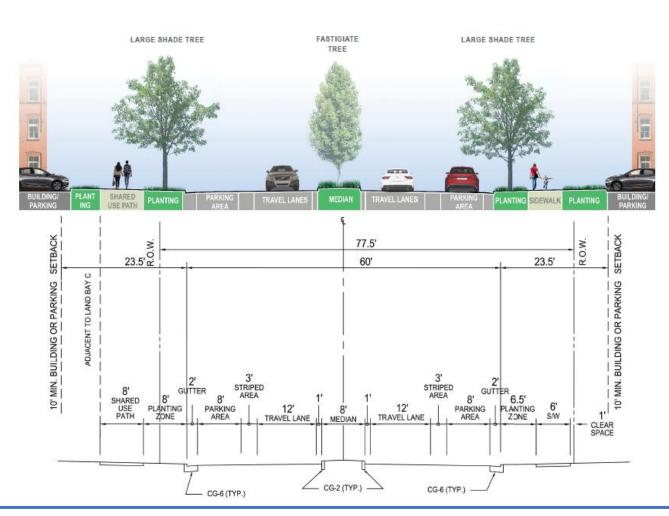








Design Speed= 30MPH

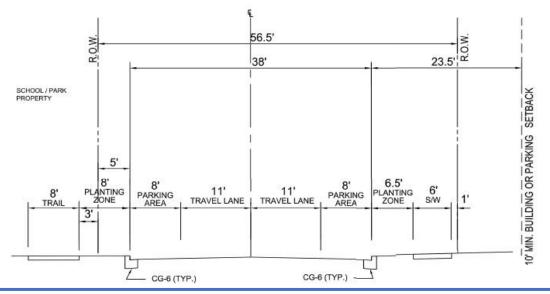






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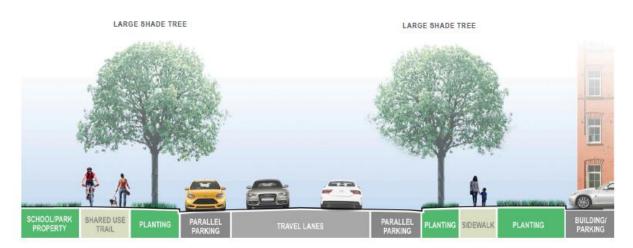


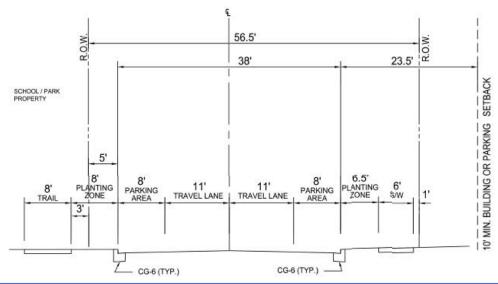




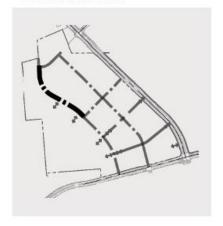


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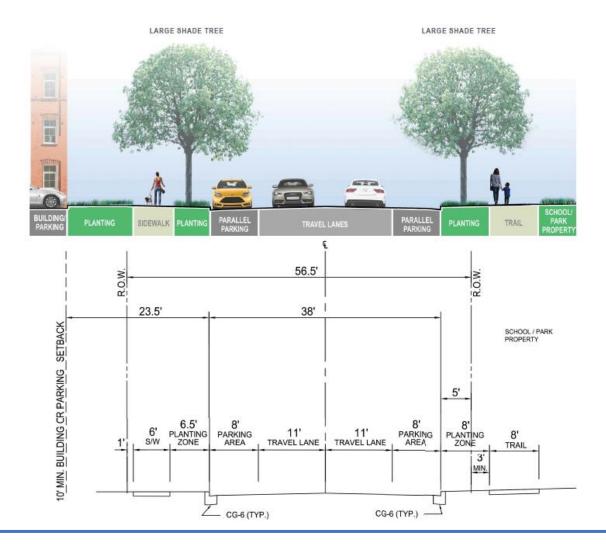




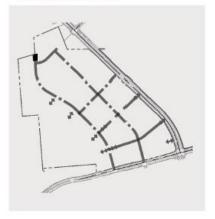




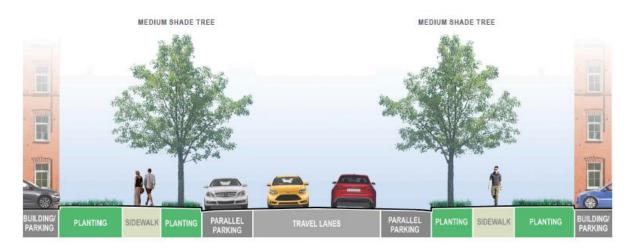
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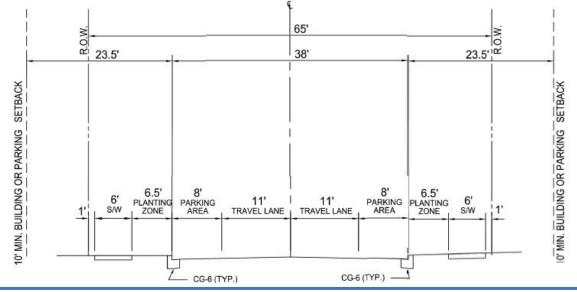






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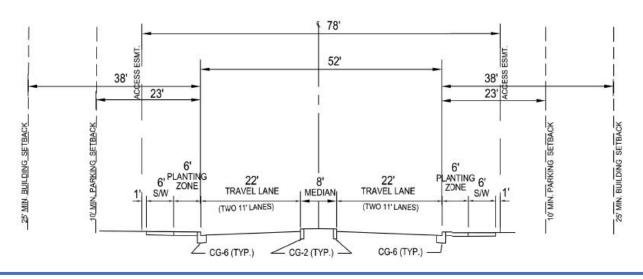




COMMUNITY INFRASTRUCTURE — PRIVATE STREET - TYPE 7A



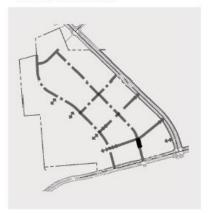


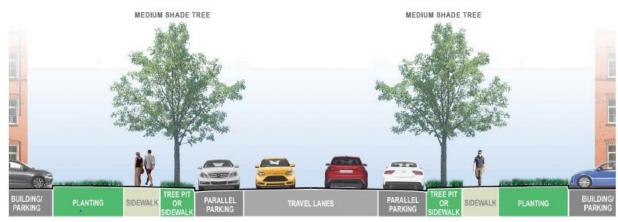


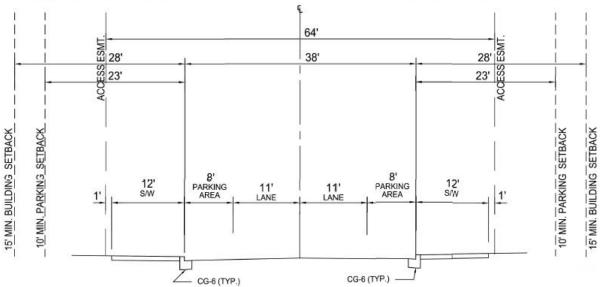


COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 7B

LOCATION MAP



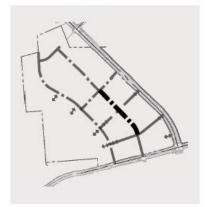




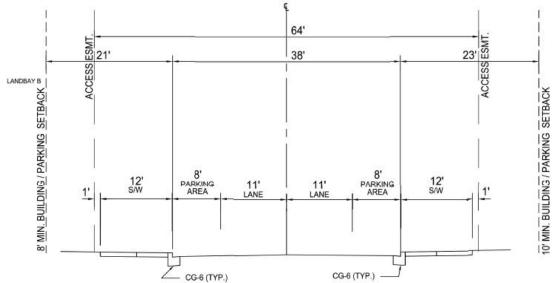


COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 7C

LOCATION MAP



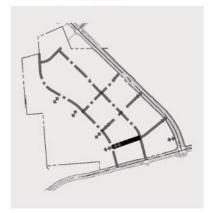




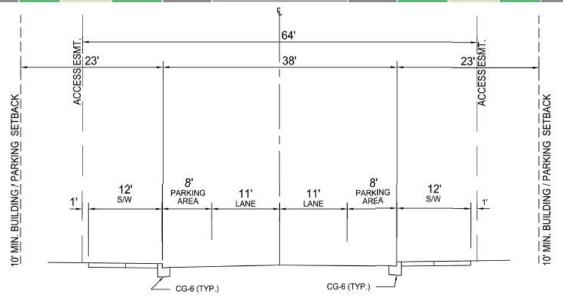


COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 8

LOCATION MAP





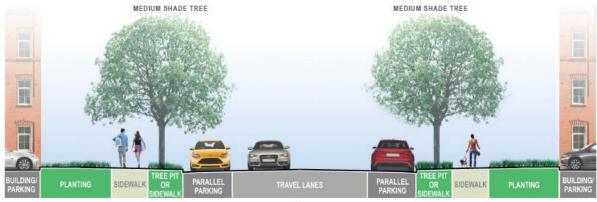




COMMUNITY INFRASTRUCTURE — PRIVATE STREET - TYPE 9A

LOCATION MAP



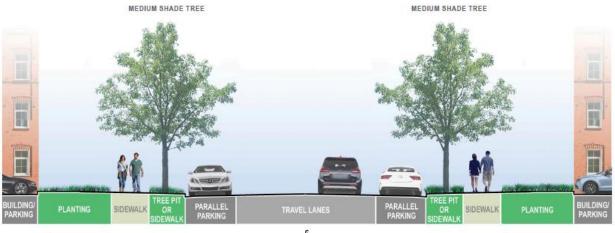


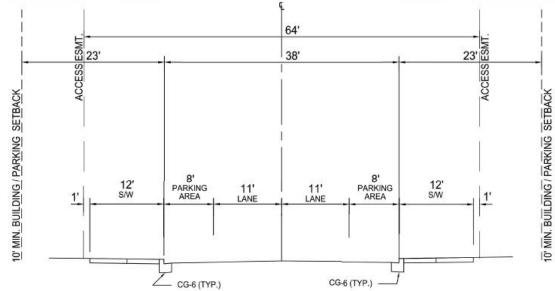
| 12 | 8 | 11 | 11 | PARKING | 12 | 12 | S.W | AREA | LANE | LANE | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | 11 | PARKING | 12 | AREA | S.W | 1 | AREA | S.W | 1



COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 9B

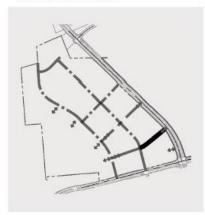
LOCATION MAP

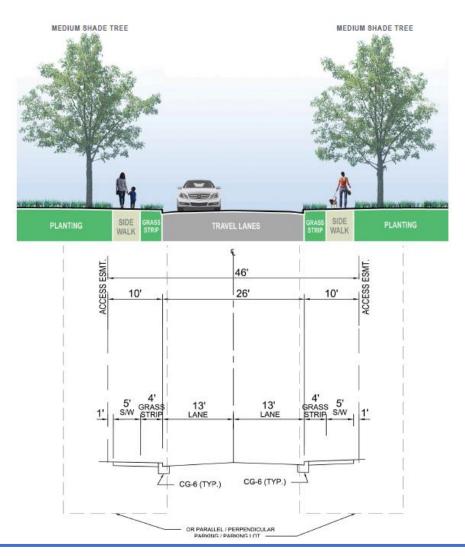






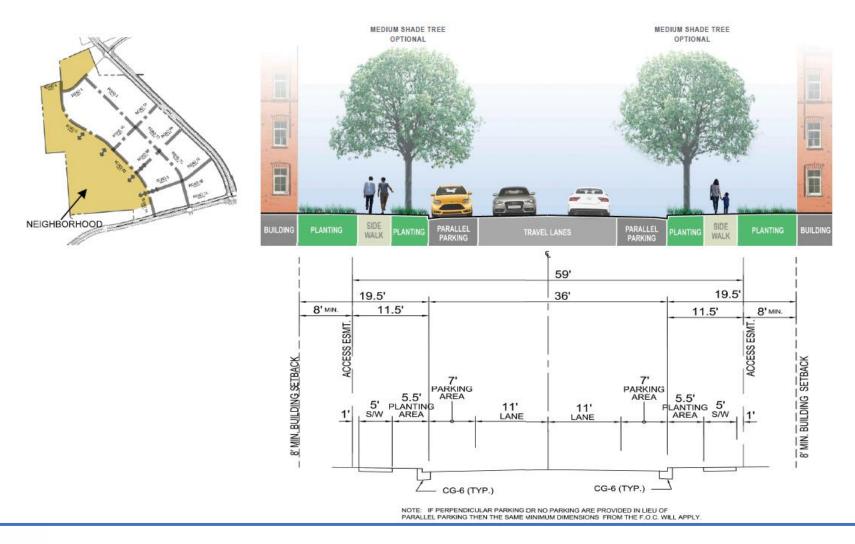
COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 10





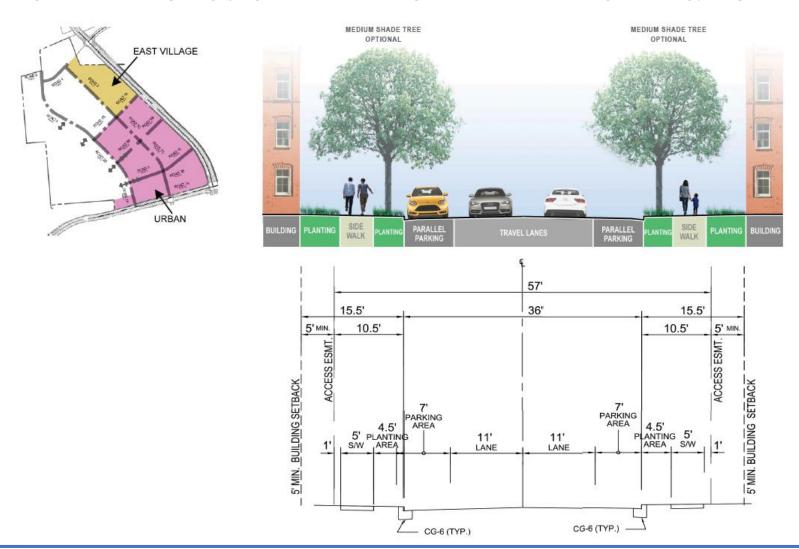


COMMUNITY INFRASTRUCTURE – PRIVATE STREET - TYPE 11 NEIGHBORHOOD



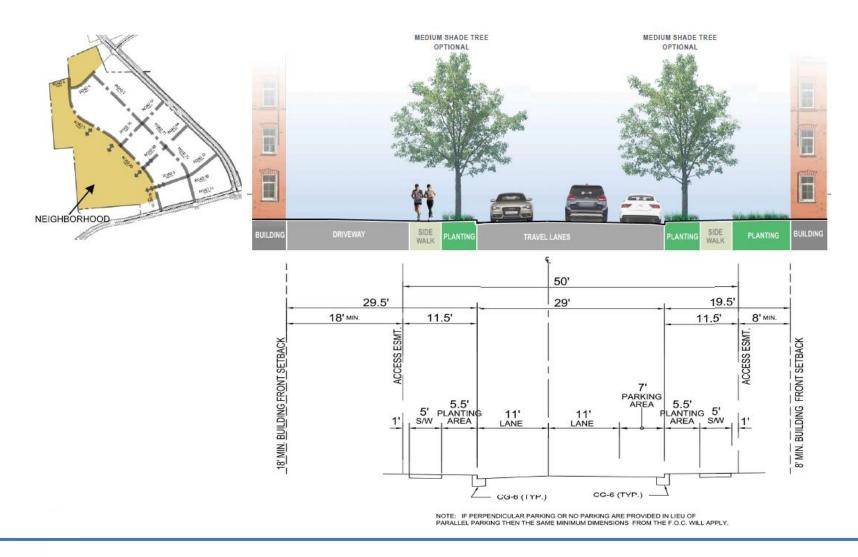


COMMUNITY INFRASTRUCTURE – PRIVATE STREET - TYPE 11 URBAN & EAST VILLAGE





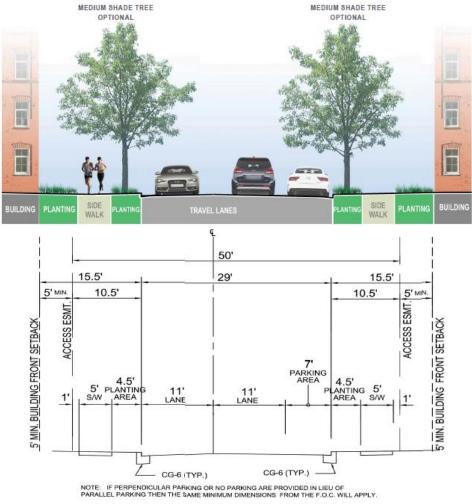
COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 12 NEIGHBORHOOD





COMMUNITY INFRASTRUCTURE – PRIVATE STREET - TYPE 12 URBAN & EAST VILLAGE





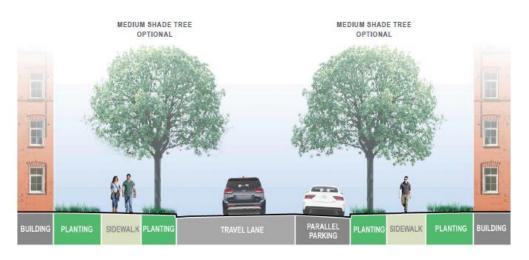


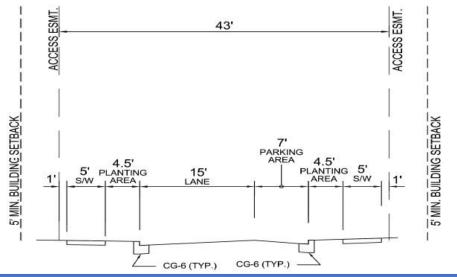


COMMUNITY INFRASTRUCTURE - PRIVATE STREET - TYPE 13 NEIGHBORHOOD & EAST VILLAGE

One Way Street with Parking



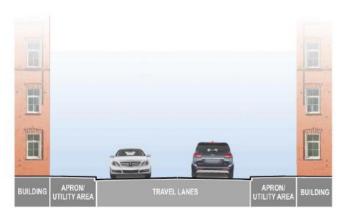


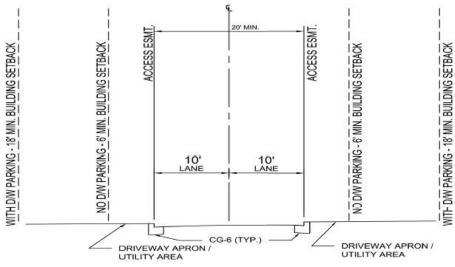




COMMUNITY INFRASTRUCTURE - ALLEY - TYPE 14 NEIGHBORHOOD, EAST VILLAGE & URBAN







NOTE: ALLEYS MAY HAVE PERPENDICULAR OR PARALLEL PARKING SPACES WHERE THERE IS NO BUILDING ACCESSING THAT PART OF THE ALLEY.

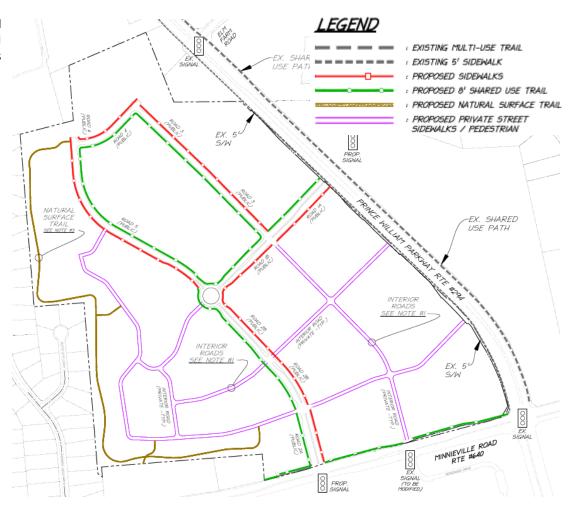


COMMUNITY INFRASTRUCTURE – PEDESTRIAN NETWORK

The Pedestrian Network will include retail streetscapes, residential sidewalks, paved shared use trails and natural surface trails as reflected on the Pedestrian Network Plan.

