



**PLANNING COMMISSION RESOLUTION**

**MOTION:**

**December 10, 2025**

**SECOND:**

**Regular Meeting**

**Res. No. 25-xxx**

**RE:**

**SPECIAL USE PERMIT AMENDMENT (SUP), SUP2024-00036, Belmont Bay  
WOODBIDGE MAGISTERIAL DISTRICT**

**ACTION:**

**WHEREAS**, this is a request for special use permit amendment associated with #99-0014, Belmont Town Center, to amend the uses within the Marina Overlook district and to remove areas from the special use permit area that are no longer part of the Town Center area on ±15.8acres; and

**WHEREAS**, the subject properties are located in the Belmont Bay community, which is east of the Route 1 (Richmond Hwy) corridor and northeast of the intersection of Route 1 and Dawson Beach Road. Marina Overlook is abutting the marina to the north, running parallel with Harbor Side Street. Taylor Point is at the southeastern point of the community with access from Belmont Bay Drive and Harbor Side Street. Belmont Landing is at the southern and southwestern portion of the community with access via Chrisswind Ave. These applications involve approximately 159.76 acres and are identified on County maps as GPINs 8492-43-5597, 8492-44-0370, 8492-44-5722, 8492-44-1022, 8492-53-7716, 8492-32-4965 (part), 8492-34-3596, 8492-43-4228 (part), and 8492-32-5196 (part); and

**WHEREAS**, the site is designated MU-4, Mixed Use with Transect 4, and RN-2/3, Residential Neighborhood, in the Comprehensive Plan; and

**WHEREAS**, the site is zoned PMD, Planned Mixed District; and within the Resource Protection Area Overlay, Environmental Resource Protection Overlay, Agritourism and Arts Overlay, 100-year and 500-year Flood Hazard Overlay Districts; and

**WHEREAS**, staff has reviewed the subject application and recommend approval, for reasons as stated in the staff report; and

**WHEREAS**, the Prince William County Planning Commission duly ordered, advertised, and held a public hearing on December 10, 2025, at which time public testimony was received and the merits of the above-referenced case were considered; and

**WHEREAS**, the Prince William County Planning Commission finds that public necessity, convenience, general welfare, and good zoning practice are served by recommending approval of this request;

**NOW, THEREFORE, BE IT RESOLVED** that the Prince William County Planning Commission does hereby close the public hearing and recommends approval of Special Use Permit Amendment, SUP2024-00036, Belmont Bay, subject to the SUP Conditions dated October 31, 2025.

Attachment: Conditions Statement dated October 31, 2025.

**Votes:**

**Ayes:**

**Nays:**

**Abstain from Vote:**

**Absent from Vote:**

**Absent from Meeting:**

**MOTION CARRIED**

Attest:

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Oly Peña  
Clerk to the Planning Commission

**SPECIAL USE PERMIT CONDITIONS**  
**#SUP2024-00036, Belmont Bay Town Center SUP Amendment**  
**Applicant: Miller & Smith**  
**Owner: Belmont Town Center Associates, LLC, Blocks B&C, LLC, Belmont Bay,**  
**L.C., & Blocks J&K, LLC**  
**Woodbridge Magisterial District Prince William County GPINs 8492-43- 5597,**  
**8492-44-0370, 8492-44-5722, 8492-44-1022, 8492-53-7716, 8492-32-4965 (part),**  
**8492-43-4228 (part), and 8492-32-5196 (part) (collectively the “Property”)**  
**Special Use Permit Area: ± 15.8 acres**  
**Zoning: PMD, Planned Mixed District**  
**Magisterial District: Woodbridge**  
**Date: October 31, 2025**

The following conditions are intended to offset the impacts of the proposal and to render the application consistent with the applicable chapters of the Comprehensive Plan and the surrounding areas and shall supersede the conditions approved with SUP #99-0014, Belmont Town Center, Marina and Boat Sales. In the event the above-referenced special use permit amendment is not granted as applied for by the Applicant, these conditions shall be withdrawn and are null and void and the conditions associated with SUP #99-0014, Belmont Town Center, Marina and Boat Sales shall be and remain in full force and effect.

If the conditions of this Special Use Permit or the Special Use Permit Plan (the “Plan”) are in conflict with the Zoning Ordinance, and/or the Design and Construction Standards Manual (DCSM), the more restrictive standards shall apply, except as specifically allowed by this special use permit.

The Applicant shall file a site plan within one (1) year of approval of this special use permit by the Board of County Supervisors and shall have up to five (5) years from the date of final site plan approval to commence the proposed use. Issuance of an occupancy permit constitutes commencement of the use.

### **SITE DEVELOPMENT**

1. Plans: The Property shall be developed in substantial conformance with the Special Use Permit Plan entitled “Belmont Bay Special Use Permit,” prepared by Dewberry Engineers Inc., dated October 31, 2025, consisting of the following sheets:
  - Cover Sheet

- Special Use Permit Plan
  - Transportation & Utility Plan
  - Street Sections
  - Road Designations
  - Marina Overlook Illustrative
2. Design Guidelines: Development on the Property shall be in substantial conformance with the design concepts and details set forth in the Design Guidelines entitled “Design Guidelines & Development Code – Marina Overlook, Belmont Landing, and Taylor Point,” dated October 31, 2025 (the “Design Guidelines”). Minor modifications to the Design Guidelines may be made at the time of final site/subdivision plan. More substantial modifications to the Design Guidelines may be approved by the Prince William County Planning Director, or its designee, who shall notify the Applicant what has been determined in regard to the modification’s consistency with the Design Guidelines. The Applicant shall not approve any such substantive amendment found to be inconsistent by the Planning Director. Changes to allow additional building materials may be approved by the Planning Director or his/her designee prior to the issuance of the building permit. Compliance with this proffer shall be evidenced with the submission of building elevations to the Development Services Land Development Division at least two weeks prior to the request for a building permit release letter.
  3. Design Review Committee: In accordance with the Design Guidelines, a Design Review Committee ("DRC") will be established to implement the Design Guidelines. The DRC shall be responsible for the review and approval of all plans for development on the Property to ensure that such plans are in conformance with the design concepts and details set forth in the Design Guidelines.
  4. Outdoor Display & Storage of Merchandise: The outdoor display and storage of merchandise shall only be allowed in accordance with Section 32-400.08 of the Zoning Ordinance.
  5. Gates for Refuse Storage: Gates for any refuse storage areas shall remain closed when refuse containers are not being accessed.
  6. Landscaping: Landscaping shall be provided in substantial conformance with the SUP Plan. A minimum of eighty-five percent (85%) of all new plantings on the Property shall be drought-resistant and native to Virginia in accordance with the

Design and Construction Standards Manual (“DCSM”). The aforementioned does not prohibit the plantings of Crape Myrtles.

7. Property Removal: As shown on the SUP Plan, portions of GPIN 8492-53-7716, 8492-32-4965, 8492-43-4228, and 8492-32-5196 shall be subject to the special use permit conditions associated with SUP #99-0014 or these special use permit conditions.

## **TRANSPORTATION**

8. FAR & Density Tabulations: At the time of sketch plan approval for each section or phase of the Town Center, a cumulative total of all land use activities in the Town Center (i.e., FAR and density tabulations) shall be provided.
9. On-street Parking: The control and enforcement of the on-street parking shall ultimately be the responsibility of homeowners associations and/or business associations established for such purpose, such control may include but shall not be limited to implementation of parking permit, stickers, meters, fines, any combination thereof or other techniques to be established by the association(s).
10. Overnight Parking: The covenants on the Property shall prohibit the overnight parking of service vehicles, recreational vehicles, boats, trailers, and heavy trucks within the development, except when the designated use is for the purpose of providing a service using the above-mentioned vehicles (i.e. boat sales). This shall not preclude parking and storage of construction vehicles during development of the Property.
11. Drainage Easement: If required by VDOT at final site plan, a perpetual maintenance/drainage easement shall be provided on streets with the right-of-way line coinciding with the back of curb.
12. Underground Stormwater Management Facility: The Applicant may have underground stormwater management facilities on the Property. In the event an underground stormwater management facility(ies) is provided on the Property, the following shall apply:
  - a. Maintenance: Any underground stormwater management facility(ies) constructed on the Property shall be fully maintained by the Homeowners

Association (“HOA”) or similar association that is responsible for the maintenance of all commonly owned facilities on the Property. The County will not maintain or accept responsibility for the maintenance of the underground stormwater management facility(ies).

- b. Escrow: The Applicant shall provide in an escrow for the benefit of the HOA an amount equal to (i) 20 years of annual maintenance for the underground stormwater management facility(ies); and (ii) 25% of the replacement costs for the underground stormwater management facility(ies). Said escrow amount shall be determined by the Applicant’s civil engineer in consultation with the Director of the Department of Public Works, or its designee, and the underground stormwater management facility(ies) manufacturer and shall be shown on the final site or subdivision plans. Prior to bond release the Applicant shall place the approved escrow amounts in an account benefiting the HOA and provide proof of deposit in said account to the Department of Public Works.

### **MARINA REGULATIONS**

13. Approval is hereby granted for the development of a marina as permitted in Section 32-401.13(13) of the Zoning Ordinance and in accordance with the following:
  - a. Special Use Permit Plan: The marina site shall be developed in substantial accordance with the Special Use Permit Plan. Minor modifications such as the location of the dock masters building and the arrangement of boat slips may be made in order to address comments of a Joint Permit Application.
  - b. Boat Slips The marina shall be limited to a maximum of 163 rental or condominium boat slips. No commercial fishing boats shall be operated from the marina. No tank barge or other fuel supply vessel shall be permitted within the marina basin. The outdoor storage of boats (inland) shall not be permitted.
  - c. Dry Storage of Boats: No dry storage of boats shall be allowed. No major boat service/repairs shall be allowed such as hull, rigging or engine repairs. Only minor repairs such as winterization of the boats shall be allowed.

- d. Hazardous Materials: No gasoline, oil, solvents, anti-freeze or other petroleum products or other hazardous substances shall be discharged into the public sewer system. The owners/operator shall employ appropriate safeguards to prevent these products from being discharged from boats into the boat sewage pump station. Waste oil and other similar pollutants shall be properly contained and disposed of in accordance with all pertinent ordinances and regulations.
- e. Underground Storage Tanks: Any underground petroleum storage tanks shall be double-walled fiberglass construction with interstitial leak detection device.
- f. Fuel Pumps: The fuel pumps shall incorporate automatic shut-off controls to prevent spills.
- g. Emergency Spill Notification: The owner/operator of the marina shall prepare an emergency spill notification contingency plan and have same posted on the premises before the issuance of any occupancy permit. The owner/operator shall be responsible for notifying the Fire Marshall's Office immediately in the event of a spill of any petroleum product or chemical waste on the Property. The owner/operator shall assume responsibility for all public or private expenses incurred in the clean-up of a product spill on the site.
- h. Boats Used as Residences: No more than ten percent (10%) of the slips shall be occupied by boats that are used as a domicile or residence.
- i. Refuse Removal: The owner/operator shall perform routine maintenance and trash pickup on the site.
- j. Loudspeakers: No loudspeakers shall be used on the site.

### **BOAT SALES**

- 14. Approval is hereby granted for boat sales and related uses in the area located around the marina as permitted in Section 32-401.13 of the Zoning Ordinance and in accordance with the following:

- a. Boat sales shall be permitted in Marina Overlook.
- b. The permanent outdoor storage or display of boats, boat parts and related equipment shall be prohibited.

### **WAIVERS & MODIFICATIONS**

15. Pursuant to Section 32-280.34 of the Zoning Ordinance, approval of the subject special use permit shall constitute a waiver/modification of the following:
  - a. Modification of Section 32-201.18 of the Zoning Ordinance to waive the 15 ft. wide landscaped area around public use sites, such as telephone switching station, power substation, etc.
  - b. Waiver of Sections 32-250.10 and 32-400.16 of the Zoning Ordinance in order to allow parking as a principal use on subdivided lots, regardless of ownership, located within the Town Center and to allow more than fifty percent (50%) of the required parking to be located on a separate parcel.
  - c. Waiver of Sections 32-250.31 of the Zoning Ordinance and 802.11 and 802.12 of the DCSM waiving the buffer between commercial, office and other nonresidential uses and residential uses within the planned development district.
  - d. Modification of Sections 32-250.40 through 32-250.44 of the Zoning Ordinance and 802.20, 802.21, 802.30 and 802.31 of the DCSM to allow implementation of tree canopy and open space requirements over the entire Property.
  - e. Modification of Section 32-280.31.7(a) of the Zoning Ordinance to allow multiple and/or tiered owner associations.
  - f. Modification of Section 32-280.31.7(b) of the Zoning Ordinance to not specifically state the Design Guidelines in the CCRC.
  - g. Modification of Section 32-280.31.7(j) of the Zoning Ordinance to allow for modification of conditions, covenants and restrictions in accordance with standard practice during developer control of project.

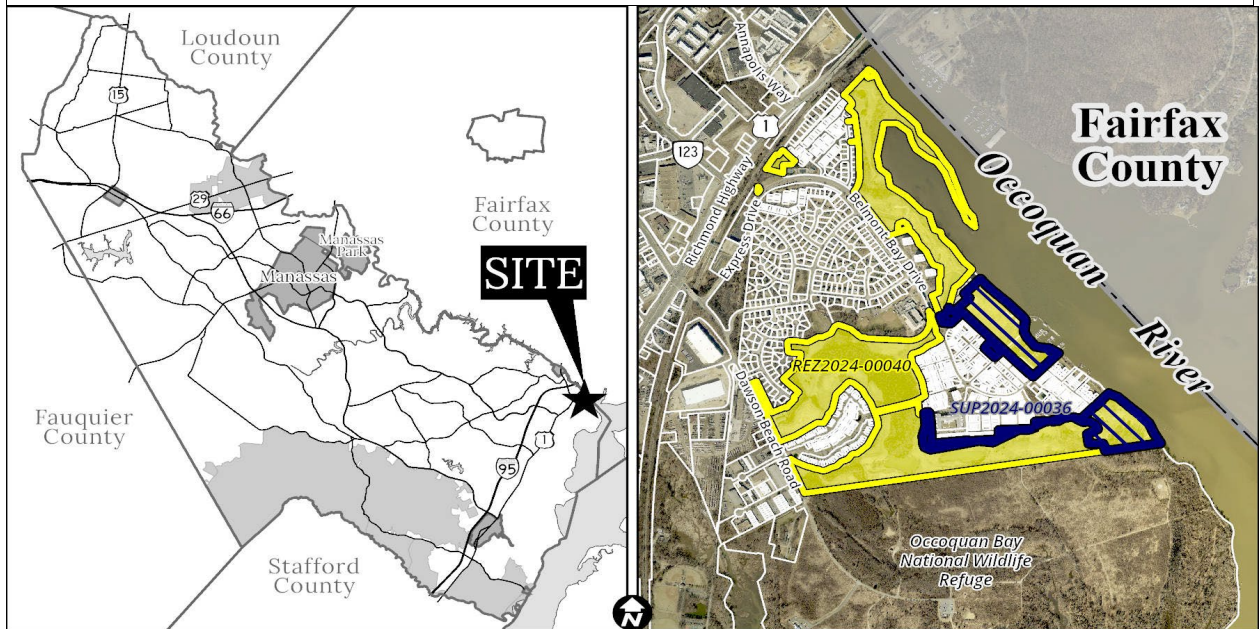
- h. Modification of Section 32-280.35 to delete the items required to be included in the Design Guidelines which are not currently included within the Design Guidelines.
- i. Modification of Section 32-280.61.2(a) of the Zoning Ordinance to allow rear yards of townhouses to be less than 30 ft.
- j. Modification of Section 32-280.61.2(e) of the Zoning Ordinance to allow a reduced building separation of 5 ft. rather than 20 ft. and to allow more than 8 townhouse units to a group.
- k. Pursuant to Section 32-300.05.4, modification of Section 32-306.12.6.F(3) to increase the maximum building height to 50 ft. the single family attached units as shown on the Development Code and in the Design Guidelines.
- l. Modification of Sections 32-306.10, 32-306.12.6.F and 32-306.12.6.G of the Zoning Ordinance to allow for the housing types, as depicted on the SUP Plan and Design Guidelines.
- m. Waiver of Sections 32-405.04(4) of the Zoning Ordinance and 802.47.B of the DCSM of the peripheral buffer in its entirety for the Property and implement the landscaping and buffers as shown on the Master Zoning Plan.
- n. Waiver of Section 32-405.04(7) of the Zoning Ordinance to allow the residential use to exceed 35% of the gross land area of the PMD as generally shown on the Land Use Plan.
- o. Modification of Section 32-700.51.1 of the Zoning Ordinance to allow the following items to be omitted from town center street grid plan: private streets, shared access driveways, delivery vehicles access, and loading.
- p. Waiver of Section 32-700.51.3 of the Zoning Ordinance requiring a Phasing Plan.
- q. Modification of Section 32-700.51.4 of the Zoning Ordinance requiring Community Facilities & Infrastructure Analysis & Plan.

- r. Waiver of Section 32-700.51.7 of the Zoning Ordinance requiring a Contingency Plan.
- s. Waiver of Section 32-700.55(1) of the Zoning Ordinance to provide flexibility with reference to the implementation of the special use permit.
- t. Modification of Section 32-700.51.2 of the Zoning Ordinance to allow the following items to be omitted from schematic land use plan; recreation and tree preservation.
- u. Modification of Section 125.01.I of the DCSM to reduce the required common or shared easement width from 15 ft. to 10 ft.
- v. Modification of Section 602.07.T. of the DCSM to allow driveways to be greater than 35% or more than 720 SF.
- w. Waiver of Section 602.14.H of the DCSM requiring a cul-de-sac at the end of privately maintained travelways.
- x. Modification of Section 610.06.B of the DSCM to allow for parking to be provided off-site or greater than 150 feet of each dwelling unit measured over sidewalks or other paved surfaces.
- y. Modification of DCSM Details 650.06 to allow street sections in accordance with the street sections shown on the MZP.
- z. Waiver of Section 32-250.31 of the Zoning Ordinance and 802.47 of the DCSM requirement for a 15-foot-wide landscape area around community recreation sites.



# STAFF REPORT

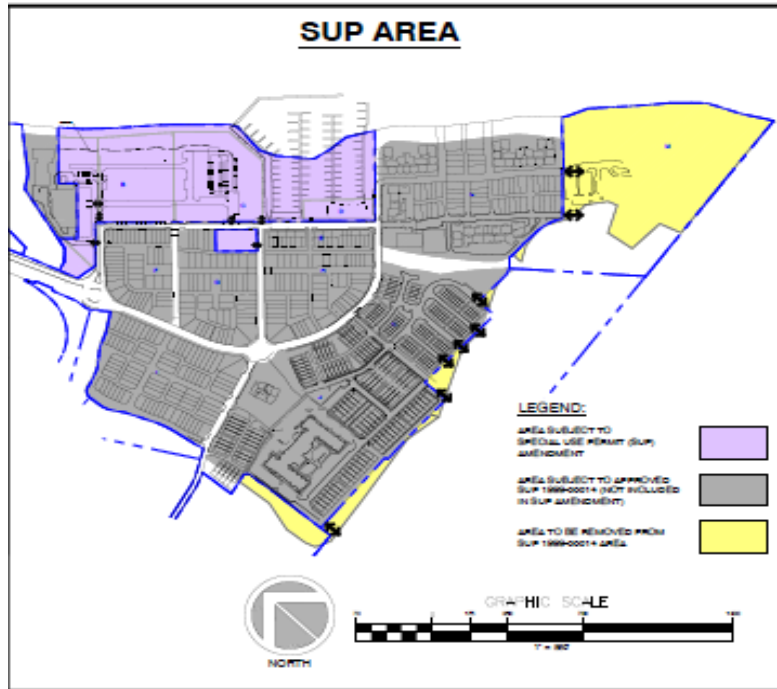
<b>PC Meeting Date:</b>	December 10, 2025
<b>Agenda Title:</b>	Proffer Amendment, REZ2024-00040 & Special Use Permit Amendment, SUP2024-00036, Belmont Bay
<b>District Impact:</b>	Woodbridge Magisterial District
<b>Requested Action:</b>	Recommend Approval of Belmont Bay Proffer Amendment, REZ2024-00040, subject to proffers dated November 12, 2025, and the MZP dated October 31, 2025, and Special Use Permit Amendment, SUP2024-00036, subject to the conditions dated October 31, 2025, and SUP Plan dated October 31, 2025.
<b>Department:</b>	Planning Office
<b>Case Planner:</b>	Alex Vanegas/Vanessa Watson



## EXECUTIVE SUMMARY

This proffer amendment request is proposing to modify the land bays and uses from Rezoning #99-0022. The Applicant is seeking to construct the remaining approved units within the Belmont Bay areas and reduce the remaining commercial uses within Belmont Bay to ±20,500 sq. ft. In addition, the Applicant is proposing a height modification as previously approved with the original rezoning and special use permit.

This is a request for a Special Use Permit Amendment to SUP#99-0014, within the town center to amend the uses within the Marina Overlook district and to remove areas from the special use permit amendment that are no longer part of the Town Center area.



The SUP Amendment area will include the existing marina, the commercial uses, parking and 74 single-family-attached townhomes (area shaded in purple above).

The residential element of these applications are for the development of 348 single-family attached dwelling units, of which 274 shall be age restricted. All dwelling units shall be between 20'-35' in width and up to 50' in height.

These applications also request a number of waivers/modifications, of which many were approved with the previous approvals and are being carried over. The two applications have been reviewed and are being presented concurrently.

The subject properties are located in the Belmont Bay community, which is east of the Route 1 (Richmond Hwy) corridor and northeast of the intersection of Route 1 and Dawson Beach Road. Marina Overlook is abutting the marina to the north, running parallel with Harbor Side Street. Taylor Point is at the southeastern point of the community with access from Belmont Bay Drive and Harbor Side Street. Belmont Landing is at the southern and southwestern portion of the community with access via Chrisswind Ave. These applications involve approximately 159.76 acres and are identified on County maps as GPINs 8492-43-5597, 8492-44-0370, 8492-44-5722, 8492-44-1022, 8492-53-7716, 8492-32-4965 (part), 8492-34-3596, 8492-43-4228 (part), and 8492-32-5196 (part).

Staff recommends that the Planning Commission recommend approval of Belmont Bay Proffer Amendment, REZ2024-00040 with proffer statement dated November 12, 2025, and the Master Zoning Plan and Design Guidelines dated October 31, 2025, and Special Use Permit Amendment, SUP2024-00036, subject to the conditions dated October 31, 2025, and SUP Plan dated October 31, 2025, to the Board of County Supervisors.

## **BACKGROUND**

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- a. Request: A request for a Special Use Permit Amendment and Proffer Amendment to modify land bays for development of 348 single-family attached dwelling units, of which 274 shall be age-restricted, as well as reducing the remaining commercial uses to  $\pm$ 20,500 square feet.
- b. Site Location: The subject properties are located in the Belmont Bay community, which is east of the Route 1 (Richmond Hwy) corridor and northeast of the intersection of Route 1 and Dawson Beach Road. Marina Overlook is abutting the marina to the north, running parallel with Harbor Side Street. Taylor Point is at the southeastern point of the community with access from Belmont Bay Drive and Harbor Side Street. Belmont Landing is at the southern and southwestern portion of the community with access via Chrisswind Ave. These applications involve approximately 159.76 acres and are identified on County maps as GPINs 8492-43-5597, 8492-44-0370, 8492-44-5722, 8492-44-1022, 8492-53-7716, 8492-32-4965 (part), 8492-34-3596, 8492-43-4228 (part), and 8492-32-5196 (part).
- c. Existing & Surrounding Zoning and Land Use: The subject site and Belmont Community is zoned PMD, Planned Mixed Use Development. The Belmont Elementary School is to the northwest and the "original" Belmont is zoned R-4. The subject site and all surrounding property is designated a mix of MU-4, Mixed Use with Transect 4, RN-2, RN-3, RN-4, Residential Neighborhood, PL, Public Land and POS, Parks and Open Space on the Comprehensive Long-Range Land Use Plan Map. The property to the south of Belmont Bay is the Occoquan Bay National Wildlife Refuge and is Federal property.
- d. Comprehensive Plan: The subject site and all surrounding property is designated a mix of MU-4, Mixed Use with Transect 4, RN-2, RN-3, RN-4, Residential Neighborhood, PL, Public Land and POS, Parks and Open Space on the Comprehensive Long-Range Land Use Plan Map. The applications are also in the North Woodbridge Small Area Plan. The North Woodbridge Small Area Plan captures the elements necessary for transforming the area into a vibrant, transit-oriented town center that celebrates and protects cultural resources and natural features while leveraging assets for economic growth and a high quality of life.
- e. Zoning: The property is PMD, Planned Mixed Use Development and within the Resource Protection Area Overlay, Environmental Resource Protection Overlay, Agritourism and Arts Overlay, 100-year and 500-year Flood Hazard Overlay Districts.

f. Surrounding Land Uses: The following table summarizes the area characteristics:

**Areas surrounding (proposed) Belmont Landing and Taylor Point**

Direction		Long Range Future Land Use	Zoning
North	Belmont Bay Community	RN-3, Residential Neighborhood with T-3 & RN-4, Residential Neighborhood with T-4 and POS, Parks and Open Space	PMD, Planned Mixed Use Development
South	Occoquan Bay Wildlife Refuge	Federal Property	Federal Property
East	Occoquan River	OW, Open Water	A-1, Agricultural
West	Open Space	POS, Parks and Open Space	PMD, Planned Mixed Use Development

**Area surrounding (proposed) Marina Overlook**

Direction		Long Range Future Land Use	Zoning
North	Occoquan River	OW, Occoquan River	A-1
South	Belmont Bay	RN-4	PMD
Southeast	Marina	OW, Occoquan River	PMD
East	Occoquan River	OW, Occoquan River	A-1
West	GM Science Museum	MU-4, Mixed Use Transect 4	PMD

Background & Context: May 18, 1999, the Board of County Supervisors approved a Rezoning REZ#99-0022, Belmont Center (the “Original Rezoning”) and a Special Use Permit for a Town Center SUP #99-0014, Belmont Bay Town Center (the “Original SUP”). The Original Rezoning allowed for a maximum of 1,548 residential units, of which a maximum of 600 would be non-age restricted. In addition, the approval was for a maximum of 550,000 square feet of non-residential space with a 505-room hotel until the construction of a grade separated intersection at the Route 1/Route 123 intersection. At the time of construction of the grade separated intersection, the permitted commercial square footage was increased to 1,550,000 sq. ft. The original approvals also include a marina, the George Mason University Science Tech Campus and a golf course.

Significant changes have transpired over the past 25 years as they pertain to land use and development in this area, but also in the North Woodbridge area as a whole. The County's Zoning Ordinance and Comprehensive Plan have both undergone substantial changes, which are reflected in the current documents, to include, The North Woodbridge Small Area Plan that was adopted in October 2019.

The 2019 North Woodbridge Small Area Plan identifies Belmont Bay as one of the Plan's three subareas (the other two are North Woodbridge Town Center and Marumscoc). The Small Area Plan includes the following key guidance (p. 70):

**VISION:** *Building on North Woodbridge's rich history and environmental resources, create a dynamic community focused on a dense, mixed-use North Woodbridge Town Center, while strengthening the existing communities of Marumscoc and Belmont Bay, oriented around a multimodal transportation network and a vibrant waterfront, nurturing a high quality of life and economic vitality for the residents and businesses of North Woodbridge.*

**PLACETYPES:** *Create a sense of place with a vertical mixed-use town center and capitalize on the waterfront as a unique feature. Create neighborhoods with a balanced mix of housing types and affordability.*

The Plan also includes the Belmont Bay Illustrative Plan that outlines development/redevelopment opportunities such as:

- *Plan for the remaining elements of the planned mixed-use community including office, commercial, and medium-density residential in the Harborside community, a resort and conference center along the Occoquan River and a mix of residential unit types in the remaining developable area at the south end of Belmont Bay Drive. (p.88)*
- *Establish an alignment for the Potomac Heritage National Scenic Trail to follow the Occoquan River shoreline providing access to a greenbelt of natural resources including the Occoquan River and the Occoquan Bay National Wildlife Refuge and connecting to the North Woodbridge Town Center to the north and Marumscoc to the south. (p.88)*

The North Woodbridge Small Area Plan includes a land use designation of Urban Neighborhood and portions of Waterfront for the subject site within the Belmont Bay subarea. As noted above, those designations were updated as part of the 2022 update of the 2040 Comprehensive Plan.

The Urban Neighborhood designation in the North Woodbridge Small Area Plan is noted in the Plan as "an opportunity to develop higher density, mixed-use residential close to transit (VRE) or town centers", that "mix housing types that meet the needs of all ages and economic groups." Additionally, the Small Area Plan's Urban Neighborhood designation notes "small-scale office, retail and service uses are integrated into the neighborhood" and that "waterfront areas of Urban Neighborhoods are

more densely developed and have a greater proportion of non-residential uses.” (p. 78) The Urban Neighborhood designation in the Small Area Plan includes the following uses:

Primary Uses	Multi-Family Residential High-Density Townhouses Retail & Service Commercial  <i>Waterfront Uses Only:</i> Hotel/Conference Center Office Government Contracting
Secondary Uses	Active Adult Retirement Communities Office  <i>Waterfront Uses Only:</i> Recreation Entertainment/Commercial

The Small Area Plan’s Economic Development Plan references Belmont Bay’s unique natural and cultural features to support future economic activity:

*The North Woodbridge area has an opportunity to leverage environmental resources and green infrastructure in establishing a cultural hub to provide related civic and economic benefits. The Belmont Bay community is home to the George Mason University Potomac Science Center. The continued interest in bringing economic development to both existing retail spaces and future land use bays in the Belmont Bay community could be targeted toward riverine science to attract new patrons to the area for both regular visits and special events, perhaps including nature walks or festivals centered on the Belmont Bay golf course property. (p. 184)*

Since the 1999 rezoning, the Marina, George Mason University Potomac Science Center, Events Center, golf course and some small commercial shops were constructed with approximately 1,403 residential dwellings, either built or under construction. Other major planned waterfront uses in the Small Area Plan, such as a hotel and conference center have not occurred to date. Additionally, the construction of the Route 1/Route 123 grade separated intersection into the community has not been constructed and to date is not funded.

The Proffer Amendment and Special Use Permit Amendment under consideration would achieve other elements of the Small Area Plan by providing the Belmont Bay community with additional housing suitable for multiple age ranges, and additional recreational and small scale commercial opportunities enhancing the area by the Marina and creating more opportunities for

families/residents to experience a true sense of place, while encouraging others to come and enjoy the commercial and recreational and natural opportunities offered by Belmont Bay.

## **STAFF RECOMMENDATION**

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Staff recommends approval of Belmont Bay Proffer Amendment #REZ2024-00040, subject to proffers dated November 12, 2025, and Master Zoning Plan and Design Guidelines dated October 31, 2025, and Special Use Permit Amendment #SUP2024-0036, subject to conditions dated October 31, 2025, and SUP Plan dated October 31, 2025.

- The surrounding uses and land use designations are compatible with the requested Proffer Amendment and Special Use Permit.
- Both applications provide consistency with the Zoning District.
- Both applications provide consistency with previous rezoning/proffer amendments regarding the property with retail/retail services uses.
- While these applications do not achieve the Small Area Plan's guidance calling for a hotel or conference center for the Belmont Bay waterfront area, the applications achieve other elements of the North Woodbridge Small Area Plan's guidance, such as high-density townhouses as a primary use along with active/adult retirement communities and entertainment/commercial as secondary uses.

## **Comprehensive Plan Consistency Analysis**

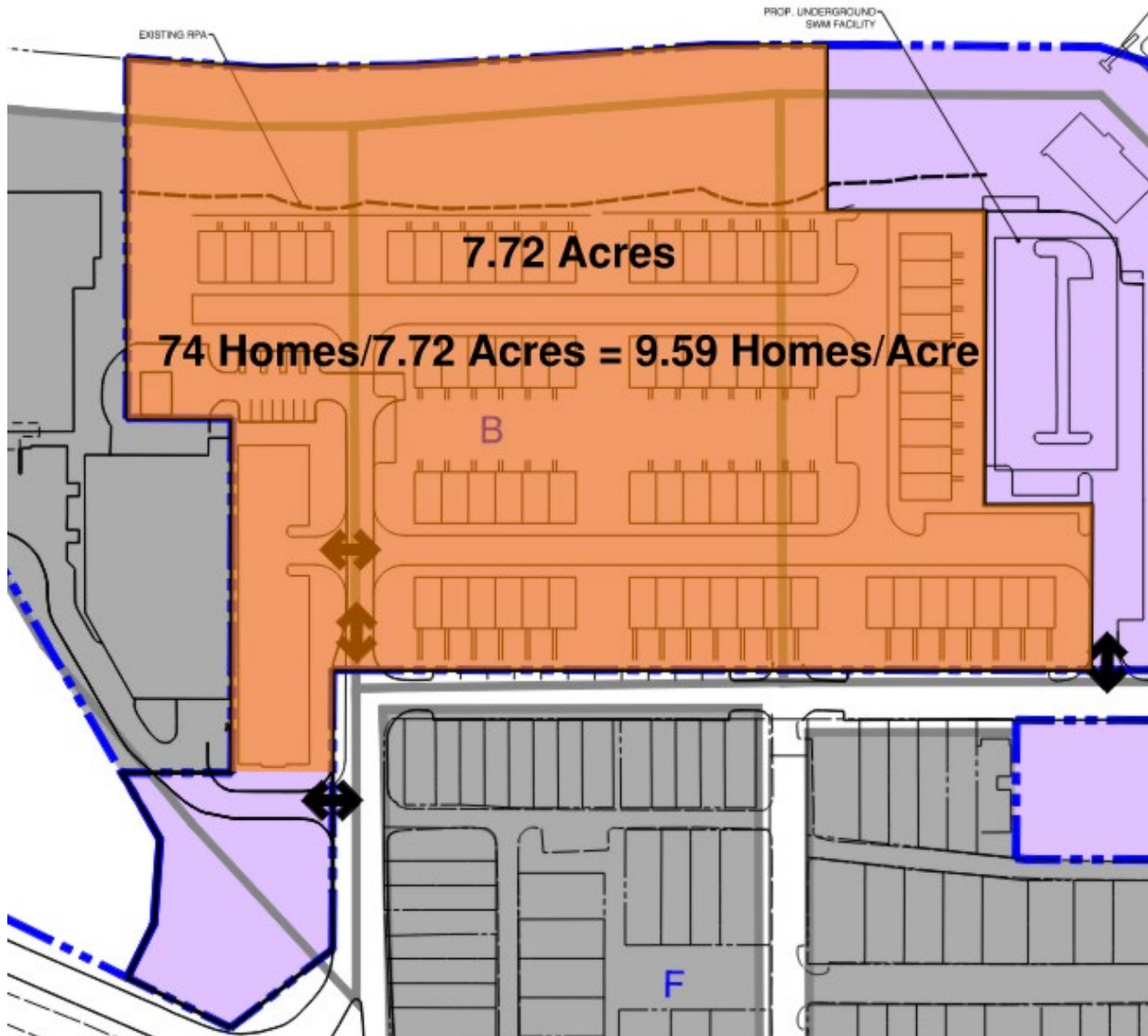
While the application's residential and commercial offerings are consistent with the land use designations, the Special Use Permit Amendment portion is inconsistent with the North Woodbridge Small Area Plan.

A major goal of the North Woodbridge Small Area Plan is to "Create a sense of place with a mixed-use town center and capitalize on the waterfront as a unique feature. Create neighborhoods with a balanced mix of housing types and affordability." This section of the Plan is integral in the development, vision, and implementation of this goal. This land use plan refers to the characteristics of density, diversity, and design present for a specific geography. A small area plan informs the linkages between several land use types and presents an overarching goal for the identity of these spaces. The framework for developing this Small Area Plan includes creating a transportation network that supports mixed-use development and a high quality of life in a waterfront community.

Long-Range Land Use: The property is designated as MU-4, Mixed Use and RN-2 & 3, Residential Neighborhood, in the Comprehensive Long-Range Land Use Plan Map. The subject site and Belmont Community is zoned PMD, Planned Mixed Use Development. The Belmont Elementary School is to the northwest and the "original" Belmont community is zoned R-4. The subject site and all surrounding property is designated a mix of MU-4, Mixed Use with Transect 4, RN-2, RN-3, RN-4, Residential Neighborhood, PL, Public Land and POS, Parks and Open Space on the Comprehensive Long-Range Land Use Plan Map. The property to the south of Belmont Bay is the Occoquan Bay National Wildlife Refuge and is Federal property.

MU-4, Mixed Use with Transect 4: Mixed Use live work centers include both residential and commercial uses arranged in a pedestrian-friendly form. These centers are locations for community commercial, entertainment destinations, and public facilities directly accessible to surrounding neighborhoods. Streets are interconnected and serve cars, cyclists, and pedestrians. Mixed Use Centers should be connected by bus transit to nearby destinations and to nearest rail transit. *Rail and bus transit are available at the Route 1 Corridor, however PRTC does not have any service into the Community.*

This land use designation allows for Townhomes as a primary use with 4-8 stories, which is in keeping with the 50', the Applicant is proposing. The density as shown on the graphic below shows the dwelling units per acre are within the guidelines of 8-24 dwelling units per acre.



Residential Neighborhood T2 & T3: Residential Neighborhoods primarily accommodate single-family attached or detached homes arranged in small to medium sized lots as well as some multi-family residential. Connections and pedestrian amenities should still be a priority for development design. These areas should also include parks, trails and open space integrated into the development in appropriate locations.

Both the Belmont Landing and Taylor Point areas are within the RN-2/3 designations with single-family-attached homes as well as parks, trails and open spaces.

## **Community Input**

Notice of the Proffer Amendment (REZ) and Special Use Permit (SUP) applications have been transmitted to the property owners within 500 feet of the subject property. At the time of this Staff Report, the County's Planning Office has received feedback from the Belmont Bay Community HOA, Concerned Citizens of Belmont Bay, LOCCA PELT, WPCCA, and the Northern Virginia Bird Alliance regarding these applications.

The Applicant had meetings prior to submitting the applications requests, including a virtual town hall meeting with Belmont Bay and the surrounding community. In addition, the Applicant had many meetings with the Belmont Bay HOA, Conservancy HOA and the HOA subassociations. They also had several meetings with George Mason University representatives during the process. Additionally, there were multiple meeting held with representatives of the Occoquan Bay National Wildlife Refuge. In May, 2024, the Applicant met with WPCCA. In person community open house meetings were held in August, 2024 and May, 2025 with all consultants present to answer questions. The Applicant made themselves available for one-on-one meetings and had approximately 40 of those meetings.

## **Other Jurisdiction Comments**

The subject site is located outside of the required notification area for adjacent jurisdictions.

## **Legal Issues**

If the Proffer Amendment (REZ) and Special Use Permit (SUP) are approved, the property could be developed as requested; if they are not approved, the property shall remain under Rezoning #99-0022 and SUP #99-0014 approvals. Legal issues resulting from the Planning Commission's action are appropriately addressed by the County Attorney's Office.

## **Timing**

The Planning Commission has until March 10, 2026, which is 90 days from the first public hearing date, to act on the Proffer Amendment and Special Use Permit request. A recommendation to approve or deny the request would meet the 90-day requirement.

Under Virginia Code Section 15.2-2232(B), failure of the Planning Commission to act within 60 days of submission, unless time is extended by the governing body, shall be deemed approval.