Sidewalks will be along most public streets, private roadways and provide links to all uses and open spaces to encourage pedestrian traffic. Internal streets shall incorporate designated pedestrian crossing areas, and primary crossing areas will be constructed with identified crosswalks. Crosswalks with alternate paving materials or surface treatments may be utilized in certain areas to be determined at site plan. Pedestrians shall have a clear and direct route from on-site parking to the buildings and the sidewalk system.

Sidewalks adjacent to internal roadways shall be part of an attractive pedestrian oriented environment incorporating design elements such as lighting, signage, canopy trees and benches. For safety, lighting concepts shall be an integral part of the overall design concept anticipating the needs of pedestrian and automobile circulation, open spaces, and residents.





COMMUNITY INFRASTRUCTURE — BICYCLE NETWORK

Quartz District will create a network of safe, attractive, and identifiable linkages for pedestrians, bicycles and automobiles to have equal opportunity to traverse the community with convenience and safety.

The bicycle network shall be a combination of dedicated bike lanes, shared use paths and primary streets and travel ways with sharrows as reflected on the Bicycle Network Plan. Shared use paths along Roads 1A, 1B, 2A and 2B should incorporate both pedestrian and bicycle symbols for twoway traffic.







COMMUNITY INFRASTRUCTURE – AMENITIES

Quartz District will provide a comprehensive collection of project amenities including parks, open spaces, recreation facilities, place making and gathering spaces, lighting, and wayfinding and project identification signage, along with a green infrastructure of both natural and landscaped areas and the previously mentioned comprehensive bicycle and pedestrian networks.

The primary amenity areas will be the dedicated 19-acre public Park/School Site, the Urban District Retail/Lakefront Areas, and the Community Clubhouse. The exact location and design of parks, plazas, trails, sidewalks, landscaping, buffers, and open spaces shall be determined at final site plan along with site lighting, benches, bike racks and other amenity components.







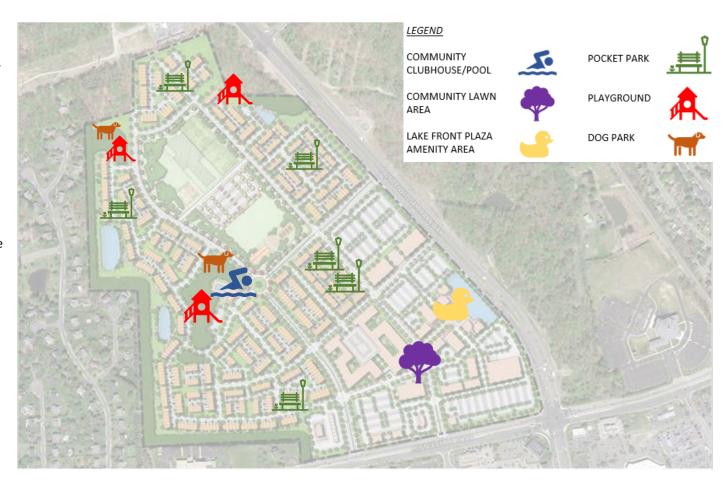




AMENITIES - PARKS & OPEN SPACES

The Neighborhood
District includes the
School/Park Site amenity
and will also provide for
numerous enjoyable
outdoor smaller open
spaces, natural areas,
and passive pocket parks
where residents and
their visitors can interact
and enjoy the outdoors.

The Urban District residential area will have smaller open spaces and pocket parks but with an urban design that will relate more to the buildings and street layout.





AMENITIES – NEIGHBORHOOD RECREATION COMPLEX

The Neighborhood District area includes numerous playground recreation areas for residents including the Community Clubhouse facility.

The Clubhouse complex will have a pool and will be contiguous with a primary tree preservation area that will incorporate one of the playground areas and a community dog park.





AMENITIES - PLACEMAKING & GATHERING SPACES

Quartz District's focus on placemaking and gathering spaces will facilitate experiential retail uses that will attract surrounding residents and create a lively hub within the overall neighborhood.

Public spaces including the lawn, sidewalks, plazas and waterfront area will be inviting and approachable while supporting a lively mix of retail offerings and public events.

Urban amenities and architectural character will offer a unique and memorable experience within the submarket.





AMENITIES – PLACEMAKING & GATHERING SPACES





LAWN

Flexible and functional, a lawn is the classic passive open space. Office workers might bring their lunches outside on a sunny day to eat in the grass, or read a book, or throw a frisbee. Or, ideally, the lawn is sized appropriately to allow all three activities to occur simultaneously. This space may be used for larger gatherings as well, such as movie nights or picnics. Ample shade and seating should be provided at the perimeter.



AMENITIES – PLACEMAKING & GATHERING SPACES





RETAIL SIDEWALKS

Sidewalks are the primary social space in any retail environment. Simple in its form, this powerful component has a physical interaction between the a retail storefront and the ground plane, thus allowing the user to enter this "in between space." The "in between" serves as an extension to the retail experience. In addition to promoting it's retail counterpart, sidewalks encourage discovery, social interaction, and a sense of community.



AMENITIES - PLACEMAKING & GATHERING SPACES





PLAZA / SHARED STREET

The plaza is the hardscape counterpoint to the lawn. A plaza typically requires more programming to remain activated, though its flexibility and functionality lends itself to many types of activities, such as markets, festivals, and small performances. Shade elements should be incorporated around the edges in ways that limit interruptions. Combining wide corners of the retail sidewalks with a raised intersection and a shared street provide the flexibility for larger events in spaces that are practical and appropriately sized for day-to-day uses.



AMENITIES - PLACEMAKING & GATHERING SPACES





WATERFRONT

Large water features, such as ponds, offer the opportunity for a variety of uses along their perimeter. Both passive and social uses – and often both – are appropriate next to water features. They can be a highly prominent placemaking feature and their openness increases both frontage and visibility into the site from nearby roads.



AMENITIES – PLACEMAKING & GATHERING SPACES

SEATING

Site furniture – benches and moveable chairs – and seat walls should be distributed liberally throughout the park. Seating may be either flexible or fixed (including ample amounts of both is best), and is an ideal opportunity to introduce vibrant colors, playful elements, and sculptural forms. Umbrellas or other shade structures should be incorporated where appropriate. Seating should be especially concentrated near retail offerings and nodes such as fountains, fireplaces, and beside the pond.







SHADE STRUCTURES

Tensile structures and shade canopies offer both shade and structure to larger public areas. These canopies help define outdoor dining areas and can be dramatic place markers that create visual interest and provide an opportunity for color. Structures can be either permanent fixtures or seasonal, temporary installations.







PUBLIC ART

Public art enhances the uniqueness of a place and establishes memorable landmarks. A strong and cohesive public art program can help forge emotional connections to a site and aid visitors in wayfinding and meetups. Instagrammable moments reinforce a place's overall brand presence.









AMENITIES - PLACEMAKING & GATHERING SPACES

PAVILIONS

Flexible, open-air structures can be a valuable amenity to any open space. In addition to providing shade and shelter, it can help define space and provide a focal point for activity. Tenants within the neighboring buildings should be encouraged to use this space for office functions. Elevating a pavilion slightly above the space around it on a podium both gives it prominence and provides additional seating at its perimeter.







FOUNTAINS

Fountains, pools, and other water features appeal to visitors of all ages. They activate outdoor environments while delighting and engaging the senses. They add white noise to public dining areas and help create unique interactive elements to public spaces.







FIREPLACES

Fireplaces and fire pits serve as approachable anchor points to an open space. They create a natural gathering place that allows for social interactions or moments of respite. This design element typically results in longer pedestrian stays and speaks to designing for year round habitation. Longer stays allow for a positive sensory memory of their experience, thus further promoting the concept of placemaking.









AMENITIES – PLACEMAKING & GATHERING SPACES

SEASONAL ACTIVITIES

Outdoor open spaces can host a variety of activities and events such as seasonal farmer's markets, concerts and food festivals. Enhancing unique site features and view corridors during events establishes a strong sense of community and identity. The combination of soft spaces, such as the lawn, and hardscape plazas allows for different types of activities to occur simultaneously.







FAMILY PLAY

A key strategy for attracting visitors on evenings and weekends, family play areas should not be misconstrued as a playground. Instead these areas should contain elements that invite activity and exploration, but are also aesthetically pleasing. Fountains, climbing structures, labyrinths, and swings appeal to children of all ages, and these elements are an excellent way to introduce bright and vibrant colors into the natural environment.







TEMPORARY RETAIL

Temporary retail in the form of market stalls, kiosks and food trucks allow for seasonal events and programming. These components are flexible and modular ranging from pop-up tents to covered pavilions. This type of retail use is best suited for events in mature retail environments, after the bricks-and-mortar retailers have been established and are thriving.











AMENITIES – STREETSCAPE FURNISHINGS



STREET LIGHT FIXTURES



TRASH RECEPTACLES





BENCHES



CONCRETE SIDEWALKS



BIKE RACKS



ACCENT PAVERS



PLANTERS



CROSSWALKS

DISCLAIMER: The selections above are intended to be representative in nature and are not considered final.



AMENITIES - MOBILITY

Quartz District will incorporate numerous current mobility and placemaking elements along with the pedestrian and bicycle networks, bike racks, cross walks and wayfinding signage. Vehicular focused mobility features will be EV charging stations, dedicated curbside delivery/pick-up spaces, designated ride-share pick-up/drop-off locations and amenities transit stop enclosures.

Transit stop enclosures will be provided in the Urban District and shall be designed as integral elements of the mixed-use environment by locating them adjacent to pedestrian and bicycle networks. Enclosures will incorporate seating, lighting and shall have design features and materials that are complementary and compatible with adjacent buildings.











AMENITIES - LIGHTING

A coordinated and complementary package of light fixtures shall be provided throughout Quartz District to contribute to a quality environment and provide a sense of security. In each development area, uniformity of fixture and pole design, color, and compatibility with other site furniture elements will help organize the landscape setting. Consistent light levels add to the sense of safety in the nighttime.

Street lighting will be stationed at intervals to provide adequate lighting levels. Pedestrian scale lighting will be utilized to enhance primary pedestrian streetscapes and plazas. Light fixtures in parking lots shall complement the pedestrian lighting. Additional accent lighting, such landscape and building lighting, may be utilized to provide additional nighttime interest and atmosphere.

All light fixtures in parking areas and on building exteriors shall be fully shielded, directed downward, and designed to prevent glare on adjacent properties and public streets. The maximum average illumination for parking lots, structured parking and exterior building lighting shall be in accordance with the applicable County requirements for the various uses proposed for the Property.











AMENITIES – SIGNAGE & WAYFINDING

With a sensitive design and appropriate placement, signage will contribute to the quality image and operation of Quartz District. A comprehensive sign package will be prepared for Quartz District and processed with the County, if required. At a minimum, all signs permitted by right and/or proposed by the sign package may include the following:

Freestanding signs should sit on grade or be attached to a base. Materials shall be compatible and complimentary with building materials and will be durable and weather resistant. The height of signs may vary according to traffic speed and volume, but lower profile or ground level signs are preferred. Signs shall not encroach into the tree canopy. To the greatest extend possible, all electrical elements such as wires, conduit, junction boxes, transformers, ballast, switches and panel boxes shall be concealed from view. Signs will also include landscaping at the base of the sign that is complementary to the sign design as well as the surrounding landscaping.

Building signs shall not be permitted on the roof or to project above the building wall. Signs shall not be allowed to overwhelm or obscure the architectural character of a building. Illumination may be either internal or indirect as permitted by the zoning ordinance or approved comprehensive sign package.

The signage program may include elements such as directories, seasonal graphics, banners, murals, and signature elements, patterns and logos. Signage may also include branded wayfinding and special event signage.









AMENITIES - BRANDING

The project will include branding with the Quartz District logo and an associated geometric pattern. Both may be used throughout the project as repeating signature identifying elements in building and hardscape materials and details, and marketing and community materials. The signage program will also incorporate wayfinding components that will be helpful for navigating Quartz District. It will also utilize the signature logo and/or pattern elements to help identify all project areas as a part of Quartz District.

The Quartz District logo may be updated or changed over time.













GREEN INFRASTRUCTURE – BUFFERS, STORMWATER & PRESERVATION AREAS

The Neighborhood District includes multiple areas that will preserve existing tree stands and specimen trees, including a 70' wide preservation area along the entire western and southern boundaries that are shared with adjacent developed properties. A natural surface path system will be created within this linear preservation area, and other preservation areas will contain or be contiguous with playground, dog park and Community Clubhouse/Pool amenities.

Stormwater Management ponds will be wet ponds and shall be designed and landscaped to appear natural, all subject to approval per Prince William County standards.





GREEN INFRASTRUCTURE – BUFFERS, STORMWATER & PRESERVATION AREAS









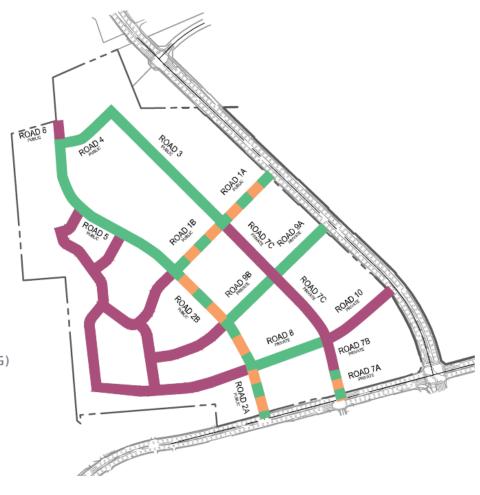


GREEN INFRASTRUCTURE – STREETSCAPE LANDSCAPING

All internal streets and public roadways shall have street trees, sidewalks, lighting, and landscape areas to encourage pedestrian activity.

Street trees are essential elements of the planned landscape improvements within Quartz District and are required on all public roads and private streets. Street trees shall be of a type that provides a canopy at maturity. Spacing will depend on final site plan engineering and ultimate sizes of trees.

- LARGE SHADE TREES (±35' O.C. SPACING)
- MEDIUM SHADE TREES (±35' O.C. SPACING)
- FASTIGIATE TREES (±25' O.C. SPACING)





GREEN INFRASTRUCTURE – STREETSCAPE LANDSCAPING

STREET TREE SELECTIONS

LARGE SHADE TREES (±35' O.C. SPACING)

London Planetree
Platanus x acerifolia

Scarlet Oak
Quercus coccinea
Littleleaf Linden
Tilia cordata 'Greenspire'

Thornless Honeylocust
Gleditsia triacanthos var. inermis



MEDIUM SHADE TREES (±35' O.C. SPACING)

Yellowwood

Cladrastris kentuckea

Black Gum

Nyssa sylvatica 'Wildfire'

Red Maple

Acer rubrum 'October Glory'

Katsura tree

Cercidiphyllum japonicum

Ginkgo

Ginkgo biloba 'Autumn Gold'











FASTIGIATE TREES (±25' O.C. SPACING)

Armstrong Maple

Acer x freemanii 'Armstrong'

Princeton Sentry Ginkgo

Ginkgo biloba 'Princeton Sentry'

Columnar Purple Beech

Fayqus sylvatica 'Dawyck'









GREEN INFRASTRUCTURE - LANDSCAPING GUIDELINES

Quartz District landscaping shall have high quality design and materials to provide an attractive and appealing environment throughout the community.

- A holistic landscape design that provides color, texture, screening and enhancement of the architecture can reinforce the standards of a high-quality development.
- The overall landscaping design shall consist of a variety of trees, shrubs and ground covers to create a wellintegrated landscape.
- Shrubs and ground covers should be planted in masses of a single species and in sufficient numbers to create bed or drifts of plants. Selected plants should provide strong seasonal and visual interest, variation in texture, habitat, color and flowering time.
- Evergreen trees or shrubs should be used for screening where possible.
 The use of indigenous plant materials should be prioritized.



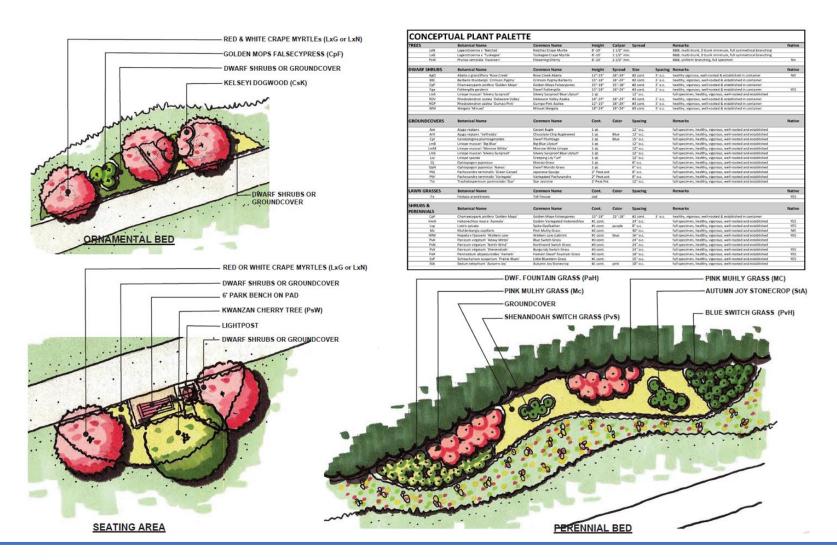








GREEN INFRASTRUCTURE - PLANTING CONCEPTS







Design Principles: Located on the west, east and north sides of Quartz District the Neighborhood District will be a transitional area between the adjacent surrounding properties to the west and the more active and urban areas of Quartz District to the east.

The Neighborhood District residential portions of Quartz District will be designed to be a neighborhood environment with more of a suburban feel, incorporating curved and less structured street patterns that will create more irregular and random internal green spaces.

The street and alley network in this area will incorporate areas of both parallel and perpendicular parking for guests and will also incorporate sharrows for bicycle connectivity to the Community Clubhouse, the Park/School Site and the Urban District.

The Neighborhood District streets will be designed to encourage slow speeds and facilitate a safe pedestrian environment.





The Neighborhood District will consist of primarily single-family attached units with high-quality more traditional architectural designs in a variety of sizes and types.

These units will include front load garage units, rear load garage units with driveways and rear loaded units without driveways. The front load units will be a minimum of 22' wide and will have their rear lot lines adjacent to the 70' tree preservation areas along the common property lines with Central Baptist Church and the Cedar Run subdivision.

There will also be rear loaded two-over-two or back-to-back type multi-family units in areas of the Neighborhood District that are closest to the Urban District.

The East Village area of the Neighborhood District will have a denser and more urban feel than the other areas of the Neighborhood District due to its location adjacent to the Urban District, the Park/School Site and Prince William Parkway. East Village will have all rear load units, many without driveways, and a more grid type framework.



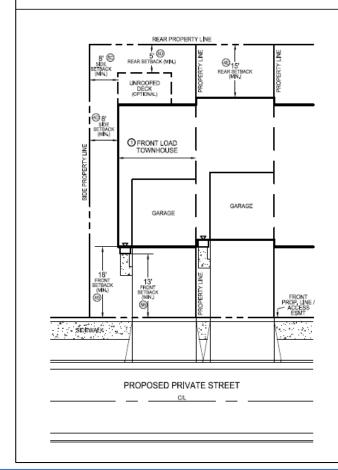








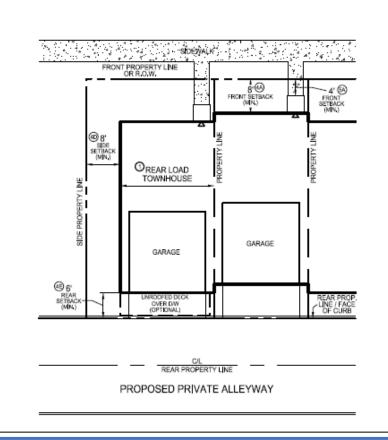
NEIGHBORHOOD DISTRICT TYPE F / TOWNHOUSE UNITS