## **STAFF CONTACT INFORMATION**

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Avanegas@pwcgov.org/vwatson@pwcgov.org

## **ATTACHMENTS**

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Attachment A – Staff Analysis  
Attachment B – Master Zoning Plan dated October 31, 2025  
Attachment C - Design Guidelines and Development Code dated October 31, 2025  
Attachment D – Special Use Permit (SUP) Plan dated October 31, 2025  
Attachment E – Belmont Bay Community Building dated February 14, 2024  
Attachment F – Refuge Buffer Concept Exhibit dated February 10, 2025  
Attachment G- Belmont Bay – Conceptual Reforestation Plan dated February 10, 2025  
Attachment H - Belmont Bay Conceptual Marina Overlook RPA Revegetation Plan  
Attachment I - Belmont Bay Conceptual Taylor Point RPA Revegetation Plan  
Attachment J – Historical Commission Resolution

**Part I. Summary of Comprehensive Plan Consistency**

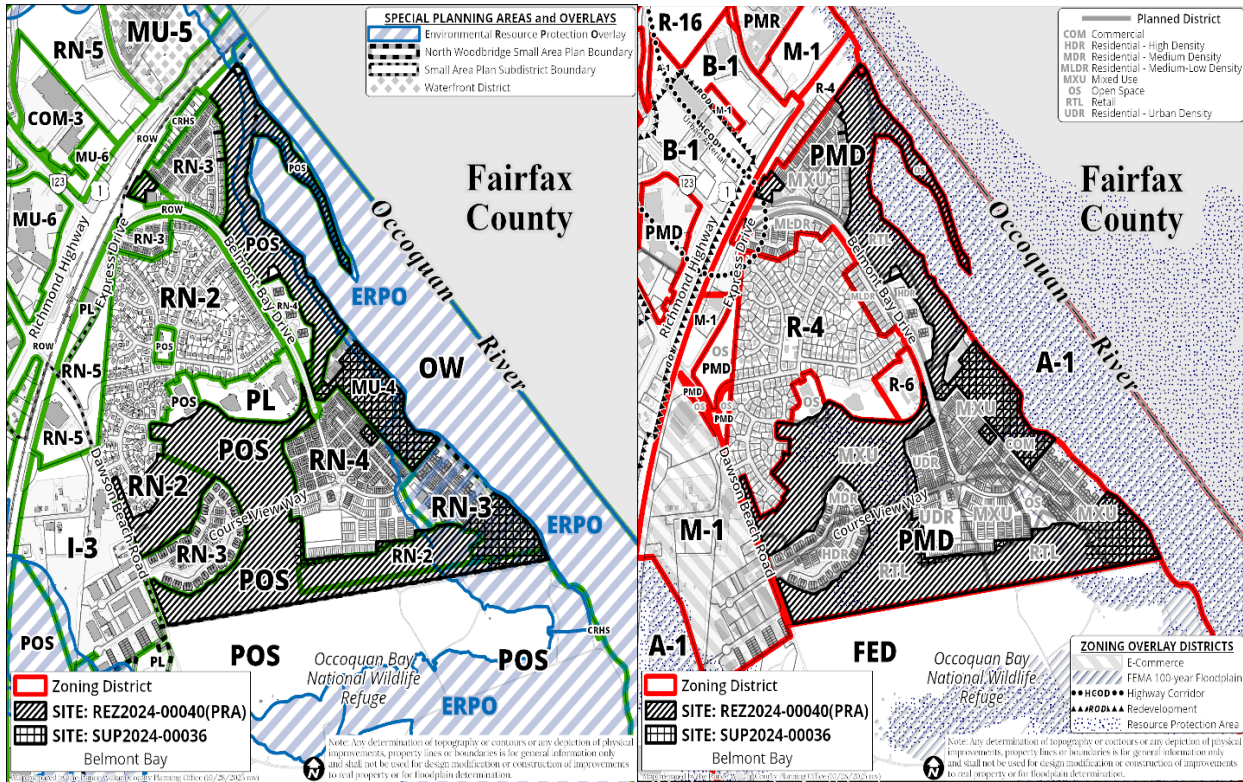
**Staff Recommendation: APPROVAL**

This summary analysis is based on the relevant Comprehensive Plan action strategies, goals, and policies. A complete analysis is provided in Part II of this report.

<b>Comprehensive Plan Sections</b>	<b>Plan Consistency</b>
Long-Range Land Use	Yes
Community Design	Yes
Cultural Resources	Yes
Economic Development	No
Environment	Yes
Fire and Rescue	Yes
Housing	Yes
Police	Yes
Potable Water	Yes
Sanitary Sewer	Yes
Schools	Yes
Transportation	Yes

**Part II. Comprehensive Plan Consistency Analysis**

The following table summarizes the area characteristics (see maps):



**Areas surrounding Belmont Landing and Taylor Point**

Direction		Long Range Future Land Use	Zoning
North	Belmont Bay Community	RN-3, Residential Neighborhood with T-3 & RN-4, Residential Neighborhood with T-4 and POS, Parks and Open Space	PMD, Planned Mixed Use Development
South	Occoquan Bay Wildlife Refuge	Federal Property	Federal Property
East	Occoquan River	OW, Open Water	A-1, Agricultural
West	Open Space	POS, Parks and Open Space	PMD, Planned Mixed Use Development

**Area surrounding Marina Overlook**

Direction		Long Range Future Land Use	Zoning
North	Occoquan River	OW, Occoquan River	A-1
South	Belmont Bay	RN-4	PMD
Southeast	Marina	OW, Occoquan River	PMD
East	Occoquan River	OW, Occoquan River	A-1
West	GM Science Museum	MU-4, Mixed Use Transect 4	PMD

**Long-Range Land Use Plan Analysis**

The County’s Long-Range Land Use Plan sets out policies and action strategies that further the County’s goal of concentrating on population, jobs, and infrastructure within vibrant, walkable, mixed-use centers serviced by transit. In addition to delineating land uses on the Long-Range Land Use Map, the Plan:

- includes smart growth principles that promote a countywide pattern of land use that encourages fiscally sound development;
- complements, preserves, protects, and respects the cultural and natural resources in the County;
- revitalizes, protects, and preserves the existing neighborhoods in the County.

The overall Proffer Amendment property is ±159.76 acres, with the Special Use Permit Amendment request encompassing ±15.8 acres of the 159.76 acres in the County’s Comprehensive Plan. The following table summarizes the uses and densities intended within the MU-4/RN-2 & RN-3 land use designations:

Long-Range Land Use Map Designation	Intended Uses and Densities
<b><i>MU-4, Mixed Use</i></b>	<p>Mixed Use live work centers include both residential and commercial uses arranged in a pedestrian-friendly form. These centers are locations for community commercial, entertainment destinations, and public facilities directly accessible to surrounding neighborhoods. Streets are interconnected and serve cars, cyclists, and pedestrians. Mixed Use Centers should be connected by bus transit to nearby destinations and to nearest rail transit.</p> <p>Target Density - T-4: 8 - 24 du/acre</p>

<p><b><i>RN-2/3, Residential Neighborhood</i></b></p>	<p>Residential Neighborhoods (T-2, T-3) primarily accommodate single-family attached or detached homes arranged in small to medium sized lots as well as some multi-family residential. Connections and pedestrian amenities should still be a priority for development design. These areas should also include parks, trails and open space integrated into the development in appropriate locations. Small office or service uses may be appropriate.</p> <p>Target Density- T-2: 1-4 du/acre Target Density- T-3: 4-12 du/acre</p>
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**Proposal's Strengths**

The applications demonstrate several strong attributes and generally align with the Comprehensive Plan and the Planned Mixed Residential District:

- Consistency with Comprehensive Plan Designations: The PMR (Planned Mixed Residential) zoning request is an implementing district within both the MU-4 (Mixed Use) and RN-2/3 (Residential Neighborhood) land use designations, where single-family attached housing is identified as a primary use.
- Appropriate Density: The proposed density falls within the target ranges for the MU-4, RN-3 and RN-2 identified in the Comprehensive Plan.
- Diversity of Housing Options: The inclusion of multiple townhome styles—complementing the existing condominiums, apartments, and single-family homes—supports a diversity of housing types suitable for a range of income levels and lifestyles.

**Proposal's Weaknesses**

While the applications exhibit several strengths, they lack alignment with key action strategies of the North Woodbridge Small Area Plan. Specifically:

- Inconsistent with Intended Intensity: The site is designated Urban Neighborhood (Transect 5), which anticipates a higher-density, mixed-use environment. The proposed development reflects a lower-density pattern than envisioned for this transect.
- Insufficient Commercial Activation: The Urban Neighborhood designation emphasizes waterfront-oriented commercial uses—such as hotel, office, government contracting, retail, and services—to support a vibrant, walkable district. The proposed 20,500 square feet of commercial space does not meet the scale needed to achieve these objectives.

- Loss of Planned Waterfront Place-Making Elements: By omitting the key waterfront uses envisioned in the Small Area Plan, the proposal removes foundational components of the area's activation and economic development strategy, diminishing the potential for Belmont Bay to evolve as a recreation and tourism destination.

### **Consistency Recommendation**

These applications are found to be consistent with the relevant components of the Long-Range Land Use Plan.

### **Community Design Plan Analysis**

The County's Community Design Plan sets out policies and action strategies that further the County's goals of providing quality development and a quality living environment for residents, businesses, and visitors, and creating livable and attractive communities. The Plan includes recommendations relating to building design, site layout, circulation, signage, access to transit, landscaping and streetscaping, community open spaces, natural and cultural amenities, stormwater management, and the preservation of environmental features.

### **Minimum Development Standards**

The requested Proffer Amendment (REZ) and Special Use Permit (SUP) do not have any conflicts with the County's Minimum Development Standards. The SUP conditions presented in this report further solidify that the requested SUP will adhere with the County's minimum development standards.

### **Proposal's Strengths**

- Management and Maintenance of the Property: The Applicant shall be charged with responsibility to oversee and coordinate development and ongoing management and maintenance of the Property,

#### **Town Center Amenities**

- Marina Overlook Development: The Town Center should be viewed as a unified, compact, mixed-use development with a special identity and sense of place. Development and use of Marina Overlook shall include the existing marina and related facilities. A minimum of two shipping containers may be used for commercial uses within Marina Overlook. A minimum of 20,500 square feet of gross floor area (including deck and terrace spaces) shall be permitted in Marina Overlook. The Applicant has agreed to proffer prohibited commercial uses referenced Proffer 7c.
- Mixed-Use Amenity Building: The Applicant shall construct a Mixed-Use Amenity Building within Marina Overlook in the approximate location shown on the MZP. Said Mixed-Use Amenity Building shall be a minimum of approximately 13,600 square feet and may include, but shall not be limited to, the police field office, retail, and recreation

## Staff Analysis

uses. The Applicant shall have the right to make the Mixed-Use Amenity Building available to the Existing BB HOA residents or a new merged master association. The Mixed-Use Amenity Building shall be completed prior to and as a condition of the issuance of the 75<sup>th</sup> residential building permit. These amenities are reflected in the Design Guidelines document.

- The Applicant has worked specifically with regards to the housing design to incorporate elements that blend with the style of the first homes constructed and the most recent. This intentionality will create a more unified community look and feel.
- The Applicant shall dedicate open space areas, 1, 3 & 4 (±99.78 acres) through the proffer statement to the Board of County Supervisors. These areas shall also have a Conservation Easement placed over them with commitment, as well through the proffer statement.
- Belmont Landing and Taylor Point Amenities: While the main amenities shall be along the marina, these areas will also have space for benches, shade structures and bocce court, etc.

### **Proposal's Weaknesses**

- There were no weaknesses with the proposed Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Community Design Requirements.

### **Consistency Recommendation**

This application is found to be consistent with and compatible to the relevant components of the County's Community Design Plan.

## **Cultural Resources Plan Analysis**

Cultural Resources are tangible links to our shared history which have shaped societal values and provide us with a better understanding of who we are. Good Cultural Resources Management (CRM) practices guide smart and sustainable development while also safeguarding the County's history and retaining a sense of place. Prince William County promotes the identification, research, evaluation, preservation, and documentation consistent with state and federal guidelines and encourages interpretation of cultural resource sites and the heritage tourism opportunities these sites present. Cultural resources are found in architectural or archaeological sites, historic districts, cemeteries, battlefields, cultural landscapes, museum objects, and archival materials; and come from all time periods and ethnicities; including minority communities. The Cultural Resources Plan's policies and action strategies provide a framework for the Board of County Supervisors, as well as boards, commissions, staff, citizens, and the development community to guide preservation decisions.

Previous Phase I investigations within the project area identified archaeological sites and cemeteries; however, a Cultural Resource Survey was not required at this time. The County's Historical Commission reviewed this request at its June 11, 2024, meeting and recommended no further work for the requested Proffer Amendment (REZ) and Special Use Permit (SUP). The Historical Commission's Resolution for this PRA & SUP is attached at the end of this report.

### **Proposal's Strengths**

- There were no strengths with the requested Proffer Amendment (REZ) or Special Use Permit (SUP) that were correlated to or associated with the County's Cultural Resources Plan.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Cultural Resources Plan.

### **Consistency Recommendation**

This application is found to be consistent with and compatible to the relevant components of the County's Cultural Resources Plan.

## **Environment Plan Analysis**

The County's Environment Plan sets out policies and action strategies that further the County's goal of preserving, protecting, and enhancing significant environmental resources and features. The Plan includes recommendations relating to the incorporation of environmentally sensitive development techniques, improvement of air quality, identification of problematic soil issues, preservation of native vegetation, enhancement of surface and groundwater quality, limitations on impervious surfaces, and the protection of significant viewsheds.

Stormwater Management and/or Best Management Practices (SWM/BMP) Controls: In accordance to the Zoning Ordinance and the DCSM, SWM/BMP shall be provided on-site and/or off-site. Locations shall be determined with final engineering during site plan review.

### **Proposal's Strengths**

- Water Quality: The applicant has agreed to conditions for a contribution of \$75.00 per disturbed acre for water quality monitoring, drainage improvements and/or stream restoration projects.
- Street Tree Planting: The Applicant shall provide street tree plantings and supplemental landscaping as established by the Design Guidelines and DCSM.

## Staff Analysis

- Reforestation: The Applicant shall provide reforestation within Open Space 4, as generally depicted on the plan entitled "Belmont Bay Conceptual Reforestation Plan," prepared by Dewberry Engineers Inc., dated February 10, 2025. Said reforestation will be installed in connection with the development of the homes in Belmont Landing.
- Revegetation: The Applicant shall provide revegetation within Taylor Point and Marina Overlook as identified below.

Taylor Point: The Applicant shall provide revegetation within Taylor Point, as generally depicted on the plan entitled "Belmont Bay Conceptual Taylor Point RPA Revegetation Plan," prepared by Dewberry Engineers Inc., dated February 10, 2025, last revised October 31, 2025. Said revegetation shall be provided in connection with the construction of the homes in Taylor Point.

Marina Overlook: The Applicant shall provide revegetation within Taylor Point, as generally depicted on the plan entitled "Belmont Bay Conceptual Marina Overlook Revegetation Plan," prepared by Dewberry Engineers Inc., dated February 10, 2025, last revised October 31, 2025. Said revegetation shall be provided in connection with the construction of the homes in Marina Overlook.

- Wetlands: All wetlands not otherwise designated as RPA's shall be identified on all grading and/or site plans submitted to the County's Land Development Division for review.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Environmental Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Environmental Plan.

## **Economic Development**

The County's Economic Development Plan sets out policies and action strategies that aims to foster the growth of environmentally sound industries to create quality jobs and diversify the nonresidential tax base, creating a climate where citizens can live and work in Prince William County. The Plan includes recommendations relating to business attraction, retention and expansion, the provision of adequate infrastructure, redevelopment of less competitive areas, telecommuting and other information-age opportunities, and recognition of tourism as an industry.

The Department of Economic Development (“DEDT”) encourages the applicant to consider waiting to coordinate with the County’s Maritime Industry & Waterfront Placemaking Market Study. This would allow further analysis and conceptualization of what a more balanced waterfront development would look like and offer strategies for implementation.

### **Proposal’s Strengths**

- The Applicant has made a commitment through the proffer statement to providing a minimum of 20,500 square feet of new commercial retail/service space.
- The Applicant has committed to bringing a new restaurant(s) and commercial opportunities to the community.
- DEDT does recognize that the applicant has made some modest changes that should be beneficial to the adjoining Belmont Bay community, notably:
  - A 5,500 SF increase in the max. square footage in the Mixed-Use Amenity Building, which will now be up to a max. 20,500 SF (total commercial uses) and include 1,360 SF for a police field office, retail and recreational.
  - The application includes a small outdoor amphitheater and outdoor small business/ vendor space for a food truck and seasonal business in two container spaces.
  - The existing Harbormaster Building will be used in part for a restaurant and part of the overall square footage.

### **Proposal’s Weaknesses**

#### **Small Area Plan Goal (p. 182):**

*“Encourage a diverse mix of retail, office, hospitality, and residential uses in a vibrant and walkable environment that capitalizes on its waterfront location and its access to regional transportation.”*

- **Weakness:** The proposal reduces commercial square footage from over 2 million square feet to just 20,500 square feet and removes previously planned hotel and office components. This fundamentally shifts the project away from a vibrant, employment-generating town center and toward a predominantly residential enclave.

#### **Action Strategy ED1:**

*“Encourage office, hotel, institutional, and service-oriented uses in the North Woodbridge Town Center **AND** waterfront areas to maximize their strategic location.”*

- **Weakness:** All hotel and most office uses have been eliminated, despite the site's strategic location on the Occoquan River and near regional transportation hubs.

#### **Action Strategy ED2:**

*“Encourage nonresidential development that serves as a destination, attracts visitors, and contributes to a vibrant and active public realm.”*

- **Weakness:** By replacing mixed-use commercial amenities with residential townhouses, the proposal fails to create a destination or active public realm, and undermines the area's potential for tourism and regional visitation as referenced in the Small Area Plan.

Developing almost purely residential on this prime commercial waterfront area will not achieve the above referenced county vision and goals in a meaningful way.

### **Consistency Recommendation**

This application is found to be inconsistent with the relevant components of the County's Economic Development Plan.

### **Fire and Rescue Plan Analysis**

The County's Fire and Rescue Plan sets out policies and action strategies that further the County's goal of protecting lives, property, and the environment through timely, professional, humanitarian services essential to the health, safety, and well-being of the community. The Plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility for fire and rescue facilities. The Plan also includes recommendations to supplement response time and reduce risk of injury or death to County residents, establishment of educational programs, such as cardio-pulmonary resuscitation (CPR) training, automatic external defibrillators (AED), and encourage installation of additional fire protection systems – such as sprinklers, smoke detectors, and other architectural modifications.

Fire/Rescue Station 2 (Occoquan/Woodbridge/Lorton) is the first due fire/rescue resource for the property. The property is located inside the required 4.0-minute travel time for Basic Life Support and Fire Suppression and inside the required 8.0-minute travel time for Advanced Life Support. In FY 2024, Station 24 responded to 3,593 incidents with a workload station capacity of 2,000 incidents per year.

### **Proposal's Strengths**

- Inside of 4.0-Minute Travel Time: The property is currently located outside the required 4.0-minute travel time for fire suppression and basic life support.
- Inside of 8.0-Minute Travel Time: The property is located within the required 8.0-minute travel time for advanced life support services.
- Monetary Contribution: The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors, the sum of \$0.61 per square foot of any new gross building floor area (over 3,668 gross square feet, the existing Harbor Master Building) for fire and rescue purposes. Said contribution shall be paid prior to and as a condition of the approval of each building permit for a building, excluding any structured parking facilities, on the Property.

### **Proposal's Weaknesses**

- Station Workload: The number of incidents in the Fire/Rescue Station #2 area and the 3,593 incidents that were responded to in FY24 were above the maximum workload capacity of 2,000 incidents.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Fire and Rescue Plan.

**Prince William County's CIP 2026 Public Safety**

## Station 2 Fire & Rescue Replacement

**Total Project Cost – \$30.0M**

**Project Description**

The project will construct a new station to replace the existing Station 2 facility currently located at 1306 F Street in Woodbridge. The existing Station 2 facility is approximately 55 years old and in need of replacement based on the recommendation of a facility condition assessment completed in 2021. The new station is planned to be constructed on the same site as the existing station. The new station will include sleeping quarters, a kitchen and dayroom, a physical fitness room, a training room, and offices. Multiple apparatus bays will allow for the best combination of response units.

**Service Impact**

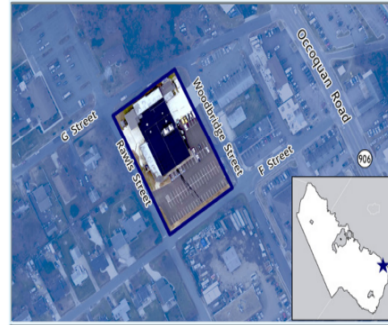
► **Station design and construction** – The new station will meet current design and construction practices and standards for fire and rescue facilities.

**Funding Sources**

► **Debt financing (supported by the fire levy)** – \$30.0M

**Project Milestones**

- **Design** is scheduled to begin in January 2030 (FY30) with completion scheduled for November 2031 (FY32).
- **Permitting and construction bidding** is scheduled to begin in November 2031 (FY32) with completion scheduled for August 2032 (FY33).
- **Construction** is scheduled to begin in August 2032 (FY33) with completion scheduled for March 2035 (FY35).
- **Occupancy** is scheduled for March 2035 (FY35).



Impact on Comprehensive Plan Chapters		
Cultural Resources	Community Design	Housing
Environment	Open Space	Mobility
<b>Safe &amp; Secure Community</b>	Utilities	Special Area Plans
Land Use	Community Education	Parks, Recreation & Tourism

Impact on Strategic Plan Goals		
Health & Wellbeing	<b>Safe &amp; Secure Community</b>	Resilient Economy
Quality Education	Environmental Conservation	Sustainable Growth
Mobility		

## Housing Plan Analysis

Prince William County is committed to clean, safe, and attractive neighborhoods for all its residents, and the elimination of neighborhood blight and substandard housing. The Housing Plan sets out policies and action strategies that further the County’s goal of identifying locations and criteria for the provision of diverse housing opportunities for all segments of our population and to promote economic development. The Plan includes recommendations relating to neighborhood preservation and improvement, affordable housing, special needs housing, and public/private partnerships to address housing needs.

The Comprehensive Plan has the following Housing policy #2: *Promote diverse mixed income housing communities throughout the County that address the demand for additional housing, the demand for a variety of housing, and the demand for affordable housing to meet the needs of residents at all income levels throughout all stages of life.*

### **Proposal's Strengths**

- Commitment to Affordable Housing: The Applicant has proffered a monetary contribution of \$2,500.00 per residential dwelling unit to be paid to the Board of County Supervisors at the time of Occupancy Permit for the County's Affordable Housing Program.

### **Proposal's Weaknesses**

- There were no weaknesses with the proposed Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Housing Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Housing Plan.

## **Police Plan Analysis**

The County's Police Plan is designed to promote Prince William County's public safety strategic goal to continue to be a safe community, reduce criminal activity, prevent personal injury and loss of life and property, and ensure effective and timely responses throughout the County. The Plan recommends educational initiatives, such as Neighborhood and Business Watch and Crime Prevention through Environmental Design (CPTED), which encourages new development to be designed in a way that enhances crime prevention. The Plan also encourages effective and reliable public safety communications linking emergency responders in the field with the Public Safety Communications Center.

At this time, the County's Police Department believes that this application will not create a significant impact on calls for service. The Applicant should coordinate with the County's Police Department as the site develops and apply the various Crime Prevention Through Environmental Design (CPTED) principles, which can be found at the following:

- <https://www.pwcva.gov/assets/documents/police/002035.pdf>

Details on the type(s) of onsite security measures should also be considered, including access control, surveillance cameras, alarm systems, security, access gates, fencing, and onsite lighting.

### **Proposal's Strengths**

- Proffer # 27 Field Office: In consultation with the Police Department, the Applicant shall provide the Police Department with "field office" facilities to be located within the Mixed-Use Amenity Building. The "field office" facilities shall consist of approximately 1,360 square feet of gross floor area with standard electric and telephone wiring in

place and shall include finished space for office space, a reception area and, a bathroom. The "field office" space shall be provided at no cost to the Police Department. The Police department shall be responsible for their electric and water fees / payments, pro rata share of their common area maintenance fees, taxes, and any expenses incurred in connection with interior improvements (Police Department fixtures and infrastructure) and/or alterations after initial occupancy by the Police Department.

### **Proposal's Weaknesses**

- There were no weaknesses with the proposed Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Police Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Police Plan.

## **Potable Water Plan Analysis**

The County's Potable Water Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound drinking water system. The Plan includes recommendations relating to system expansion, required connections to public water in the development area, and the use of private wells or public water in the Rural Area.

Regarding the Marina Overlook land bay, the Service Authority has an existing 12-inch water main located in Harbor Side Street. Regarding the Taylor Point land bay, the Service Authority has an existing 12-inch water main on site. Regarding the Belmont Landing land bay, the Service Authority has a 12-inch water main located in Chrisswind Avenue and an 8-inch water main located in Clear Lake Circle. The developer will be required to provide a looped water supply connecting the two existing water mains located in Chrisswind Avenue and Clear Lake Circle for increased reliability and water quality. All connections to the public water system shall be in accordance with the Service Authority's USM requirements and restrictions.

### **Proposal's Strengths**

- The subject Proffer Amendment (REZ) & SUP site shall be connected to the Service Authority/Prince William Water public water with the Applicant bearing all costs and responsibilities for such connections including planning, designing, and constructing the public water service to the property, as reflected in the Proffer Statement #28.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) Amendment that were correlated to or associated with the County's Potable Water Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Potable Water Plan.

### **Sanitary Sewer Plan Analysis**

The County's Sanitary Sewer Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound sanitary and stormwater sewer system. The Plan includes recommendations relating to system expansion, required connections to public sewer in the development area, and the use of either private or public sewer systems in locations classified as Semi-Rural Residential (SRR) and the Rural Area.

Regarding the Marina Overlook land bay, the Service Authority has an existing 12-inch gravity sewer main located in Harbor Side Street. Regarding the Taylor Point land bay, the Service Authority has an existing 10-inch gravity sewer main located on site. Regarding the Belmont Landing land bay, the Service Authority has an existing 8-inch gravity sewer main located in Chrisswind Avenue, with availability of capacity determined in conjunction with plan submission. All connections to the public sewer system shall be in accordance with the Service Authority's USM requirements and restrictions. The Service Authority is also requesting the applicant to proffer 0.43 acres on the northern portion of the site as shown on sheet 5 of 6 of the Belmont Bay Transportation & Utilities Plan for a sewer lift station within 6 months of county application approval. The Applicant has committed to this request in the Proffer statement

Proffer #29. Sewer Lift Station Dedication: For the public facility approved in connection with Public Facility Review #PFR2022-00004, Occoquan Creek Pump Station as may be amended, the Applicant shall dedicate, at no cost, approximately 0.43 acres of land within OSP 1 as depicted on the MZP, to Prince William Water. Said dedication shall be made within six months of Final Rezoning approval.

The Applicant shall design and construct all new on-site and off-site water and sanitary sewer utility improvements necessary to develop the subject property and the above listed requirements in accordance with the Service Authority's USM, and County and State requirements, standards, and regulations. The sizing and configuration of on-site and off-site utility system improvements will be determined during the preliminary and final plan review process, based on existing and proposed zonings of surrounding properties and the policies of the County Comprehensive Plan and Service Authority planning documents. The design shall be supported by appropriate engineering analysis/modeling of affected existing utility systems and the proposed new facilities.

### **Proposal's Strengths**

- The subject Proffer Amendment (REZ) & SUP site will be connected to the Service Authority/Prince William Water public sewer with the Applicant bearing all costs and responsibilities for such connections including planning, designing, and constructing the public sewer service to the property. This commitment is reflected in the Proffer Statement #28.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) that were correlated to or associated with the County's Sanitary Sewer Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Sanitary Sewer Plan.

## **Schools Plan Analysis**

A high-quality education system serves not only the students and their families, but the entire community by attracting employers who value educational opportunities for their employees. The Schools Plan sets out policies and action strategies that further the County's goal of providing quality public education to our school-aged population. The Plan includes recommendations relating to facility size and location, sitting criteria, compatible uses, and community use of school facilities.

Projected enrollment at the assigned elementary school and high school exceeds 100% of capacity. The addition of anticipated students generated from this application will further exacerbate this condition. However, the approved CIP includes the "Woodbridge Area" Elementary School and the 14th High School which will provide additional capacity to address overcrowding at these school levels.

### **Proposal's Strengths**

This impact analysis provides an update to the previously approved 74 market rate units already calculated in student generation numbers.

- The majority of these residential dwelling units shall be 55+ and not generate any school age children.
- Current and projected enrollment at the assigned middle school indicates available space for the anticipated students generated from this application.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) associated with the County's Transportation Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's School's Plan.

**Current and Projected Student Enrollment and Capacity Utilization for Schools in the Attendance Area of the Proposed Rezoning**

Under the Division’s 2024-25 school attendance area assignments, students generated from the Proposed Rezoning will attend the following schools:

Assigned Schools	Available Space			2024-25			2025-26			2029-30		
	Planning Capacity	Program Capacity	Portable Class-rooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Belmont Elementary	---	551	0	478	73	86.8%	500	51	90.7%	607	-56	110.2%
Fred M. Lynn Middle	---	1,170	0	959	211	82.0%	929	241	79.4%	785	385	67.1%
Freedom High	2,053	---	12	2,239	-186	109.1%	2,311	-258	112.6%	2,362	-309	115.1%

Note: Capacities of schools reported do not include the temporary capacity provided by any portable classrooms present on the school site. Portable classrooms do not add permanent capacity and are not included in the calculation of a school’s capacity or the assessment of a school being able to adequately accommodate students.

**Current and Projected Student Enrollment and Capacity Utilization for Schools in the Attendance Area, Including Projected Student Yields from the Proposed Rezoning**

Assigned Schools	Available Space			2024-25			2025-26			2029-30		
	Planning Capacity	Program Capacity	Portable Class-rooms	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)	Students	Space Available (+/-)	Util. (%)
Belmont Elementary	---	551	0	478	73	86.8%	515	36	93.5%	622	-71	112.9%
Fred M. Lynn Middle	---	1,170	0	959	211	82.0%	935	235	79.9%	791	379	67.6%
Freedom High	2,053	---	12	2,239	-186	109.1%	2,318	-265	112.9%	2,369	-316	115.4%

Note: Capacities of schools reported do not include the temporary capacity provided by any portable classrooms present on the school site. Portable classrooms do not add permanent capacity and are not included in the calculation of a school’s capacity or the assessment of a school being able to adequately accommodate students.

**Strategic Plan**

“The 2025–2028 Strategic Plan reflects the community’s current desired outcomes and focuses on how PWC can transform to meet the needs of a growing and diverse community. This plan will serve as the guiding vision and action plan for PWC over the next four years. As such, it provides key policy guidance for service delivery and resource allocation decisions during the Board of County Supervisors’ term in office.” (*Prince William County, 2025–2028 Strategic Plan, p. 13*)

**The Smart Growth Goal** - Advance strategic and intentional community-oriented growth that strengthens and enhances the diversity and sustainability of the local economy and the County.

**Objective 1**, Promote high-quality architectural design, thoughtful placemaking, and appealing public spaces to create vibrant and attractive environments for living, working,

Strategies:

1. Provide design guidance in the Comprehensive Plan, small area plans, and regulatory tools to promote and facilitate high-quality design of development and public spaces.

**Objective 2,** Ensure the County's growth holistically addresses the impact on natural resources and infrastructure and enhances community and economic resilience.

Strategies:

1. Develop clear guidelines to mitigate development impeding natural resources.
2. Redevelop and increase density on underutilized infill sites for housing, services, and other needed uses.

### **Transportation Plan Analysis**

By providing a multi-modal approach to traffic circulation Prince William County promotes the safe and efficient movement of goods and people throughout the County and surrounding jurisdictions. The Transportation Plan sets out policies and action strategies that further the County's goal of creating and sustaining an environmentally friendly, multi-modal transportation system that meets the demands for intra- and inter-county trips, is integrated with existing and planned development, and provides a network of safe, efficient, and accessible modes of travel. The Plan includes recommendations addressing safety, minimizing conflicts with environmental and cultural resources, maximizing cost effectiveness, increasing accessibility of all travel modes, minimizing projected trip demand, and providing sufficient network capacity. The County recognizes that it is not possible to address congestion through road investments alone and has reduced the acceptable standard to (LOS) of "E" specifically in Small Area Plans, in Activity Centers, and on Arterials Projects should include strategies that result in a level of service LOS "D" or better on all roadway, corridors, and intersections, reduce traffic demand through transportation demand management strategies, dedicate planned rights-of-way, provide and/or fund transit infrastructure, pedestrian, and bicycle pathways, and improved and coordinated access to transit facilities.

Access to the property will be from Dawson Beach Road at Express Drive and Course View Way.

A Deferral for a Traffic Impact Analysis was submitted with the initial application with the Department of Transportation (DOT) approving the deferral request. DOT staff determined the proposed uses represent a significant reduction of site trips compared to the approved uses.

#### **Proposal's Strengths**

- **Town Center Public Streets:** These streets will be dedicated for public use and designed to ensure capacity for adequate circulation among the range of planned land uses. Conventional parallel parking is encouraged on the Town Center's public streets for convenient access.
- **Pedestrian Circulation:** The Belmont Town Center will function as a pedestrian friendly environment favoring traditional neighborhood design concepts in which residents, the

workforce, and visitors have access to amenities, goods and services. This includes a shared use path along the east side of Dawson Beach Road between Course View Way south and the Occoquan Bay National Wildlife Refuge property.

- Woodbridge VRE/Amtrak and Bus Service Station: This station is located at the Dawson Beach Road and Express Way intersection adjacent to Route 1. This station includes a covered parking garage for convenient access.

### **Proposal's Weaknesses**

- There were no weaknesses with the requested Proffer Amendment (REZ) or Special Use Permit Amendment (SUP) associated with the County's Transportation Plan.

### **Consistency Recommendation**

This application is found to be consistent with the relevant components of the County's Transportation Plan.

### **Materially Relevant Issues**

This section of the report is intended to identify issues raised during the review of the proposal, which are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but which are materially relevant to the County's responsibilities in considering land use issues. The materially relevant issues in this case are as follows:

#### **Occoquan Bay National Wildlife Refuge**

The Occoquan Bay National Wildlife Refuge abuts portions of these applications. Planning/Environmental staff and the Applicant have conducted a number of meetings with the Refuge staff managers. The last meeting Planning staff had with the Refuge was in August 2025. In our discussions, the manager, Mr. Adams relayed that he could not offer official support for the application, however, he was pleased with the efforts the Applicant is planning with the removal of invasive species and the commitment to using dark sky lighting in all the townhomes along the Refuge buffer.

Mr. Adams provided the following in an email dated August 2025:

*I appreciate the efforts your company is making to be a good neighbor to Occoquan Bay NWR. We recognize the importance of collaboration in promoting conservation efforts that will benefit both the refuge and your interests.*

*Should your project receive approval, we would be eager to explore partnership opportunities with you, in areas including invasive plant control and native plant establishment along our boundary that was shared in your planning documents with me and my staff.*

Commitments to the Refuge are reflected in the Proffer Statement presented with this application under are as follows.

22. Buffer:
- a. The Applicant shall coordinate with the Occoquan Bay National Wildlife Refuge (the "Refuge") to partner to provide a minimum 50-foot buffer adjacent to the Refuge property. The coordination with the Refuge shall also include installing plantings between the common property line and the existing Refuge fence. Said buffer shall be planted in general conformance with the Refuge Exhibit. In lieu of said plantings and in coordination with the Refuge, the Applicant may provide a monetary donation in the amount of \$65,000 for the plantings. The plantings shall be provided at the time the adjacent portions of the Property are developed and shall be shown on each respective final site plan. In the event the Refuge determines that the monetary contribution is more appropriate, said monetary contribution shall be paid prior to and as a condition of the 50<sup>th</sup> residential occupancy permit on Taylor Point.
  - b. All new plantings within said buffer shall be plant species native to Virginia.
  - c. The Applicant shall coordinate with the Refuge to partner to remove invasive species on the Refuge property between the common property line and the existing Refuge fence.
23. Lighting: All external, exterior lighting fixtures shall be Dark Sky or an equivalent standard on the Property along the Refuge boundary.
24. Connectivity: Trail connections between the Property and Refuge property may be provided as depicted on the MZP or such other locations agreed upon in consultation with the Refuge.

### **Waivers and Modifications**

Pursuant to Zoning Ordinance section 32-700.25, a planned development zoning district may include waivers and/or modifications to specific standards of the Subdivision Ordinance, the Zoning Ordinance or the Design and Construction Standards Manual. A written justification is provided, and the characteristics of the specific property are depicted in the application materials. The waivers or modifications proposed will not conflict with the fulfillment of the purpose of the requested Planned Mixed Residential zoning district. The appropriate County agencies have reviewed the waivers/modifications requested and are in agreement to their approval. A number of the waivers and modifications were previously approved and being carried over. As well, a number are shown as duplicates as they are for both applications.

**Proffer Amendment – REZ2024-00040**

Pursuant to Section 32-700.25 of the Prince William County Zoning Ordinance (the “Zoning Ordinance”), the following waivers and modifications to the requirements of the Zoning Ordinance and DCSM shall be deemed granted and approved.

**1. Modification** of Section 32-201.18 of the Zoning Ordinance to waive the 15-foot-wide landscaped area around public use sites, such as telephone switching station, power substation, etc.

**Justification** The urban character of the Town Center is not consistent with the implementation of landscaped areas surrounding public facilities. The specific type and location of a facility will determine the appropriate treatment in terms of screening, landscaping and architecture or fencing, as such treatment is determined at the time of site plan.

**2. Waiver** of Sections 32-250.10 and 32-400.16 of the Zoning Ordinance in order to allow parking as a principal use on subdivided lots, regardless of ownership, located within the Town Center. and to allow more than 50% of the required parking to be located on a separate parcel.

**Justification:** This waiver was previously approved. The design and construction of the Town Center is an urban village design that may require various uses to be on separate parcels. However, the proposed project provides sufficient parking for the entire project.

**3. Waiver** of Sections 32-250.31 of the Zoning Ordinance and 802.11 and 802.12 of the DCSM waiving the buffer between commercial, industrial, office and other nonresidential uses and residential uses within the planned development district.

**Justification:** This waiver was previously approved. Belmont Bay was designed to be a cohesive development. As discussed with County staff, the goal of the development is to provide an integrated community within Belmont Bay.

**4. Modification** of Sections 32-250.40 through 32-250.44 of the Zoning Ordinance and 802.20, 802.21, 802.30 and 802.31 of the DCSM to allow implementation of tree canopy and open space requirements over the entire Property.

**Justification:** This waiver was previously approved. As part of the development of Belmont Bay, site plans have been submitted with overall development tabulations. With the open space and parks and recreation land bays provided, there is sufficient open space and tree canopy on the Property.

**5. Modification** of Sections 32-306.10, 32-306.12.6.F and 32-306.12.6.G of the Zoning Ordinance to allow for the housing types, as depicted on the MZP and Design Guidelines.

**Justification:** This modification allows for flexibility and alternative housing types as well as the design contemplated with this development. The development standards of the housing types on the Property will be in accordance with the MZP and Design Guidelines.

**6. Modification** of Sections 32-300.05.4 and Section 32-306.12.6.F(3) of the Zoning Ordinance to increase the maximum building height to 50 ft. the single family attached units as shown in the MZP and Design Guidelines.

**Justification:** The original rezoning and special use permit allowed for a maximum height of 50 ft. in the Development Code. The Applicant is memorializing the already permitted height within Belmont Bay. This height allows for the housing types shown on the MZP and in the Design Guidelines.

**7.Waiver** of Sections 32-405.04(4) of the Zoning Ordinance and 802.47.B of the DCSM of the peripheral buffer in its entirety for the Property and implement the landscaping and buffers as shown on the MZP.

**Justification:** A similar waiver was approved with the previous Rezoning and SUP. Creation of the buffers between various uses would contrast with the objective of the town center and overall Belmont Bay development. This is a standard waiver associated with town center developments.

**8.Waiver** of Section 32-405.04(7) of the Zoning Ordinance to allow the residential use to exceed 35% of the gross land area of the PMD as generally shown on the MZP.

**Justification:** The Applicant is proposing a development with a mix of uses both residential and commercial with a town center core. The Comprehensive Plan, as approved, has more than 35% of the land area designated as residential. The proposed development is compatible with, and consistent with, the Comprehensive Plan.

**9.Modification** of Section 125.01.I of the DCSM to reduce the required common or shared easement width from 15 ft. to 10 ft.

**Justification:** To accommodate the lot sizes of the townhome units and incorporate this urban style setback and allow the homes to sit close to the street the utility easement will need to be reduced to 10 ft.

**10.Modification** of Section 602.07.T. of the DCSM to allow driveways to be greater than 35% or more than 720 SF.

**Justification:** The project is Urban in nature with smaller lots and decreased setbacks. Allowing greater than 35% of the minimum required front yard to be improved parking surface would allow the project to maintain the urban feel.

**11.Waiver** of Section 602.14.H of the DCSM requiring a cul-de-sac at the end of privately maintained travelways.

**Justification:** Cul-de-sacs at the ends of streets within the project would result in additional impervious area. Additionally, the presence of cul-de-sacs would encroach into or decrease buffers resulting in less planting and screening. Streets are private and a modified curb return/street ending will be provided for passenger vehicles accessing the driveways at the ends of streets to allow for typical vehicular movements. Note – The Applicant has sought this waiver from County Transportation and County Transportation has recommended it for Board of County Supervisors action.

**12.Modification** of DCSM Details 650.06 to allow street sections in accordance with the street sections shown on the MZP.

**Justification:** This development is within a community with previously constructed residential and commercial areas with modified street sections. In order to maintain continuity in the redevelopment areas these modified, narrow street sections shall continue to be permitted. Furthermore, more narrow street sections promote low impact design strategies such as reducing impervious surface areas, increasing green space and vegetative areas, and reducing runoff. Note – The Applicant has sought this waiver from County Transportation and County Transportation has recommended it for Board of County Supervisors action.

**13.Modifications** as approved with SUP#2024-00036 and any subsequent amendments thereto.

**Justification:** This waiver was previously approved and allows for the special use permit conditions to prevail in Marina Overlook.

**14.Waiver** of Section 32-250.31 of the Zoning Ordinance and 802.47 of the DCSM requirement for a 15-foot-wide landscape area around community recreation sites.

**Justification:** This waiver provides flexibility to integrate community recreation sites / facilities into the planned mixed development design, rather than separate out such uses. Community recreation sites / facilities should be incorporated into the overall development.

### **SUP Amendment SUP2024-00036**

Pursuant to Section 32-280.34 of the Zoning Ordinance, approval of the subject special use permit shall constitute a waiver/modification of the following:

**1.Modification** of Section 32-201.18 of the Zoning Ordinance to waive the 15 ft. wide landscaped area around public use sites, such as telephone switching station, power substation, etc.