BUILDING TYPE:	TYPE F / TOWNHOUSE (FRONT-LOAD)		
	PMR REQUIREMENT	PROPOSED REQUIREMENT	DEPICTED (YES / NO)
(1) MINIMUM LOT WIDTH:	20 FEET	NO CHANGE	YES
(2) GROUP SETBACK:	20 FEET	15 FEET	NO
(3) MAXIMUM BUILDING HEIGHT:	35 FEET	35 FEET	NO
(4) MINIMUM SETBACKS:			
(a) FRONT (WITH OFF-STREET PARKING)	10 FEET	NO CHANGE	NO
(b) FRONT (WITH GARAGE)	20 FEET	18 FEET	YES
(c) SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FEET	10 FEET	NO
(d) SIDE (END UNIT)	10 FEET	8 FEET	YES
(e) REAR	20 FEET	15 FEET	YES
(5) MINIMUM SETBACK FOR UNROOFED DECKS, STOOPS, LANDINGS AND SIMILAR FEATURES:			
(a) FRONT (WITH OFF-STREET PARKING)	5 FEET	NO CHANGE	NO
(b) FRONT (WITH GARAGE)	15 FEET	13 FEET	YES
(c) SIDE (END UNIT)	10 FEET	8 FEET	YES
(d) REAR	8 FEET	5 FEET	YES
(6) MINIMUM BUILDING FOOTPRINT	720 SQ. FT.	NO CHANGE	NO



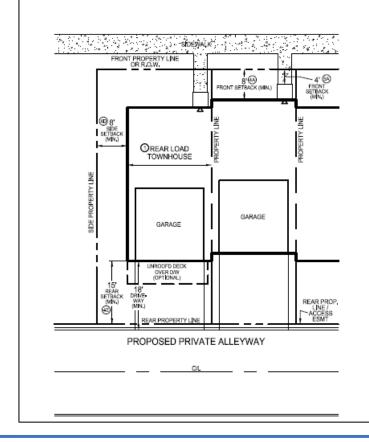
NEIGHBORHOOD DISTRICT TYPE F / TOWNHOUSE UNITS



BUILDING TYPE:	TYPE F / 1	TYPE F / TOWNHOUSE (REAR-LOAD)		
	PMR REQUIREMENT	PROPOSED REQUIREMENT	DEPICTED (YES / NO)	
(1) MINIMUM LOT WIDTH:	20 FEET	16 FEET	YES	
(2) GROUP SETBACK:	20 FEET	15 FEET	NO	
(3) MAXIMUM BUILDING HEIGHT:	35 FEET	35 FEET	NO	
(4) MINIMUM SETBACKS:				
(a) FRONT (WITH OFF-STREET PARKING)	10 FEET	8 FEET	YES	
(b) FRONT (WITH GARAGE)	20 FEET	N/A	NO	
(c) SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FEET	10 FEET	NO	
(d) SIDE (END UNIT)	10 FEET	8 FEET	YES	
(e) REAR	20 FEET	6 FEET	YES	
(5) MINIMUM SETBACK FOR UNROOFED DECKS, STOOPS, LANDINGS AND SIMILAR FEATURES:				
(a) FRONT (WITH OFF-STREET PARKING)	5 FEET	4 FEET	YES	
(b) FRONT (WITH GARAGE)	15 FEET	N/A	NO	
(c) SIDE (END UNIT)	10 FEET	8 FEET	NO	
(d) REAR	8 FEET	0 FEET	NO	
(6) MINIMUM BUILDING FOOTPRINT	720 SQ. FT.	NO CHANGE	NO	

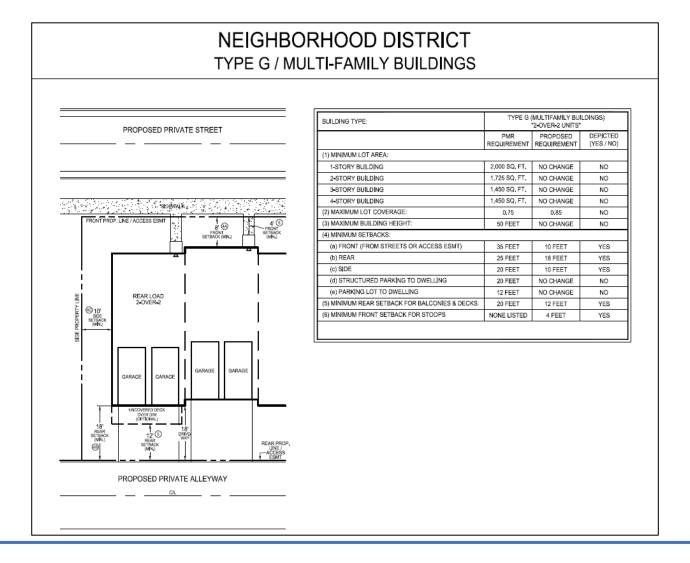


NEIGHBORHOOD DISTRICT TYPE F / TOWNHOUSE UNITS



BUILDING TYPE:	TYPE F / TOWNHOUSE (REAR-LOAD)		
	PMR REQUIREMENT	PROPOSED REQUIREMENT	DEPICTED (YES / NO)
(1) MINIMUM LOT WIDTH:	20 FEET	16 FEET	YES
(2) GROUP SETBACK:	20 FEET	15 FEET	NO
(3) MAXIMUM BUILDING HEIGHT:	35 FEET	35 FEET	NO
(4) MINIMUM SETBACKS:			
(a) FRONT (WITH OFF-STREET PARKING)	10 FEET	8 FEET	YES
(b) FRONT (WITH GARAGE)	20 FEET	N/A	NO
(c) SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FEET	10 FEET	NO
(d) SIDE (END UNIT)	10 FEET	8 FEET	YES
(e) REAR	20 FEET	15 FEET	YES
(5) MINIMUM SETBACK FOR UNROOFED DECKS, STOOPS, LANDINGS AND SIMILAR FEATURES:			
(a) FRONT (WITH OFF-STREET PARKING)	5 FEET	4 FEET	YES
(b) FRONT (WITH GARAGE)	15 FEET	N/A	NO
(c) SIDE (END UNIT)	10 FEET	8 FEET	NO
(d) REAR	8 FEET	NO CHANGE	NO
(6) MINIMUM BUILDING FOOTPRINT	720 SQ. FT.	NO CHANGE	NO







NEIGHBORHOOD DISTRICT – ARCHITECTURAL DESIGN GUIDELINES

Design Principles: Architectural design, materials, and detailing of the Neighborhood District must be aesthetically pleasing on all facades with particular attention being given to end wall elevations and rear elevations which are visible from streets and public gathering places.
 Building form, scale, and proportions shall be harmonious with the design of adjacent buildings and shall be "pedestrian friendly" in orientation rather than vehicular and have a more suburban townhouse community character in overall feel.

Standards: All building facades shall incorporate design detail, articulation and quality materials and honor the following standards:

- Exterior building materials utilized for all residential structures may include brick, real wood, cementitious or fiber board siding or a similar material. Vinyl siding is not permitted.
- Trim and detailing must be precast, wood, simulated wood product, painted pre-finished, the same on three sides and shall reinforce the character or architectural style of the building.
- An unbroken, single plane façade which comprises the entire front elevation of single-family attached units shall be prohibited.
- Windows shall be included on all end elevations of single-family attached units and multi-family two over two units.
- The rear elevations of all residential structures shall not face major roadways.

- Sides of high-visibility end unit single-family attached and/or multifamily two over two residential structures shall contain the same materials as front elevations.
- Rear elevations of single-family attached and/or multi-family two over two residential structures visible from the public right-of-way shall have upgraded trim details levels and shall have no more than two adjacent units with the same siding color.
- · Windows may be vinyl or vinyl clad.
- Front elevations of buildings containing single-family attached units, multi-family two over two units, or back-to-back units shall incorporate multiple features like dormers, reverse gables, feature window features, covered entrances, stoops, or porches to add interest and character to the overall building elevation.
- All roof pitches on single family attached and multi-family two over two residential structures should be proportional with the design of the building. Intermixing gable roofs and hipped roofs is encouraged to promote a visually exciting and animated streetscape. Where architecturally appropriate, roofs shall provide articulation and variation to divide the massiveness of the roof.
- Gutters and downspouts should be integrated into the architectural design in color, shape, and location. For multi-family structures, all flashing, attic ventilators, metal chimneys, and vents must be painted to complement the roof or the material they are adjacent to.



NEIGHBORHOOD DISTRICT – ARCHITECTURAL DESIGN GUIDELINES

Accessory structures, porches, balconies, decks and driveways: Traditional uses of intermediate spaces between the public and private realms, such as porches and balconies help to create streetscape appeal and facilitate community interaction. Residents do have a right to create private areas and spaces but should do so in conjunction with the following guidelines:

- Any detached structures shall be of complimentary design and materials as the primary structure and subject to Homeowners' Association approval.
- Wing walls may be used for creating privacy if consistent with the architectural style but should be integrated and not appear as separate or add-on elements.
- Porches and balconies on front, sides, and in view of streets or public places shall be painted, unless the architecture is integrated with the style and specific design of the home.
- Decks and balconies should be designed to highlight unit identity and to help create residential scale. They should be an integral part of the design and should not appear as separate or add-on elements.
- All walks are to be concrete or brick/concrete pavers.
- Driveways shall be either asphalt or concrete or brick/concrete paver. Placement of driveways will be such that street tree plantings are not eliminated. The concrete driveway apron should be extended into the driveway to match the sidewalk so that visually the sidewalk is not divided by a portion of asphalt.













URBAN DISTRICT – COMMERCIAL SITE PLANNING PRINCIPLES

The Urban District will contain the retail and commercial core of the community and will incorporate urban type amenities and architectural character. It will include inviting and approachable public spaces that will support both a lively mix of retail offerings and public events that will provide a unique and memorable experience in this market area. The Urban District will also contain a residential component with an urban character and density.

This mixed-use core of the community will be sited at the main intersection of Prince William Parkway and Minnieville Road and contain over 300,000 sq. ft. of commercial uses as well as lawn areas, retail sidewalks, a plaza and waterfront gathering areas creating a place where employers, employees, residents and visitors will all share the active social programming, public spaces, streetscapes and retail amenities of the Quartz District.

The Urban District's urban-like character will distinguish it from nearby suburban shopping and residential areas, and pedestrian walkability will be a key design feature. A focus on experiential retail and placemaking elements will appeal to visitors of all ages, will drive repeat visits from surrounding residents and create a lively hub within the overall Quartz District community and the surrounding region.

The Urban District's street grid framework is designed to accommodate future redevelopment and transit opportunities when development patterns and market values will allow structured parking to replace surface parking.