**Justification:** The character of the Town Center is not consistent with the implementation of landscaped areas surrounding public facilities. The specific type and location of a facility will determine the appropriate treatment in terms of screening, landscaping and architecture or fencing, as such treatment is determined at the time of site plan.

**2.Waiver** of Sections 32-250.10 and 32-400.16 of the Zoning Ordinance in order to allow parking as a principal use on subdivided lots, regardless of ownership, located within the Town Center and to allow more than fifty percent (50%) of the required parking to be located on a separate parcel.

**Justification:** This waiver was previously approved. The design and construction of the Town Center is an urban village design that may require various uses to be on separate parcels. However, the proposed project provides sufficient parking for the entire project.

**3.Waiver** of Sections 32-250.31 of the Zoning Ordinance and 802.11 and 802.12 of the DCSM waiving the buffer between commercial, office and other nonresidential uses and residential uses within the planned development district.

**Justification:** This waiver was previously approved. Belmont Bay was designed to be a cohesive development. As discussed with County staff, the goal of the development is to provide an integrated community within Belmont Bay.

**4.Modification** of Sections 32-250.40 through 32-250.44 of the Zoning Ordinance and 802.20, 802.21, 802.30 and 802.31 of the DCSM to allow implementation of tree canopy and open space requirements over the entire Property.

**Justification:** This waiver was previously approved. As part of the development of Belmont Bay, site plans have been submitted with overall development tabulations. With the open space and parks and recreation land bays provided, there is sufficient open space and tree canopy on the Property.

**5. Modification** of Section 32-280.31.7(a) of the Zoning Ordinance to allow multiple and/or tiered owner associations.

**Justification:** This waiver was previously approved. The complexity of multiple owners and the variety of residential and commercial uses make multiple and/or tiered owners associations appropriate. The Applicant reserves the right to have multiple owner associations, including but not limited to an umbrella association and various residential and nonresidential associations and sub-associations based on future decisions as to the responsibility for the maintenance of facilities and related controls on a land bay by land bay basis.

**6. Modification** of Section 32-280.31.7(b) of the Zoning Ordinance to not specifically state the Design Guidelines in the CCRC.

**Justification:** This waiver was previously approved. The special use permit conditions will control significant development parameters and the Design Guidelines will be incorporated into the CCRCs by reference. In the event of a transfer of the right to modify the Design Guidelines by the Design Review Committee as set forth in the Design Guidelines, such transfer and authority to make such changes shall be only to an architectural review board of an umbrella or master association and not the various sub associations individually.

**7. Modification** of Section 32-280.31.7(j) of the Zoning Ordinance to allow for modification of conditions, covenants and restrictions in accordance with standard practice during developer control of project.

**Justification:** This waiver was previously approved. The complexity of multiple owners and the variety of residential and commercial uses make multiple and/or tiered owners associations appropriate. The Applicant reserves the right to have multiple owner associations, including but not limited to an umbrella association and various residential and nonresidential associations and sub-associations based on future decisions as to the responsibility for the maintenance of facilities and related controls on a land bay by land bay basis.

**8. Modification** of Section 32-280.35 to delete the items required to be included in the Design Guidelines which are not currently included within the Design Guidelines.

**Justification:** This modification was previously approved. The Applicant as part of the proposal has substantially used the same Design Guidelines and made them applicable to Marina Overlook, Taylor Point, and Belmont Landing.

**9. Modification** of Section 32-280.61.2(a) of the Zoning Ordinance to allow rear yards of townhouses to be less than 30 ft.

**Justification:** This waiver was previously approved. The unique design of many of the townhouse units and overall density of the development contemplated for the Town Center, does not accommodate the rear yards. Reduced setbacks and separations allow for an urban development consistent with the Town Center.

**10. Modification** of Section 32-280.61.2(e) of the Zoning Ordinance to allow a reduced building separation of 5 ft. rather than 20 ft.

**Justification:** This waiver was previously approved. The reduced separation provides flexibility to create character consistent with Town Center concept. Any reduced building separations will be subject to building code requirements and Fire and Rescue Approval.

**11. Modification of Section 32-300.05.4 and** Section 32-306.12.6.F(3) of the Zoning Ordinance to increase the maximum building height to 50 ft. the single family attached units as shown on the Development Code and in the Design Guidelines.

**Justification:** This was previously approved and allowed for a maximum height of 50 ft. in the Development Code. The Applicant is memorializing the already permitted height within Belmont Bay. This height allows for the housing types shown on the MZP and in the Design Guidelines.

**12. Modification** of Sections 32-306.10, 32-306.12.6.F and 32-306.12.6.G of the Zoning Ordinance to allow for the housing types, as depicted on the SUP Plan and Design Guidelines.

**Justification:** This modification allows for flexibility and alternative housing types as well as the design contemplated with this development. The development standards of the housing types on the Property will be in accordance with the MZP and Design Guidelines.

**13. Waiver** of Sections 32-405.04(4) of the Zoning Ordinance and 802.47.B of the DCSM of the peripheral buffer in its entirety for the Property and implement the landscaping and buffers as shown on the Master Zoning Plan.

**Justification:** A similar waiver was previously approved. Creation of the buffers between various uses would be in contrast with the objective of the town center and overall Belmont Bay development.

**14. Waiver** of Section 32-405.04(7) of the Zoning Ordinance to allow the residential use to exceed 35% of the gross land area of the PMD as generally shown on the Land Use Plan.

**Justification:** The Applicant is proposing a development with a mix of uses both residential and commercial with a town center core. The Comprehensive Plan, as approved, has more than 35% of the land area designated as residential. The proposed development is compatible with, and consistent with, the Comprehensive Plan.

**15. Modification** of Section 32-700.51.1 of the Zoning Ordinance to allow the following items to be omitted from town center street grid plan: private streets, shared access driveways, delivery vehicles access, and loading.

**Justification:** This waiver was previously approved. These characteristics will be provided in full detail during the site plan stage.

**16. Waiver** of Section 32-700.51.3 of the Zoning Ordinance requiring a Phasing Plan.

**Justification:** This waiver was previously approved. A majority of the infrastructure and road network have already been constructed within Belmont Bay.

**17. Modification** of Section 32-700.51.4 of the Zoning Ordinance requiring Community Facilities & Infrastructure Analysis & Plan.

**Justification:** This waiver was previously approved. The community facilities are shown in the Design Guidelines which meets this requirement.

**18. Waiver** of Section 32-700.51.7 of the Zoning Ordinance requiring a Contingency Plan.

**Justification:** This waiver was previously approved.

**19. Waiver** of Section 32-700.55(1) of the Zoning Ordinance to provide flexibility with reference to the implementation of the special use permit.

**Justification:** This waiver was previously approved. Due to the interrelated nature of the various components of the Town Center flexibility is necessary to allow for implementation of the special use permit over an extended period of time, particularly in the absence of an interpretation and clarification of this requirement with reference to commencement of the use.

**20.Modification** of Section 32-700.51.2 of the Zoning Ordinance to allow the following items to be omitted from schematic land use plan; recreation and tree preservation.

**Justification:** This modification was previously approved. A majority of the Belmont Bay development has already been constructed. The tree preservation areas within portions of the golf course will remain. However, a majority of the parks and open space land bays will be dedicated to the County for its parks and recreation use.

**21.Modification** of Section 125.01.I of the DCSM to reduce the required common or shared easement width from 15 ft. to 10 ft.

**Justification:** To accommodate the lot sizes of the townhome units and incorporate this urban style setback and allow the homes to sit close to the street the utility easement will need to be reduced to 10 ft.

**22.Modification** of Section 602.07.T. of the DCSM to allow driveways to be greater than 35% or more than 720 SF.

**Justification:** The project is Urban in nature with smaller lots and decreased setbacks. Allowing greater than 35% of the minimum required front yard to be improved parking surface would allow the project to maintain the urban feel.

**23.Waiver** of Section 602.14.H of the DCSM requiring a cul-de-sac at the end of privately maintained travelways.

**Justification:** Cul-de-sacs at the ends of streets within the project would result in additional impervious area. Additionally, the presence of cul-de-sacs would encroach into or decrease buffers resulting in less planting and screening. Streets are private and a modified curb return/street ending will be provided for passenger vehicles accessing the driveways at the ends of streets to allow for typical vehicular movements. Note – The Applicant has sought this waiver from County Transportation and County Transportation has recommended it for Board of County Supervisors action.

**24.Modification** of Section 610.06.B of the DCSM to allow for parking to be provided off-site or greater than 150 feet of each dwelling unit measured over sidewalks or other paved surfaces.

**Justification:** As designed, the Town Center is designed to have shared parking between the commercial and non-commercial uses. For this proposal, the Applicant is providing more parking than required and is not taking credit for the parallel parking spaces on Harbor Side Street. However, some of the parking spaces will be more than 150 ft. of some of the dwelling units. Note – The Applicant has sought this waiver from County Transportation and County Transportation has recommended it for Board of County Supervisors action.

**25.Modification** of DCSM Details 650.06 to allow street sections in accordance with the street sections shown on the MZP.

**Justification:** This development is within a community with previously constructed residential and commercial areas with modified street sections. In order to maintain continuity in the redevelopment areas these modified, narrow street sections shall continue to be permitted. Furthermore, more narrow street sections promote low impact design strategies such as reducing impervious surface

areas, increasing green space and vegetative areas, and reducing runoff. Note – The Applicant has sought this waiver from County Transportation and County Transportation has recommended it for Board of County Supervisors action.

**26.Waiver** of Section 32-250.31 of the Zoning Ordinance and 802.47 of the DCSM requirement for a 15-foot-wide landscape area around community recreation sites.

**Justification:** This waiver provides flexibility to integrate community recreation sites / facilities into the planned mixed development design, rather than separate out such uses. Community recreation sites / facilities should be incorporated into the overall development.

### **Agency Comments**

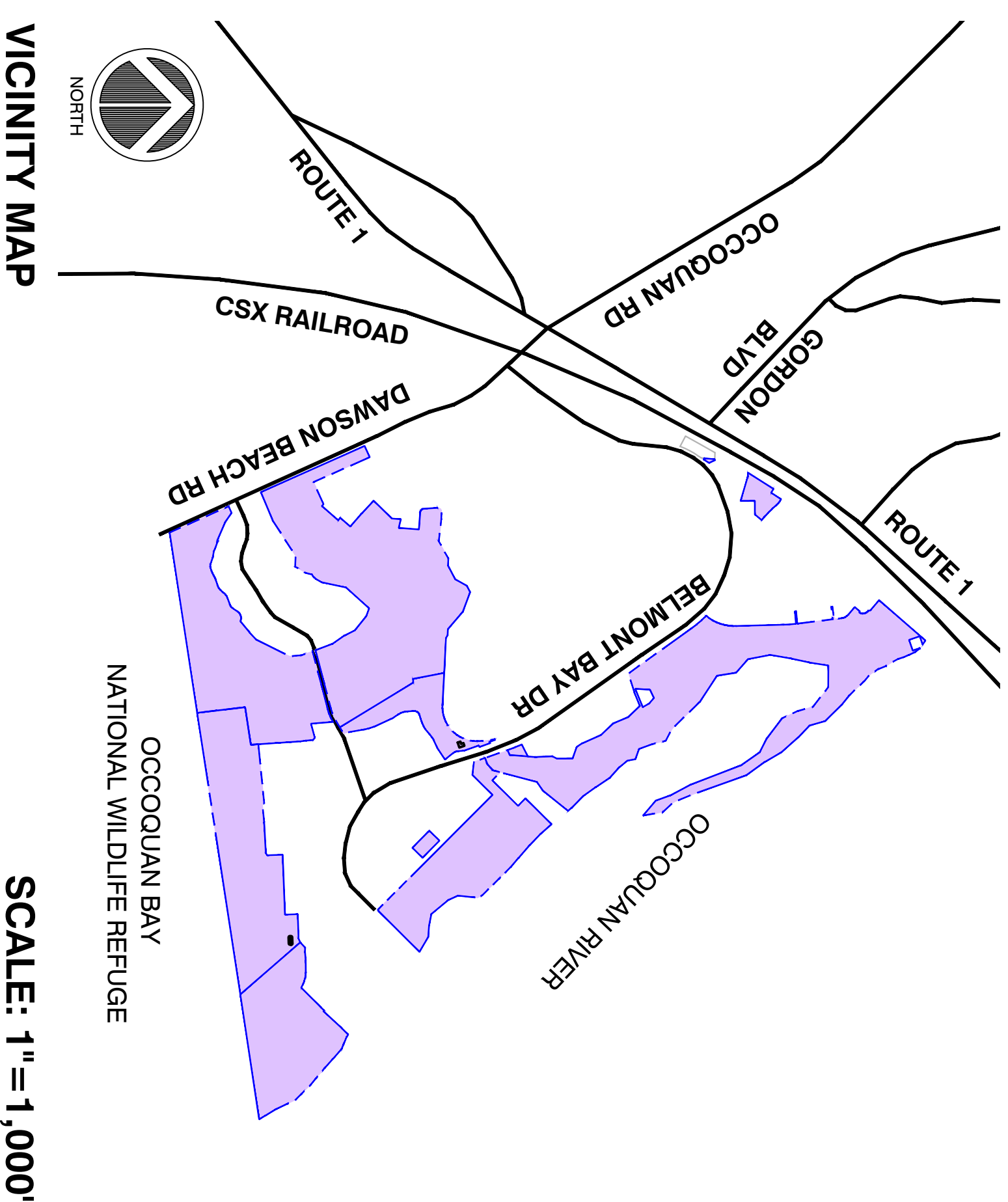
The following agencies have reviewed the proposal, and their comments have been summarized in relevant comprehensive plan chapters of this report. Individual comments are in the case file in the Planning Office:

- PWC Planning Office - Archaeologist
- PWC Building Official
- PWC Economic Development
- PWC Fire Marshal Office
- PWC Historical Commission
- PWC Housing & Community Development
- PWC Development Services, Land Development Case Manager
- PWC Library Systems
- PWC Parks and Recreation Department
- PWC Planning Office – Land Use Review Case Manager
- PWC Planning Office – Countywide Planning
- PWC Planning Office – Community Planning & Revitalization
- PWC Police / Crime Prevention
- PWC Public Works – Environmental Services / Watershed Management
- PWC Service Authority/Prince William Water
- PWC School Board
- PWC Transportation Department
- Virginia Department of Transportation (VDOT)
- PWC Development Services – Zoning Administration

# BELMONT BAY

## PROFFER AMENDMENT

### REZZ2024-00040



SCALE: 1" = 1,000'

#### NOTES

1. SITE OWNERS: BELMONT BAY LLC, BLOCKS B&C LLC, BELMONT TOWN CENTER ASSOCIATES LLC, & BLOCKS J&K LLC, PRESTON C. CARLITHERS, TRUSTEE
2. APPLICANT: MILLER AND SMITH HOMES, INC.
3. GPIN #s - 8492-44-0370, 8492-53-7716, 8492-44-1022, 8492-44-5722, 8492-43-5597, 8492-34-3696
4. & PORTIONS OF 8492-32-5196, 8492-32-4965, 8492-43-4228 SITE AREA +/- 159.76 ACRES
5. EXISTING ZONING: PLANNED MIXED USE DISTRICT (PMD)
6. PROPOSED ZONING: PLANNED MIXED USE DISTRICT (PMD)
7. LANDBAY AREAS ARE PROVIDED IN GROSS ACRES AND MAY INCLUDE FLOODPLAIN, RPA, EASEMENTS, POSSIBLE RIGHT-OF-WAY DEDICATION AREA, ETC.
8. BACKGROUND INFORMATION AND DATA COURTESY OF PRINCE WILLIAM COUNTY GIS DATA 2020

#### SHEET INDEX

1. COVER SHEET
2. DEVELOPMENT CODE
3. LAND USE PLAN
4. BELMONT LANDING ILLUSTRATIVE
5. TAYLOR POINT ILLUSTRATIVE
6. MARINA OVERLOOK ILLUSTRATIVE
7. OVERALL ILLUSTRATIVE
8. OPEN SPACE PLAN
9. LANDSCAPE & BUFFER PLAN
10. TRANSPORTATION & UTILITIES PLAN
11. ROAD DESIGNATIONS
12. STREET SECTIONS

## BELMONT BAY

### COVER SHEET

DATE: MARCH 15, 2024      PROFFER AMENDMENT REZ# 2024-00040  
REVISED: OCTOBER 31, 2025

WOODBRIDGE MAGISTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

Dewberry  
5375 HERTHOTE BLVD.  
GAINESVILLE, VA 20155-6893  
PHONE: 703.468.2211  
FAX: 703.468.2272

SHEET 1 OF 12

# DEVELOPMENT CODE

PROPOSED LAND BAY DESIGNATION	FORMER LAND BAY DESIGNATION (REZ #99-0022)	LAND BAY SIZE (ACRES)	ZONING DESIGNATIONS	PROJECTED LAND USE <sup>1</sup>	RESIDENTIAL DENSITY BY USE		RESIDENTIAL DENSITY BY LAND BAY		MAXIMUM NON-RESIDENTIAL (Gross Floor Area or Slips)	YARDS (MINIMUM) FRONT LOAD TOWNHOME			YARDS (MINIMUM) REAR LOAD TOWNHOME			YARDS (MAXIMUM)			LOT WIDTH		LOT DEPTH		BUILDING HEIGHT		LOT COVERAGE <sup>2</sup> MAX	PARKING REQUIREMENTS MIN. OFF-STREET	BUFFER YARD	
					MIN DU/acre	MAX DU/acre	MAX DU/acre	MAX DU/acre		FRONT	SIDE	REAR	FRONT	SIDE	REAR	FRONT	SIDE	REAR	MIN	MAX	MIN	MAX	MIN	MAX				
MARINA OVERLOOK	PART LAND BAY 6 PART LAND BAY 14	16.22	B-1	RESTAURANTS, RETAIL, MARINA, BOAT SALES, PARKING, MIXED USE, COMMUNITY RECREATION AND/OR CIVIC FACILITIES PUBLIC USES AND FACILITIES	N/A	N/A	6	74	20,200 SF <sup>1</sup> - 163 SLIPS + 37 LET SKI TENDERS	18'	4'	6'	8'	4'	4'	30'	N/A	N/A	25'	20'	20'	35'	50'	20'	50'	N/A	PER DCSM	0'
BELMONT LANDING	PART LAND BAY 17 PART LAND BAY 18	21.89	HDR	SINGLE-FAMILY ATTACHED ACTIVE ADULT	6	16	16	178	N/A	18'	4'	11'	8'	4'	18'	30'	20'	25'	20'	20'	35'	50'	20'	50'	N/A	PER DCSM	SEE OPEN SPACE & BUFFER PLAN	
TAYLOR POINT	PART LAND BAY 7	15.23	HDR	SINGLE-FAMILY ATTACHED ACTIVE ADULT	6	16	16	96	N/A	18'	4'	6'	7'	4'	4'	30'	20'	25'	20'	35'	50'	20'	50'	N/A	PER DCSM	SEE OPEN SPACE & BUFFER PLAN		
OPEN SPACE 1	LAND BAY 2 LAND BAY 20 LAND BAY 21	37.17	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
OPEN SPACE 2	PART LAND BAY 12 PART LAND BAY 15	5.44	OS	OPEN SPACE, PUBLIC USES, COMMUNITY RECREATION, AND/OR CIVIC FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
OPEN SPACE 3	PART LAND BAY 13 PART LAND BAY 15 LAND BAY 16	38.29	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
OPEN SPACE 4	PART LAND BAY 17	24.32	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
OPEN SPACE 5	LAND BAY 2 LAND BAY 3	1.35	OS	OPEN SPACE, PUBLIC USES, COMMUNITY RECREATION, AND/OR CIVIC FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
OPEN SPACE 6	LAND BAY 22	0.05	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSM	0'		
APPLICATION PROPERTY TOTAL ACRES										348										MAXIMUM DWELLING UNITS FOR THE APPLICATION PROPERTY		MAXIMUM NON-RESIDENTIAL GFA FOR THE APPLICATION PROPERTY		163		MAXIMUM MARINA SLIPS		

## TYPICAL LOT DIAGRAMS

### SINGLE FAMILY ATTACHED LOT DETAIL - FRONT LOADED

- DEVELOPMENT STANDARDS
- MIN LOT WIDTH: 20 FT
  - MAX BUILDING HEIGHT: 50 FT
  - MIN SETBACKS
    - FRONT: 18 FT
    - SIDE: 4 FT
    - REAR: 6 FT

\*ALL UNITS WILL HAVE A GARAGE

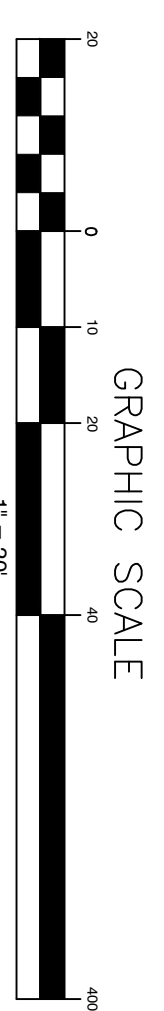
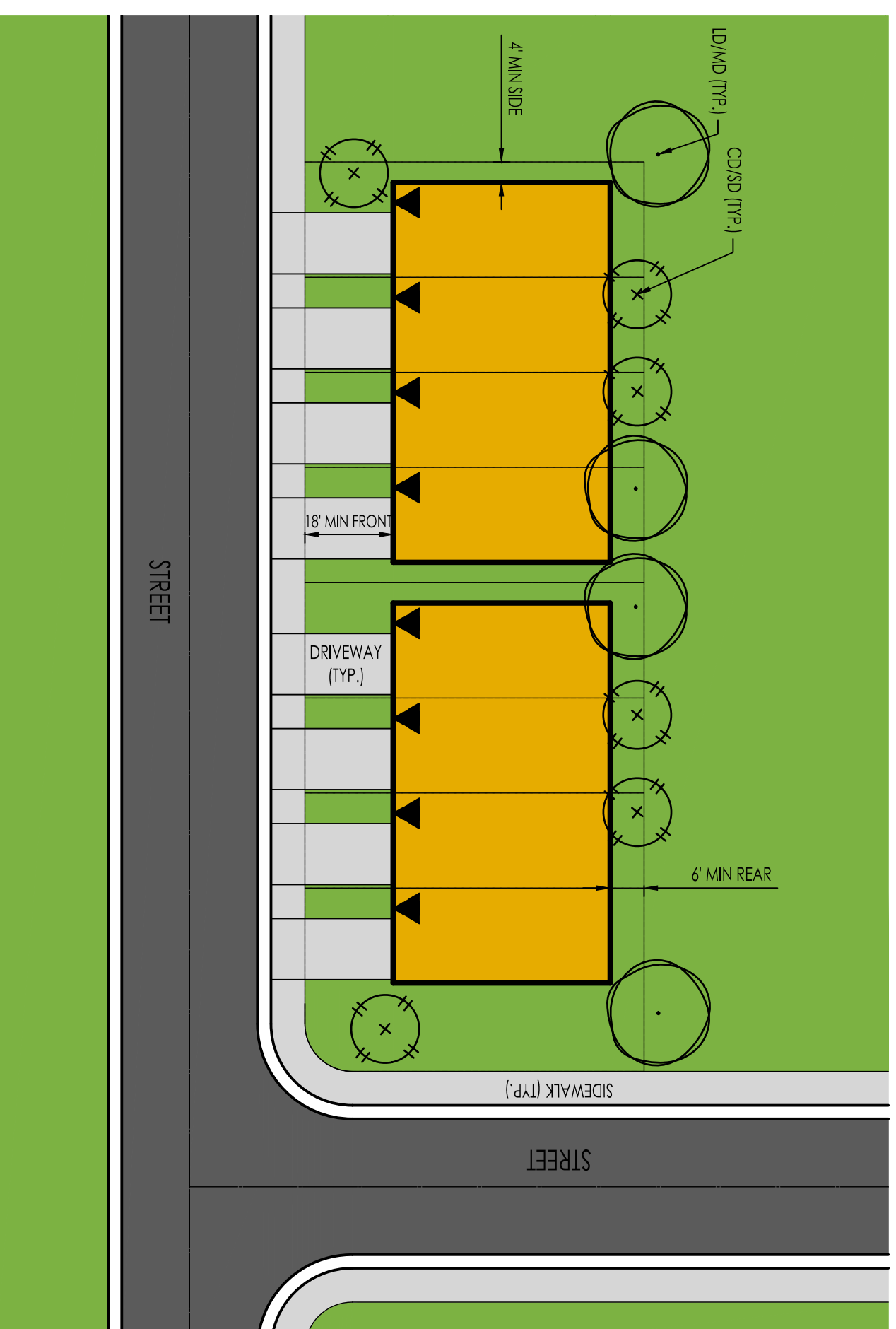
#### LANDSCAPING:

A MINIMUM OF ONE (1) LARGE DECIDUOUS (LD) OR ONE (1) MEDIUM DECIDUOUS (MD) TREE PER END UNIT AND ONE (1) COMPACT OR SMALL DECIDUOUS TREE (CD/SD) PER UNIT.

FINAL LOCATION OF ALL PLANT MATERIAL TO BE DETERMINED AT TIME OF FINAL SITE PLAN.

IF SPACE DOES NOT PERMIT, THE LD AND MD REQUIRED PER END UNIT MAY BE LOCATED IN COMMON OPEN SPACE FOR THE SITE. EXISTING SHADE TREES EXCEEDING TWO-INCH CALIPER, LOCATED ANYWHERE IN THE OPEN SPACE AREA, MAY BE COUNTED ON A ONE-TO-ONE BASIS TOWARDS FULFILLING THE REQUIREMENT FOR LARGE AND MEDIUM DECIDUOUS TREES ON THAT SITE.

NOTE: PARKING WILL BE PROVIDED IN ACCORDANCE WITH DCSM TABLE 6-8



### SINGLE FAMILY ATTACHED LOT DETAIL - REAR LOADED

- DEVELOPMENT STANDARDS
- MIN LOT WIDTH: 20 FT
  - MAX BUILDING HEIGHT: 50 FT
  - MIN SETBACKS
    - FRONT: 7 FT
    - SIDE: 4 FT
    - REAR: 4 FT

\*ALL UNITS WILL HAVE A GARAGE

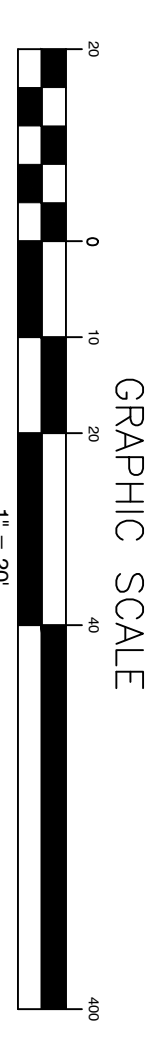
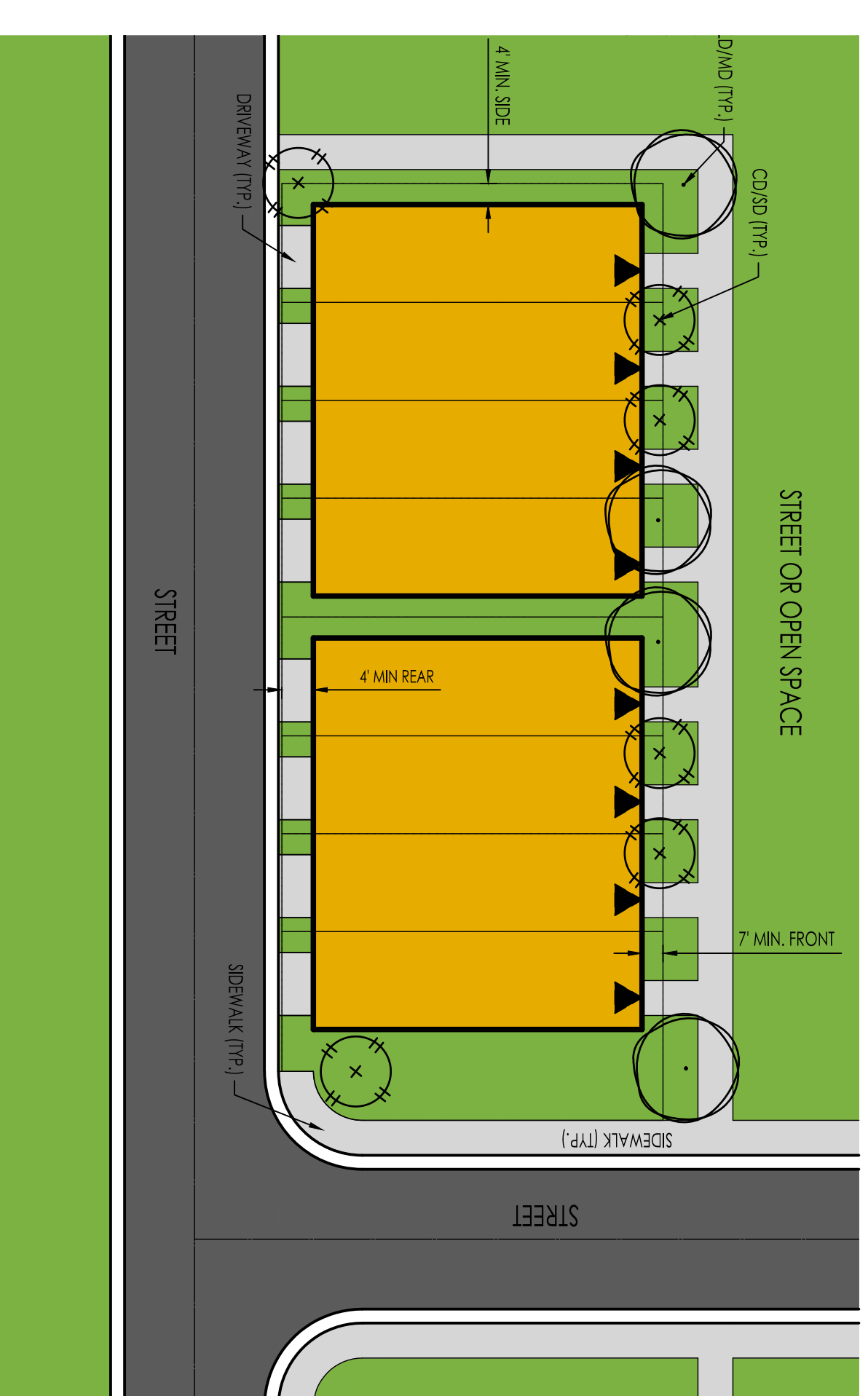
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NOTE: PARKING WILL BE PROVIDED IN ACCORDANCE WITH DCSM TABLE 6-8



## BELMONT BAY

### DEVELOPMENT STANDARDS & TYPICAL LOT DIAGRAMS

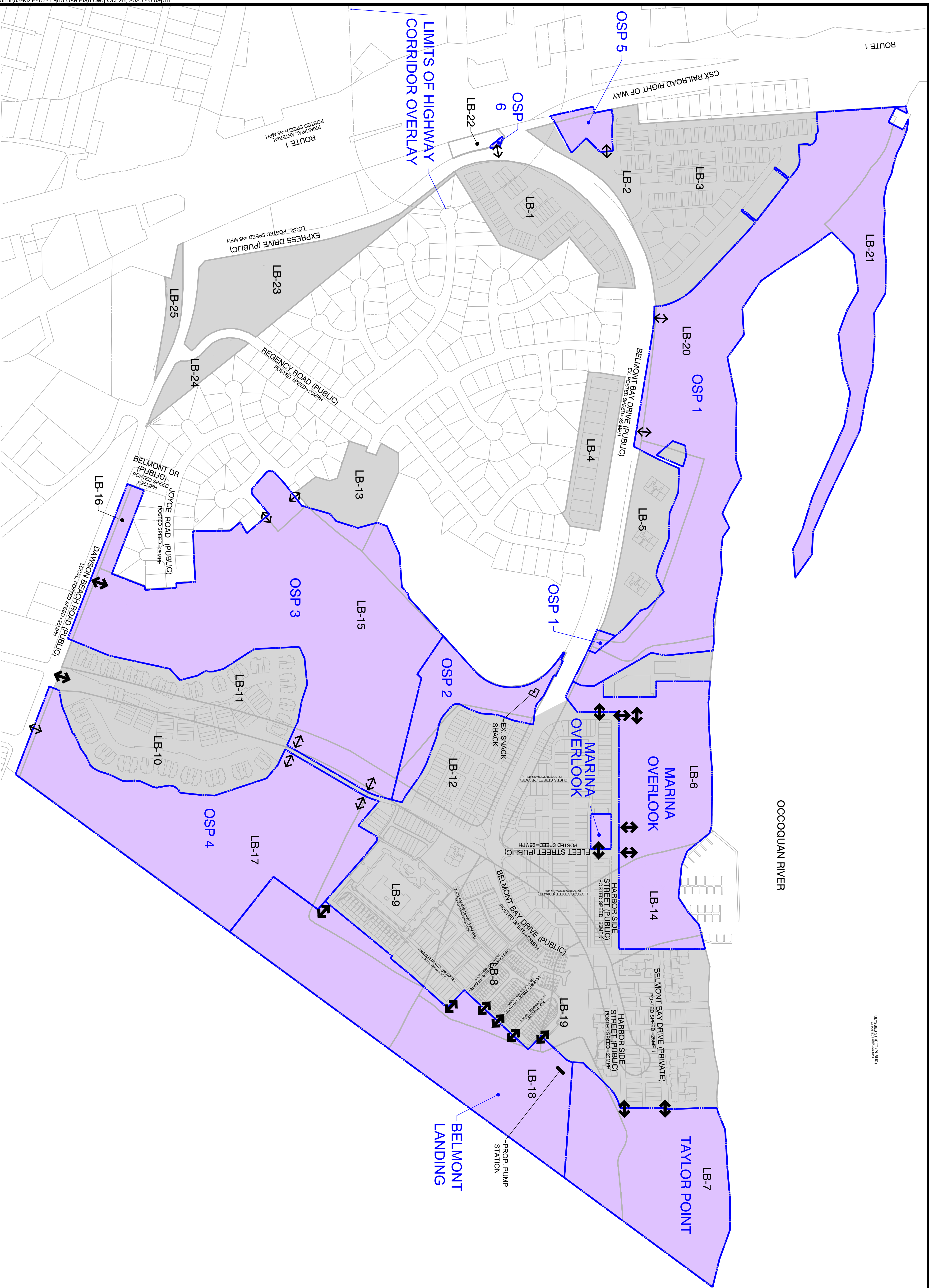
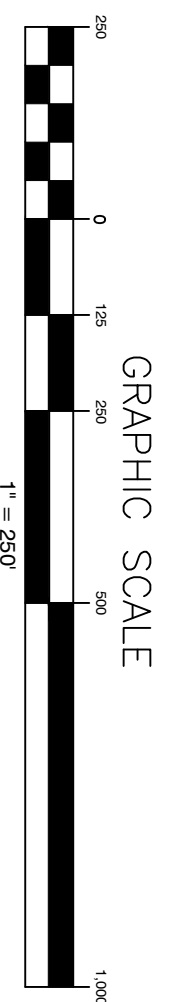
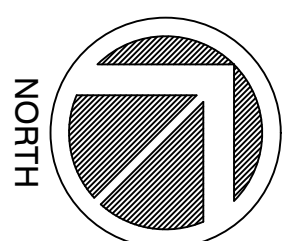
WOODBRIDGE MAGISTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

DATE: MARCH 15, 2024  
REVISED: OCTOBER 31, 2025

PROFFER AMENDMENT REZ# 2024-00040

SHEET 2 OF 12

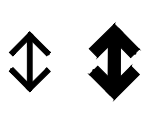




OCCOQUAN RIVER

UNIVERSITY STREET (PUBLIC)  
POSTED SPEED - 25MPH

**LEGEND:**  
 PROJECT BOUNDARY/REVISED LANDRAVS  
 LANDRAY BOUNDARIES  
 ENTRANCE LOCATION\*  
 MAINTENANCE ACCESS LOCATION\*  
 AREA SUBJECT TO PROFFER CONDITION AMENDMENT (PCA)  
 AREA SUBJECT TO APPROVED PROFFERS PER REZ 99-00022  
 AREA NOT INCLUDED WITHIN BELMONT BAY REZONING

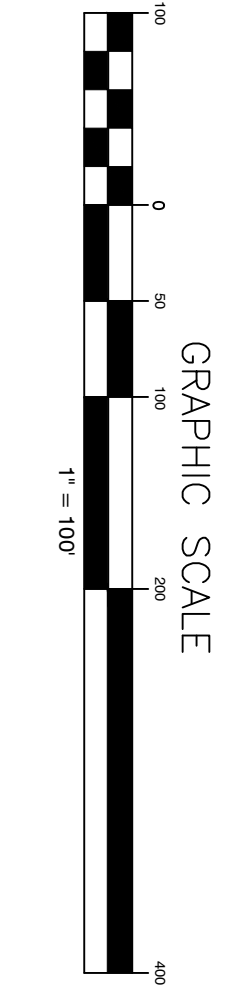
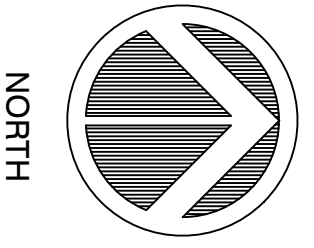


\* APPROXIMATE LOCATION, SUBJECT TO CHANGE WITH FINAL ENGINEERING.

# BELMONT BAY LAND USE PLAN

DATE: MARCH 15, 2024    PROFFER AMENDMENT REZ# 2024-00040  
 REVISED: OCTOBER 31, 2025  
 SCALE: 1"=250'  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 3 OF 12





**LEGEND:**

- VEHICLES PER DAY
- PROPOSED SIDEWALK
- PROPOSED TRAIL
- EXISTING TRAIL

# BELMONT BAY

## BELMONT LANDING ILLUSTRATIVE

DATE: MARCH 15, 2024

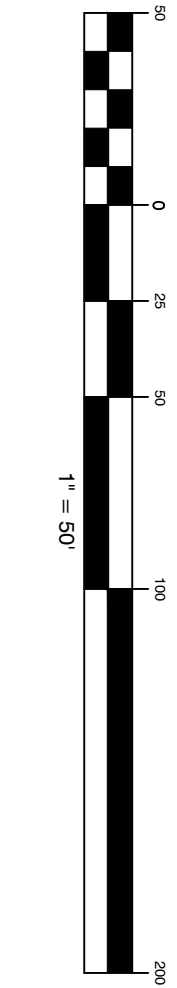
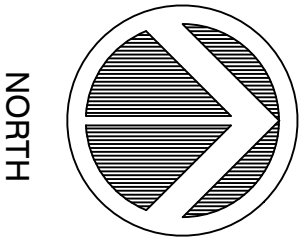
PROFFER AMENDMENT REZ# 2024-00040

REVISED: OCTOBER 31, 2025

WOODBRIDGE MASTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 4 OF 12





GRAPHIC SCALE



**LEGEND:**

- VEHICLES PER DAY
- PROPOSED SIDEWALK
- PROPOSED TRAIL
- EXISTING TRAIL
- HERITAGE TRAIL

# BELMONT BAY

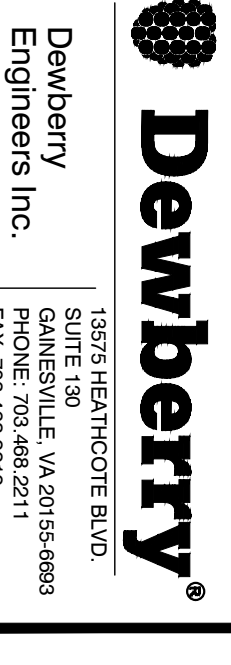
## TAYLOR POINT ILLUSTRATIVE

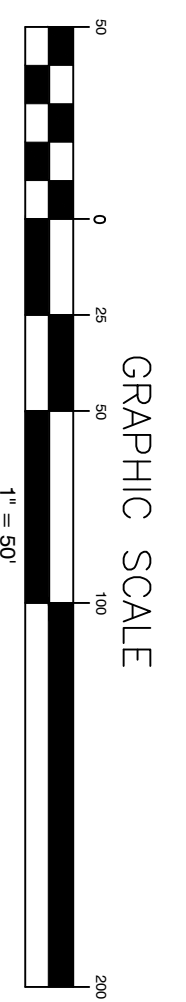
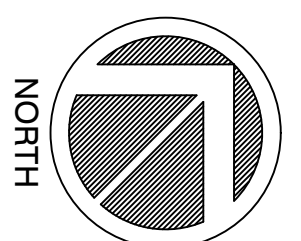
DATE: MARCH 15, 2024      PROFFER AMENDMENT REZ# 2024-00040

REVISED: OCTOBER 31, 2025

WOODBRIDGE MASTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 5 OF 12





**LEGEND:**

- RESIDENTIAL PARKING SPACE •
- VEHICLES PER DAY <img alt="Symbol for vehicles per day: a rectangle with 'xxx' inside." data-bbox="95 635 115 645"/>
- PROPOSED SIDEWALK <img alt="Symbol for proposed sidewalk: a pink line." data-bbox="85 635 95 645"/>
- PROPOSED TRAIL <img alt="Symbol for proposed trail: a yellow line." data-bbox="75 635 85 645"/>
- EXISTING SIDEWALK <img alt="Symbol for existing sidewalk: an orange line." data-bbox="65 635 75 645"/>
- EXISTING TRAIL <img alt="Symbol for existing trail: a red line." data-bbox="55 635 65 645"/>
- HERITAGE TRAIL <img alt="Symbol for heritage trail: a dashed grey line." data-bbox="45 635 55 645"/>

# BELMONT BAY

## MARINA OVERLOOK ILLUSTRATIVE

DATE: MARCH 15, 2024      PROFFER AMENDMENT REZ# 2024-00040

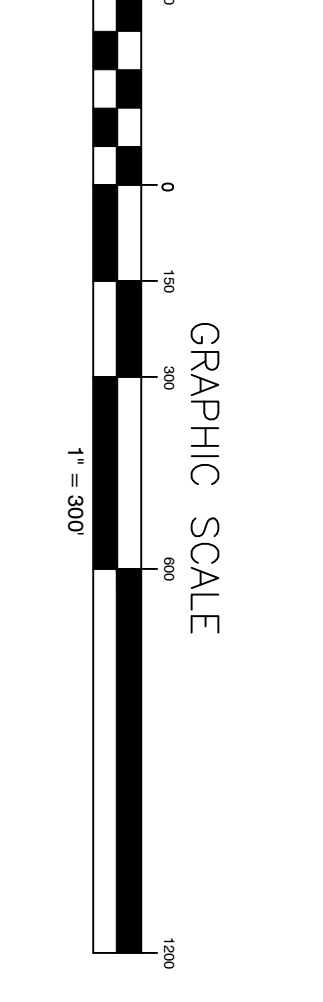
REVISED: OCTOBER 31, 2025

WOODBRIDGE MASTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 6 OF 12



Dewberry  
Engineers, Inc.  
5375 HERNDOTE BLVD.  
GAINESVILLE, VA 20155-6883  
PHONE: 703.468.2211  
FAX: 703.468.2212



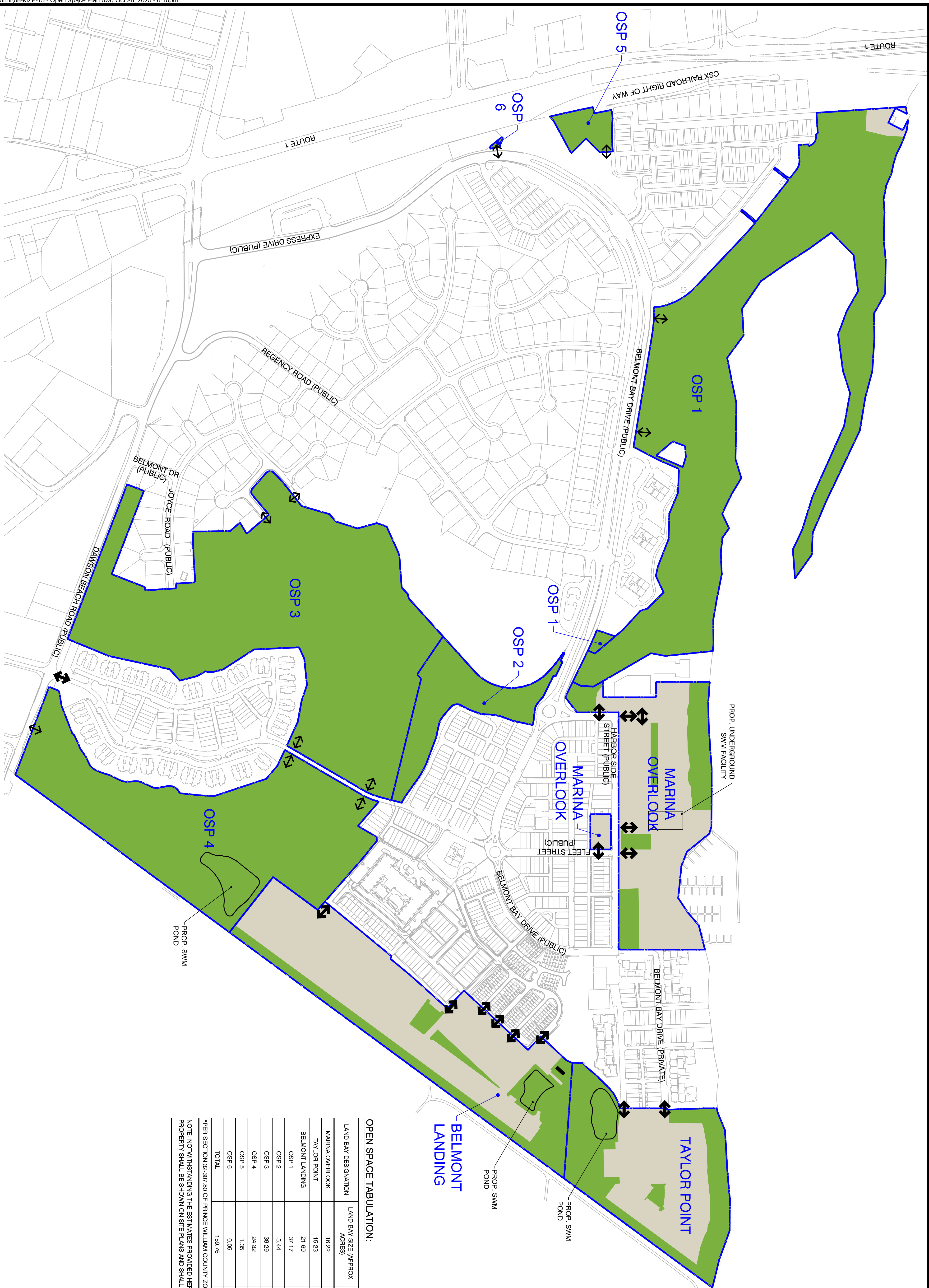
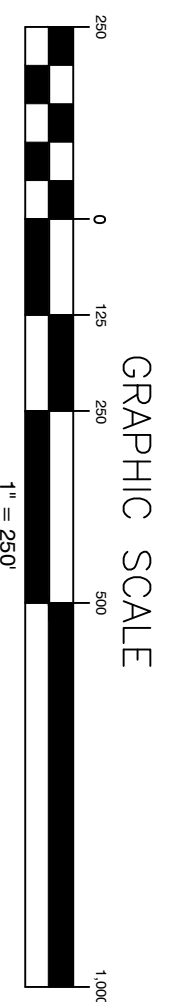
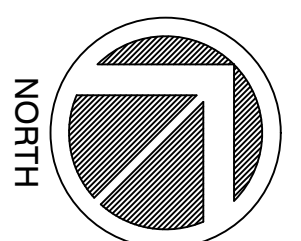
**LEGEND:**

- PROPOSED TRAIL
- EXISTING TRAIL
- PWC HERITAGE TRAIL

# BELMONT BAY OVERALL ILLUSTRATIVE

DATE: MARCH 15, 2024    PROFFER AMENDMENT REZ# 2024-00040  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 7 OF 12





- LEGEND:**
- PROJECT BOUNDARY/REVISED LANDRAVS
  - ENTRANCE LOCATION\*
  - MAINTENANCE ACCESS LOCATION\*
  - OPEN SPACE AREAS
  - NON-OPEN SPACE AREAS

- NOTES:**
1. OPEN SPACE SHOWN HEREON IS APPROXIMATE AND SUBJECT TO MODIFICATION AT TIME OF FINAL SITE PLAN SO LONG AS DEVELOPMENT STANDARDS AND DCSM REQUIREMENTS ARE MET.
  2. GENERAL OPEN SPACE AND NATURAL OPEN SPACE AREAS SHOWN HEREON DO NOT PROHIBIT THE CONSTRUCTION, INSTALLATION OR PRESENCE OF UTILITIES, SIDEWALKS, STORMWATER MANAGEMENT FACILITIES, SIDEWALKS, AND SHARED USE PATHS.

**OPEN SPACE TABULATION:**

LAND BAY DESIGNATION	LAND BAY SIZE (APPROX. ACRES)	OPEN SPACE REQUIRED (ACRES)	OPEN SPACE REQUIRED (%)	OPEN SPACE PROVIDED (ACRES)	OPEN SPACE PROVIDED (%)
MARINA OVERLOOK	16.22	N/A	N/A	2.51	15.47%
TAYLOR POINT	15.23	N/A	N/A	7.48	49.11%
BELMONT LANDING	21.69	N/A	N/A	5.90	27.20%
OSP 1	37.17	N/A	N/A	36.54	98.31%
OSP 2	5.44	N/A	N/A	5.44	100.00%
OSP 3	38.29	N/A	N/A	38.29	100.00%
OSP 4	24.32	N/A	N/A	24.32	100.00%
OSP 5	1.35	N/A	N/A	1.35	100.00%
OSP 6	0.05	N/A	N/A	0.05	100.00%
<b>TOTAL</b>	<b>159.76</b>	<b>20.00%</b>	<b>31.95</b>	<b>121.89</b>	<b>76.29%</b>

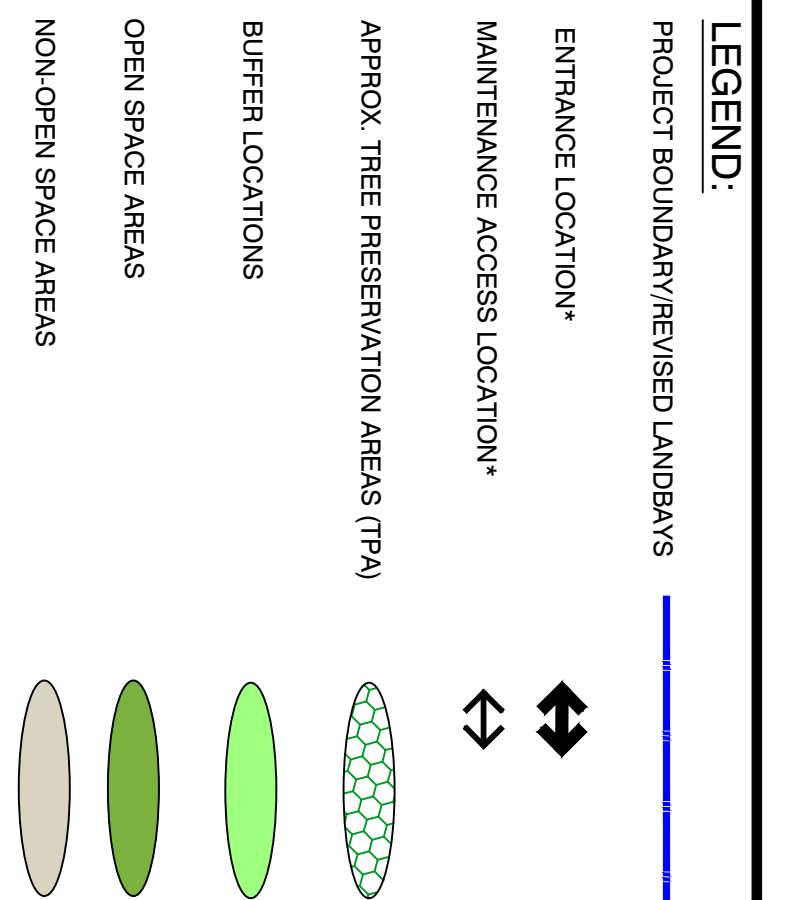
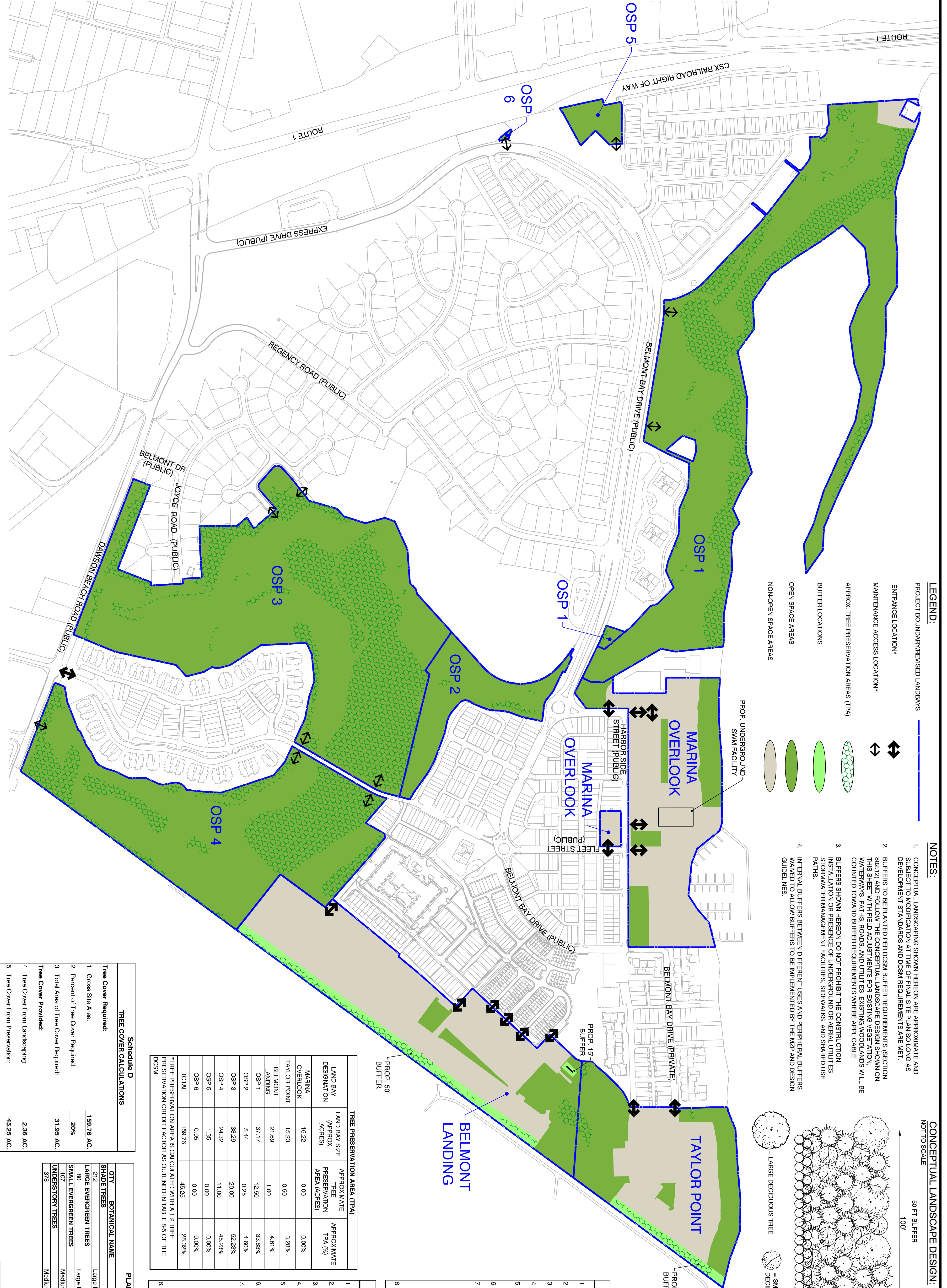
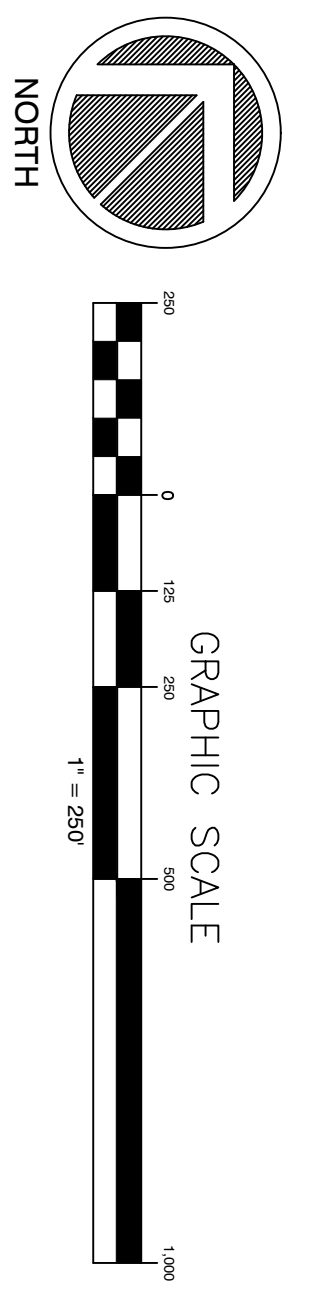
\*PER SECTION 32-307.80 OF PRINCE WILLIAM COUNTY ZONING ORDINANCE

NOTE: NOTWITHSTANDING THE ESTIMATES PROVIDED HEREON, THE FINAL LOCATIONS AND ACREAGE OF OPEN SPACE TO BE PROVIDED ON THE PROPERTY SHALL BE SHOWN ON SITE PLANS AND SHALL BE A MINIMUM OF 31.95 ACRES (20%).

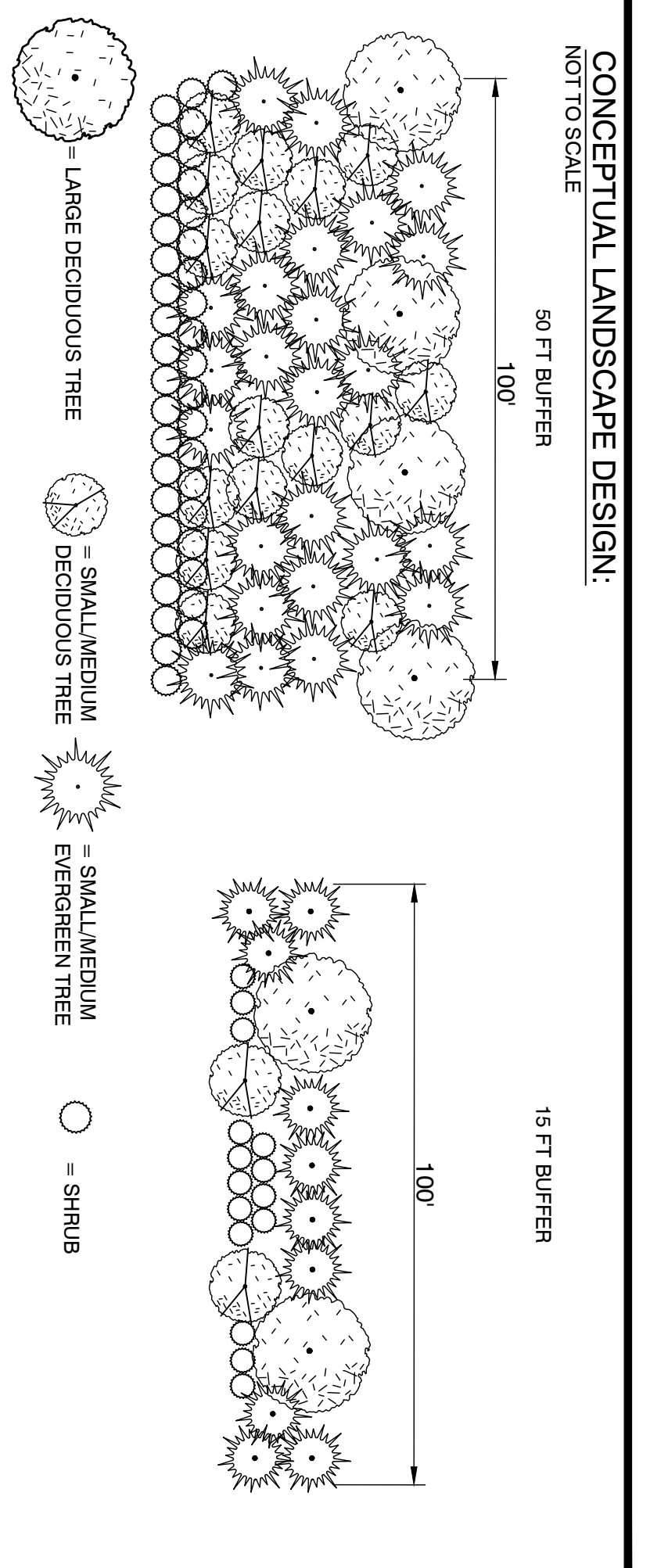
# BELMONT BAY OPEN SPACE PLAN

DATE: MARCH 15, 2024      PROFFER AMENDMENT REZ# 2024-00040  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 8 OF 12





- LEGEND:**
- PROJECT BOUNDARY/REVISED LANDBAYS
  - ENTRANCE LOCATION\*
  - MAINTENANCE ACCESS LOCATION\*
  - APPROX. TREE PRESERVATION AREAS (TPA)
  - BUFFER LOCATIONS
  - OPEN SPACE AREAS
  - NON-OPEN SPACE AREAS



- NOTES:**
- CONCEPTUAL LANDSCAPING SHOWN HEREON ARE APPROXIMATE AND SUBJECT TO MODIFICATION AT TIME OF FINAL SITE PLAN SO LONG AS DEVELOPMENT STANDARDS AND DCSM REQUIREMENTS ARE MET.
  - BUFFERS TO BE PLANTED PER DCSM BUFFER REQUIREMENTS SECTION 802.12) AND FOLLOW THE CONCEPTUAL LANDSCAPE DESIGN SHOWN ON THIS SHEET WITH FIELD ADJUSTMENTS FOR EXISTING VEGETATION, WATERWAYS, PATHS, ROADS, AND UTILITIES. EXISTING WOODLANDS WILL BE COUNTED TOWARD BUFFER REQUIREMENTS WHERE APPLICABLE.
  - BUFFERS SHOWN HEREON DO NOT PROHIBIT THE CONSTRUCTION, INSTALLATION OR PRESENCE OF UNDERGROUND OR AERIAL UTILITIES, PATHS, SWAMP/MANAGEMENT FACILITIES, SIDEWALKS, AND SPAVED USE PATHS.
  - INTERNAL BUFFERS BETWEEN DIFFERENT USES AND PERIPHERAL BUFFERS WAIVED TO ALLOW BUFFERS TO BE IMPLEMENTED BY THE MAP AND DESIGN GUIDELINES.

**Schedule F**  
RESIDENTIAL REQUIREMENTS

Development Type:	SFA
1. Development Type:	348
2. Number of Lots:	
3. Number of trees required per lot:	1/END UNIT = 1200 Trees (SD/CO Trees) 1/INTERIOR UNIT = 400 Trees (AT Trees)
4. Total number of trees provided:	110 (LD/MD Trees) 238 (SD/CO Trees) 0 (AT Trees)

**Schedule A**  
50' LANDSCAPE BUFFER

Minimum required buffer area (A, B, or C)	C
1. Minimum required buffer area (A, B, or C)	50 FT.
2. Minimum width of landscaped yard:	2075 FT.
3. Linear feet of buffer strip required along property line and/or right-of-way:	50 %
4. Percentage of required buffer area occupied by existing woodland	
5. Fence or wall or berm employed in buffer area: Yes XX No	
6. Total number of plant units required in buffer strip:	3320 p.u.
7. Number of large deciduous trees provided: Number of large evergreen trees: Number of evergreen trees understorey (medium, small or compact): Number of deciduous understorey trees (medium, small or compact): Number of shrubs: Number of ornamental grasses: Number of perennials:	102 x 10 p.u. = 1020 p.u. 80 x 10 p.u. = 800 p.u. 80 x 5 p.u. = 400 p.u. 140 x 5 p.u. = 700 p.u. 200 x 2 p.u. = 400 p.u. 0 x 1 p.u. = 0 p.u. 0 x 0.25 p.u. = 0 p.u.
8. Total number of plant units provided in buffer strip:	3320 p.u.

**Schedule A**  
15' LANDSCAPE BUFFER

Minimum required buffer area (A, B, or C)	A
1. Minimum required buffer area (A, B, or C)	15 FT.
2. Minimum width of landscaped yard:	120 FT.
3. Linear feet of buffer strip required along property line and/or right-of-way:	0 %
4. Percentage of required buffer area occupied by existing woodland	
5. Fence or wall or berm employed in buffer area: Yes XX No	
6. Total number of plant units required in buffer strip:	132 p.u.
7. Number of large deciduous trees provided: Number of large evergreen trees: Number of evergreen trees understorey (medium, small or compact): Number of deciduous understorey trees (medium, small or compact): Number of shrubs: Number of ornamental grasses: Number of perennials:	0 x 10 p.u. = 0 p.u. 0 x 10 p.u. = 0 p.u. 27 x 5 p.u. = 135 p.u. 0 x 5 p.u. = 0 p.u. 0 x 1 p.u. = 0 p.u. 0 x 0.25 p.u. = 0 p.u.
8. Total number of plant units provided in buffer strip:	135 p.u.

**Schedule D**  
TREE COVER CALCULATIONS

Tree Cover Required:	159.76 AC.
1. Gross Site Area:	159.76 AC.
2. Percent of Tree Cover Required:	20%
3. Total Area of Tree Cover Required:	31.95 AC.
<b>Tree Cover Provided:</b>	
4. Tree Cover From Landscaping:	2.36 AC.
5. Tree Cover From Preservation:	45.25 AC.
6. Total Tree Cover Provided:	29.80% 47.61 AC.

**TPA PRESERVATION AREA (TPA)**

LAND BAY DESIGNATION	LAND BAY SIZE (APPROX. ACRES)	APPROXIMATE TREE PRESERVATION AREA (ACRES)	APPROXIMATE TPA (%)
MARINA OVERLOOK	16.22	0.00	0.00%
TAYLOR POINT	15.23	0.50	3.28%
BELMONT LANDING	21.89	1.00	4.61%
OSP 1	37.17	12.50	33.63%
OSP 2	5.44	0.25	4.60%
OSP 3	38.29	20.00	52.23%
OSP 4	24.32	11.00	45.23%
OSP 5	1.35	0.00	0.00%
OSP 6	0.05	0.00	0.00%
<b>TOTAL</b>	159.76	45.25	28.32%

\*TREE PRESERVATION AREA IS CALCULATED WITH A 1:2 TREE PRESERVATION CREDIT FACTOR AS OUTLINED IN TABLE 8-5 OF THE DCSM

**PLANT LIST-SCHEDULE H**

QTY	BOTANICAL NAME	REMARKS	SIZE	COVERAGE CALCULATIONS	TOTAL CREDIT
212	SHADE TREES	B&B	2" Cal	200 S.F.	42400 S.F.
80	LARGE EVERGREEN TREES	B&B	7-8" HT	150 S.F.	12000 S.F.
107	SMALL EVERGREEN TREES	B&B	7-8" HT	100 S.F.	10700 S.F.
376	UNDERSTOREY TREES	B&B	2" Cal	100 S.F.	37600 S.F.
<b>*TOTAL</b>				102,900.00 S.F.	2.36 AC.

**NOTE:** LANDSCAPE TABULATIONS SHOWN HEREON ARE PROVIDED TO DEMONSTRATE CONFORMANCE WITH DCSM SECTION 800 LANDSCAPE REQUIREMENTS ONLY. REQUIREMENTS AND QUANTITIES OF PROPOSED LANDSCAPE PLANTING ARE SUBJECT TO CHANGE AT THE TIME OF FINAL SITE PLAN SO LONG AS FINAL SITE PLANS ARE IN CONFORMANCE WITH DCSM SECTION 800.

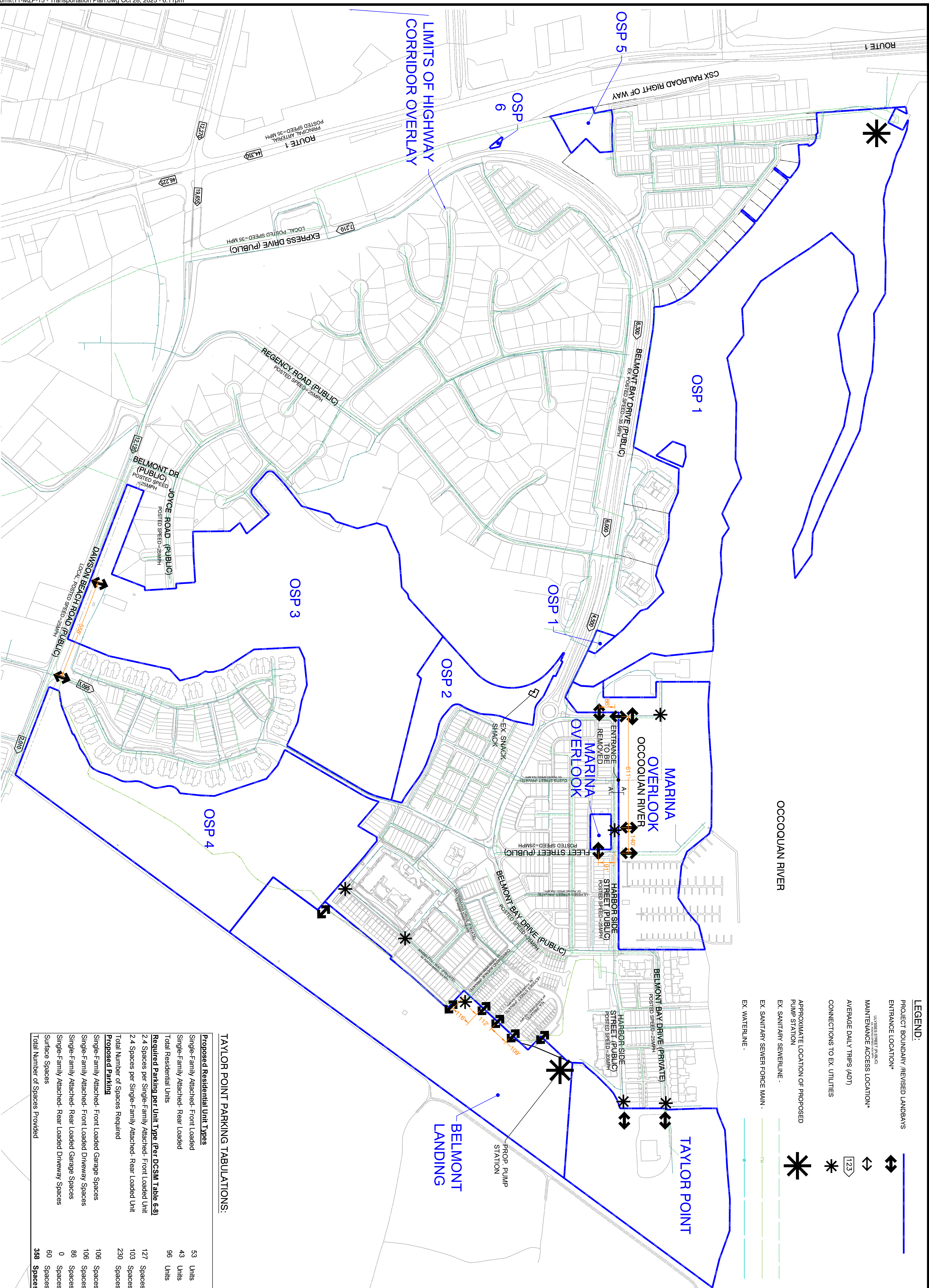
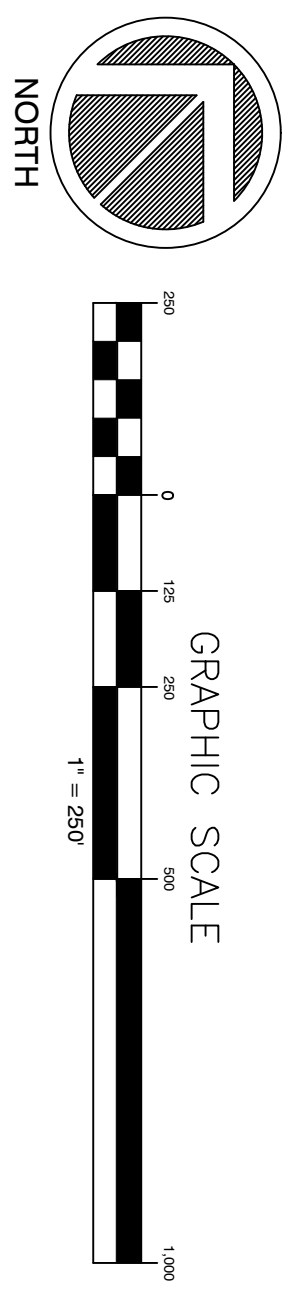
**DATE:** MARCH 15, 2024    **PROFFER AMENDMENT REZ#** 2024-00040  
**REVISED:** OCTOBER 31, 2025

**WOODBRIDGE MAGISTERIAL DISTRICT**  
**PRINCE WILLIAM COUNTY, VIRGINIA**

**LANDSCAPE & BUFFER PLAN**  
**SHEET 9 OF 12**

**Dewberry**  
 13375 HERSCHOLT BLVD  
 GAINESVILLE, VA 20155-6893  
 PHONE: 703.468.2211  
 FAX: 703.468.2232

MZP-19



- LEGEND:**
- PROJECT BOUNDARY / REVISED LANDBAYS
  - ENTRANCE LOCATION\*
  - MAINTENANCE ACCESS LOCATION\*
  - AVERAGE DAILY TRIPS (ADT)
  - CONNECTIONS TO EX UTILITIES
  - APPROXIMATE LOCATION OF PROPOSED PUMP STATION
  - EX SANITARY SEWERLINE
  - EX SANITARY SEWER FORCE MAIN
  - EX WATERLINE

- NOTES:**
1. POSSIBLE WATER AND SANITARY SEWER CONNECTIONS SHOWN HEREON ARE PROPOSED TO PROVIDE SERVICE TO THE PROJECT AREA.
  2. ALIGNMENT AND SIZE OF PROPOSED WATER AND SANITARY SEWER LINES WILL BE DETERMINED WITH FINAL ENGINEERING AND WILL BE BASED UPON CAPACITY NEEDED TO SERVE THE PROJECT.
  3. ALIGNMENT OF EXISTING WATER AND SANITARY SEWER LINES MAY BE REVISED WITH FINAL ENGINEERING.
  4. ALL DEVELOPMENT WITHIN THE PROJECT LIMITS REQUIRING PUBLIC WATER OR SANITARY SEWER SHALL BE SERVED WITH PUBLIC WATER AND SANITARY SEWER.
  5. PROPOSED UTILITY CONNECTION POINTS ARE PRELIMINARY, SUBJECT TO CHANGE WITH FINAL ENGINEERING, AND ARE SUBJECT TO APPROVAL BY THE PRINCE WILLIAM COUNTY SERVICE AUTHORITY.
  6. OFF-SITE UTILITY CONNECTIONS SHALL REQUIRE APPLICABLE EASEMENT ACQUISITION PRIOR TO FINAL SITE PLAN APPROVAL PROPOSING SUCH UTILITY.
  7. STORMWATER MANAGEMENT AND BMPs FOR MARINA OVERLOOK, TAYLOR POINT, AND BELMONT LANDING WILL BE PROVIDED IN ACCORDANCE WITH PRINCE WILLIAM COUNTY AND THE COMMONWEALTH OF VIRGINIA REQUIREMENTS OR AS APPROVED WITH FINAL SITE PLANS.
  8. ALL EXISTING STREETS WITHIN THE BELMONT BAY DEVELOPMENT AREA ARE ASSUMED TO BE CLASSIFIED AS LOCAL AND ALL PROPOSED STREETS WITHIN THE DEVELOPMENT ARE ANTICIPATED TO BE CLASSIFIED AS PRIVATE STREETS.
  9. THE APPLICANT SHALL HAVE THE RIGHT TO CONSTRUCT VEHICULAR ENTRANCES AND ACCESS POINTS IN THE LOCATIONS GENERALLY SHOWN ON THIS SHEET. THE APPLICANT RESERVES THE RIGHT TO ADD, SUBTRACT, MODIFY OR MOVE AN ENTRANCE OR ACCESS POINT AT THE TIME OF FINAL SITE PLAN.
  10. UNDERGROUND SWM/BWP SHALL BE ALLOWED FOR RESIDENTIAL DEVELOPMENTS.
  11. PRIVATE ENTRANCES ON PUBLIC ROADS WILL BE CONSIDERED COMMERCIAL ENTRANCES AND MUST COMPLY WITH COMMERCIAL ENTRANCE REQUIREMENTS INCLUDING CG-11 STANDARDS.