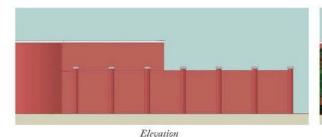


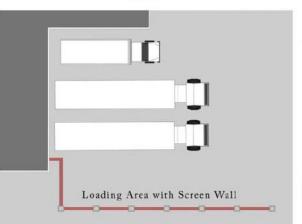


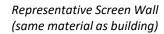
URBAN DISTRICT – COMMERCIAL SITE PLANNING PRINCIPLES

Non-Residential Standards: A goal of Quartz District is to create a sense of place and human scale through strategic placement of buildings adjacent to internal streets, travel ways and or roadways by considering the following guidelines:

- Building setbacks shall be tailored to the desired roadway character with smaller setbacks along interior streets and larger setbacks with buffers along major streets. In general, buildings shall be located closer to internal streets to frame views, provide a strong streetscape and help reinforce the pedestrian setting along sidewalks.
- Along private streets, front yard setbacks shall be set between building and roadway curb. Building setbacks along internal streets shall provide adequate room for streetscape improvements such as bench seating, outdoor cafes, etc.
- Service areas, including dumpsters and loading areas, shall be orientated away from roadways towards a rear parking storage area. Service bays and dumpsters shall be screened from view of adjacent roadways, sidewalks, trails and parking areas either with landscape materials or with materials to match the building architecture.









Elevation



Representative Landscaping Screen



URBAN DISTRICT – COMMERCIAL SITE PLANNING PRINCIPLES

Parking: Parking is the biggest consumer of land in suburban development. While visually unappealing, it is critical to retail operations and project success that shoppers, workers, and visitors can move through the Quartz District and enjoy the nearby pedestrian friendly streetscapes, building facades, window displays, and parks. The following guidelines should be incorporated where possible:

- Parking should not become visually dominated until a visitor chooses to park his car and move from a street or travel way to the interior parking areas.
- Where possible, parking lots will be separated from roadways with buildings or screened with landscape improvements. Views of major parking fields adjacent to roadways shall be broken up with buildings and/or landscape buffers or features.
- Parking fields shall be subdivided into smaller, visually contained areas with perimeter landscaping and landscape islands. The parking lot shall have a landscaped island or row of islands between every 3rd or 4th parking aisle or equivalent. The typical size of these subdivided parking areas shall range from I00 to 150 cars.
- If parking lots are located directly adjacent to private roadways, the streetscape design shall encourage pedestrian activity using pedestrian amenities such as seating areas or landscaping.
- Screening of parking lots adjacent to roadways shall consist of a low wall or evergreen hedge that will screen bumpers, wheels, and paving, while allowing surveillance.
- Parking areas shall be illuminated for security.
- Future structured parking shall be located behind buildings and shielded from the street whenever possible. If structured parking is not shielded from the street by a building, the first floor of the parking structures shall have a building-like façade design.



Typical parking lot with landscape separation



Commercial Buildings

Design Principle: Commercial buildings will be strategically placed and designed to create a sense of place and emphasize human interaction and scale.

Design Standards

- Within all areas there shall be compatible and complementary architectural design element details, materials, and colors, which create themes within the individual development areas.
- Building setbacks along internal streets shall provide adequate room for streetscape improvements.
- Buildings whose side or rear facades face public streets shall incorporate enhanced landscaping and/or architectural walls to screen views.
- Rooflines shall be designed to give visual relief by varying height and/or materials.
- Retail building front elevations with frontage on streets must include larger or display type windows.
 Where possible, window openings of appropriate scale shall be encouraged on side and rear elevations that do not face streets, but it is not anticipated that retail buildings will have any rear windows.
- The primary entrances of buildings will be highlighted with an entry feature such as an awning, roof feature, paving or other decorative elements.



- Long building facades shall provide incremental setback variations and a variety of façade designs or materials to create shadows and visual interest.
- Rooftop mounted mechanical equipment should be screened from view or set back from building perimeter such that it is not visible from adjacent streets and travel ways.
- The "back-of-the-house" facilities and equipment that serve buildings include
 utilities, maintenance equipment, telecommunications equipment and service
 areas for loading, trash pickup and recycling. Such facilities and equipment should
 be located in a manner that minimizes their impact on the pedestrian and visual
 environments, including views from adjacent residences or office buildings, while
 still enabling easy access by utility and service providers.



Commercial Building Materials

Design Principle: Well-crafted buildings using quality materials can visually cater to the senses of the public looking for special experiences and is one of the most important elements in creating an image for a planned mixed-use project. Quality materials that respect the special features and characteristics of the community will help to enhance the project. A sense of quality and durability should play a role in the selection of all building materials.

Materials Standards

- Freestanding buildings shall be clad with appropriate exterior materials and, where possible, contain windows on at least two sides. Party wall buildings shall be finished and contain windows on elevations facing onto streets.
- Exterior materials for all buildings shall not include unpainted non-architectural concrete block, corrugated metal, or exterior plywood. Vinyl and aluminum siding should not be used.
- Exterior building materials utilized for the in-line or attached buildings may include brick, split-faced block, fluted block tile, concrete block, real wood, glass, metal, stucco, and EIFS.
- Standard concrete masonry block shall not be used for the facades of the in-line or attached commercial buildings.
- Use high quality finishes and materials that will both respect the rest of the building and will provide for a more durable and lower-maintenance facade.









Commercial Building Materials

Materials Standards Continued:

- Use high quality finishes and materials that will both respect the rest of the building and will provide for a more durable and lower-maintenance facade.
- Coordinate the types and colors of materials for signs, door and window frames, cornices, walls, awnings, and other storefront elements.
- Use materials can withstand graffiti, dirt and dust.
- Materials should wrap a corner before a change in materials occurs.
- A mix of materials should be incorporated into the design.
 Intense colors should only be used as accents. A change in material or color should be designed with a change in wallplane.











Commercial Storefronts and Grade-Level Spaces

Design Principle: The architecture along the street frames the public domain, while its detailing acknowledges the people that walk along its length. The optimal street environment allows continuous communication to occur between the inside and outside of buildings and its detailing encourages public exchanges and community. Awnings and canopies are accents to architectural features, and mark thresholds between inside and outside and the transition from public to private. They therefore should communicate on two levels: as a definer of the public realm, and as an expression of the establishment's individuality. They should entice the public into the shops, restaurants and businesses they enhance.

Standards

- Weather-protection features such as awnings, canopies, porticos, and entry elements should be provided at building entrances. Canopies typically refer to elements extending perpendicular from the main building entry towards the street. Awnings typically refer to elements which extend over and shade storefront windows of commercial businesses. Awnings may be used as decorative architectural features, such as in the mid-height floor windows of a hotel.
- Canopies should frame entrances. Posts which support a canopy should not interfere with the clear movement zone of the sidewalk. Consider the design of other methods of structural support, such as cables or rods attached to the building and extended out to hold the canopy from above.



- A series of awnings provided along an establishment's façade should maintain a consistent design.
- Awnings maybe located at grade and second-level windows. The width
 of an awning would typically match the width of the building's opening
 for the window. Other locations for awnings may be considered.
- Canopies and awnings should be of fire-resistant material, or of metal and/or glass treated to withstand oxidation, corrosion, and deterioration from airborne salts. Awning fabrics will vary, and the basis for selection should include color retention and durability.
- Awnings can be of various forms and sizes but should not extend more than 4 feet from the face of the building and should not be lower than 8 feet above finished grade.



URBAN DISTRICT - RESIDENTIAL SITE PLANNING PRINCIPLES

The residential areas are located on the west and north sides of the Urban District and will be designed as an urban neighborhood environment with a grid of straight streets and alleys to create more structured and smaller internal green spaces. The streets will incorporate parallel parking and an urban pedestrian friendly streetscape and will have sharrows for bicycle connectivity with other areas of Quartz District. The Urban District streets will be designed to encourage slow speeds and facilitate a safe pedestrian environment.

The Urban District will consist of both single-family attached units and multi-family units that will have high-quality, more contemporary, urban architectural designs. The single family attached units and for sale two-over-two/back-to-back multifamily units will all be neo-traditional type with rear load garages and alley. Most units will not have driveways, to create a denser more compact urban layout and pedestrian friendly streetscapes without garage doors.



There will also be a multi-family apartment component in the urban residential area. This rental apartment project will have its own indoor and outdoor recreational amenities and will initially be surface parked, with a surface lot layout that is designed to facilitate ultimate structured parking and future development potential.



URBAN DISTRICT – RESIDENTIAL ARCHITECTURAL DESIGN GUIDELINES

Design Principles: Architectural design, materials, and detailing of the Urban District residential areas will have a contemporary urban appearance. Building form, scale, and proportions shall be harmonious with the design of adjacent commercial, retail and multifamily apartment buildings. Flat roof architecture appearance will be provided, and roof top decks will be offered as options. Streets shall be pedestrian friendly in orientation rather than vehicular and have a more urban character in overall feel. There will be no front facing garage doors as all units will have rear alley load garages. Building design, materials and details must be aesthetically pleasing on all facades with particular attention being given to end wall elevations and rear elevations which are visible from streets and public gathering places.

Standards: All building facades shall incorporate design detail, articulation and quality materials and honor the following standards:

- Exterior building materials utilized for all residential structures may include brick, real wood, cementitious or fiber board siding or similar material. Vinyl and aluminum siding is not permitted.
- Trim and detailing must be precast, wood, simulated wood product, painted pre-finished, the same on three sides and shall reinforce the character or architectural style of the building.
- An unbroken, single plane façade which comprises the entire front elevation of single-family attached units may be considered in the Urban District only.
- Windows shall be included on all end elevations of single-family attached units and multi-family two-over-two units.
- The rear elevations of all residential structures shall not face major

roadways.