**MARINA OVERLOOK PARKING TABULATIONS:**

Proposed Residential Unit Types	Required Parking per Unit Type (Per DCSM Table 6-9)	Total Residential Units	Required Parking	Proposed Parking	Surface Spaces	Total Number of Spaces Provided
Single-Family Attached- Rear Loaded	2.4	178	427	178	0	178
Single-Family Attached- Front Loaded	2.4	148	355	148	0	148
Single-Family Attached- Rear Loaded Driveway Spaces	0	55	0	55	55	55
Single-Family Attached- Rear Loaded Driveway Spaces	0	55	0	55	55	55
<b>Total</b>		<b>436</b>	<b>782</b>	<b>436</b>	<b>110</b>	<b>546</b>

**TAYLOR POINT PARKING TABULATIONS:**

Proposed Residential Unit Types	Required Parking per Unit Type (Per DCSM Table 6-9)	Total Residential Units	Required Parking	Proposed Parking	Surface Spaces	Total Number of Spaces Provided
Single-Family Attached- Front Loaded	2.4	127	305	127	0	127
Single-Family Attached- Rear Loaded	2.4	103	247	103	0	103
Single-Family Attached- Rear Loaded Driveway Spaces	0	230	0	230	60	230
Single-Family Attached- Front Loaded Garage Spaces	0	106	0	106	60	106
Single-Family Attached- Front Loaded Driveway Spaces	0	86	0	86	0	86
Single-Family Attached- Rear Loaded Garage Spaces	0	0	0	0	0	0
Single-Family Attached- Rear Loaded Driveway Spaces	0	60	0	60	60	60
<b>Total</b>		<b>612</b>	<b>552</b>	<b>612</b>	<b>120</b>	<b>732</b>

**BELMONT LANDING PARKING TABULATIONS:**

Existing Commercial Parking	Required Commercial Parking	Proposed Commercial Parking	Surface Spaces	Total Number of Spaces Provided
78 Spaces	45 Spaces	78 Spaces	15 Spaces	114 Spaces

**TAYLOR POINT PARKING TABULATIONS:**

Proposed Residential Unit Types	Required Parking per Unit Type (Per DCSM Table 6-9)	Total Residential Units	Required Parking	Proposed Parking	Surface Spaces	Total Number of Spaces Provided
Single-Family Attached- Front Loaded	2.4	127	305	127	0	127
Single-Family Attached- Rear Loaded	2.4	103	247	103	0	103
Single-Family Attached- Rear Loaded Driveway Spaces	0	230	0	230	60	230
Single-Family Attached- Front Loaded Garage Spaces	0	106	0	106	60	106
Single-Family Attached- Front Loaded Driveway Spaces	0	86	0	86	0	86
Single-Family Attached- Rear Loaded Garage Spaces	0	0	0	0	0	0
Single-Family Attached- Rear Loaded Driveway Spaces	0	60	0	60	60	60
<b>Total</b>		<b>612</b>	<b>552</b>	<b>612</b>	<b>120</b>	<b>732</b>

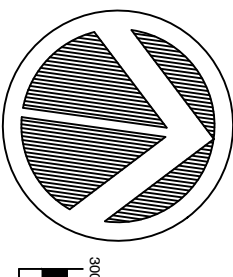
**BELMONT LANDING PARKING TABULATIONS:**

Proposed Residential Unit Types	Required Parking per Unit Type (Per DCSM Table 6-9)	Total Residential Units	Required Parking	Proposed Parking	Surface Spaces	Total Number of Spaces Provided
Single-Family Attached- Front Loaded	2.4	99	238	99	0	99
Single-Family Attached- Rear Loaded	2.4	83	199	83	0	83
Single-Family Attached- Rear Loaded Driveway Spaces	0	178	0	178	166	178
Single-Family Attached- Front Loaded Garage Spaces	0	190	0	190	190	190
Single-Family Attached- Front Loaded Driveway Spaces	0	166	0	166	0	166
Single-Family Attached- Rear Loaded Garage Spaces	0	166	0	166	0	166
Single-Family Attached- Rear Loaded Driveway Spaces	0	130	0	130	130	130
<b>Total</b>		<b>726</b>	<b>437</b>	<b>726</b>	<b>386</b>	<b>1112</b>

# BELMONT BAY TRANSPORTATION & UTILITIES PLAN

DATE: MARCH 15, 2024  
 REVISED: OCTOBER 31, 2025  
 SCALE: 1"=250'  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 100F-12





NORTH



GRAPHIC SCALE  
1" = 300'



**LEGEND:**

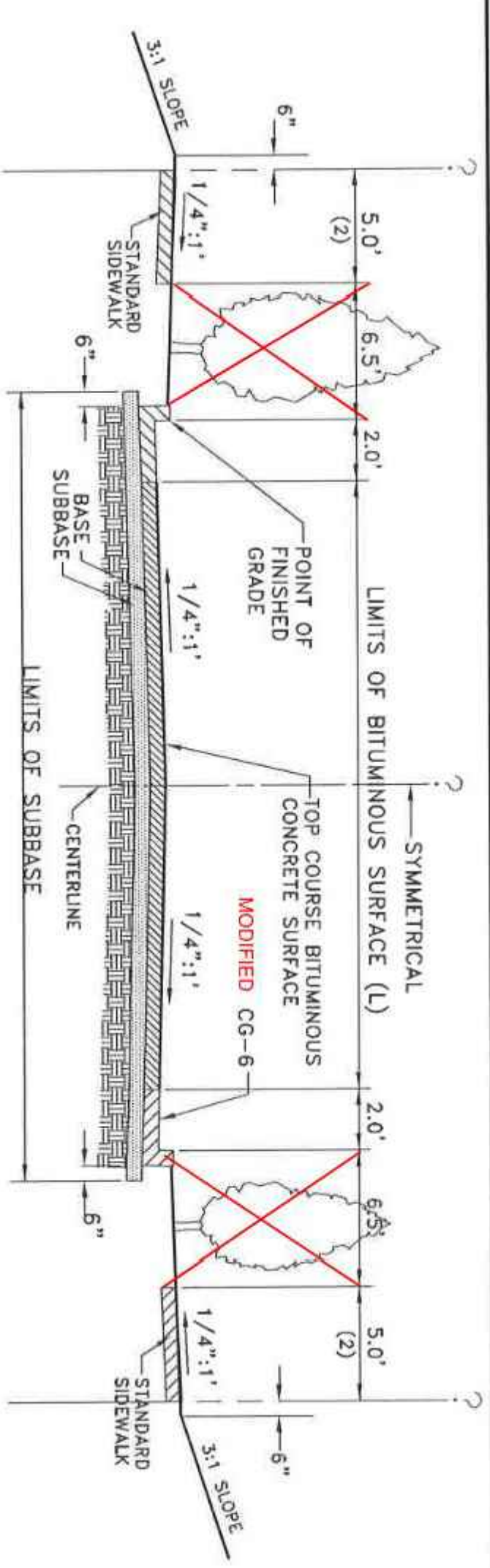
- EXISTING PUBLIC ROADS —
- EXISTING PRIVATE ROADS —
- PROPOSED PRIVATE ROADS —
- EXISTING PUBLIC ROAD ENTRANCE/ INTERSECTION TO PROJECT AREA ✱

# BELMONT BAY ROAD DESIGNATIONS

DATE: MARCH 15, 2024    PROFFER AMENDMENT REZ# 2024-00040  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MASTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 11 OF 12



# MODIFIED TS-1 ROAD SECTION

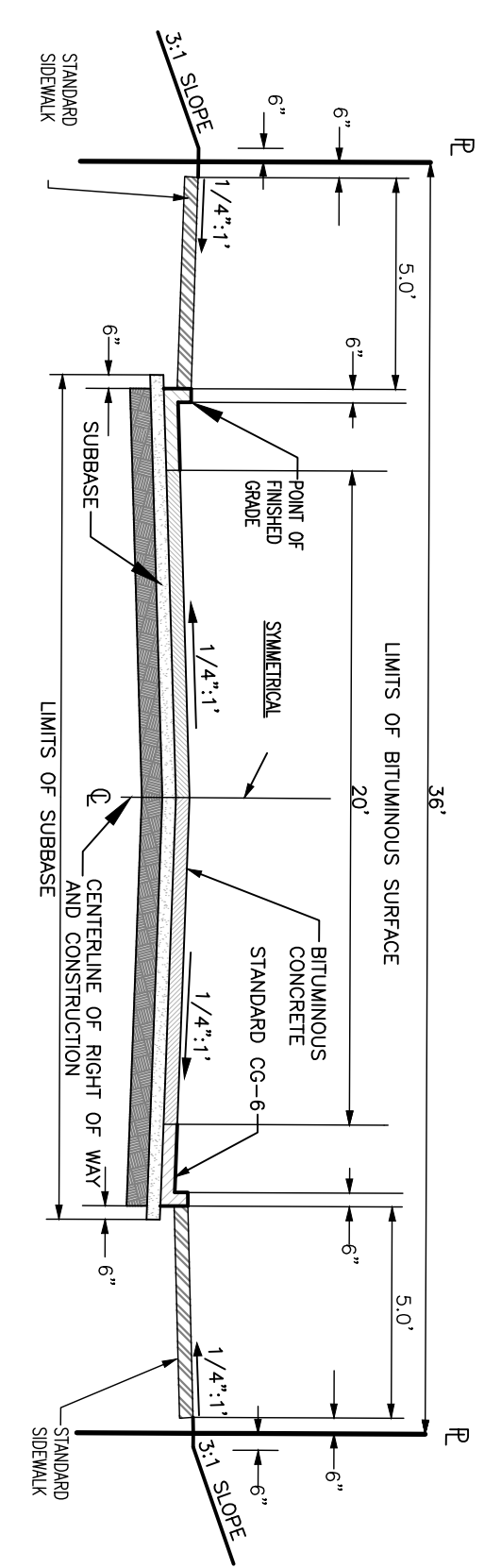


TRAFFIC VOLUME (VPD)	PARKING	PAVEMENT SECTION
0 TO 250	No Parking Allowed	20" CATEGORY I
0 TO 250	Parallel (one side)	20" CATEGORY I
0 TO 250	Parallel (both sides)	20" CATEGORY I
0 TO 250	Perpendicular	20" CATEGORY I
251 TO 400	Perpendicular (both sides)	20" CATEGORY II
251 TO 400	No Parking Allowed	20" CATEGORY II
401 TO 600	Parallel (one side)	20" CATEGORY III
401 TO 600	Perpendicular	20" CATEGORY III
601 TO 8000	No Parking Allowed	20" CATEGORY III

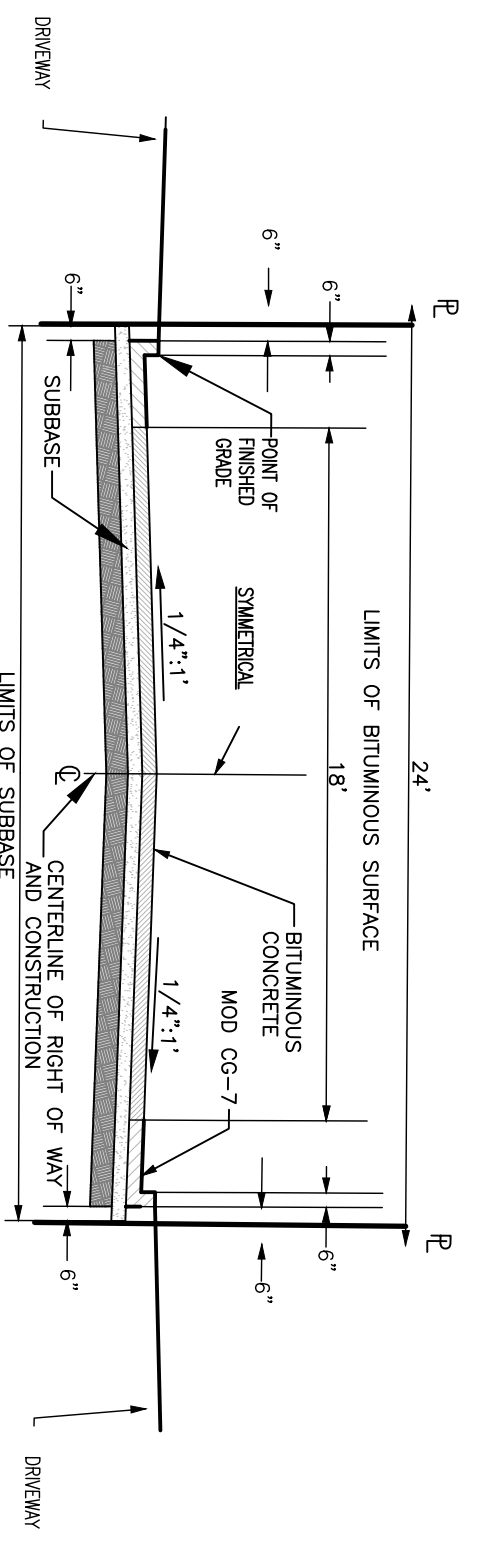
- GENERAL NOTES:
1. These dimensions are for travelways only, exclusive of parking spaces.
  2. Sidewalk location to be determined during final site plan review.
  3. Stone material shall be extended under the curb and gutter a minimum of six inches (6 in.), beyond the back of curb. The aggregate thickness under the curb and gutter shall be in excess of the depth of the gutter face or a minimum of four inches (4 in.), whichever is greater.
  4. Refer to Detail 650.01 for alternative pavement sections.
  5. Maximum grade eight percent (8%).
  6. Design of this type of roadway shall conform to current VDOT requirements.
  7. If optional street tree plantings are not provided per Section 802.45, the right-of-way may be reduced by 5 feet.

Detail No.	TS-1	COUNTY OF PRINCE WILLIAM VIRGINIA	TRAVELWAY STANDARDS FOR INDUSTRIAL, INSTITUTIONAL, OFFICE, COMMERCIAL, SINGLE FAMILY ATTACHED, AND MULTIFAMILY DEVELOPMENTS	Date 7/15/14
650.06				

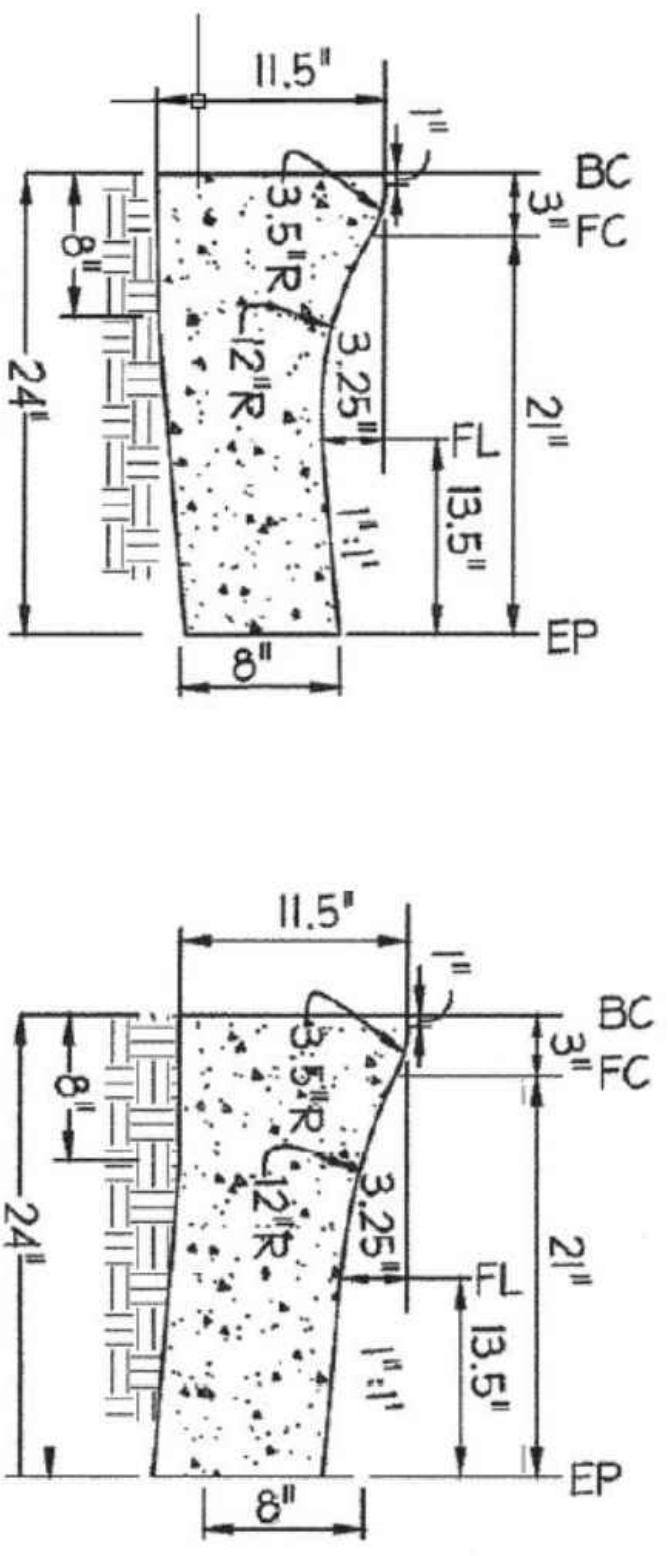
TYPICAL PRIVATE STREET  
MOD. TS-1



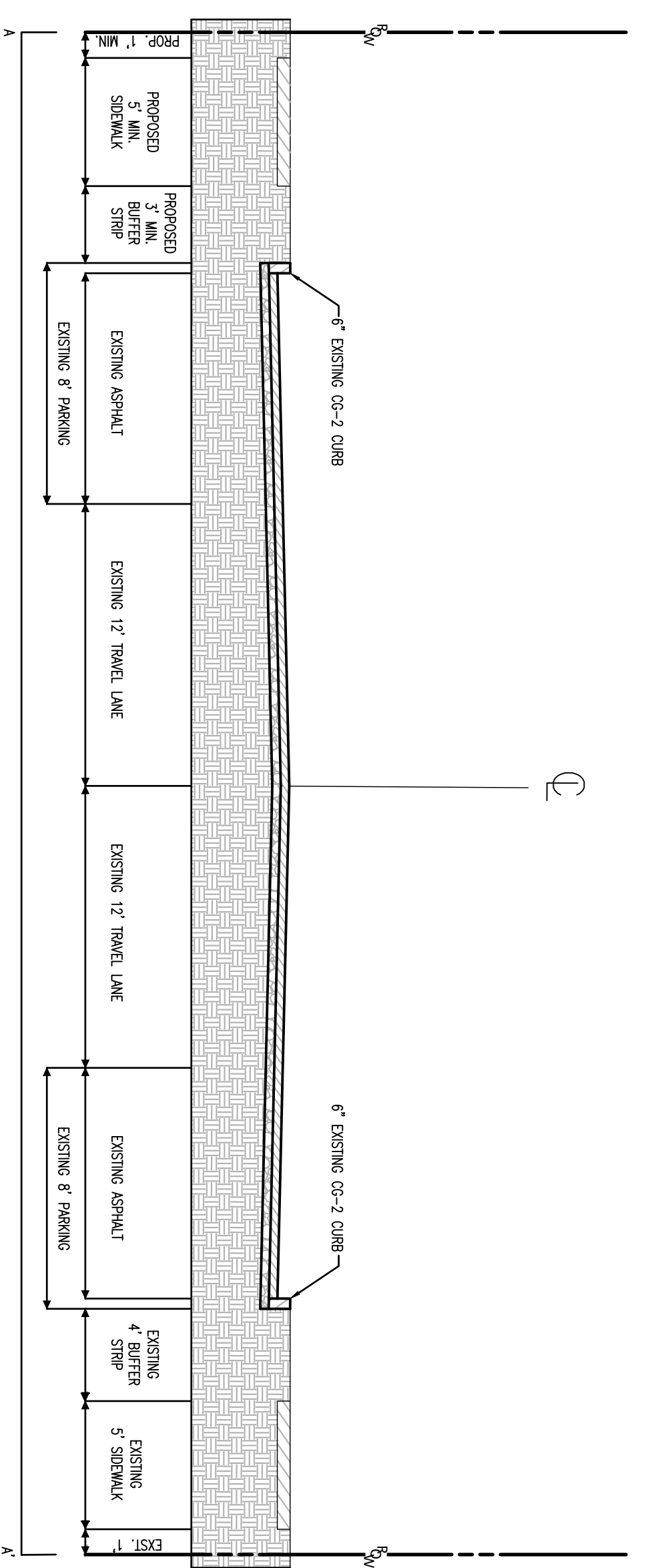
TYPICAL PRIVATE ALLEY  
MOD. TS-1



# MODIFIED CG-6



# HARBOR SIDE STREET SECTION A-A' BETWEEN PALISADES STREET AND FLEET STREET



# BELMONT BAY STREET SECTIONS

DATE: MARCH 15, 2024  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 12 OF 12





■ DESIGN GUIDELINES & DEVELOPMENT CODE ■

# MARINA OVERLOOK, BELMONT LANDING, AND TAYLOR POINT



*Prepared for Miller and Smith  
Limited to Rezoning Area  
REZ2024-00040 and  
SUP2024-00036  
October 31, 2025*

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**Mixed-Use Amenity Building**

**Marina Overlook**

**Taylor Point**

**Belmont Landing**

**Section I:  
INTRODUCTION:**

The Design Guidelines & Development Code for Marina Overlook, Belmont Landing, and Taylor Point establish the development framework for these areas. Upon the adoption of the proposed rezoning for these areas, the 2024 Design Guidelines & Development Code will guide future development for these areas. The applicable sections of the 1999 Design Guidelines are included in this document for continuity in design approaches and for the convenience of the reader.

The 1999 Design Guidelines, with any subsequent amendments, remain in effect for all other portions of the Belmont Bay Town Center.

When there is a conflict between any provisions, approvals by the Development Review Committee, applicable government agencies, and the 2024 amendments for the rezoning areas, these will supersede the 1999 Design Guidelines.

Although Belmont Landing and Taylor Point are not within the Town Center (or included within the Town Center special use permit area), for purposes of this Design Guidelines & Development Code, the terms “Town Center or Belmont Bay Town Center” shall mean Marina Overlook, Belmont Landing, and Taylor Point.

**Block and Use Group Designations**

<b>Block</b>	<b>Block Size (approx. acres)</b>	<b>SUP Land Bay Designations</b>	<b>Projected Land Use</b>	<b>Maximum Density Du/ac FAR</b>
<b>Rezoning Areas:</b>				
Marina Overlook	16.2	MDR & B-1	Residential TH Retail/Restaurant/Community	6 0.25
Taylor Point and Belmont Landing	36.9	HDR	Residential TH/FAR	16

**LAND USE SUMMARY**

	<b>Residences</b>	<b>Nonresidential</b>
<b>Marina Overlook</b>	<b>74</b>	
<b>Belmont Landing</b>	<b>178</b>	
<b>Taylor Point</b>	<b>96</b>	
<b>Residential Total</b>	<b>348</b>	
<b>Retail/Restaurant/Office</b>		<b>20,200 SF</b>
<b>Marina</b>		<b>163 slips 37 jet ski tenders</b>

# MARINA OVERLOOK, BELMONT LANDING, AND TAYLOR POINT



**STREET GRID SCHEMATIC AND LAND USE PLAN**

## **SECTION II: THE BELMONT DEVELOPMENT CODE**

### **Philosophy**



From a philosophical perspective, the design guidelines for the Belmont Town center establish an integrated “code” to optimally organize the development of human habitats, workplaces, streetscapes, and public spaces. Towards that end, this code represents the “urban regulations” which are intended to reach beyond merely aesthetic and architectural principles. The code is intended to promote rich physical, social, and economic “values” by promulgating a consistent and uniformly high level of quality throughout the Town Center. It is believed that the vitality of this new town will emerge from the well coordinated pursuit of streetscapes which are compact, consistent, beautiful, and walkable, with natural systems that are integrated on a human scale. The organizing principles of these guidelines are not new but, rather, represent a return to the timeless precepts of “urbanism” around which so many of Virginia’s treasured cities, towns and villages have evolved.

The Town Center land uses have been organized into the discrete planning units described on the Schematic Land Use Plan. These neighborhoods may also be referred to as “blocks” and “streetscapes” as the case may be. The blocks serve as the principal physical areas around which most architectural and site design elements will be structured. To define the basic parameters for image, design, and massing, the design guidelines ensure that the individual neighborhoods and blocks are developed with an integrity towards the quality, compatibility, and cohesion envisioned for all development in the Town Center.

### **Design Review Committee**

From a more pragmatic standpoint, the Development Code is intended to fulfill and amplify the requirements of the County’s Town Center special use permit requirements, and, more importantly, to provide the necessary and sufficient “urban regulations” to guide and assist builders, architects, and designers in preparing coherent and appropriate development plans. In addition to the County’s statutory plan review process, the Belmont Town Center Design Review Committee (DRC) has been organized to fulfill a heightened design review role. To ensure the integrity of the Town Center, the Design Review Committee is established in addition to separate covenants creating a Belmont Town Center Property Owners Association.

### **Comprehensive Planning Objectives**

Integrating both a regional planning and site-specific planning perspective, the Town Center plan has evolved from years of study by both the County and the landowner. Belmont Bay is well-grounded in the underlying objectives and initiatives of the County’s Comprehensive Plan and its Waterfront Action Plan. Out of these publicly-stimulated efforts have grown a New Urbanism philosophy which undergirds the guiding principles upon which these design guidelines are based.



The overriding philosophy for the Town Center seeks to fulfill the following commonly-held goals and objectives:

- *Create a new town serving as both a home and destination for County residents, workers, shoppers, tourists, and visitors in keeping with the County's Strategic Plan Update and Waterfront Action Plan.*
- *Develop a new town based on traditional neighborhood development (TND) precepts that merits national recognition yet is tailored to the characteristics of the Town Center site, and the adjoining community.*
- *Create an atmosphere which promotes compact, close-knit, human-scaled neighborhoods located within a short walk of the harbor and downtown retail area.*
- *Establish enhanced expectations for public and private development based on standards which enhance, maintain, and promote the environmental and cultural integrity of the Occoquan River waterfront area.*
- *Ensure the consistent application of superior design and implementation of improvements, thus protecting and enhancing the investment of all those at Belmont Town Center.*
- *Ensure harmonious relationships among natural and manmade features which promotes economic, environmental and cultural sustainability.*
- *Minimize disruption to neighboring properties both adjacent to and within the overall Belmont Bay community.*
- *Promote the use of transit and shared parking facilities in and around the Town Center to increase transportation efficiency and reduce unnecessary impervious areas along the waterfront.*
- *Ensure that the Town Center's schematic land use plan, included with the special use permit application requirements, will serve as the guide for the master planning process on a project-by-project basis.*
- *Facilitate development and minimize delays by establishing a rational basis and sequencing of tasks for preparing and evaluating proposed Town Center development.*

## Guiding Principles for Urban Design



This design philosophy shall be applied consistently throughout the project by both the developer and the individual site developer/builder to meet the following guiding principles:

- *Each streetscape and block should be designed to promote well integrated and comfortable pedestrian movement between buildings, as well as to offer a memorable, safe, and attractive experience.*
- *The Town Center's architectural and urban design elements should be selected and expressed in a manner that should be enduring and make a positive contribution to the project's image, viability, and sustainability.*
- *All aspects of the Town Center should promulgate high standards of urban design and architectural quality, seeking a careful balance amongst variety, consistency, and contrast within the community.*
- *The Town Center should be viewed as a unified, compact, mixed-use development with a special identity and sense of place. To reinforce this concept, buildings and uses at Belmont should be both visually and functionally compatible.*
- *The esthetic ambience of the Town Center should evolve from architectural styles, building orientations, building materials, graphic identity systems, landscaping, and a common set of site and building elements.*
- *Federal, Charleston, and Georgian interpretative stylings and vernacular designs embracing colonial or revival elements should be employed to emulate traditional Virginia architecture. Designs may include the Portofino elements suggested by the Centerbrook perspective included in the Waterfront Action Plan.*
- *The functional relationships of Town Center uses should be reinforced by the development of pedestrian interconnections, coordinated vehicular circulation, parking, utility corridors, and physical connections between buildings.*

The guidelines shall serve as the basis for review of site development components, streetscape improvements, public infrastructure, and architectural plans prepared by both the owners and individual neighborhood and block developers. They are to be administered and interpreted by the Town Center's Design Review Committee (DRC.) These standards shall guide architects, planners, and the DRC to cultivate design excellence, coordination, and integration in design by balancing the overall project image, identity, and theme of each use with the unique needs of each building site, streetscape, and tenant.

## ORGANIZATION OF THE BELMONT DEVELOPMENT CODE

In establishing the urban regulations around which future decisions are based, the organization of the code extends beyond the scope of traditional governmental regulations and rezoning conditions. As drawn from the guiding philosophy for the development of the Town Center, the Development Code adopted with the Special Use Permit conveys the intended urban design concepts and principles for Belmont Bay in a manner which can be either advisory or mandatory. The Code is organized into the three major sections:

### Streetscape Infrastructure Guidelines



#### Section III: Streetscape Infrastructure Guidelines

This section describes development standards for the Town Center's transportation, pedestrian circulation, infrastructure, and landscape organization, including:

1. *Vehicular transportation systems and street grid*
2. *Pedestrian circulation systems*
3. *Landscape development*
4. *Site furnishings*
5. *Signage*
6. *Utility services*
7. *Construction procedures*

### Architecture & Development Code



#### Section IV: Architectural and Development Code

This section describes architectural guidelines and a Development Code to be applied within the Town Center, addressing the following:

1. *Guiding architectural principles*
2. *The "Development Code"*
3. *Block and building orientations*
4. *Architectural design and materials*
5. *Parking structures systems*

### Design Review

#### Section V: Design Review and Approval Process

The final section describes a procedure for architectural review within the Belmont Town Center.

1. *The concept plan*
2. *The preliminary development plan*
3. *The final development plan*

## Code Interpretation

The Belmont Town Center Development Code and related design guidelines have been formulated and adopted by the the County, the Belmont Bay owners and their consultants as the growth management regulation for the Town Center. The code is subject to subsequent refinements, modifications and amendments by the DRC provided that no such modification shall either conflict with the guiding philosophy and intent of the guidelines or affect any approval granted by County prior to the date of such refinement, revision or modification.

In addition to the design standards and development code, other considerations such as state, federal and local governmental regulations, zoning proffers, and special use permit conditions must be observed by the site or block developer. This document does not enumerate all applicable regulations, nor does it list all conditions surrounding the rezoning of the new town and surrounding community. Each site or block developer is responsible, however, for meeting all applicable governmental requirements and additional stipulations of the DRC.

As used in this document, the word "should" describes *discretionary* guidelines and "shall" describes *mandatory* standards to be applied by the DRC.

**SECTION III:  
STREETSCAPE INFRASTRUCTURE GUIDELINES**

**MAJOR TRANSPORTATION SYSTEMS AND THE STREET GRID**

The planned vehicular circulation system for Belmont Town Center provides for the coordinated development of individual parcels employing both public and private transportation systems. There are six aspects of transportation and streetscape improvements within the Town Center which must be given special attention.

1. *Town Center Public Streets*
2. *Private Streets, Passages, and Alleys*
3. *Office and Residential Motor Courts*
4. *Surface Parking Areas*
5. *Parking Structures*
6. *Public Transportation*

**Town Center  
Public Streets**

Belmont Bay Drive, a spine road serving the community, gives primary access into the Town Center. As it approaches the Town Center, this four-lane divided thoroughfare reduces to a three-lane, undivided urban street. The extension of Belmont Bay Drive, along with the parallel Main Street and the perpendicular connecting streets comprise the major elements of the traditional grid streetscape network of the Town Center. Supplementing the street system design guidelines are the following three companion exhibits, as prepared by The Cox Company:

- *Town Center Street Grid Plan*
- *Town Center Traffic Impact Assessment and Transportation Improvements Plan*
- *Town Center Schematic Land Use Plan*

These streets shall be dedicated for public use and designed to ensure capacity for adequate circulation among the range of planned land uses. Conventional parallel parking is encouraged on the Town Center's public streets. These tree lined roads will provide an attractive streetscape and address for individual buildings fronting thereon. All building sites shall have access to either public or private streets at the locations designated on final sites plans and subdivision plats. Reasonable variations in the exact location of pedestrian and vehicular access points may be permitted, subject to the approval of the Design Review Committee and the County.

The guidelines for the Town Center's public streets include the following characteristics. Refer to the Special Use Permit application package for further street design parameters.

**1. Belmont Bay Drive - Town Center Section**

- *Movement: Free, un-signalized, two-way pattern*
- *Design Speed: 25 mph*
- *Lanes: 2-3 travel lanes (12' lane width), with 11' center turn lane*
- *Parking: Parallel, 1 or 2 Sides, 8' x 22' parking stalls*
- *Right of Way: Variable, back of curb to back of curb*
- *Sidewalks: 6'-20' at curb line (subject to specific use)*
- *Drainage: Curb inlets, underground conveyance*
- *Curbs: Raised, with gutter pan optional*
- *Curb returns: 15'-25' radius (per intersectional requirements)*

**2. Main Street and Town Center Major Streets**

- *Movement: Free, un-signalized, two-way pattern*
- *Design Speed: 20 mph*
- *Lanes: 2 Lanes (12' lane width, with no turn lanes)*
- *Parking: Parallel; 2 Sides, 8' x 22' parking stalls*
- *Right of Way: Variable, back of curb to back of curb*
- *Sidewalks: 6'-20' at curb line (subject to specific use)*
- *Drainage: Curb inlets, underground conveyance*
- *Curbs: Raised, with gutter pan optional*
- *Curb returns: 15' radius*

**3. Minor Town Center Commercial and Residential Streets**

- *Movement: Free, un-signalized, one or two-way pattern*
- *Design Speed: 20 mph*
- *Lanes: 2 Lanes (11' lane width, with no turn lanes)*
- *Parking: Parallel 1 or 2 sides, 8' x 22' parking stalls*
- *Right of Way: Variable, back of curb to back of curb*
- *Sidewalks: 4'-8' at Curb Line (subject to specific use)*
- *Drainage: Curb inlets, underground conveyance*
- *Curbs: Raised, with gutter pan optional*
- *Curb returns: 15' radius*

## Private Streets & Alleys



A system of alleyways and private streets may be constructed within the interior of Town Center properties. They should be generally parallel to the orthogonal street system and provide access for both residents and service. These private streets, alleys and special vehicular passageways may be constructed within the interior individual building sites in order to connect the proposed building(s), motor courts, and/or parking structures to a public street in accordance with the following standards:

- *Streets to conform to minimum VDOT and County standards, for pavement design*
- *Private street locations and geometry shall be in accord with final site plans*
- *Minimum pavement widths: 10' - 11' per lane*
- *Parking: On-street, parallel with 8' x '22' parking stalls; perpendicular and angled parking may be permitted in low traffic zones*
- *Paving materials to match those of adjacent public streets, provided that specialty pavers are permitted per approval of the Design Review Committee*

## Motor & Pedestrian Courts



While New Urbanism development implies that the typical pedestrian / vehicular interface occurs at the public street, certain uses within the Town Center (such as the hotel/conference center, the Virginia Science Museum, and corporate office complex) may dictate specialized access arrangements. These buildings may require multi-purpose motor and pedestrian courts that combine both (1) a “plaza” or “courtyard”-like entrance from the adjacent public street, and (2) a visitor or handicap drop-off area at the building’s entry or its porte cochere. Final design solutions for each motor/pedestrian court should be reviewed for scope, placement, and configuration by the Design Review Committee and County. Subject to County and VDOT site plan review and approval, motor courts may be classified as either public or private.

A limited number of short term VIP visitor/delivery/handicap parking spaces may be provided adjacent to the motor court, which shall be designed as dual purpose pavements to ensure safe and attractive pedestrian use. If provided, off-street parking should be landscaped and screened by walls or landscaping from the public street views. A drop-off area should be integrated with the motor court in close proximity to the building entry way. Parking decks may be accessed directly from motor court areas. Motor courts shall not be used for through traffic or long-term parking.



Materials should indicate to motorists that they have entered a pedestrian area. One or more materials may be used as the dominant material for motor court paving, including:

- *Integrally colored concrete with special accent bands*
- *Precast concrete unit pavers*
- *Rustic terrazzo (integral exposed aggregate concrete)*
- *Clear or stress finished concrete, with special jointing patterns and accent bands of another material*
- *Stone pavers*
- *Imprinted paving patterns, such as stamped concrete*
- *Asphalt*

The color of vehicular/pedestrian court paving should compliment and be a part of both its immediate streetscape and the overall family of colors within the Town Center. Accent colors that directly relate to the individual building lobby or skin materials are encouraged to provide the linkage between each motor court at its immediately-visible surroundings. Asphalt is permitted.

Header curbs are required at the edge of all motor court vehicular driving surfaces to separate vehicular travelways from pedestrians. They shall be constructed of concrete or stone. Asphalt curbs are not permitted.

## Street & Hardscape Furnishings



The use of variety of coordinated street furnishings or hardscape improvements, is strongly encouraged throughout the Town Center. The final selection of the palette of materials will be finalized by the Design Review Committee prior to commencement of construction of first phase public improvements.

Refer to the Appendix of this document for selected details and manufacturers' cut sheets of prototypical standards. The following general guidelines shall be applied:

Bollards may be used in a number of applications, provided that they are a minimum twelve (12) to eighteen (18) inches in diameter. Wood, concrete and metal are acceptable materials. No fiberglass materials should be employed.

Metal flagpoles to match the existing street lighting are permitted for the flying of the American, State, County, international, festival, and corporate flags. Flagpole heights should not exceed fifty (50) feet.

Light fixture illumination at motor courts should be provided from indirect sources such as landscape, accent up-lighting and down-lighting. Where used, pole mounted sources shall have a controlled cutoff reflector and shall match public street lighting. Light sources shall

produce a white light. Colored lenses will be generally prohibited. Lighting shall be positioned and/or screened to minimize light spill-over to adjacent sites and communities.

Sculpture may be used but shall be appropriate in scale, form, and texture. It should not produce noise (other than that which can be generated from moving water) or contain flashing or moving lights. Fountains and water features may be appropriate in pedestrian/motor courts and plaza areas.

The Design Review Committee reserves the right to review and approve all street and hard-scape furnishings.



### **Surface Parking Areas**



At-grade parking areas, as well as medians, will be graded to ensure the preservation of existing vegetation as close as possible to the edge of the parking lot. A maximum side slope of 2:1 will be allowed for this purpose; retaining walls may also be required in some instances to preserve existing vegetation. A maximum slope and cross slope of five (5) percent is recommended within the surface parking areas, and a minimum of three (3) feet will be allowed between buildings and parking lots or structures.

If parking lot medians are to be provided, bermed and landscaped medians at least ten (10) feet in width should be provided. Landscaped medians will also be provided between parking areas and service drives. Interior parking shall provide adequate turnaround areas for emergency and delivery vehicles. Individual off-site parking spaces within commercial areas should not have direct individual access to any public street other than for parallel parking.

### **Parking Structures**

Free-standing and integrated parking decks will be employed at strategic locations throughout the Town Center to accommodate the parking needs of the mixed uses. The architectural guidelines address the design considerations to be incorporated into the parking decks and structures.

### **Public Transportation**

Bus stops and locations for other forms of public ground transportation transfer points may be located within the Town Center in accord with final site plans. At full development of the Belmont Bay community, intra-community ground transit may be provided between the Town Center and the Woodbridge VRE/Amtrak Station.

## PEDESTRIAN CIRCULATION

Belmont Town Center shall function as a pedestrian-friendly environment favoring traditional neighborhood design precepts in which residents, the work force, and tourists have equal access to amenities, goods, and services. An integrated set of standards is promoted in the Development Code for major pedestrian system elements such as sidewalks, covered walkways, through-building and garage connections, and pedestrian trails. This common set of guidelines includes color, lighting graphics, materials, and critical dimensions.

### Harbor Plaza



Together, the harbor plaza and waterfront walk serve as the core public activity zone within the Belmont Town Center. Incorporating passive park features and urban landscaped spaces, these spaces will establish the center of outdoor activity for residents, shoppers, tourists, and the town's work force. Pedestrian access to the plaza and marina area will be aligned with important lines of sight within the Town Center. Complementary landscaping will be organized to accentuate, not block, views of major buildings and public spaces.

### Urban Sidewalks



Urban sidewalks shall be provided in the Town Center along all public streets. Private walks shall be provided by individual developers connecting the urban sidewalks, motor courts, buildings, and civic spaces. Private residential walks shall be a minimum of four (4) feet wide and shall generally match the public sidewalks in scoring patterns and materials. Residential entry areas shall be connected to the commercial area sidewalks, which shall be designed to appropriate, but varying widths. Covered walkways should be provided by individual developers at building entries and porte cocheres. Connections shall also be provided to buildings and their parking lots or structures. Consistent graphics and lighting programs shall be used to identify public sidewalks and private walkways.

### Crosswalks



Street crosswalks should be provided across all public streets, private vehicular accessways and service drives located between surface parking areas and building entrances. Crosswalks should provide both clues to the pedestrian and driver to promote "traffic calming." Crosswalks shall be at least six (6) feet wide and located at all major crossing points on the Town Center streets, access drives, motor courts, service drives, and parking lots. Walks may be denoted by contrasting paving materials or appropriate painted striping. While it is not the intent to create uniformity throughout the Town Center, a common vocabulary of complementary crosswalk materials and colors will be adopted and implemented by the Design Review Committee.

## Trails

Trails and related forms of pedestrian access will be provided within the Town Center through public spaces and common areas, forming a circulation system connected to the traditional public infrastructure sidewalks. In coordination with the design and development of the Heritage Trail, these paths may be located along the edges of the Town Center and the river at varying distances and horizontal curvature to heighten visual interest. Where appropriate within environmentally sensitive areas, a curvilinear alignment and sensitive layout accenting major views of the waterfront should be employed to make the system a neighborhood amenity.



## STREETSCAPE AND LANDSCAPE GUIDELINES



A distinguishing feature of the new town is its New Urbanism comprehensively planned streetscape and landscape development program. The application of the urban landscape and streetscape concepts begins at the Town Center's gateways and continues throughout, communicating a changing scale and zonal character as the user enters the more intimate areas of the residential neighborhoods or the more intensive areas of the activity centers.

The master streetscape and landscape architecture theme for the public elements, consisting of the gateways, the tree-lined main street, and minor streets, is to be implemented by the Town Center developer in conjunction with its public improvements. The development of each building site shall reinforce and add to this image by employing complementary landscape forms, patterns, and materials to create wall plantings, squares, plazas, specimens, allees, hedges, bosques or groves, as appropriate.

The purpose of this section is to define the expectations and qualities of landscaping which are intended to be seamlessly implemented throughout the project. The areas of landscape responsibility for individual property owners are defined herein as well as in the Town Center property association documents. Each development is encouraged to retain a qualified professional landscape architect to complement the project design team. The landscape plan shall be reviewed and approved by the DRC.

### Site Grading

Buildings should be slightly elevated above the public street whenever possible to improve image, visibility, and site drainage. The grading of adjacent buildings shall be coordinated to meet common facilities such as parking structures, lobbies, and motor courts at the same grade.

Buildings within the same neighborhood grouping or cluster should have the same finished floor elevation and/or massing and details which relate the individual buildings to each other. Berms, if applied, should be gently curved and rolling at an appropriate scale, and artificial-looking earth "mounds" are expressly prohibited.

To reduce environmental impacts, site designs will attempt to minimize the extent of mass grading within the community. All site grading will be done in a way that enhances the preservation of mature vegetation (or where otherwise called for on approved site plans or by the Design Review Committee). A maximum side slope of 2:1 in and around buildings will be allowed, provided that retaining walls may also be necessary in some instances to preserve existing vegetation.

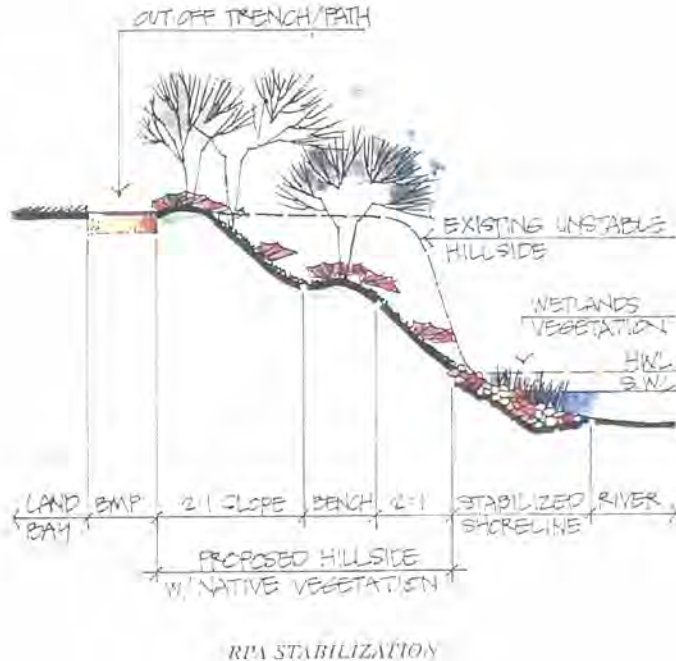
## BMPs



Best Management Practices (BMPs) shall be developed in accord with the BMP Master Plan for Belmont Center (as originally prepared by The Cox Company and amended for inclusion and adoption with the SUP application). In conjunction with Belmont Bay's initial development plans, the BMP Master Plan was adopted as a comprehensive approach to addressing the water quality issues inherent to an environmentally sensitive, waterfront property. The Town Center has a relatively minor impact on community BMPs, with the outlying Belmont Bay land bays and the golf course serving as the location for the high capacity BMP facilities which have been implemented for the entire project.

Due to the heavier weighting attached to these outlying land bays for water quality performance, the Town Center needs only to achieve nominal pollutant reduction target levels in its "on-site" BMP system. Towards this end, the Town Center will employ urban BMPs, where feasible, to the extent necessary to achieve the BMP master plan's pollutant reduction target levels. To ensure compliance, BMPs may be incorporated into the drainage designs of private streets and parking lots in order to supplement the community-wide BMP water quality objectives and to minimize cumulative, untreated runoff concentrations.

A maintenance program will be imposed in the property owners agreements to mandate provisions for maintaining permanent BMPs and erosion and sediment control facilities.

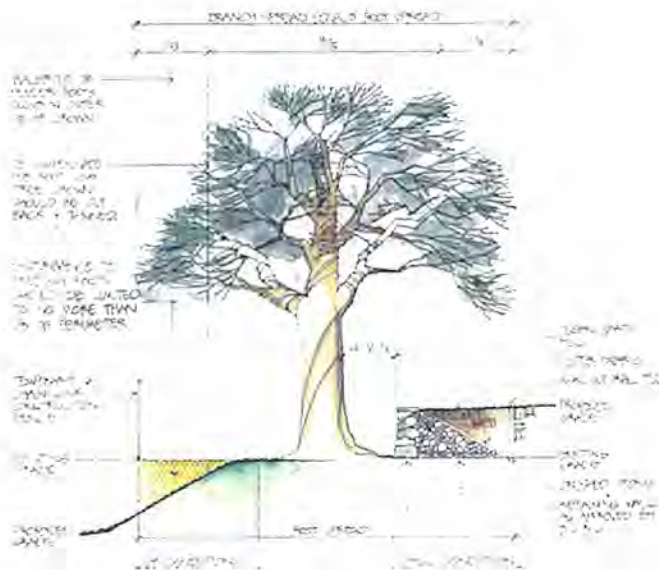


## Landscape Protection



Existing wooded areas, particularly those within Resource Protection Areas and wetlands, are to be preserved to the maximum extent possible. Existing natural areas situated outside building zones will be organized into a comprehensive community landscape design program to enhance the visual character of the Town Center and its surrounds. To achieve this objective, grade changes at the edge of building zones will be minimized and the elevations of building pads, streets, parking lots, and other paved areas will be established to be compatible with the existing “flat” topography of the Town Center property. “Tree save” areas should be incorporated into the landscape design plan to serve as “passive” settings for open space, where possible.

All existing trees of eight (8) inch caliper or greater will be field located and typed and will be depicted on site plans for individual development activities. These plans will indicate specimen trees to remain and those to be removed. To determine which trees will require special protection (such as tree wells), a circle with a radius equal to one foot for each inch of caliper will be drawn for each existing tree to be retained. This area represents a normal branch and critical root spread. If two-thirds of this area can remain undisturbed at original grades, no special treatment will be required to protect the tree. Otherwise, tree protection fencing shall be provided.



## **Common Landscape Areas**

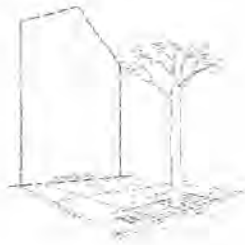
The Town Center's common landscape areas will consist of plazas and courtyards, passive open spaces, and existing natural areas that are intended to be incorporated into the final site design by the owner. These are to be maintained by the Belmont Town Center Property Owners' Association. These areas fall into three categories:

- *Street Tree Landscape Easements: These consist of the street tree plantings, sidewalks, and lawn areas adjacent to the Town Center's major and minor public streets which have been dedicated by the developer to the Virginia Department of Transportation.*
- *Landscape Preserves: These are the peripheral areas of Belmont Town Center, primarily Resource Protection Areas, which consist of wooded areas, grassy slopes, and BMP facilities. A trail system will provide access to some of these areas. Improvements to the Landscape Preserves will be provided by the developer and maintenance will be provided by the Belmont Town Center Property Owners' Association.*
- *Waterfront/Marina Overlook Landscapes: The waterfront open spaces and community center for Belmont Bay residents establish the "urban edge" of the Belmont harbor and marina. Hardscape and softscape within this area will be designed to amplify the beauty of the Town Center's public improvements along the Occoquan River setting and includes a small amphitheater. At full development, the area will be maintained by the Belmont Town Center Property Owners' Association.*

## **Transition Areas**

Transitional landscape areas consist of the areas internal to individual blocks and streetscapes, including utility easements and parking lots, which are located between the buildings and the common landscape areas. This landscape development shall be implemented by the developers of individual neighborhoods, or blocks, and shall be compatible with the adjacent landscape material in color, form, and texture. The horticultural intent is to provide interesting landscape color and texture throughout the year for the project as a whole, as reviewed and approved by the DRC.

Landscape development should be sympathetic to and coordinated with the architectural scheme and context with respect to scale, color, and organization. If required, buffers should be employed to separate different site-specific uses in order to obtain both a level of privacy and individualization, as well as to minimize noise pollution levels. Service areas and public utilities should be screened from view with vegetation. All screening should be integrated with existing vegetation stands wherever possible.



## LANDSCAPE MATERIALS

### Landscape Classification



The following guidelines shall apply to the major categories of landscape materials in order to create the desired visual harmony among the individual development sites. All plant material shall be selected from varieties visually compatible with or from the native vegetation and free of odorous or messy fruits or seeds. Species with chronic diseases, pest problems, and/or structural weaknesses shall be avoided. The following categories are addressed:

- *Canopy and Street Trees*
- *Ornamental Trees*
- *Evergreen Trees*
- *Deciduous Shrubs*
- *Evergreen Shrubs*
- *Ground Cover*
- *Turf*

### Streetscape Canopy & Street Trees



Major deciduous trees shall be used in organized streetscape designs between the building zone and the street edge. They may be located either within sidewalk planters or within planting zones between the sidewalk and curb.

Also, these street trees should be used in parking islands of surface and structured lots, and on lawns of corporate offices and the Town Center's destination uses, where they do not interfere with the landscape architectural design intent of the street tree plantings. Minimum preferred size: 3" - 3 1/2" caliper.

### Ornamental Trees



Ornamental trees that normally grow from 12' to 25' tall at maturity should be liberally planted for accents and visual emphasis. The flowering habits of ornamental trees are very effective when used on corporate office lawns or in the vicinity of Town Center buildings where space is adequate.

Ornamental trees are most effective with a dark background provided by architecture or evergreen trees. Multiple or single stem, small scale trees may be used in small scale pedestrian locations where space is limited and an intimate feeling is appropriate. Minimum preferred size: 10'-12' height.

## Evergreen Trees



Evergreen trees shall be used at strategic locations within the Town Center and should be designed into group plantings to enhance “winter” seasonal interest, screen objectionable views of service areas and parking structures, and as a backdrop for ornamental trees.

Evergreen trees shall be selected from species native to the area or similar in appearance. See selection list on following pages for recommended species. Minimum preferred size: 8'-10' height.

## Deciduous Shrubs

Deciduous shrubs should be used as accents throughout the Belmont Town Center to create seasonal color interest. Due to their informal appearance, they should not be used in high image areas where a manicured formal image is desired. Deciduous shrubs should be maintained with a "natural" growth habitat and shall not be used as formal hedges, except in occasional streetscape settings. Minimum preferred size: 3'-4' height.

## Evergreen Shrubs

These shall be selected from varieties of winter hardy shrubs commonly found in the mid-Atlantic region area. They may be used where a low level screen or hedge is desired at parking lots, sidewalks or as mass plantings in the landscape; they may also be used as effective ground covers on slopes. Minimum preferred size: 18"-24" spread.

## Ground Covers

*Euonymus fortunei* “Coloratus” (Evergreen Wintercreeper) is recommended for ground cover plantings fronting on the Town Center’s public streets. Plant 18” on center. Use on slopes steeper than a rise of one foot in three feet of run. Minimum preferred size: Vines - 2 1/4" pots, Shrubs - 18"-24" spread.

## Turf

Lawns and other turf zones within the Town Center’s public street rights of way and other first phase development areas may be seeded, with sod as an optional substitute. Subsequent to this, sod should be used on all disturbed areas in individual block development projects not otherwise planted or in mulched beds. A standard turf shall be established for the entire project which may be modified by the Design Review Committee. Where sod is employed, it shall be of a quality meeting the certification requirements of the Virginia Association of Turf Growers.



**Recommended  
Plant  
Materials**



**Canopy Trees**

*Acer rubrum*  
*Acer saccharum*  
*Chionanthus virginicus*  
*Cladrastis lutea*  
*Fagus grandifolia*  
*Fraxinus americana*  
*Fraxinus pennsylvanica* "Newport"  
*Ginkgo biloba*  
*Gleditsia triacanthos var. inermis*  
*Liquidambar styraciflua*  
*Nyssa sylvatica*  
*Oxydendrum arborium*  
*Platanus occidentalis*  
*Quercus alba*  
*Quercus coccinea*  
*Quercus palustris*  
*Quercus phellos*  
*Quercus rubra*  
*Quercus shumardii*  
*Taxodium disticum*  
*Tilia cordata*  
*Zelkova serrata*

Red Maple  
 Sugar Maple  
 White Fringetree  
 American Yellowwood  
 American Beech  
 White Ash  
 Newport Green Ash  
 Ginkgo (male)  
 Thornless Honeylocust  
 Sweet Gum  
 Sourgum  
 Sourwood  
 Sycamore  
 White Oak  
 Scarlet Oak  
 Pin Oak  
 Willow Oak  
 Red Oak  
 Shumard Oak  
 Common Baldcypress  
 Littleleaf Linden  
 Japanese Zelkova

**Ornamental Trees**

*Amelanchier canadensis*  
*Cercis canadensis*  
*Chionanthus virginicus*  
*Cornus florida*  
*Cornus kousa*  
*Crataegus sp.*  
*Halesia carolina*  
*Lagerstroemia indica*  
*Magnolia soulangeana*  
*Magnolia stellata*  
*Magnolia virginiana*  
*Malus sp.*  
*Prunus serrulata*  
*Stewartia ovata*  
*Styrax japonicum*  
*Ilex opaca*  
*Pinus nigra*  
*Pinus strobus*  
*Pinus thunbergii*  
*Tsuga canadensis*

Shadblow Serviceberry  
 Eastern Redbud  
 White Fringetree  
 Flowering Dogwood  
 Kousa Dogwood  
 Hawthorn  
 Carolina Silverbell  
 Crapemyrtle  
 Saucer Magnolia  
 Star Magnolia  
 Sweetbay Magnolia  
 Flowering Crabapple  
 Flowering Cherry  
 Mountain Stewartia  
 Japanese Snowbell  
 American Holly  
 Austrian Pine  
 White Pine  
 Japanese Black Pine  
 Canadian Hemlock

**Recommended  
Plant  
Materials**



**Evergreen Trees**

*Ilex opaca*  
*Pinus nigra*  
*Pinus strobus*  
*Pinus thunbergi*  
*Tsuga canadensis*

American Holly  
Austrian Pine  
White Pine  
Japanese Black Pine  
Canadian Hemlock

**Deciduous Shrubs**

*Berberis* sp.  
*Cotoneaster* sp.  
*Euonymus* sp.  
*Forsythia x intermedia*  
*Fothergilla gardenii*  
*Hamamelis virginiana*  
*Jasminum nudiflorum*  
*Potentilla fruticosa*  
*Pyracantha coccinea* 'lowboy'  
*Rhododendron* sp.  
*Rhododendron* sp.  
*Viburnum* sp.

Barberry  
Cotoneaster  
Euonymus  
Forsythia  
Dwarf Fothergilla  
Witchhazel  
Winter Jasmine  
Shrubby Cinquefoil  
Firethorn  
Azalea  
Rhododendron  
Viburnum

**Evergreen Shrubs**

*Ilex* sp.  
*Ilex glabra* "Shamrock"  
*Juniperus* sp.  
*Kalmia latifolia*  
*Mahonia aquifolium*  
*Pieris japonica*  
*Taxus* sp.

Holly  
Inkberry  
Juniper  
Mountain Laurel  
Oregon Grapeholly  
Japanese Pieris  
Yew

**Groundcovers**

*Ajuga reptans*  
*Euonymus fortunei* "Coloratus"  
*Hedera helix*  
*Hosta* sp.  
*Hypericum* sp.  
*Liriope muscari*  
*Liriope spicata*  
*Pachysandra terminalis*  
*Vinca minor*

Buglewort  
Purple Wintercreeper  
English Ivy  
Hosta  
Hypericum  
Liriope  
Spreading Liriope  
Japanese Spurge  
Periwinkle

## **Irrigation**

An automated irrigation system may be installed by the developer in the landscape easement areas. Individual building site owners shall install, operate and maintain similar systems, where required, in landscape areas within their site, as approved by the DRC.

## **Planting Practices**

All plants should be nursery grown in accordance with the highest standards of horticultural practices and should be grown under climatic conditions similar to those of the site for at least two years. Plants shall be typical of their species or variety and should have normal growth habits. Plants shall be free of disease and shall have healthy, well-developed root systems. All plants shall conform to the *American Standard for Nursery Stock* as published by the American Association of Nurserymen.

## **Edging Material**

The use of a plant bed edging or mowing strip is required. The following materials are acceptable: steel (black), concrete, stone, and brick.

## **Mulching Material**

Mulch shall be bark from a pine, fir, or hardwood source. Maximum dimension for chunk type should not exceed 1.5 inches. Maximum length for shredded type should not exceed 4 inches.

## **Maintenance & Covenants**

The Town Center Property Owners' Association will maintain common areas described in the Declaration of Protective Covenants. Individual owners shall be responsible for executing a landscape maintenance program for landscape areas within their site. The program shall include the following and be coordinated with the maintenance of common areas:

- *Prune trees and shrubs as necessary to maintain an attractive shape; remove dead branches and provide clearance for vehicles and pedestrians.*
- *Replace dead and disfigured plant material immediately with plants of equal size except in environmentally sensitive areas at the discretion of the Belmont Town Center Design Review Committee.*
- *Fertilize, mulch, water, and weed plant beds. Apply insecticides and fungicides as necessary to maintain plant vigor and appearance.*
- *Lawns shall be watered, mowed, and maintained in a dense, weed-free condition. Lawns shall be edged adjacent to paved surfaces.*
- *Remove fallen leaves, branches, paper, and other debris from lawn, shrub, and pavement areas.*

## STREETSCAPE AND SITE FURNISHINGS

A coordinated system of appropriate streetscape and site furnishings with similar color, scale, and materials will help to unify the Belmont Town Center. Consistent with New Urbanism design principles for retail and public spaces, restraint should be applied to the use of overly elaborate hardscapes as well as the extent and placement of site furnishings.

### Lighting

A coordinated hierarchy of lighting sources and intensities is required to maximize the benefits of “night architecture” in the Town Center. This hierarchy should respond to the relative building heights and architectural features. A white colored light source should provide an appropriate foot-candle level of illumination which satisfies the standards set by the Illuminating Engineering Society of North America. Lighting design criteria shall apply to:

- *Town Center Public Streets*
- *Private Streets, Alleys, and Access Drives*
- *Pedestrian and Landscaped Areas*
- *Waterfront Plaza, Marina, and Harbor Retail Area*
- *Motor/Pedestrian Courts and Plazas*
- *Structured Parking*

Public streets within the Town Center should be illuminated by the owner/developer using a controlled light distribution pattern. A lighting plan shall be prepared with the public improvements plans. Public streets which are intended to serve a dual use for occasional “festival” activities and “outdoor markets” should include supplemental lighting and appropriate electrical conduit and service connections to fulfill this purpose.

Private streets, alleys, and access drives shall use pole mounted lighting to match public street lighting in form and color. Landscape up-light and down-light accents shall be provided, where appropriate. In a fashion similar to public streets, those private streets and alleys which are intended to serve a dual use for occasional “festival” activities and “outdoor markets” shall include supplemental lighting and appropriate electrical conduit and service connections to fulfill this purpose.

Pedestrian and landscape areas shall have lighting in pedestrian plazas, pedestrian connections, and sitting areas to provide reasonable levels of nighttime visibility. Accent lighting should be used at special locations such as stairs, entry signage, and sculpture.





Waterfront Plaza, Marina, and Harbor Retail areas should be illuminated in a fashion similar to other private pedestrian landscape areas. Illumination intensities should be somewhat greater than minimal standards to ensure nighttime “vibrancy” in and around the plaza. The urban waterfront areas shall have down-lighting and bollard lighting dispersed throughout the plaza, the marina walk, and activity areas adjacent to the harbor. Accent lighting will be used at special locations such as stairs, entry signage, and sculpture. Motor courts will receive some illumination from adjacent buildings. Additional lighting within courtyards should be provided by hidden source landscape up-light and down-light accents. Those areas which are intended to serve “festivals” and “outdoor markets” shall include supplemental lighting and appropriate electrical conduit and service connections.

Those areas which are intended to serve “festivals” and “outdoor markets” shall include supplemental lighting and appropriate electrical conduit and service connections.

## Fencing



Fencing may be employed with discretion and restraint. In general, New Urbanism regulations should approach fencing with broad flexibility. However, it is commonly accepted that fencing will be strongly discouraged on commercial building sites (except during the construction phase.) Fencing in residential areas is appropriate, with the design of fences for residential neighborhoods and other blocks to be coordinated and approved on a case-by-case basis by the DRC.



In keeping with Federal and Georgian traditions, it is recommended that fences and gates be constructed of iron (or other painted metal), provided that fences of wood or wood-like painted surfaces may be permitted where architecturally compatible with building forms and materials. Chain link fence and similar contemporary fence types should not be used.

Brick walls and fences are not encouraged for use on individual lot frontages within the Town Center. Brick provides an appropriate treatment for garden walls and rear privacy walls in certain townhouse applications.

Brick or stone ornamental fences may be incorporated into gateway features and other areas of special emphasis. In instances where permanent enclosure or screening is necessary, walls complimentary with the adjacent architecture should be employed within the Town Center.

## Bus Stops



The design and location of bus stop facilities on public streets shall be in accord with the locations designated on the Town Center's public improvements plans. The location of any bus facilities, including parking, on private property shall be approved by the Belmont Town Center Design Review Committee.

## Benches Tables & Chairs



Benches, tables and chairs should be provided within the Town Center's public and quasi-public spaces. Color and style shall complement and be coordinated with the building and paving materials. Additional informal outdoor seating in plaza areas is encouraged as long as the seating location does not distract from the image of Belmont Town Center. Benches and other related furnishings shall be located outside of the public right of way unless approved by the Virginia Department of Transportation and shall be maintained by the Town Center Property Owners' Association.

## Bicycle Racks



Bicycle racks should be provided inside parking garages and within the Town Center's waterfront plaza and other commercial areas. Where bike racks are to be installed in residential areas, they should be sited to minimize their appearance from the public streetscape.

Racks will be located outside of the public right of way unless approved by the Virginia Department of Transportation and shall be maintained by the Belmont Town Center Property Owners' Association. Approved materials are metal and precast concrete.

## Trash Receptacles



Trash receptacles will be provided in public plaza areas and other sections of the Town Center where people will congregate. Approved materials are stone, precast concrete, and metal. To the extent feasible, trash receptacle colors and materials should compliment and be coordinated with adjacent buildings and other site furnishings.



Trash receptacles will be located outside of the public right of way unless otherwise approved by the Virginia Department of Transportation and will be maintained by the Belmont Town Center Property Owners' Association.

**Vending  
Machines**

Large outdoor vending machines, such as food and drink machines and mail drop boxes, are prohibited outside of buildings and parking structures without permission from the Belmont Town Center Design Review Committee. Newspaper vending machines are permitted at locations solely approved by the DRC. Vending machines shall be located outside of the public right of way unless approved by the Virginia Department of Transportation and shall be maintained by the Town Center Property Owners' Association.

**Lot  
Landscaping  
Guidelines**

The proposed lots in Marina Overlook, Taylor Point and Belmont Landing will use the applicable Prince William County Design & Construction Standards Manual (DCSM) lot landscaping guidelines (Table 8-6) shown below. Homeowner modifications throughout Belmont Bay will follow the current process and guidance from the Landscape Materials section.

Excerpt:

TABLE 8-6 ON-SITE RESIDENTIAL PLANTING REQUIREMENTS		
Townhouses, single-family attached, two-family	1 LD or MD per end unit and 1 SD or CD per unit	If space does not permit, the LD and MD required per end unit may be located in common open space for the site. Existing shade trees exceeding two-inch caliper, located anywhere in the open space area, may be counted on a one-to-one basis towards fulfilling the requirement for large and medium deciduous trees on that site.
*LD = Large Deciduous Tree; MD = Medium Deciduous Tree; SD = Small Deciduous Tree; CD = Compact Deciduous Tree; AT = Trees from any category of the Plant Selection and Cover Guide.		

## TOWN CENTER SIGNAGE

The Signage Guidelines for the Belmont Town Center incorporate measures which have been organized and designed to achieve a coherent image within the community. Although the signs will physically occupy little space, their visual impact on the overall image of the Town Center will be very important. For this reason, the signage regulations have been structured to provide a consistent image along the public streets, while providing more flexibility with building signage and within the pedestrian areas accessing commercial areas.

All signage concepts and designs shall be submitted to the Design Review Committee for approval prior to installation. All signs must conform to the signing provisions of the Prince William County Zoning Ordinance in addition to the criteria in this section. The signage system has been divided into the following signage categories -- Free Standing Building Identity, Building Mounted Signs, and Small Regulatory Signs.

### **Free Standing Building Identity**

Because of frequency and exposure, this sign type is the most important of all recommended signage within the Town Center. While one free standing building identity sign is permitted for each detached commercial building, discretion should be employed in the use of any free standing signs within the retail and festival areas of the Town Center. Free standing signs are acceptable for uses such as those planned in Blocks A, B, C, L and M. A standard, two-faced sign is recommended due to ease of visibility. This standard should be used as the only free standing building identity sign. Although the sign housing will be standardized, the graphic parameters will be designed to permit maximum design flexibility on the sign faces by the individual user. The services of a graphic designer are strongly recommended in laying out the sign face. All signs are located at approximately the same locations (to be set by developer) relative to the entrance drives. This consistent sign placement in conjunction with the standard sign housing creates a subtle but effective unified image of the entire site.

One tenant directory sign may be used per building. This sign is to incorporate the standard type sign housing at the prescribed location. It should be designed from the same materials as the building identity sign to ensure compatibility. These signs shall be limited to identifying the name of the building and/or the individual enterprises located therein, trademark or identifying symbol or any combination thereof. These signs are not intended as vehicles for advertising.

No other free standing signs, other than directional signs, will be permitted.

### **Building Mounted Signs**

In addition to the single free standing signs, building mounted signs will be permitted. Fun-

damental to Design Review Committee approval is that the building mounted signs are compatible with the architectural design of the building. In general, building mounted signs should not be visible from the river or adjacent properties.



Building mounted signs may be located on the surface of building facades in accordance with the following specific guidelines:

- *Signs may be located in the horizontal base band of the building. This area is defined as extending from the ground level to the floor slab of the second level of the building, or an area not exceeding twenty (20) feet upward from the ground level, whichever is less. Only the building facade containing the major entrance to the building may be used for signing in the base band.*
- *Signs may be located in the top horizontal band of a building. This area is defined as the top horizontal band containing the uppermost two occupied levels of the building. In the top band, unlike the base band, signs may be located on any of the building's facades, as long as the total permitted signage area is not exceeded. Only one (1) sign per facade is permitted in the top band.*
- *No signs shall be located on a penthouse, structural extension, or other architectural accessory and/or decorative building feature.*
- *No part of any sign shall extend above or beyond the perimeter of the building parapet or roof.*
- *Signs may be located on the vertical face of a marquee, but no part of the sign shall extend above or below the vertical face. The bottom of a marquee sign shall be no less than ten (10) feet above a walkway or grade, at any point.*
- *The total maximum number of building mounted signs per building is four (4).*



The total sign area for all building mounted signs cannot exceed one and one half (1-1/2) square feet of sign area for each of the first one hundred (100) linear feet of building frontage plus one square foot of sign area for each linear feet of building over one hundred (100) feet of frontage. The "building frontage" is the facade which is architecturally designed as the front of the building and which contains the main entrance for use by the general public. However, no one sign shall have a sign area in excess of thirty-six (36) square feet in the building base band and eighty (80) square feet in the building's top band.

Except as qualified below, the area of a sign shall be computed as the entire area within a single continuous rectilinear perimeter of not more than eight (8) straight lines enclosing the extreme limits of writing, representation, emblems, or a figure of similar character together with all material, color, or lighting forming an integral part of the display or used to differen-

tiate the sign from the background against which it is placed.

The area of building-mounted signs composed of individual letters and/or symbols shall be computed in accordance with one of the following methods:

- *If the space between the proposed individual letters and/or symbols is less in dimension than the width of the largest unit, the computation shall be made in accordance with a strict application of the provisions above.*
- *If the space between the proposed individual letters and/or symbols is greater than the width of the largest unit, the computation may be based on the total area within rectangular enclosures surrounding each individual unit.*
- *Maximum lettering and image size is 24" on the building's base band. In the building's top band the maximum image or lettering height shall be no larger than 1/2" of the letter or logo height per one foot of building height measured from final grade to roof top.*

Only the following types of building-mounted signs will be permitted:

### **Illuminated Signs**

Illuminated signs of the following characteristics are permitted:

- *Individual dimensional word or metal back-lit ("halo-effect") letters. The illumination behind such letters should be white neon unless approved by the DRC.*
- *Internally-illuminated channel letters with opaque metal sides and white translucent acrylic faces.*

### **Non-Illuminated Signs**

Non-illuminated signs of the following characteristics are permitted:

- *Individual dimensional letters or gold or silver leafed letters applied directly to glass or other signing surfaces with other colors to be approved by the DRC. Painted wood or painted metal is encouraged.*
- *Letters formed from thin metal plate or similar materials and pin-mounted at a distance approximately 1 inch from the signing surface. The face of such signs should not extend more than three inches from the signing surface.*
- *Boxed or cabinet-type signs will be permitted on the buildings base band only. This sign type must be totally recessed and the sign background must be opaque and darker than the image.*

- *The following types of illumination are prohibited: (1) Any arrangement of exposed tubing or a series of lights in rows, strings, patterns affixed to any portion of a building or structure, (2) Any sign displaying flashing or intermittent lights, or lights of changing degrees of intensity of color or moving copy (exclusive of temporary Christmas and seasonal holiday lighting).*
- *Signs shall be limited to identifying the name of the building and/or the individual enterprises located therein, trademark or identifying symbol or any combination thereof.*
- *Signs in the top horizontal band shall be limited to the logo, logo type, or initials of the enterprise.*

### **Small Regulatory Signs**

Small regulatory signs shall incorporate common design themes and complementary textures and colors throughout the Town Center. These signs are defined as the following:

- *Small signs displayed for the direction or convenience of the public, such as signs which direct traffic or identify the location of freight entrances or parking areas. These signs should not exceed two (2) square feet in area.*
- *Signs warning the public against trespassing, swimming, or the like. These signs should not exceed four (4) square feet in area and should be posted at eye level.*
- *Small signs, above grade, which identify parking for the handicapped as required by the Prince William County Zoning Ordinance. This sign type should not exceed one and one half (1 1/2) square feet in area.*



## UTILITY SERVICES

### **Water & Sewer**

Service mains shall be constructed by the developer within public street rights of way and other utility easements. These will be sized in accord with approved final site plans. These service mains may be dedicated to Prince William County for maintenance. Individual block developers will be responsible to connect service laterals into these systems.

### **Electricity & Telephone**

Service easements have been located between the streetscape easements and the individual building sites. Services shall be located in these “utility corridors” to serve all buildings within the Town Center. In some instances, these utilities will have to be located within street rights of way. All service connections shall be located underground and away from existing and proposed trees and special features. Exterior electric service conduits and connections shall be provided along designated streets to provided accessible receptacles for use during public events and festivals.

### **Transformers**

Transformers shall be adequately enclosed by masonry walls, landscaping, or other materials that are complementary with the adjacent building, or placed in underground vaults. The placement, design, and construction of all transformers will comply with all appropriate Prince William County regulations. Furthermore, transformers shall not be placed in highly visible locations.

### **Ground Antennae**

Ground mounted antennae shall require approval of the Belmont Town Center Design Review Committee and must conform to Prince William County standards.

### **Solid Waste**

Waste storage and pick-up facilities for commercial sites should be located in areas with minimum visibility from roadways and pedestrian circulation, preferably within a building. When located on the building exterior, it shall be enclosed by masonry walls or other materials complementary with the adjacent building. The waste storage and pick-up area should be located with other service and loading facilities, preferably within alleys or contiguous to Town Center minor streets. Other rules may be promulgated for residential sites.

## CONSTRUCTION PROCEDURES

Belmont Town Center will be developed over an extended period of time, and, from time to time, new construction will be underway adjacent to completed and occupied buildings. This section establishes minimum standards to limit the negative impact of new construction. Some of the standards may be in addition to those required by Prince William County. A demolition and phasing plan shall be prepared and approved by the Design Review Committee for each individual land use and development activity.

### Circulation

Safe, clean vehicular, and unobstructed pedestrian access should be maintained in the vicinity of the site. Protected and covered temporary walkways shall be constructed in high density construction areas. All construction parking and access shall be physically separated from permanent parking, employing temporary barriers, etc. Construction parking in adjacent parking lots or decks without permission of the Design Review Committee shall be prohibited. Where granted, construction parking shall be maintained in a neat, orderly and dust-free condition. Development should take place on both sides of the street so that streetscapes are complete.

### Equipment Access

A maximum of two construction access points for any parcel shall be permitted with each site/subdivision plan approved. Construction traffic should be minimized during peak traffic hours.

### Construction Fencing

Orange safety fencing or other construction fencing required by the County's Design and Construction Standards Manual and Zoning & Subdivision Ordinances. These fences shall be removed at completion of construction.

### Erosion Control & BMPs

Methods of controlling erosion and sedimentation shall be required during construction in compliance with applicable State and County laws and regulations. The controls will be planned as an integral part of the construction operation and will be included as part of the project's contract documents.

Both topsoil and fill material stockpiled on the site shall, within fourteen (14) days of disturbance, be seeded, mulched, or covered and appropriately graded to avoid erosion and blowing. Stockpiles shall be maintained and kept weed-free and dust shall be controlled.

The following precautions should be taken during construction to minimize the causes of erosion and siltation:

- *Control dust on a daily basis.*
- *To ensure the effectiveness of silt basins, they shall be installed at locations to intercept water before it enters catch basins or outfall structures. Silt basin cleaning may be required after each substantial rainfall.*
- *Temporary berms, swales, settling ponds, and other control mechanisms shall be removed and the underlying area shall be reshaped and planted to blend with the surrounding environment when the project has been completed.*

Belmont Bay has constructed many BMP facilities and related improvements in conjunction with its infrastructure development program. Individual land bays are served by and benefit from the existing BMPs. It shall be the responsibility of each site or block developer to protect existing BMPs during construction and to ensure that the BMP design capacity is maintained. Upon completion of construction, the developer shall inspect, clean, rehabilitate, and otherwise reestablish the BMPs to their original design capacity.

Where permanent erosion and sediment control measures and BMPs are not otherwise maintained by Prince William County, the maintenance shall be the responsibility of the Belmont Town Center Property Owners' Association.

## **Construction Debris**

Construction debris should be concealed during construction by placing it in a visually screened location. If a debris pit is used during construction, protective fencing will be required. Open burning of debris shall not be permitted. Central trash collection areas shall be emptied on a regular basis. Streets shall be washed and swept regularly to prevent mud or dust from spreading to passing vehicles or adjacent paved areas. No construction parking shall be permitted on public streets.

Paper and other debris shall be picked up and removed on a frequent basis. After construction is completed, temporary barriers, surplus materials, and all trash, debris, and rubbish shall be removed from the site. All backfill shall be cleared of building material, stone, and rubbish.

**Existing  
Vegetation**

Protective tree barriers (snow fencing) for existing trees shall be installed. Barriers shall be placed a minimum of ten (10) feet from the tree trunk, or at the drip line, whichever is greater. No trespassing shall be permitted within the tree barrier area by equipment, materials, or temporary structures. Provide drainage for vegetation masses at the base of fill slopes.

**Temporary  
Structures**

Temporary structures, portable offices, and other related facilities shall be painted in a single, neutral color (i.e. white, light gray, beige), maintained in good repair, and arranged in a compact and organized manner on the construction site and shall not be allowed for more than twenty-four (24) months. These facilities shall be situated so as to minimize unsightly appearance when seen from the road or adjacent properties. All temporary structures and portable facilities shall be removed within sixty (60) days from issuance of occupancy permit. As needed, the contractor shall provide shoring along adjacent driveway structures or landscaped areas.

**Toxic  
Materials**

Do not dispose of any toxic materials within the Belmont Town Center site. No open burning shall be permitted. All applicable County, state and federal regulations shall be satisfied.

**Equipment  
Storage**

The area designated for storage of equipment and materials shall be visually minimized from the streets and adjacent properties. Screened, off-site storage is encouraged.

**Temporary  
Utilities**

All temporary utilities on the construction site should be arranged in an unobtrusive alignment. Distribution to the various areas of construction shall be from an approved on-site location.

**Temporary  
Signs**

One (1) sign is permitted per site. The sign must be free standing and limited to an area of thirty-two (32) square feet and a maximum height of eight (8) feet. All fonts used must be white or light in color on a darker background. It should be limited to identifying the name of the proposed building, the character of the building(s), enterprise(s), or the purpose for which the building(s) is intended. In a subordinate manner, the sign may identify the architects, engineers, contractors, realtors, and other individuals or firms involved with the construction but shall not include any advertisement of any project. All temporary construction signs must be removed within fourteen (14) days following completion of construction.



## **SECTION IV: ARCHITECTURAL AND DEVELOPMENT CODE**

As indicated on the Schematic Land Use Plan, Belmont Town Center incorporates wide diversity into the land use orientations within the 96-acre “new town.” The harborfront commercial and cultural buildings are planned to be the dominant focus of the Town Center, and these should be flanked by other residential, office, hospitality, and tourism destination uses.

The various building configurations should be integrated into this mixed use development by focusing on the creation of a complementary, yet dynamic, set of architectural and streetscape interrelationships. However, it is recognized that, because of the diversity of uses and their density, scale, and function, the individual buildings cannot all come in the same “wrapper.” On the other hand, each building design should fulfill the Town Center streetscape objectives while attempting to reflect the rich historical character, traditional architectural elements, and environmental character of other towns in the County and state.

### **Guiding Principles**



In fulfillment of the precepts of the County’s Town Center goals, the vision for the architecture and urban design components for the Town Center should embody the following:

- *The Town Center should have the “look and feel” of a traditional Virginia town, with streetscape designs seeking to emulate the historic and cultural elements found in nearby communities such as Occoquan, Manassas and Alexandria.*
- *The esthetic ambience of the Town Center should evolve from architectural styles, building orientations, building materials, graphic identity systems, landscaping, and a common set of site and building elements. Federal, Charleston, and Georgian interpretative stylings and vernacular designs embracing colonial or revival elements should be employed to emulate traditional Virginia architecture. Designs may include Portofino elements per the Centerbrook illustration.*
- *Buildings should be developed with entrances and frontage on a grid of traditional streets that “bind” the town together.*
- *The commercial and residential buildings which face the harbor should have a consistent “building wall” to better define the space and create a “great outdoor room” or “festival area” between the east building facades and the marina.*
- *Buildings and other structures should be set at, or close to, the street edge, with a sidewalk and, in many instances, a planting zone between building walls and curbs.*

- *Heights of buildings should be predominantly established as low-to-mid rise, with buildings of three to five stories encircling the harbor.*
- *Buildings within the Town Center should be predominantly brick, complemented by a mixture of painted surfaces and stucco.*
- *To set the Town Center apart from other urban areas, architectural emphasis should be placed on “memorable” roofscapes, which may be composed of accent elements such as gables, rooftop cupolas, steeples, towers, and penthouses.*
- *Traditional urban architectural elements, such as awning, benches, custom cast streetlights, bollards, and signage, should be compatible and coordinated with architectural stylings.*
- *Buildings around the harbor should be designed to accommodate specialty retail and commercial activities, and they should be located at the ground level and clustered around the harborfront plaza.*
- *In key locations, residential uses, including apartments or condominiums above first floor commercial spaces, may be located in mixed use structures .*

Buildings throughout the Town Center should be of “traditional” architectural themes, but incorporating “state-of-the-art” building technologies with the finest design and support facilities available. To ensure these concepts as well as the functional and environmental quality of the project, the Design Review Committee intends to consistently apply the following design and development precepts throughout the life of the Town Center.

This section describes the architectural standards that contribute to the overall design excellence of the Town Center. The building form, silhouette, orientation, and material standards reinforce an urban design concept. The objective of architectural design criteria is to ensure a continuity and compatibility amongst all architectural elements and to maintain a high level of quality and design excellence. Further, it is a primary objective to provide for highly functional and flexible buildings which are sensitive to market considerations by being cost effective and responsive to varying tenant requirements.

## Architectural Design Fundamentals

Architects and urban designers of individual properties in the Town Center are encouraged to provide creative interpretations of traditional Virginia architecture. However, certain design and artistic fundamentals must be adhered to from project-to-project in order that the Town Center evolve with due respect for the dominant themes expressed herein. The fundamentals which designers must keep clearly in their sights include **scale, order, rhythm, balance, and proportion.**

### Scale



Scale is the ratio of the size of one object as compared to the size of another. Important scale relationships in architecture include:(1) the size of the parts of a building related to that of the whole building, (2) the size of the parts of a building compared to that of a human figure, and (3) the buildings size in relation to its setting.

The intended intimacy of the Town Center and its pedestrian scaled building treatments are to provide definition of space, interest, and articulation along the ground floor facades. The intended sense of enclosure at the harborfront is to be provided by a distinctive set of building facades framing the harbor and creating an great and inviting “urban room.” At the same time, the scale of architectural elements within the buildings’ forms, such as doors, windows, and columns, should be consistent among the facades at the harborfront. Through the use of scale, the group of buildings may be understood as a unified whole, while the hierarchy of its parts is made visually clear.

The perceived scale of a building, as seen from a specific vantage point, may be controlled through careful siting and architectural treatment at the ground floor level. By aligning building edges at the harborfront, a new structure’s visual impact is enhanced. The scale and visual impact of a building may also be controlled by breaking larger volumes into smaller components and architectural detailing.

### Order



Visual order is achieved through a consistency of balance, rhythm, and proportion among architectural components. It enables one to visually understand the relationships between the parts of a building as well as the relationships between distinct buildings as well. Within the Town Center, this is an essential concept for architects who will be challenged to create and maintain “order” among a wide range of buildings with varying uses, orientations, scale, densities, and massing.

The striking symmetry of traditional Virginia architectural style creates a strong sense of order. The hierarchy of scale between the central block and its dependencies, as well as the axial relationship between the main entry reinforce that order. The consistency of the windows’ and doors’ head and sill heights contributes to the composition’s overall unity.

## Balance

Balance is achieved when the desired focal point of a composition is apparent. Balance is readily achieved in a symmetrical building when the focal point is located at its center. Where a building composition is asymmetrical, balance may be achieved through the use of rhythm and repetition, often combined with localized symmetry or a discontinuity in the typical patterns at the focal point itself.

## Rhythm



Rhythm is fundamental to the architectural integrity of the Town Center. At its essence, rhythm is the consistent repetition of building forms or architectural components. The orderly repetition of similar architectural elements such as columns, doors, and windows contributes to the balance and order perceived in a building or group of buildings. Once a rhythmic pattern has been established, an interruption in that pattern becomes a focal point.

Within the Town Center, rhythmic patterns are established by the regular spacing of similarly sized openings within individual buildings and blocks of buildings. Rhythms established at the ground floor level are often represented the upper levels. Variations in rhythm are established to provide acute visual interest in specific instances such as in rooflines visible from the river and the harborfront.

## Proportion



When specific scale ratios between associated architectural components are held to constant values, a proportional system results. In essence, the “proportional system” represents the means of quantification of “what feels good” about a given building, group of buildings or streetscape. Proportion, sadly, is missing from most of Northern Virginia suburban commercial and residential areas. Architects for the Town Center will be challenged by the DRC to ensure that proportional systems for any given building are fully integrated into the overall streetscape. With such a system, the visual relationships between the components of the Town Center will be better understood.

Proportional relationships demonstrated in building elevations will include consistent window width-to-height ratios, consistent roof slopes at related roof forms, and uniformity in the percentage of void (window and door) area to overall area of the building elevations. Scale relationships between the Town Center blocks and their adjacent dependencies also support the established proportional system.

## **Regulations**

The two major areas of architectural design and urban regulations are prescribed in the following sections:

- *Town Center Streetscape, Block and Building Orientations*
- *Town Center Architectural Design and Materials*

## **STREETSCAPE, BLOCK AND BUILDING ORIENTATIONS**

Buildings shall be organized by general use groups. The planned uses are consistent with the rezoning and use group designations.



Marina Overlook: Town Center Mixed Use and Residential

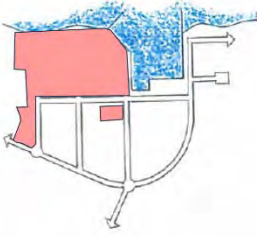
Taylor Point and Belmont Landing: Age Restricted Residential

As depicted on the Schematic Land Use Plan, each use group will have a unique mix, density, and orientation of uses, and architectural features. This section provides the guiding principles for each use group as well as the general architectural guidelines. The development code provides a matrix tabulation of the land use performance requirements -- bulk, density, setback, height, coverage and parking standards for block development, while the ensuing text provides block-by-block architectural and building guidelines for the designated Land Bay use groups. With these standards serving as a point of departure for the architectural review process, the Design Review Committee shall be responsible for final decisions related to architectural design.