- Sides of high-visibility end unit single-family attached and/or multifamily two over two residential structures shall contain the same materials as front elevations.
- Rear elevations of single-family attached and/or multi-family two over two residential structures visible from the public right-of-way shall have upgraded trim details levels and shall have no more than two adjacent units with the same siding color.
- Windows may be vinyl or vinyl clad.
- Front elevations of buildings containing single-family attached units, multi-family two over two units, or back-to-back units shall incorporate multiple urban features like feature windows, covered entrances, stoops and industrial look details and materials to add contemporary interest and character to the overall building elevations.
- All buildings shall have flat roofs or roof parapets that when viewed from street level appear as flat roof urban buildings. Any roof features and accents on single family attached and multi-family two over two residential structures should be proportional with the design of the building. Where architecturally appropriate, roofs shall provide articulation and variation to divide the massiveness of the roof.
- Gutters and downspouts should be integrated into the architectural design in color, shape, and location.
- For multi-family structures, all flashing, attic ventilators, metal chimneys, and vents must be painted to complement the roof or the material they are adjacent to.



URBAN DISTRICT - RESIDENTIAL ARCHITECTURAL DESIGN GUIDELINES

Accessory structures, porches, balconies, decks and driveways: Traditional uses of intermediate spaces between the public and private realms, such as porches, stoops and roof top decks help to create streetscape appeal and facilitate community interaction. Residents do have a right to create private areas and spaces but should do so in conjunction with the following guidelines:

- Wing walls may be used for creating privacy if consistent with the architectural style but should be integrated and not appear as separate or add-on elements.
- Stoops and covered entrances on front, sides, and in view of streets or public places shall be painted, unless the architecture is integrated with the style and specific design of the home.
- Roof top and rear decks should be designed to highlight unit identity and to help create residential scale. They should be an integral part of the design and should not appear as separate or add-on elements.
- All walks are to be concrete or brick/concrete pavers.
- Driveways shall be either asphalt, concrete or brick/concrete paver.





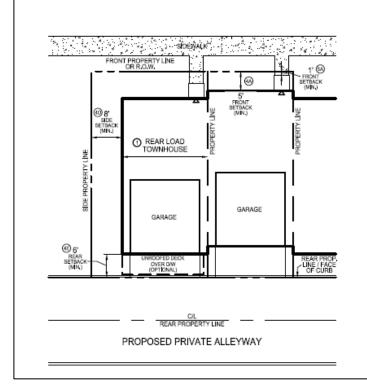






URBAN DISTRICT – RESIDENTIAL ARCHITECTURAL DESIGN GUIDELINES

URBAN DISTRICT TYPE F / TOWNHOUSE UNITS

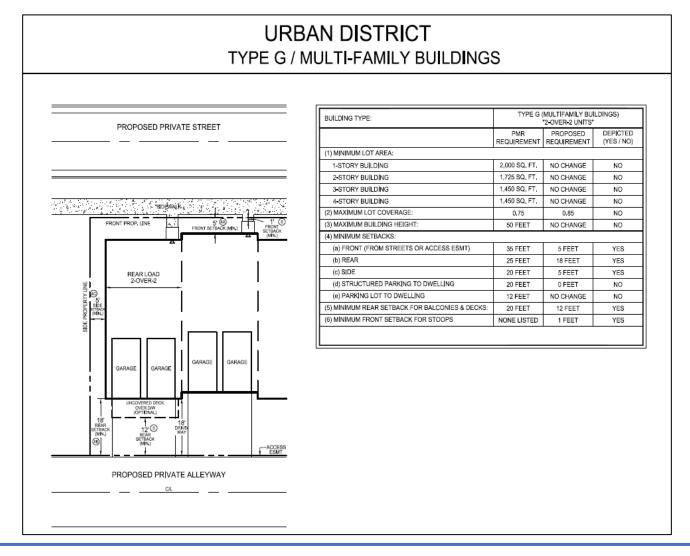


BUILDING TYPE:	TYPE F /	TYPE F / TOWNHOUSE (REAR-LOAD)		
	PMR REQUIREMENT	PROPOSED REQUIREMENT	DEPICTED (YES / NO)	
(1) MINIMUM LOT WIDTH:	20 FEET	16 FEET	YES	
(2) GROUP SETBACK:	20 FEET	10 FEET	NO	
(3) MAX MUM BU LD NG HE GHT:	35 FEET	35 FEET	NO	
(4) MINIMUM SETBACKS:	•			
(a) FRONT (WITH OFF-STREET PARKING)	10 FEET	5 FEET	YES	
(b) FRONT (WITH GARAGE)	20 FEET	N/A	NO	
(c) SETBACK FROM ANY PUBLIC RIGHT-OF-WAY	20 FEET	5 FEET	NO	
(d) SIDE (END UNIT)	10 FEET	8 FEET	YES	
(e) REAR	20 FEET	6 FEET	YES	
(5) MINIMUM SETBACK FOR UNROOFED DECKS, STOOPS, LANDINGS AND SIMILAR FEATURES:				
(a) FRONT (WITH OFF-STREET PARKING)	5 FEET	1 FEET	YES	
(b) FRONT (WITH GARAGE)	15 FEET	N/A	NO	
(c) SIDE (END UNIT)	10 FEET	8 FEET	NO	
(d) REAR	8 FEET	0 FEET	NO	
(6) MINIMUM BUILDING FOOTPRINT	720 SQ. FT.	NO CHANGE	NO	

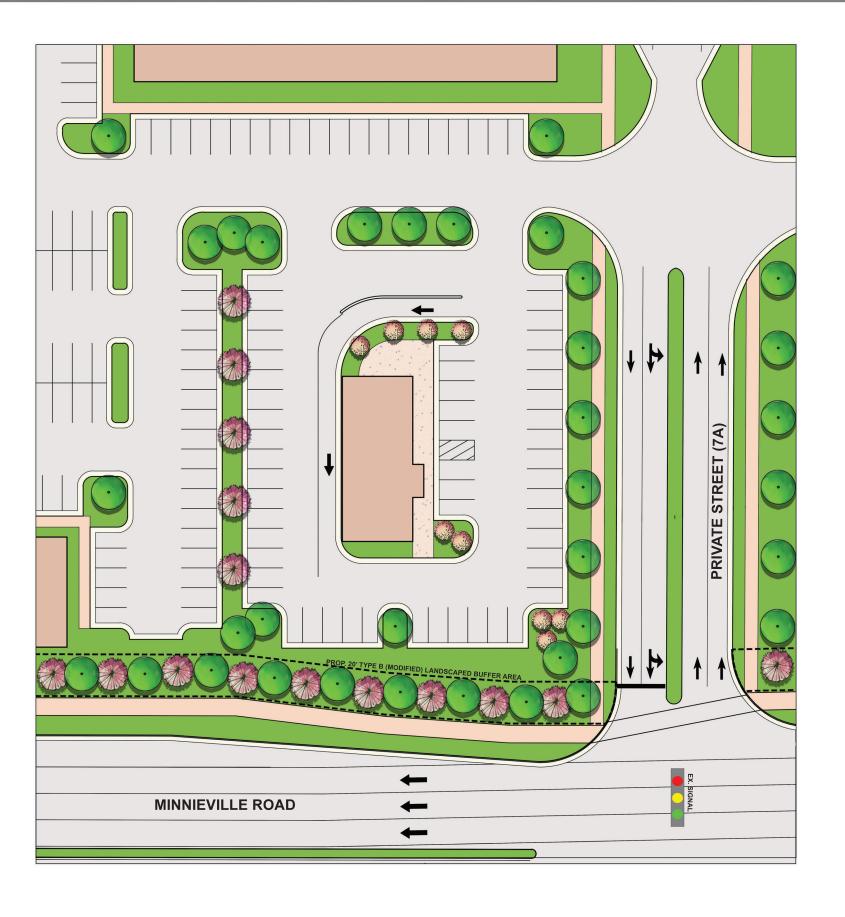
NOTE: FRONT YARDS TO BE FULLY LANDSCAPED WITH SHRUBS AND APPROPRIATE GROUNDCOVER, SEE PROFFERS FOR ADDITIONAL INFORMATION.



URBAN DISTRICT - RESIDENTIAL ARCHITECTURAL DESIGN GUIDELINES







Attachment H



The QUARTZ DISTRICT

ILLUSTRATIVE DRIVE THROUGH AND MOTOR VEHICLE FUEL STATION RENDERINGS WOODBRIDGE, Virginia



Notes:

- This plan is preliminary in nature and is subject to change based on site surveying and final site engineering.
- Boundary and existing base information were obtained from multiple sources including data from this firm.

Interior landscaping will be determined and finalized at final site plan.

Date: 08-03-2022

Project #: 06109.009.00

Drawing #: 111986





The QUARTZ DISTRICT
ILLUSTRATIVE DRIVE THROUGH AND MOTOR VEHICLE FUEL STATION RENDERINGS WOODBRIDGE, Virginia

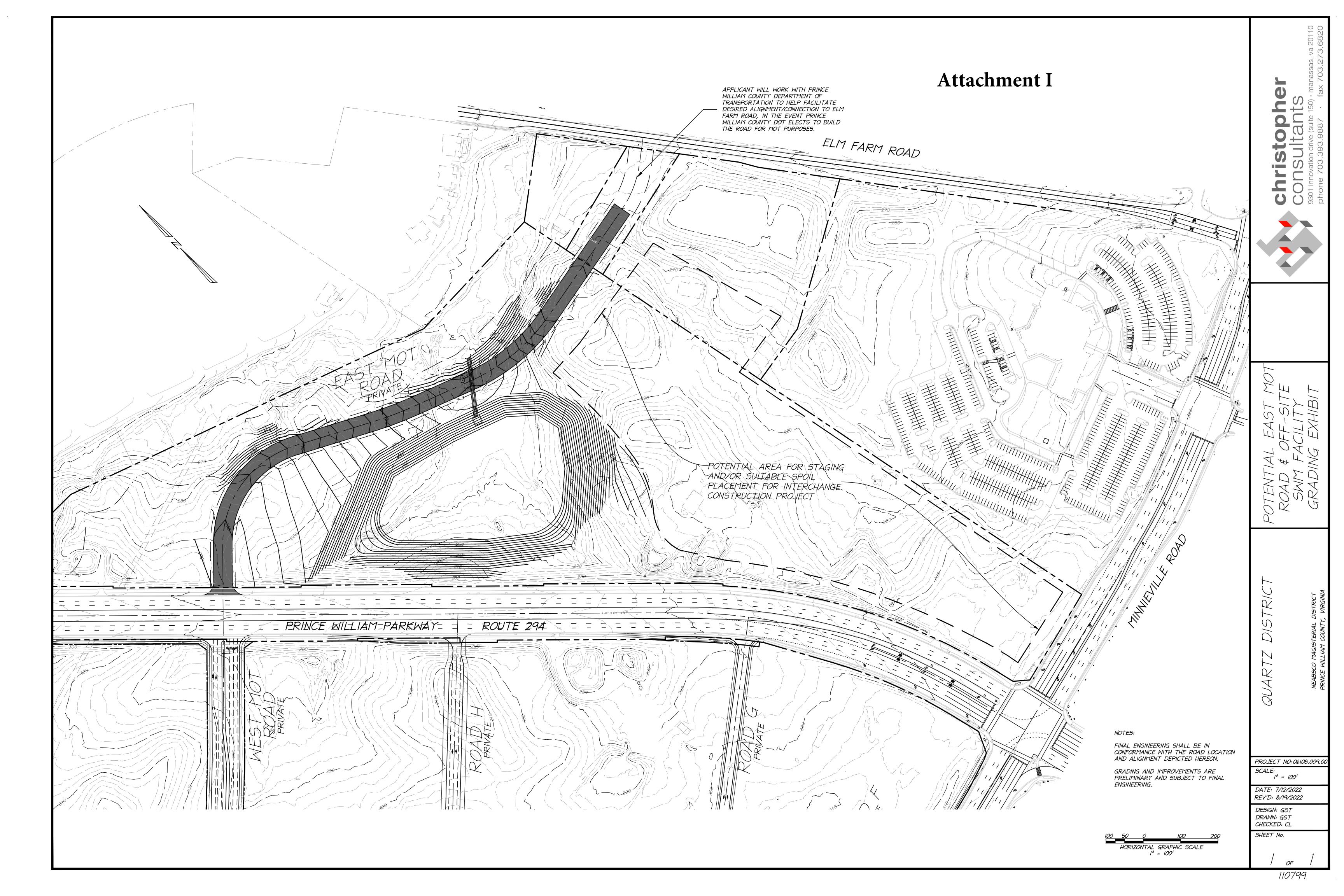


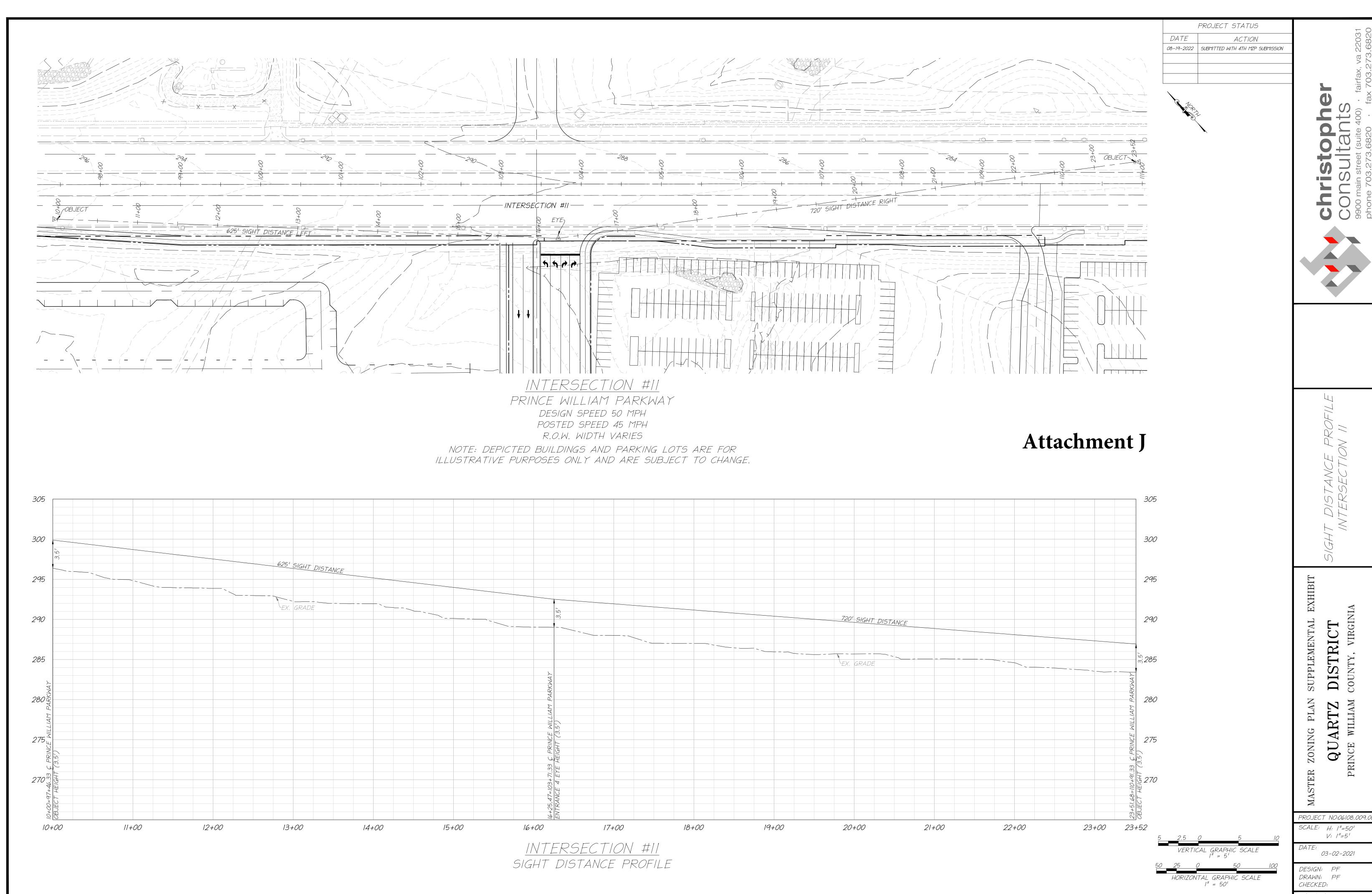
- 1. This plan is preliminary in nature and is subject to change based on site surveying and final site engineering.
- 2. Boundary and existing base information were obtained from multiple sources includng data from this firm.
- 3. Interior landscaping will be determined and finalized at final site plan.

Date: 08-03-2022

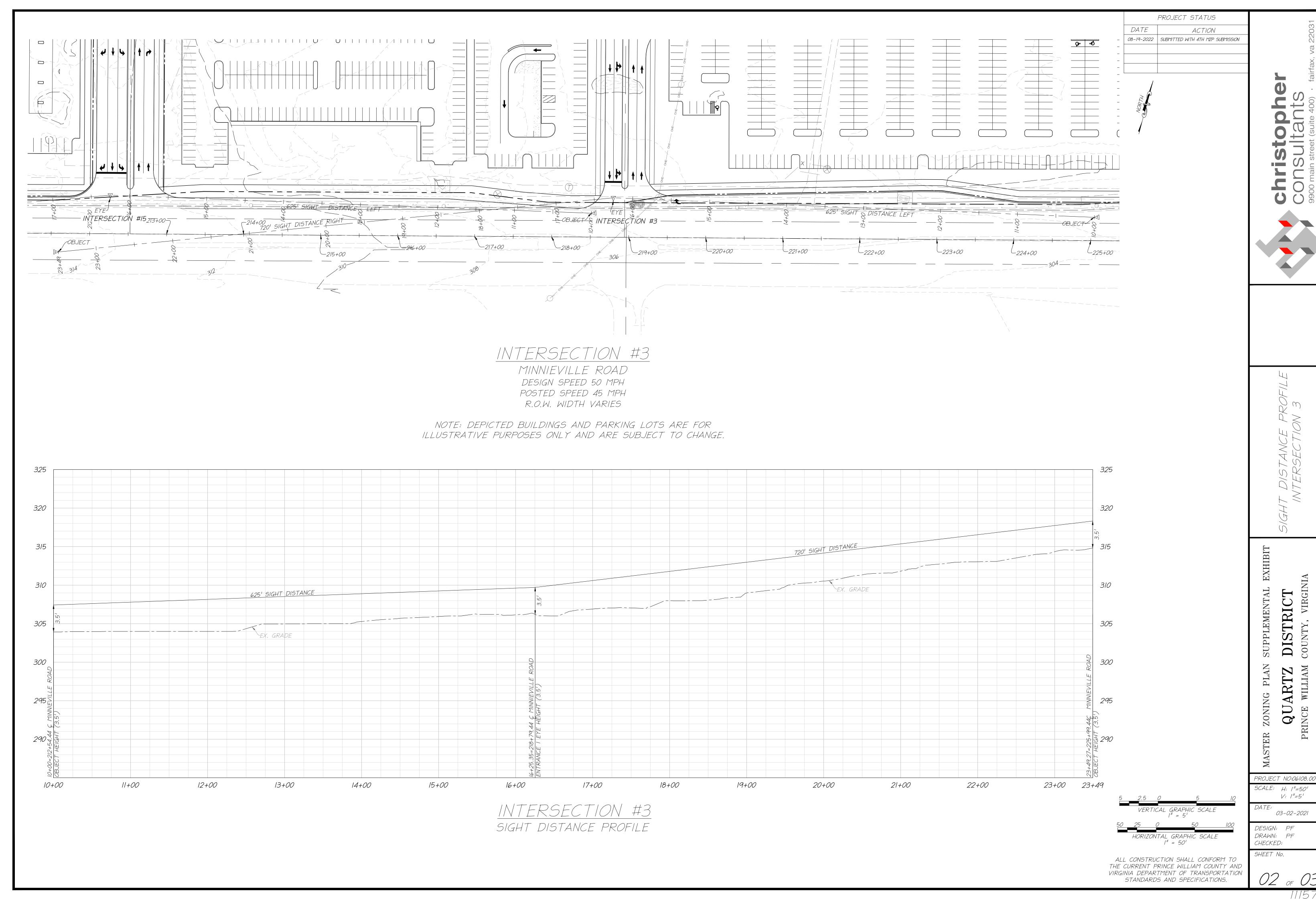
Project #: 06109.009.00

Drawing #: 111986





ALL CONSTRUCTION SHALL CONFORM TO THE CURRENT PRINCE WILLIAM COUNTY AND VIRGINIA DEPARTMENT OF TRANSPORTATION STANDARDS AND SPECIFICATIONS.



PROJECT NO:06108.009.0