## DEVELOPMENT CODE

PROPOSED LAND BAY DESIGNATION	FORMER LAND BAY DESIGNATION (REZ #99-0022)	LAND BAY SIZE (ACRES)	ZONING DESIGNATIONS	PROJECTED LAND USE <sup>1</sup>	RESIDENTIAL DENSITY BY USE		RESIDENTIAL DENSITY BY LAND BAY		MAXIMUM NON-RESIDENTIAL (Gross Floor Area or Slips)	YARDS (MINIMUM FRONT LOAD TOWNHOME)			YARDS (MINIMUM REAR LOAD TOWNHOME)			YARDS (MAXIMUM)			LOT WIDTH		LOT DEPTH	BUILDING HEIGHT		LOT COVERAGE <sup>2</sup>	PARKING REQUIREMENTS	BUFFER YARD
					MIN DUs/Acre	MAX DUs/Acre	MAX DUs/Acre	MAX DUs		FRONT	SIDE	REAR	FRONT	SIDE	REAR	FRONT	SIDE	REAR	MIN	MAX	MIN	MIN	MAX	MAX	MIN, OFF-STREET	
MARINA OVERLOOK	PART LAND BAY 6 PART LAND BAY 14	16.22	MDR	SINGLE FAMILY ATTACHED	4	6	6	74	20,200 SF*, 163 SLIPS + 17 JET SKI TENDERS	18'	4'	6'	8'	4'	4'	30'	20'	25'	20'	35'	50'	20'	50'	N/A	PER DCSSM	0'
			B-1	RESTAURANTS, RETAIL, MARINA, BOAT SALES, PARKING, MIXED USE, COMMUNITY RECREATION AND/OR CIVIC FACILITIES, PUBLIC USES AND FACILITIES	N/A	N/A				N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	10'	45'	95%	PER DCSSM	
BELMONT LANDING	PART LAND BAY 17 PART LAND BAY 18	21.69	HDR	SINGLE-FAMILY ATTACHED ACTIVE ADULT	6	16	16	175	N/A	18'	4'	11'	8'	4'	16'	30'	20'	25'	20'	35'	50'	20'	50'	N/A	PER DCSSM	SEE OPEN SPACE & BUFFER PLAN
TAYLOR POINT	PART LAND BAY 7	15.23	HDR	SINGLE-FAMILY ATTACHED ACTIVE ADULT	6	16	16	96	N/A	18'	4'	8'	7'	4'	4'	30'	20'	25'	20'	35'	50'	20'	50'	N/A	PER DCSSM	SEE OPEN SPACE & BUFFER PLAN
OPEN SPACE 1	LAND BAY 2 LAND BAY 20 LAND BAY 21	37.17	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
OPEN SPACE 2	PART LAND BAY 12 PART LAND BAY 15	5.44	OS	OPEN SPACE, PUBLIC USES, COMMUNITY RECREATION, AND/OR CIVIC FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
OPEN SPACE 3	PART LAND BAY 13 PART LAND BAY 15 LAND BAY 16	36.29	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
OPEN SPACE 4	PART LAND BAY 17	24.32	OS	OPEN SPACE, PUBLIC USES AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
OPEN SPACE 5	LAND BAY 2 LAND BAY 3	1.35	OS	OPEN SPACE, PUBLIC USES, COMMUNITY RECREATION, AND/OR CIVIC FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
OPEN SPACE 6	LAND BAY 22	0.05	OS	OPEN SPACE, PUBLIC USES, AND FACILITIES	N/A	N/A	N/A	N/A	N/A	35'	35'	35'	35'	35'	35'	N/A	N/A	N/A	N/A	N/A	N/A	10'	30'	15%	PER DCSSM	0'
		159.76	APPLICATION PROPERTY TOTAL ACRES						348	MAXIMUM DWELLING UNITS FOR THE APPLICATION PROPERTY						* MAY BE INCREASED WITH ADEQUATE PARKING PROVIDED IN ACCORDANCE WITH THE DCSSM										
								20,200 SF	MAXIMUM NON-RESIDENTIAL GFA FOR THE APPLICATION PROPERTY																	
								163	MAXIMUM MARINA SLIPS																	

## Marina Overlook Use Group



## Marina Overlook

Marina Overlook's buildings and urban spaces will represent a core feature of the Town Center. The buildings facing the harbor and open spaces serve as major destination points for Belmont Bay residents and visitors. Similar to Block E, this block will have a strong visual and functional orientation to the harbor's pedestrian plaza, the marina, the river, and public streetscapes. Vernacular building forms, massing, and proportion should reflect the historic and cultural elements of the region. Marina Overlook includes Blocks B and C, along with portions of A and G.

Architectural elements within Marina Overlook which have frontage on the marina and the Harbor Street streetscape should be both diverse and authentic, employing the ideas of a variety of architects throughout the range of buildings to be constructed.

The siting of residential townhouses and other residences uses should establish a strong streetscape for the adjoining Town Center streets, with parking permitted both on-street and off street. The extension of the Town Center street network and sidewalks completes the interconnection of the Town Center to the southern portion of the trail system. The townhouses should be designed with a massing, orientation, and vernacular styling similar to those found in Old Town Alexandria and Georgetown. See Appendix A for proposed pocket park concepts that will be finalized in the site plan. See Appendix C for architectural elevations to be finalized for building permits.”

### PWC SUP/Zoning Code Classification

- *MDR*
- *B-1*

### Planned Uses

- *Mixed-Use Amenity Building*
- *Harbormaster Building/Commercial/Restaurant*
- *Retail*
- *Townhouse residential buildings*

### Guiding Principles for Building Architecture

- *Traditional Virginia architecture elements, with a recommended vernacular and interpretative Georgian, Charleston, or Federal emphasis in building styles and motifs*
- *Building facades should be varied and articulated for pedestrian visual interest*
- *Building skin should avoid exterior reflective materials and mirrored glass*
- *Building massing and landscaping should relate strongly to adjoining blocks*
- *Roofscapes should be delineated by gables, cupolas, steeples, or towers*
- *Roof slopes should be symmetrical with slopes between 6:12 and 8:12*
- *Arcades, porches, balconies, and bays, encouraged and of proper scale*
- *Windows should be vertically proportioned or square*
- *Flush mounted windows not encouraged*
- *Opening in gables should be centered*

- *Environmental sustainability principles should be incorporated into building design*
- *Exposed foundation walls along street frontages should be clad with brick or stone water tables*
- *Roof penetrations, such as vents and stacks, should be placed on rear roof slopes, where practicable*

### **Mix of Uses**

- *Retail*
- *Mixed Use*
- *Townhouses (condominium and/or fee-simple)*

### **Massing and Density**

- *Maximum density: see “Development Code” for SUP limitations*
- *No more than 8 townhouses within a single structural grouping*
- *Townhouse widths: range 20’ to 35’*
- *Fronts of townhouse buildings shall be minimum 3 stories (2 stories for villa style).*
- *Fencing allowed in rear and side yards only; limited to 8’ in height*

### **Setbacks**

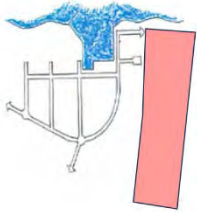
- *Minimum front yard setback: 18’ for residential and 0’ for non-residential*
- *Maximum residential front yard setback: 30’*
- *Maximum commercial setback: 0’*
- *Rear yard setback: 6’ for front garage and 4’ for rear garage homes*
- *10’ minimum separation between residential buildings*
- *Refer to “Development Code” for more details*

### **Heights**

- *20’ minimum, provided that single story structures shall be employed only as supporting elements which are integral to larger building masses.*
- *50’ maximum for townhouses*
- *Townhouses: 3-4 stories recommended, inclusive of first level garage*
- *45’ maximum height for mixed use and retail*
- *Mixed use and retail: 2-4 stories*

### **Parking Accommodations**

- *Surface and garage parking, with opportunities for shared parking*
- *On-street parking within public rights of way or private easements*
- *Detached garages optional at rear of building*



## Taylor Point and Belmont Landing

Taylor Point and Belmont Landing provide desirable locations for active adult living with easy access to the waterfront, trail system, Occoquan National Wildlife Refuge, and the Harborfront. Taylor Point and Belmont Landing includes Block M along with portions of the former golf course and Blocks J & K.

The siting of residential townhouses and other residences uses should establish a strong streetscape for the adjoining Town Center streets, with parking permitted both on-street and off street. The extension of the Town Center street network and sidewalks completes the interconnection of the Town Center to the southern portion of the trail system. See Appendix A for proposed pocket park concepts that will be finalized in the site plan. See Appendix C for architectural elevations to be finalized for building permits.

The townhouses should be designed with a massing, orientation, and vernacular styling similar to those found in Old Town Alexandria and Georgetown.

The Osprey's at Belmont Bay private event facility will be demolished, but may remain as an approved use until the neighborhood is constructed.

### PWC SUP/Zoning Code Classification

- *HDR*

### Planned Uses

- *Townhouse residential buildings*

### Guiding Principles for Building Architecture

- *Traditional Virginia architecture elements, with a recommended vernacular and interpretative Georgian, Charleston, or Federal emphasis in building styles and motifs*
- *Building facades should be varied and articulated for pedestrian visual interest*
- *Building skin should avoid exterior reflective materials and mirrored glass*
- *Building massing and landscaping should relate strongly to adjoining blocks*
- *Roofscapes should be delineated by gables, cupolas, steeples, or towers*
- *Roof slopes should be symmetrical with slopes between 6:12 and 8:12*
- *Arcades, porches, balconies, and bays, encouraged and of proper scale*
- *Windows should be vertically proportioned or square*
- *Flush mounted windows not encouraged*
- *Opening in gables should be centered*
- *Environmental sustainability principles should be incorporated into building design*
- *Exposed foundation walls along street frontages should be clad with brick or stone water tables*
- *Roof penetrations, such as vents and stacks, should be placed on rear roof slopes, where practicable*

### **Mix of Uses**

- *Not regulated*

### **Massing and Density**

- *Maximum density: see "Development Code" for SUP limitations*
- *No more than 8 townhouses within a single structural grouping*
- *Townhouse widths: range 20' to 35'*
- *Fronts of townhouse buildings shall be minimum 3 stories (2 stories for villa style).*
- *Fencing allowed in rear and side yards only; limited to 8' in height*

### **Setbacks**

- *Minimum front yard setback: 18'*
- *Maximum residential front yard setback: 30'*
- *Rear yard setback:*
  - Rear Garage Homes 18' in Belmont Landing & 4' in Taylor Point*
  - Front Garage Homes: 11' in Belmont Landing & 6' in Taylor Point*
- *10' minimum separation between residential buildings*
- *Refer to "Development Code" for more details*

### **Heights**

- *20' minimum, provided that single story structures shall be employed only as supporting elements which are integral to larger building masses.*
- *50' maximum for townhouses*
- *Townhouses: 3-4 stories recommended, inclusive of first level garage*

### **Parking Accommodations**

- *Garage parking*
- *On-street parking within public rights of way or private easements*
- *Detached garages optional at rear of building*

## ARCHITECTURAL DESIGN AND MATERIALS

The appearance of unified, harmonious building exteriors as recommended by this section will strongly portray the commitment to design excellence and building quality embodied throughout the Town Center.

### Building Materials



The exterior building skin should be composed of one dominant facing material and one or two additional materials as accents. Brick should be the preferred dominant material for all buildings, with the exception of commercial office or cultural buildings which may be of natural stone or pre-cast materials. Materials such as stone or glass would be acceptable for certain applications as a dominant material subject to review by the Design Review Committee. The dominant material shall comprise a minimum of 50% of any elevation. No building should have more than two facing materials in addition to glass. Material selections shall be submitted to the Design Review Committee along with plans and specifications. Sample building material panels shall be constructed for DRC site review purposes. The following materials are not recommended as dominant building materials:

- ceramic tile
- aluminum siding
- glass curtain walls

### Colors



One dominant color comprising over fifty (50) percent of the building elevation and two accent colors will be permitted. White (excluding marble and limestone), black and bright primary colors are not recommended in any material for the dominant material. Recommended dominant colors and tones are as follows:

- *Stone:* warm and cool tones
- *Glass:* warm and cool tones, semi-reflective glass, except for highly reflective, mirrored, bronze, and/or gold glass.
- *Precast:* warm and cool tones
- *Metal:* warm and cool, brushed and neutral tones
- *Wood (for residential only):* warm and cool, neutral tones

In all cases, flashy or showy colors should be avoided. The color of all proposed materials shall be indicated on each plan submission for review and approval. Buildings within each land bay should have compatible, coordinated color schemes.

### **Setbacks & Height Limits**

All structures shall be in conformance with the approved Development Code and special use permit. Internally, all office, parking, and hotel structures shall be set back in accordance with the approved Schematic Land Use Plan. No buildings should encroach or cause alteration of any landscape easement. The height limit for each building and parking structure shall be in conformance with the approved special use permit. Total square footage of development shall only be increased from the Schematic Land Use Plan if it is amended and approved by the Design Review Committee and County and provided that it does not reduce the potential of any other property without that owner's consent. Buildings approved under REZ2024-00040 and SUP2024-00036 shall not exceed 50 feet in height.

### **Roof Treatments**



Mechanical equipment and roof color shall be compatible and well-integrated with the building design. Roof mounted equipment is discouraged, where feasible. If necessary, it shall be masked behind discreet screening materials. All roof-mounted mechanical equipment should be screened and painted to blend the approved roofing color. Equipment should be arranged in an orderly, clustered manner, grouped behind one parapet screen. At a minimum, a single continuous parapet wall without louvers or penetrations shall screen, or preferably hide entirely, all roof mechanical equipment from ground views. Antennae, disks, solar panels, etc., if required by building occupants, shall be grouped in an orderly manner behind the mechanical screen referenced above unless specifically approved by the Design Review Committee. Roof top flag poles shall be permitted subject to the approval of the Design Review Committee.

### **Lighting**

Except for hotels and the restaurants, flood lighting of building exteriors shall be prohibited without the approval of the Design Review Committee. An illumination impact plan may be required by the DRC prior to building approval.

### **Ground Floor Transitions**

Where appropriate, the design of buildings should provide a transition from the upper floors to the pedestrian level by the use of materials, building articulation, and level of detailing. When seen from the boulevards and streets, a transition zone should add interest at the vehicular and pedestrian scale, thereby reinforcing the urban street quality of the street grid system.

### **Site and Building Integration**

Belmont Town Center is a fully integrated multi-use development, with a strong emphasis placed on coordinated pedestrian linkages. Therefore, it is important that equal attention be paid to pedestrian and vehicular circulation throughout the project. An integrated multi-use development provides greater opportunities to share facilities and infrastructure systems, thereby creating greater convenience and economies for users.

### **Circulation**

The previous section describes the goals for ground level pedestrian and vehicular circulation. The Design Review Committee shall ensure that an integrated system of through-building passages, garage connections, pedestrian concourses, and covered walkways completes the overall Town Center circulation system.

### **Utilities**

Utility and service integration must be considered in the development of all blocks and parcels within subdivided blocks to allow the most effective connection to the infrastructure systems. These considerations should include the installation of sleeves for future extension of piping, shared, and oversized utility connections between buildings and shared entry courts or service areas.

## Accessibility

All requirements for barrier-free handicap access, including the guidelines contained in the Americans with Disabilities Act (ADA), shall be observed.

No building, improvement, structure, parking structure or lot, wall or fence, sign, utility or drainage system, exterior lighting, landscaping, or other site improvement of any kind should be started, erected, placed, removed, or altered on any building site until all plans and associated specifications have been formally submitted to and approved by the Design Review Committee.



**SECTION V:  
DESIGN REVIEW AND APPROVAL PROCESS**

**Plan  
Review**

Drawings and samples of building materials are to be submitted to the Design Review Committee in three phases: Conceptual Plan, Preliminary Development Plan, and Final Development Plan. Submissions to the Belmont Town Center Design Review Committee (DRC) are to be made in triplicate so that each member of the DRC may review plans independently.

**Time of  
Response**

The review of each submission by the DRC will generally be completed within five (5) working days from the date of each submission. Notification of the Committee's action for approval or disapproval will be provided in writing to the builder, developer or representative within an additional five (5) days. The subsequent phase of the process should not be undertaken until the submitted phase has been approved by the Committee or substantial consultation has taken place. Final Development Plan approval by the Committee is required prior to the undertaking of any site improvements, or site preparation work.

**Resubmittals**

In cases where a portion of a plan is not approved, resubmission of that portion is required. Any part of the submission which has been approved will not be required to be resubmitted. The DRC requires resubmission to correct the following deficiencies:

- *Failure to comply with the Belmont Town Center Schematic Land Use Plan*
- *Failure to provide complete information regarding architectural design and site development materials and details*
- *Insufficient design intent and quality of proposed landscaped areas*
- *Inappropriate exterior signage and lighting designs*
- *Incomplete compliance with any other requirement pursuant to the Design Guidelines and Development Code*
- *Inconsistency with any local, state, or federal regulation or prior approval*

## **Pre-Design Meeting**

Prior to submittal of a conceptual plan for review, there shall be a coordination meeting for the purpose of introducing the builder/developer's team members, including architect, to the DRC. At this meeting, the applicant shall present a proposed critical path for the project and apply it to the County's Development Review process and procedures and the Belmont Town Center Design Guidelines. This is in addition to schedules for the preparation of the project's own design documents, construction documents, and specifications.

## **Conceptual Plan**

Once the applicant (or block developer) has reviewed these Design Guidelines, the Applicant shall submit three (3) copies of the Concept Plan, containing the following information, to the DRC:

- *A site location plan at 1"=100' scale identifying the relationship to adjacent site features and other major physical characteristics of the Town Center. This drawing is to be based on the Town Center Schematic Land Use Plan, as approved by the County*
- *A conceptual site plan at 1"=30' scale identifying building and parking structures on the site. Vehicular circulation, parking lot layout, number of parking spaces, landscaping, building placement, conceptual grading, and pedestrian walkways should also be shown. This plan should also show schematic utility locations. Metric plans shall be provided upon request.*
- *Architectural floor plans and elevations at 1/16"=1'-0" scale of proposed building massing and elevation treatments*
  - *Lists and samples of materials proposed for exterior facades, with renderings*
- *An estimate of the gross square footage of all construction*
- *A statement that the proposed construction complies with the Belmont Town Center Concept Plan and all applicable regulations of Prince William County and these Design Guidelines*
- *In cases where the block is being subdivided into smaller parcels or units for purposes of ownership, a preliminary plat of the subdivision shall be included in the submission for approval by the Design Review Committee*

## **Preliminary Development Plan**

The block builder/developer shall submit three (3) copies of the Preliminary Development Plan, containing the following information, to the DRC:

- *Notice of Concept Plan Approval by the Design Review Committee*
- *A preliminary site plan at 1"=20' scale showing the building pad with all site improvements, grading, and landscaping. This shall include the relationship of the building(s) to parking lots and/or structures, service areas, and utilities. The Plan shall also show proposed storm drainage, location of any exterior lighting, and locations of all significant trees. Outdoor recreational facilities such as swimming pools and tennis courts shall also be included*
- *Schematic architectural drawings of the building(s) and parking structure, if any, at 1/16"=1'-0" or 1/8"=1'-0" scale, showing typical floor plans, entries, exterior elevations with proposed finishes and other features as required such as lobby and proposed structural grid, if appropriate to the structure*
- *Plans and elevations showing all proposed exterior signage locations, sizes, and materials*
- *A statement that the proposed construction complies with the Belmont Town Center Concept Plan and all applicable regulations of Prince William County and these Design Guidelines*
- *A revised estimate of the construction gross square footage*
- *A scale model showing massing and building-to-site relationships, upon request*
- *Parking impact analysis to ensure compatibility with Town Center requirements*

The block developer's submittal shall be reviewed by Prince William County in accord with established procedures. Upon receiving a complete submittal with responses to Staff's comments, a presentation by the applicant and its design team will be scheduled for the full Design Review Committee and shall take place within fourteen (14) days after receipt of the submittal to DRC, or as soon thereafter as the parties can arrange. This presentation is mandatory. Should the DRC request significant revisions pursuant to this document or other requirements, a second presentation may be required.

## **Final Development Plan**

Drawings for this submission should be at a level of detail and completion suitable to be included as part of the Construction Contract Documents for the project. Each sheet shall include a graphic scale and north arrow. The applicant (or block developer) shall submit three (3) copies of the Final Development Plan, containing the following information, to the DRC:

- *Notice of Preliminary Development Plan Approval by the DRC*
- *A complete site development plan at 1"=20' scale showing existing and proposed contours at two (2) foot intervals, including one hundred fifty (150) feet of overlap into adjacent land bays; first floor elevations for all main buildings; and floor elevations noted for all parking structure levels. Metric plans shall be submitted upon request.*
- *The exact locations of all buildings or footprints and other roofed structures, including parking structures, surface parking areas with individual parking spaces and access lanes shown, landscaped areas, external trash collection points, and ground mounted building equipment, building service and loading areas, water features, any proposed locations for sculpture or signs, and any other site development elements*
- *Public infrastructure and storm drainage plans and profiles, including an engineering report addressing design capacity and compatibility with existing systems*
- *A detailed site landscape architecture plan and a streetscape improvements plan, if required*
- *Best Management Practices plan and maintenance program, including a description of how existing BMPs will be protected and rehabilitated upon completion of construction*
- *Erosion and sediment control plan, including maintenance plan*
- *A conservation and environmental protection plan for Resource Protection Areas, wetlands, and designated tree preserve areas, as required*
- *Parking impact analysis to ensure compliance with Town Center requirements*
- *Details of all walkways, courtyards, and other exterior features, including sample materials to be used if appropriate*

- *Exterior lighting system, including locations, mounting heights, and actual manufacturer's catalog cuts of proposed fixtures*
- *Architectural drawings of all structures at 1/8" = 1'-0" or 1/4" = 1'-0" scale, showing all typical floor plans and elevations of the project, noting all materials*
- *A complete description of all exterior materials, including composition, surface textures and colors, and assemblage to one another; including the submission of actual samples of proposed exterior construction materials if requested by the Design Review Committee*
- *Scaled drawings of the entry lobby and elevator door materials for all commercial buildings*
- *A general construction demolition and construction phasing plan*
- *Proposed exterior signs, including location, size, elevations, face design, colors, illumination, materials and method of installation*
- *A colored rendering of the building facades and the site plan*
- *Scale drawing of construction sign showing materials, colors, and type face*
- *Early site grading plan or land disturbing plan, if required*
- *Written specifications relating to any of the above items. Calculations showing gross and rentable building footage, number of parking spaces provided (separate calculations for surface and structured parking), the ratio of parking spaces provided to gross square footage of building (in thousands) and the distribution of parking spaces by stall sizes*
- *A statement that the proposed construction complies with the Belmont Town Center Concept Plan and all applicable regulations from Prince William County and these Design Guidelines*

## **Other Submittals**

The design approvals by the Design Review Committee are independent of the normal review and approval process for Prince William County. Applicable codes, ordinances, and regulations must be followed to obtain the necessary permits prior to construction. It is recommended that conceptual design approval by the Design Review Committee be obtained before any submission to County, State, and Federal agencies.

**Pre-  
Construction  
Meeting**

Only upon final approval of those items that require approval shall the block developer be allowed to begin construction. Prior to the start of construction, the block developer shall schedule a Design Review Committee meeting with the selected contractor to discuss overall site construction, staging, phasing, safety, and environmental issues. The Contractor shall be prepared to discuss each of the requirements presented in these guidelines at the above-referenced Design Review Committee meeting.

The following diagram provides a relative tracking of the activities and responsibilities required by all parties involved with the approval processes for Prince William County and the Design Review Committee.

**DRC  
Membership**

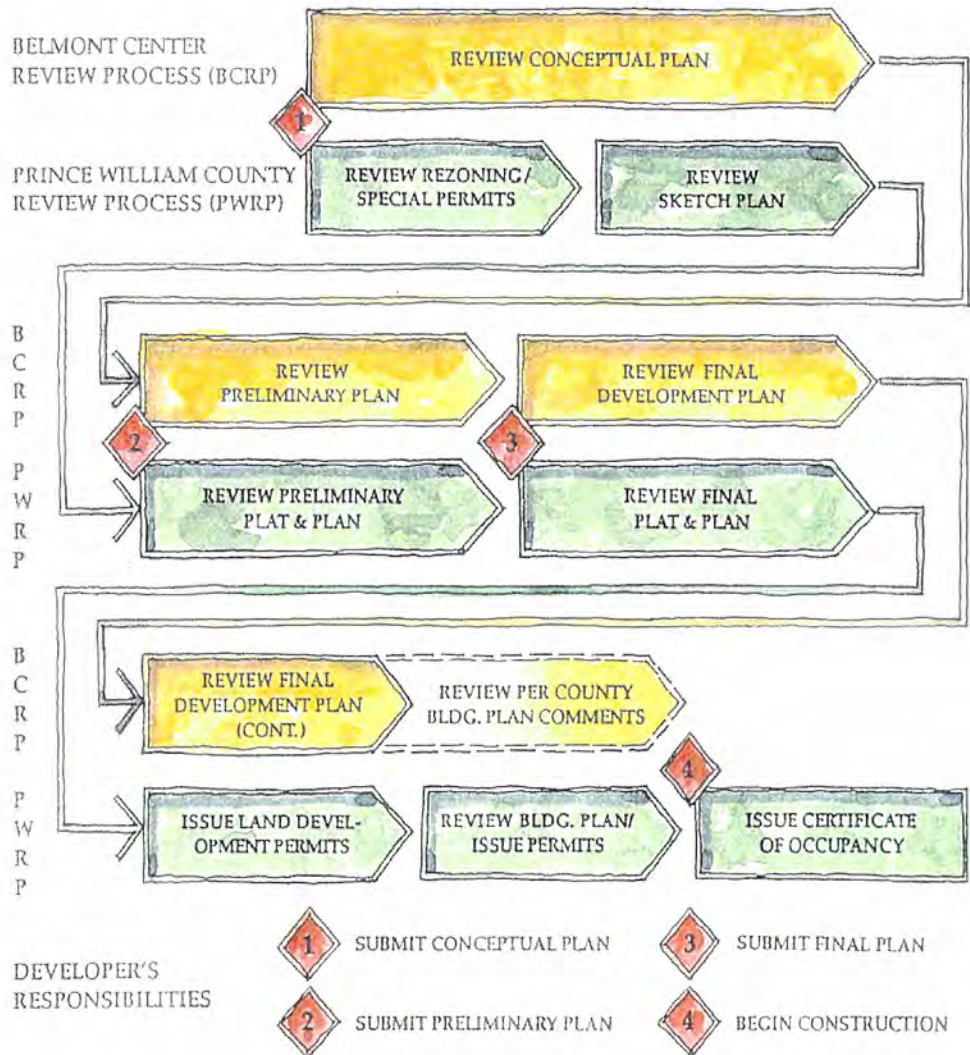
Membership in the Belmont Town Center Design Review Committee shall not be less than three members, with qualifications and terms to be established by and at the discretion of the owners of Belmont Bay, L.C., the current owner of the substantive portion of that property representing the Town Center.

Belmont Bay, L.C. shall appoint members of the DRC. Upon completion of the development of the Town Center, the obligations and rights created herein may, at the option of Belmont Bay, L.C. be assigned to the Architectural Review Board (which is created and formed in connection with the covenants, conditions and restrictions associated with the Belmont Town Center. All action of the DRC shall be controlled by a majority vote of the DRC.

**Amendments  
to Code &  
Guidelines**

Amendments, revisions, and waivers must be approved by a majority vote of the full membership of the DRC, with documentation provided to ensure that the design intent of the amendment can be clearly understood and implemented. This includes the preparation of architectural, landscape, and engineering standards and criteria, as appropriate, to be included in the revised document. No amendment, revision or waiver shall be approved which is not consistent with the conditions of zoning, proffers and the special use permit.

**DRC  
Flow Chart**

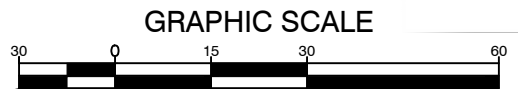


*Appendix A*

POCKET PARK CONCEPTS TO BE FINALIZED DURING SITE PLAN APPROVAL



- 1 SHADE STRUCTURE
- 2 BOCCE COURT
- 3 SIDEWALK
- 4 BENCH
- 5 SHADE TREE, TYP.
- 6 UNDERSTORY TREE, TYP.
- 7 SHRUB & HERBACEOUS PLANTINGS, TYP.
- 8 GRASS LAWN, TYP.



**BELMONT BAY AMENITY EXHIBIT**  
**BELMONT LANDING CENTRAL PARK**

DATE: OCTOBER 31, 2025



- 1 SIDEWALK
- 2 BENCH
- 3 SHADE TREE, TYP.
- 4 UNDERSTORY TREE, TYP.
- 5 SHRUB & HERBACEOUS PLANTINGS, TYP.
- 6 GRASS LAWN, TYP.



NORTH

GRAPHIC SCALE



**BELMONT BAY AMENITY EXHIBIT**  
**BELMONT LANDING PROMENADE**

DATE: OCTOBER 31, 2025



- 1 SIDEWALK
- 2 BENCH ON OVERLOOK
- 3 SHADE TREE, TYP.
- 4 UNDERSTORY TREE, TYP.
- 5 SHRUB & HERBACEOUS PLANTINGS, TYP.

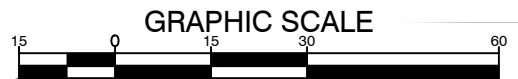


**BELMONT BAY AMENITY EXHIBIT**  
**BELMONT LANDING OVERLOOK**

DATE: OCTOBER 31, 2025



- 1 BENCH
- 2 EVERGREEN SCREEN
- 3 SHADE TREE, TYP.
- 4 UNDERSTORY TREE, TYP.
- 5 SIDEWALK
- 6 SHRUB & HERBACEOUS PLANTINGS, TYP.
- 7 GRASS LAWN, TYP.



**BELMONT BAY AMENITY EXHIBIT**  
**TAYLOR POINT CENTRAL PARK**

DATE: OCTOBER 31, 2025

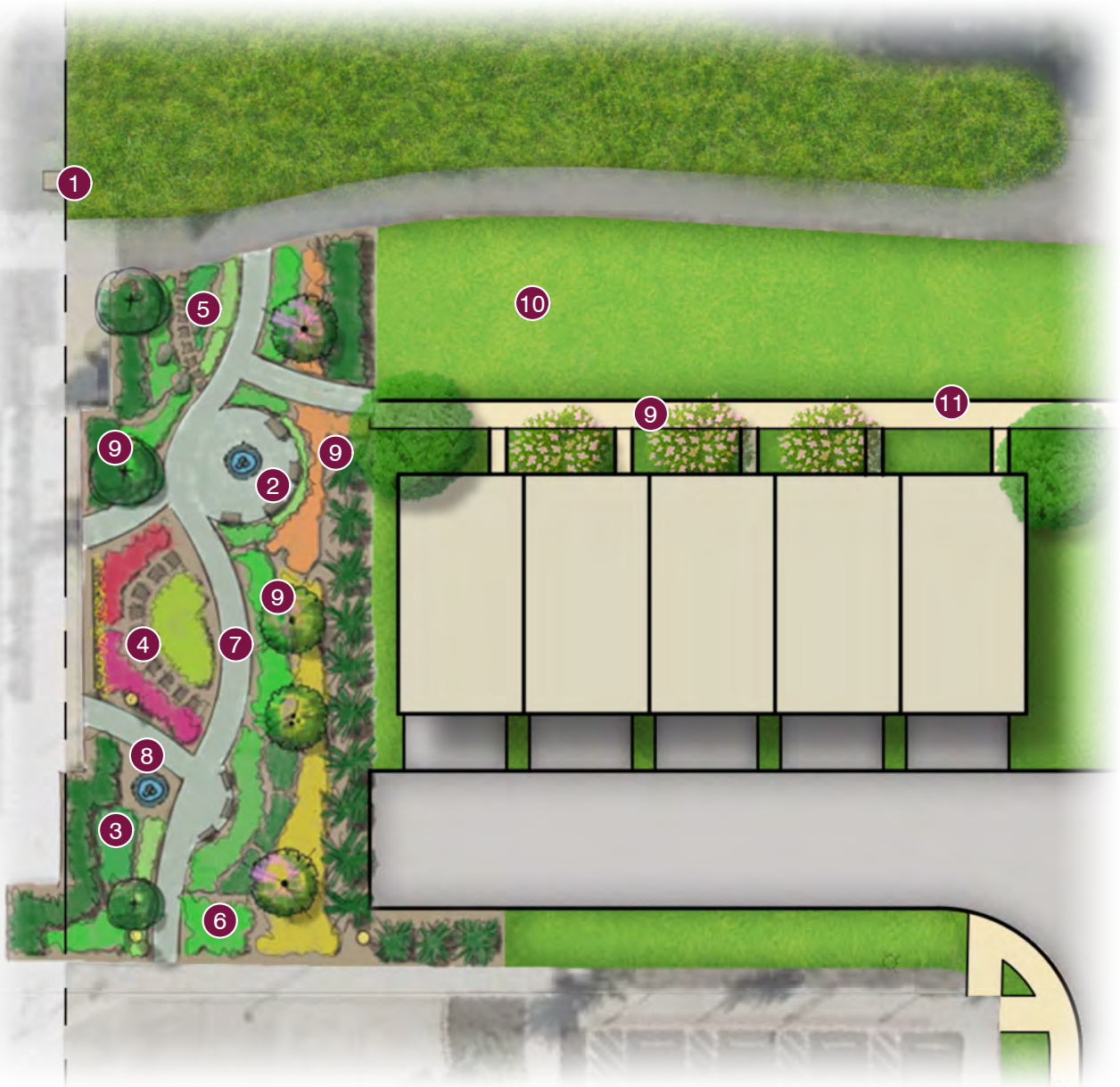


- ① EXISTING TREE
- ② SIDEWALK
- ③ SHADE TREE, TYP.
- ④ EVERGREEN, TYP.
- ⑤ UNDERSTORY TREE, TYP.
- ⑥ SHRUB & HERBACEOUS PLANTINGS, TYP.
- ⑦ GRASS LAWN, TYP.



**BELMONT BAY AMENITY EXHIBIT**  
**TAYLOR POINT RIVERFRONT PARK**

DATE: OCTOBER 31, 2025



NORTH

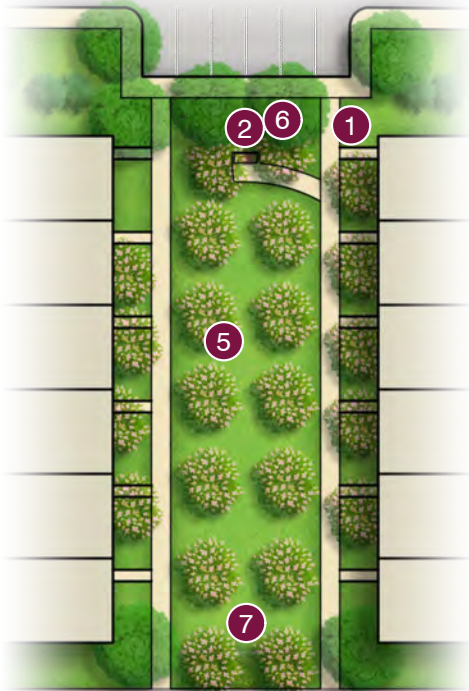
GRAPHIC SCALE



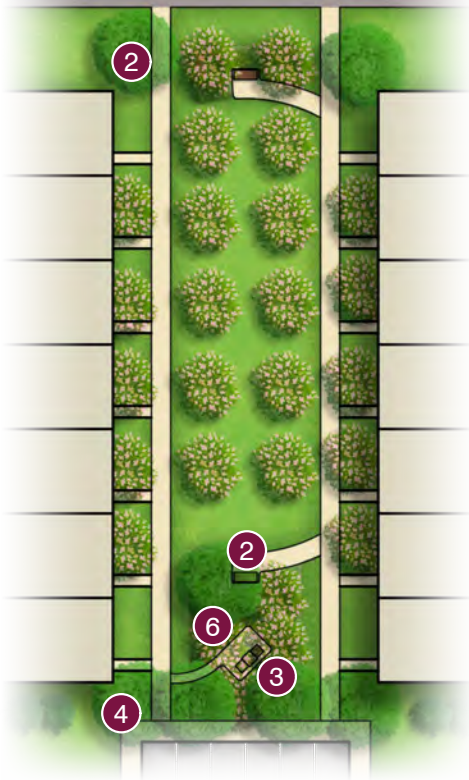
- |   |                                     |    |                             |
|---|-------------------------------------|----|-----------------------------|
| 1 | MANUFACTURED OSPREY NEST (BY OTHER) | 7  | DECOMPOSED GRANITE PATH     |
| 2 | SEMI-CIRCULAR AREA                  | 8  | STONE GARDEN FOUNTAIN, TYP. |
| 3 | SMALL RAIN GARDEN                   | 9  | TREE, TYP.                  |
| 4 | STEPPING STONES, TYP.               | 10 | GRASS LAWN, TYP.            |
| 5 | SEDUM AND ROCK GARDEN               | 11 | SIDEWALK, TYP.              |
| 6 | POLLINATOR GARDEN                   |    |                             |

**BELMONT BAY AMENITY EXHIBIT**  
**MARINA GMU CONNECTION**

DATE: OCTOBER 31, 2025



- 1 SIDEWALK
- 2 BENCH
- 3 CAFE TABLE
- 4 SHADE TREE, TYP.
- 5 UNDERSTORY TREE, TYP.
- 6 SHRUB & HERBACEOUS PLANTINGS, TYP.
- 7 GRASS LAWN, TYP.

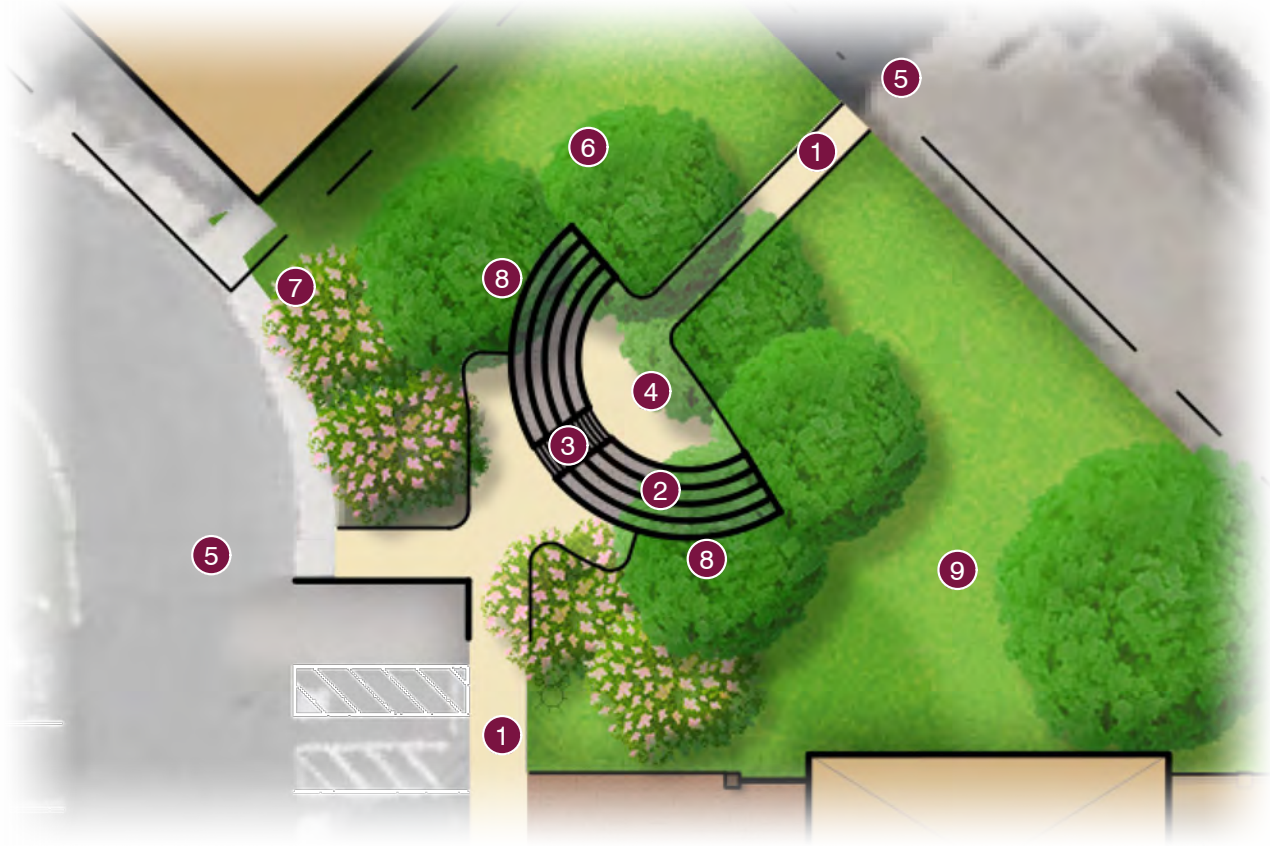


GRAPHIC SCALE



**BELMONT BAY AMENITY EXHIBIT**  
**MARINA CENTRAL PARK**

DATE: OCTOBER 31, 2025



- 1 SIDEWALK
- 2 AMPHITHEATER WALL
- 3 AMPHITHEATER STEPS
- 4 AMPHITHEATER PLATFORM
- 5 EXISTING TO REMAIN
- 6 SHADE TREE, TYP.
- 7 UNDERSTORY TREE, TYP.
- 8 EVERGREEN SCREEN
- 9 GRASS LAWN, TYP.



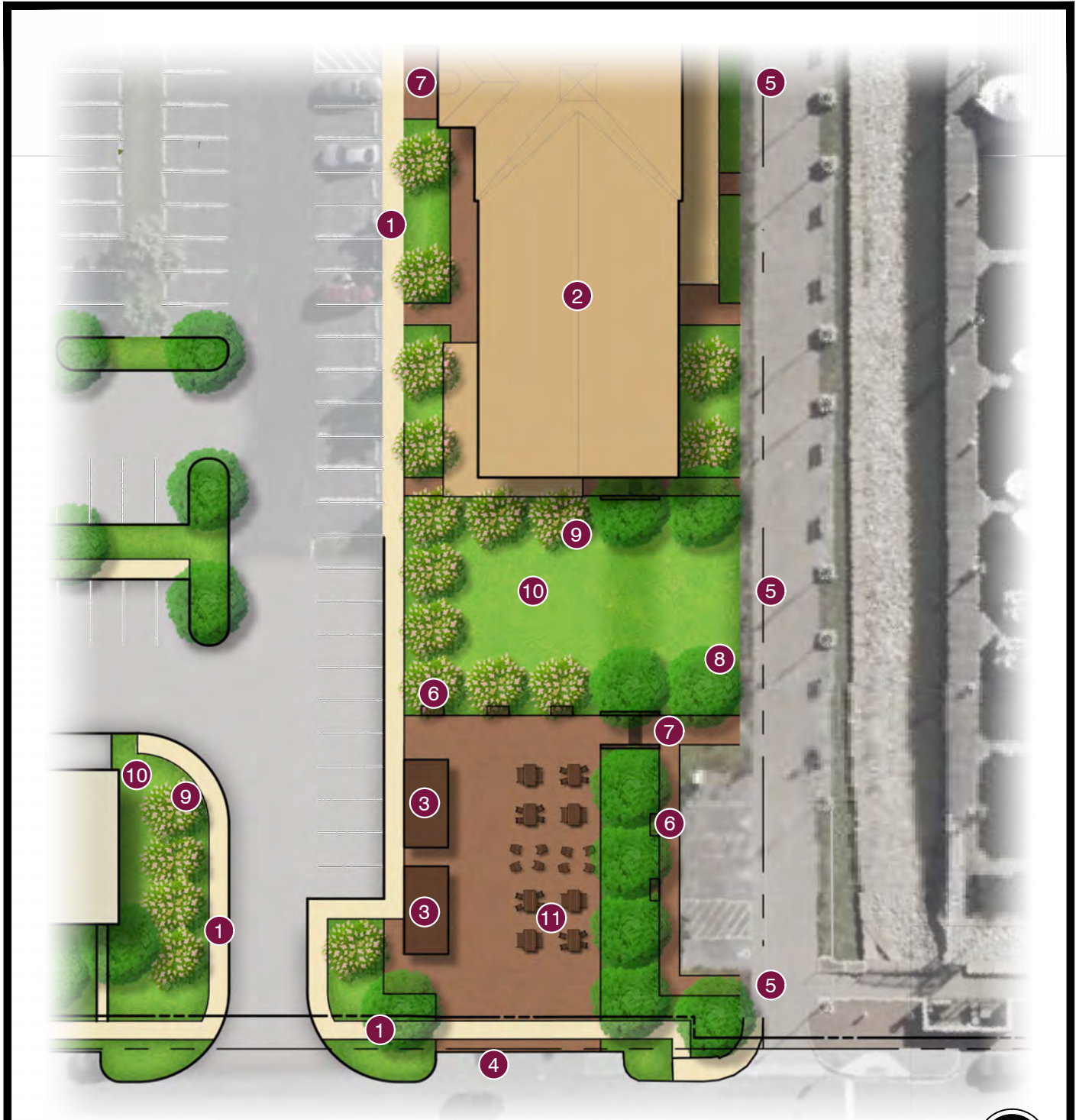
NORTH

GRAPHIC SCALE

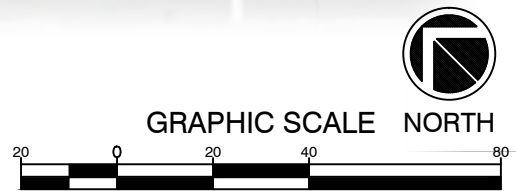


**BELMONT BAY AMENITY EXHIBIT**  
**MARINA AMPHITHEATER**

DATE: OCTOBER 31, 2025

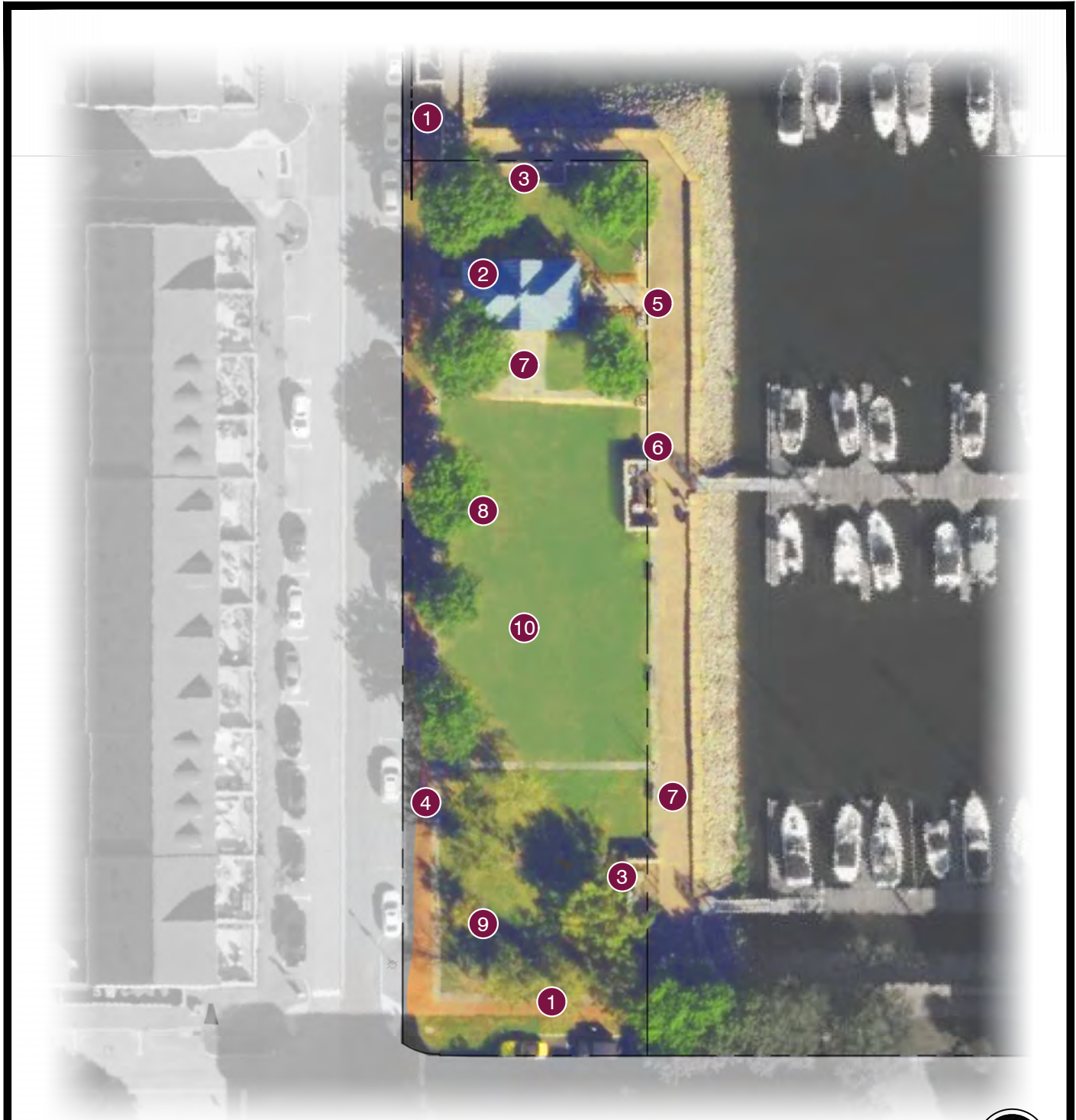


- |   |                    |    |                         |
|---|--------------------|----|-------------------------|
| 1 | SIDEWALK           | 7  | DECORATIVE PAVERS, TYP. |
| 2 | MIXED-USE BUILDING | 8  | SHADE TREE, TYP.        |
| 3 | VENDOR OUTPOST     | 9  | UNDERSTORY TREE, TYP.   |
| 4 | FOOD TRUCK SPACE   | 10 | GRASS LAWN, TYP.        |
| 5 | EXISTING TO REMAIN | 11 | PUBLIC COURTYARD        |
| 6 | BENCH, TYP.        |    |                         |

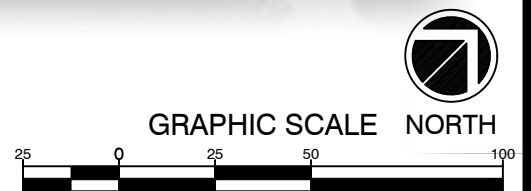


**BELMONT BAY AMENITY EXHIBIT**  
**MIXED USE BUILDING PUBLIC SPACE**

DATE: OCTOBER 31, 2025

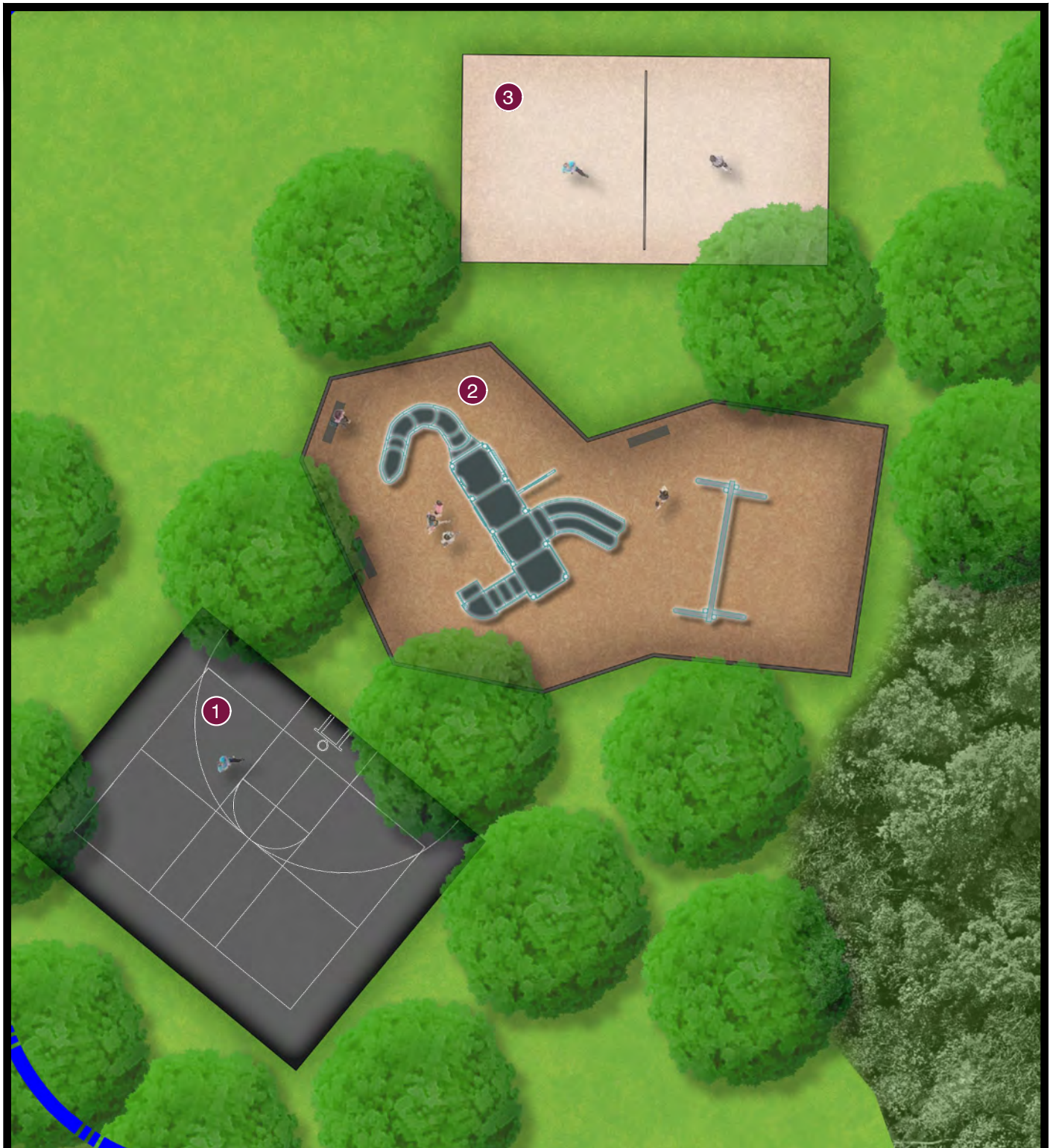


- |                      |                              |
|----------------------|------------------------------|
| 1 EX. SIDEWALK, TYP. | 6 EX. TRASH RECEPTACLE, TYP. |
| 2 EX. PAVILION       | 7 EX. TRAIL, TYP.            |
| 3 EX. WALL, TYP.     | 8 EX. SHADE TREE, TYP.       |
| 4 EX. BENCH, TYP.    | 9 EX. UNDERSTORY TREE, TYP.  |
| 5 EX. POLE, TYP.     | 10 EX. GRASS LAWN, TYP.      |



**BELMONT BAY AMENITY EXHIBIT**  
**HARBOR PLAZA**

DATE: OCTOBER 31, 2025



- 1 BASKETBALL COURT
- 2 PLAYGROUND
- 3 VOLLEYBALL COURT



GRAPHIC SCALE

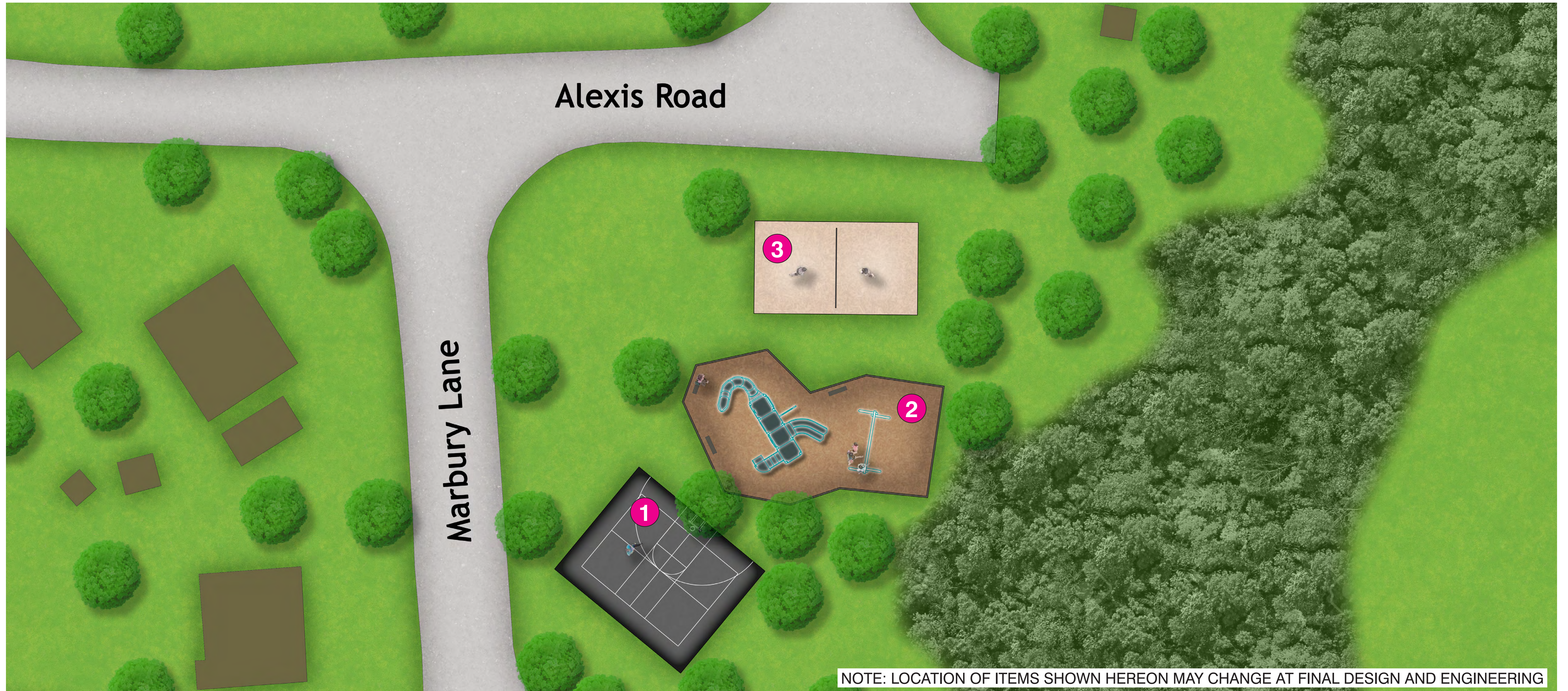


**BELMONT BAY AMENITY EXHIBIT**  
**EXISTING PARK IMPROVEMENTS**

NOTE: LOCATION OF ITEMS SHOWN HEREON MAY  
 CHANGE AT FINAL DESIGN AND ENGINEERING

DATE: JULY 30, 2025

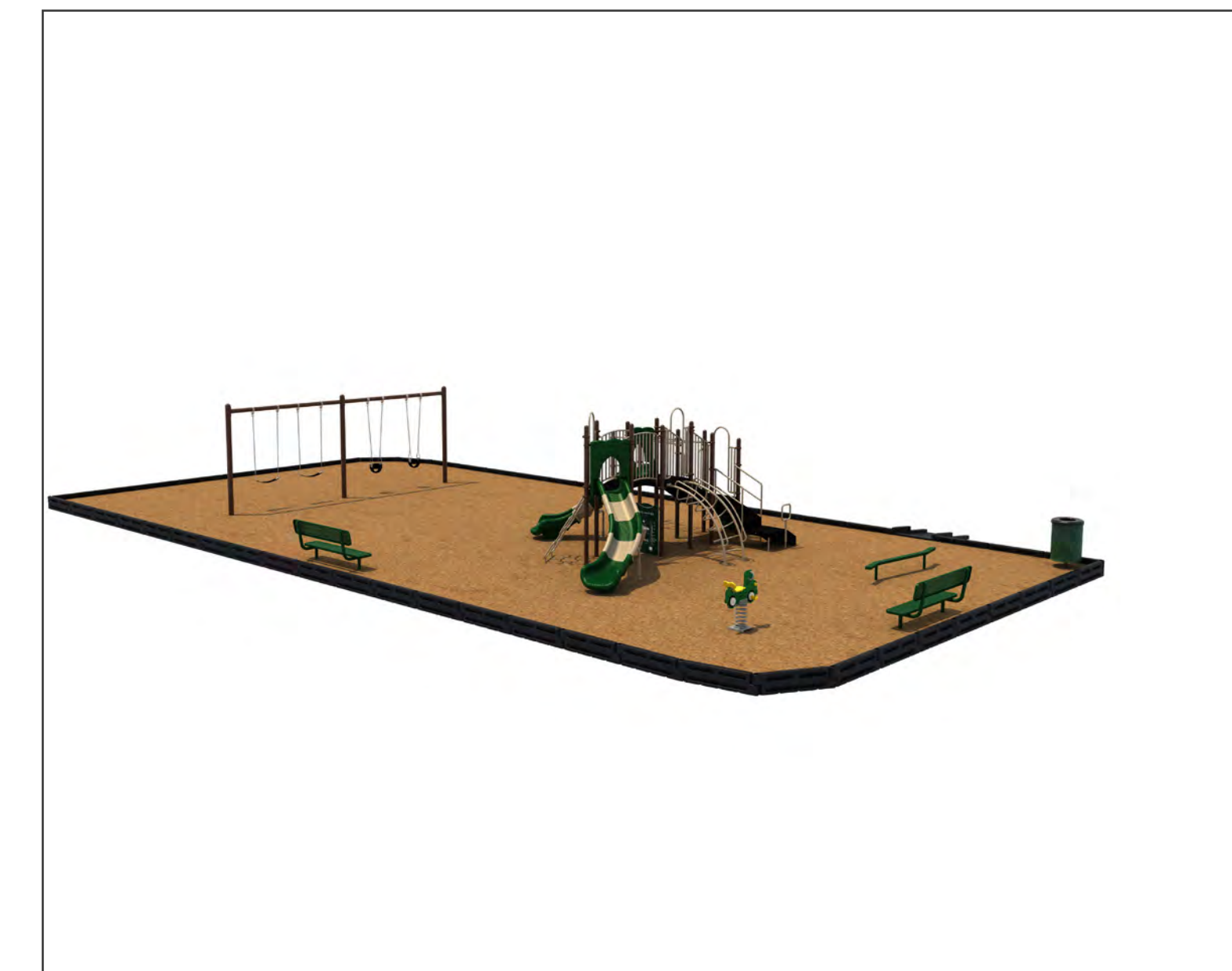
# Existing Park Improvements



**1** Half-Court



**2** Playground



**3** Volleyball Court



## Appendix B

### TYPICAL LOTS

#### SINGLE FAMILY ATTACHED LOT DETAIL- FRONT LOADED

##### DEVELOPMENT STANDARDS

- (1) MIN LOT WIDTH: 20 FT
- (2) MAX BUILDING HEIGHT: 50 FT
- (3) MIN SETBACKS
  - (3)a. FRONT- 18 FT
  - (3)b. SIDE- 4 FT
  - (3)c. REAR - 6 FT

\*ALL UNITS WILL HAVE A GARAGE

##### LANDSCAPING:

-A MINIMUM OF ONE (1) LARGE DECIDUOUS (LD) OR ONE (1) MEDIUM DECIDUOUS (MD) TREE PER END UNIT AND ONE (1) COMPACT OR SMALL DECIDUOUS TREE (CD/SD) PER UNIT.

-FINAL LOCATION OF ALL PLANT MATERIAL TO BE DETERMINED AT TIME OF FINAL SITE PLAN.

-IF SPACE DOES NOT PERMIT, THE LD AND MD REQUIRED PER END UNIT MAY BE LOCATED IN COMMON OPEN SPACE FOR THE SITE. EXISTING SHADE TREES EXCEEDING TWO-INCH CALIPER, LOCATED ANYWHERE IN THE OPEN SPACE AREA, MAY BE COUNTED ON A ONE-TO-ONE BASIS TOWARDS FULFILLING THE REQUIREMENT FOR LARGE AND MEDIUM DECIDUOUS TREES ON THAT SITE.

NOTE: PARKING WILL BE PROVIDED IN ACCORDANCE WITH DCSSM TABLE 6-8



#### SINGLE FAMILY ATTACHED LOT DETAIL- REAR LOADED

##### DEVELOPMENT STANDARDS

- (1) MIN LOT WIDTH: 20 FT
- (2) MAX BUILDING HEIGHT: 50 FT
- (3) MIN SETBACKS
  - (3)a. FRONT- 7 FT
  - (3)b. SIDE- 4 FT
  - (3)c. REAR - 4 FT

\*ALL UNITS WILL HAVE A GARAGE

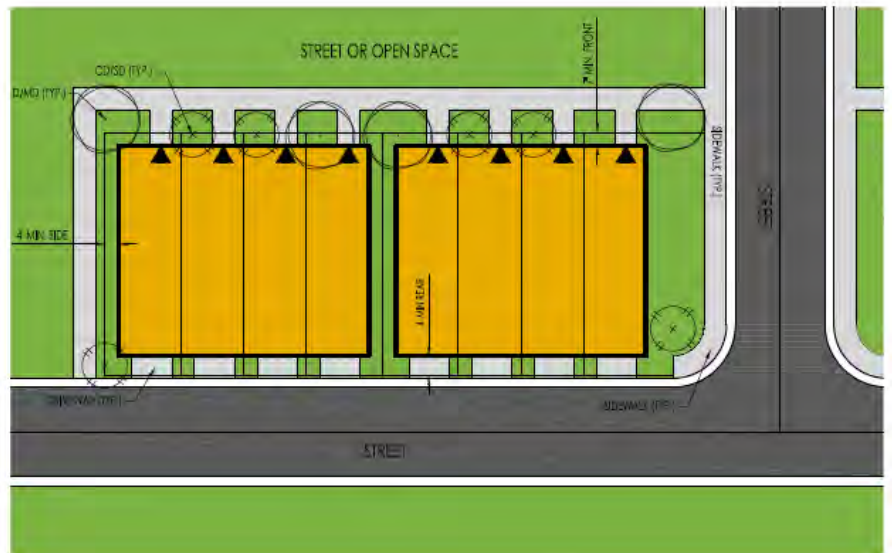
##### LANDSCAPING:

-A MINIMUM OF ONE (1) LARGE DECIDUOUS (LD) OR ONE (1) MEDIUM DECIDUOUS (MD) TREE PER END UNIT AND ONE (1) COMPACT OR SMALL DECIDUOUS TREE (CD/SD) PER UNIT.

-FINAL LOCATION OF ALL PLANT MATERIAL TO BE DETERMINED AT TIME OF FINAL SITE PLAN.

-IF SPACE DOES NOT PERMIT, THE LD AND MD REQUIRED PER END UNIT MAY BE LOCATED IN COMMON OPEN SPACE FOR THE SITE. EXISTING SHADE TREES EXCEEDING TWO-INCH CALIPER, LOCATED ANYWHERE IN THE OPEN SPACE AREA, MAY BE COUNTED ON A ONE-TO-ONE BASIS TOWARDS FULFILLING THE REQUIREMENT FOR LARGE AND MEDIUM DECIDUOUS TREES ON THAT SITE.

NOTE: PARKING WILL BE PROVIDED IN ACCORDANCE WITH DCSSM TABLE 6-8



*Appendix C:*

**ELEVATIONS TO BE FINALIZED FOR BUILDING PERMITS**



***FRONT ELEVATION***



***RIGHT SIDE ELEVATION***



***REAR ELEVATION***



***LEFT SIDE ELEVATION***



PINNACLE DESIGN & CONSULTING INC.

***BELMONT BAY COMMUNITY BUILDING***  
***MILLER AND SMITH HOMES, INC.***

1/8"=1'-0"

02/14/2024

Mixed-Use Amenity Building



BELMONT BAY LAND BAY C - MARINA TOWNHOMES - 3 STORY MODELS

SHEET 1

WOODBIDGE, VA 22191

02/15/2024



BELMONT BAY LAND BAY C - MARINA TOWNHOMES - 4 STORY MODELS

SHEET 2

WOODBIDGE, VA 22191

02/15/2024

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Marina Overlook  
*To be Finalized  
for Building  
Permits*



22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 1

22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 3

22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 2

22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 1

22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 2

22'x60' REAR LOAD - LANDBAY E -  
ELEVATION 3

FRONT ELEVATION - REAR LOAD SCALE 3/16" = 1'-0" 1

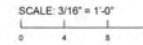


Architecture + Planning  
888.456.5949  
kitgy.com

**Miller & Smith.**  
ONE VISIT CAN CHANGE EVERYTHING.

**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 08, 2025



**LANDBAY E - TH - 22'x60' RL**  
*Belmont Landing*  
To be Finalized for  
Building Permits

**A1.3**



24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 2

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 1

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 3

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 2

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 3

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 1

FRONT ELEVATION - FRONT LOAD SCALE: 3/16" = 1'-0" 1



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**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 08, 2025

SCALE: 3/16" = 1'-0"  
0 4 8 12

**LANDBAY E - TH - 24'x45' FL**

**Taylor Point**  
To be Finalized for  
Building Permits

**A1.4**



24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 2

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 1

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 3

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 2

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 3

24'x45' FRONT LOAD - LANDBAY E -  
ELEVATION 1

FRONT ELEVATION - FRONT LOAD SCALE: 3/16" = 1'-0" 1



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**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 08, 2025

SCALE: 3/16" = 1'-0"  
0 4 8 12

**LANDBAY E - TH - 24'x45' FL**  
**Taylor Point**  
To be Finalized for  
Building Permits

**A1.5**



22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 3

22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 1

22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 2

22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 2

22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 1

22'x60' REAR LOAD - RIVERSIDE  
LANDBAY D - ELEVATION 3

FRONT ELEVATION - REAR LOAD SCALE: 3/16" = 1'-0" 1

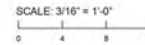


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**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 08, 2025



**LANDBAY D - TH - 22'x60' RL  
RIVERSIDE**  
**Taylor Point**  
*To be Finalized for  
Building Permits*

**A1.0**



22'x60' REAR LOAD - LANDBAY D - ELEVATION 1

22'x60' REAR LOAD - LANDBAY D - ELEVATION 3

22'x60' REAR LOAD - LANDBAY D - ELEVATION 2

22'x60' REAR LOAD - LANDBAY D - ELEVATION 3

22'x60' REAR LOAD - LANDBAY D - ELEVATION 1

22'x60' REAR LOAD - LANDBAY D - ELEVATION 2

FRONT ELEVATION - REAR LOAD SCALE 3/16" = 1'-0" 1



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**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 08, 2025

SCALE: 3/16" = 1'-0"  
0 4 8 12

**LANDBAY D - TH - 22'x60' RL**

**Taylor Point**  
To be Finalized for  
Building Permits

**A1.1**



24'x45' FRONT LOAD - LANDBAY D - ELEVATION 1

24'x45' FRONT LOAD - LANDBAY D - ELEVATION 3

24'x45' FRONT LOAD - LANDBAY D - ELEVATION 2

24'x45' FRONT LOAD - LANDBAY D - ELEVATION 3

24'x45' FRONT LOAD - LANDBAY D - ELEVATION 2

24'x45' FRONT LOAD - LANDBAY D - ELEVATION 1

FRONT ELEVATION - FRONT LOAD SCALE: 3/16" = 1'-0" 1



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**BELMONT BAY**  
PRINCE WILLIAM COUNTY, VA  
# 20230470.00

**SCHEMATIC DESIGN**  
JANUARY 06, 2025

SCALE: 3/16" = 1'-0"  
0 4 8 12

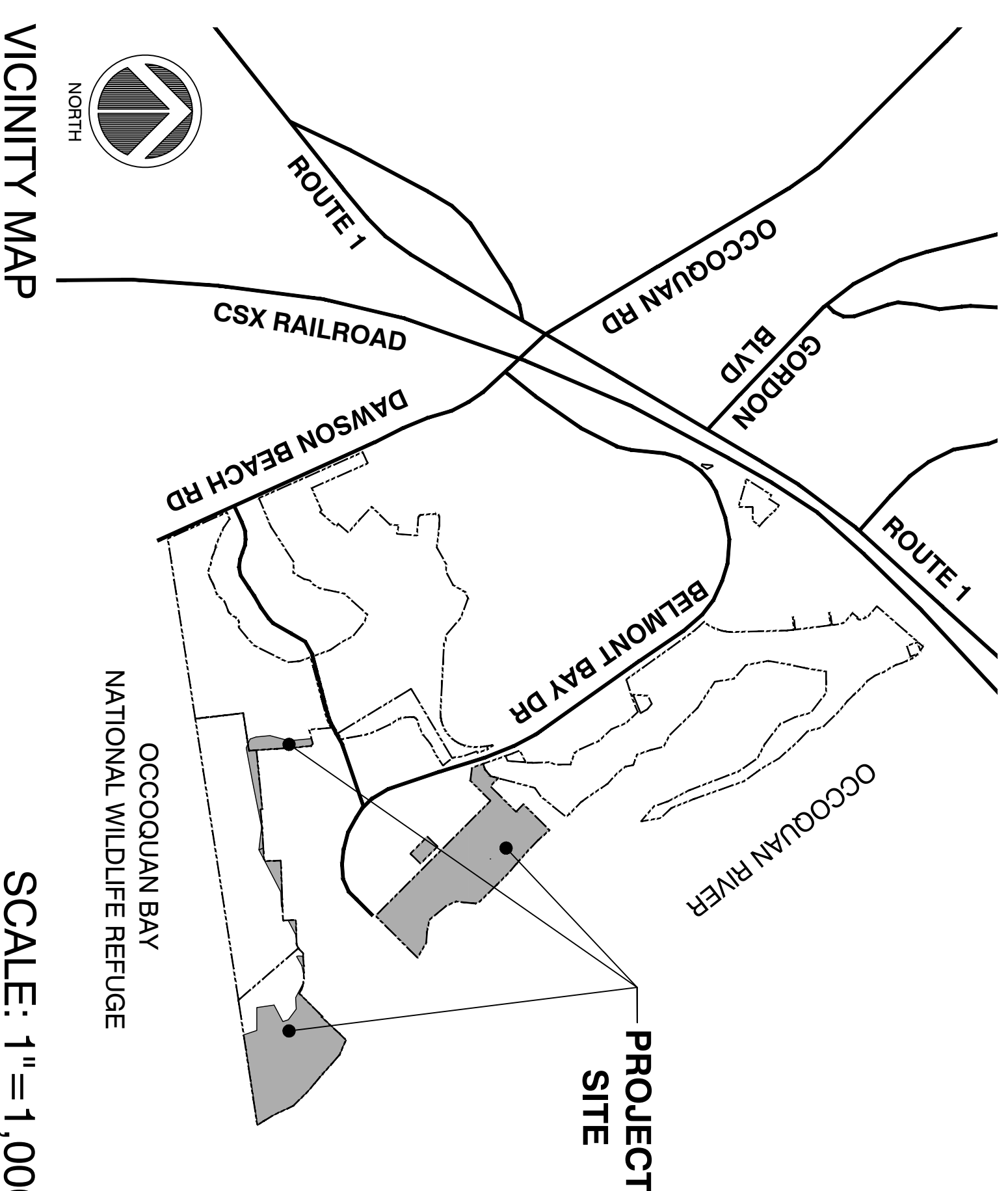
**LANDBAY D - TH - 24'x45' FL**  
**Taylor Point**  
To be Finalized for  
Building Permits

**A1.2**

# BELMONT BAY

## SPECIAL USE PERMIT

### SUP2024-00036



#### NOTES

1. SITE OWNERS: BELMONT BAY LC, BLOCKS B&C LLC, & BELMONT TOWN CENTER ASSOCIATES LLC, & BLOCKS J&K LLC
2. APPLICANT: MILLER AND SMITH HOMES, INC.
3. GPIN #'s - 8492-44-0370, 8492-53-7716, 8492-44-1022, 8492-44-5722, 8492-43-5597, & PORTIONS OF 8492-32-4965, 8492-43-4228, & 8492-32-5196
4. AREA TABULATION:
  - ORIGINAL SUP 1999-00014 AREA - +/- 92.93 ACRES
  - AREA 1 - SUBJECT TO SUP AMENDMENT - +/- 16.22 ACRES
  - AREA 2 - AREA TO BE REMOVED FROM SUP 1999-00014 - +/- 13.61 ACRES
  - NET SUP 1999-00014 AREA - +/- 63.10 ACRES
5. EXISTING ZONING: PLANNED MIXED USE DISTRICT (PMD).
6. FINAL LOCATION OF EXISTING UTILITY STRUCTURES SHALL BE DETERMINED AT FINAL SITE PLAN.
7. RPA LIMITS FROM PASA (ASP2020-00046) PREPARED BY TNT ENVIRONMENTAL, APPROVED JUNE 30, 2020.
8. FEMA FLOOD ZONES PER FIRM MAP PANELS 511530C0238E & 511530C0238E.
9. SWM/BMP FOR BELMONT BAY WILL BE PROVIDED WITH A STORMWATER MANAGEMENT AND BMP PLAN PROVIDED WITH OR BEFORE FINAL SITE PLAN.
10. DIRECTIONAL ARROWS HERON ARE FOR INFORMATION PURPOSES ONLY AND SHALL NOT BE REQUIRED TO BE INSTALLED.
11. PORTIONS OF THE SUP AREA ARE LOCATED WITHIN THE UPPER OCCOQUAN DAM INUNDATION ZONE (VA DAM#153004).

#### SHEET INDEX

1. COVER SHEET
2. SPECIAL USE PERMIT PLAN
3. TRANSPORTATION & UTILITY PLAN
4. STREET SECTIONS
5. ROAD DESIGNATIONS
6. MARINA OVERLOOK ILLUSTRATIVE

## BELMONT BAY

### COVER SHEET

DATE: MARCH 15, 2024 SPECIAL USE PERMIT SUP # 2024-00036  
REVISED: OCTOBER 31, 2025

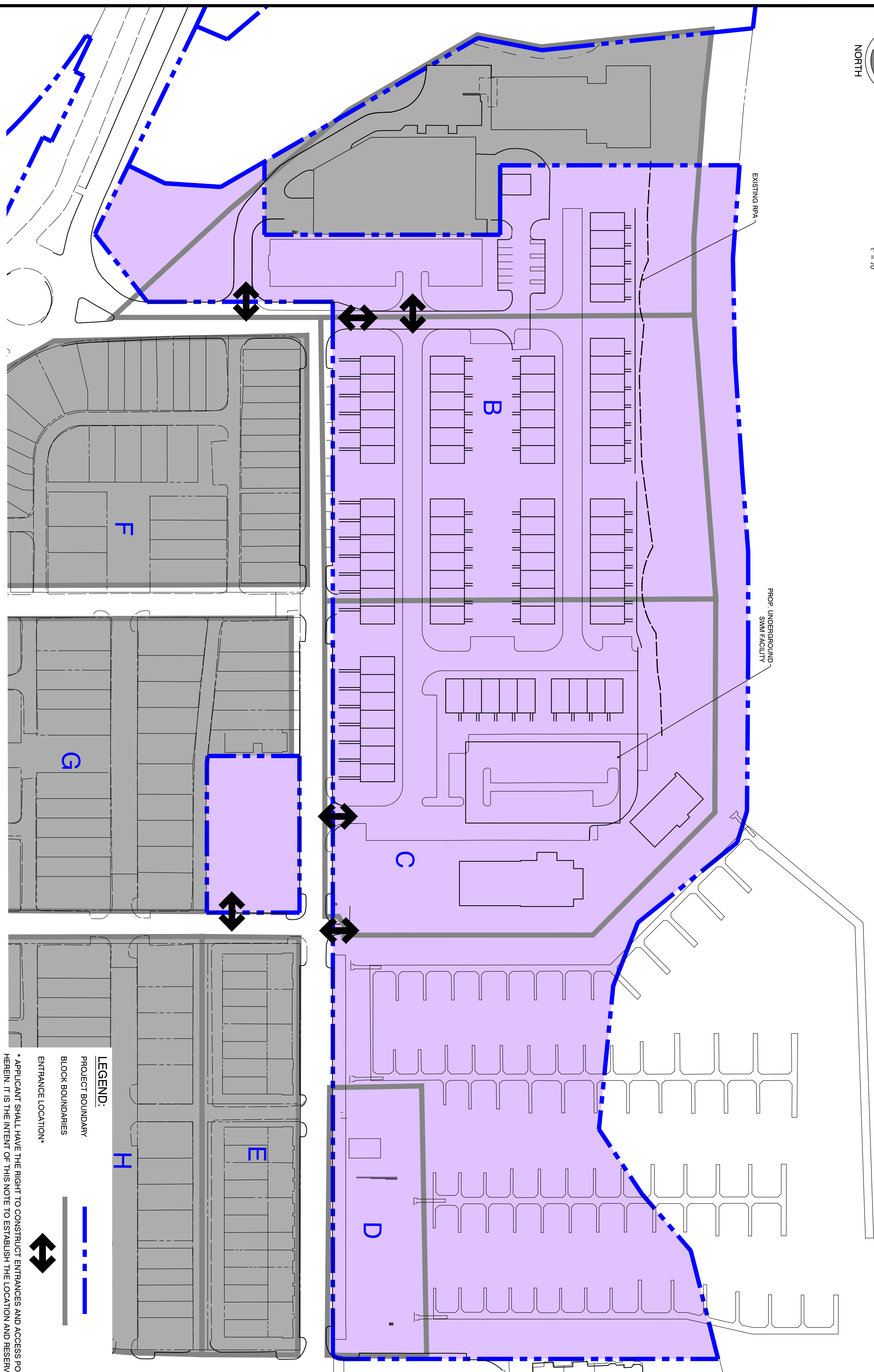
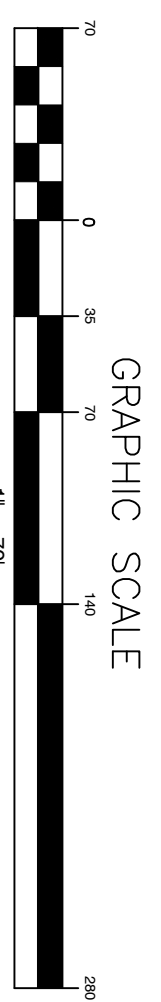
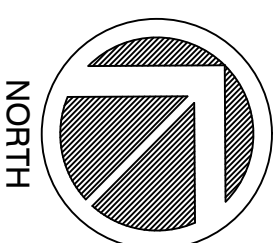
WOODBRIDGE MASTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA



Dewberry  
Engineers Inc.

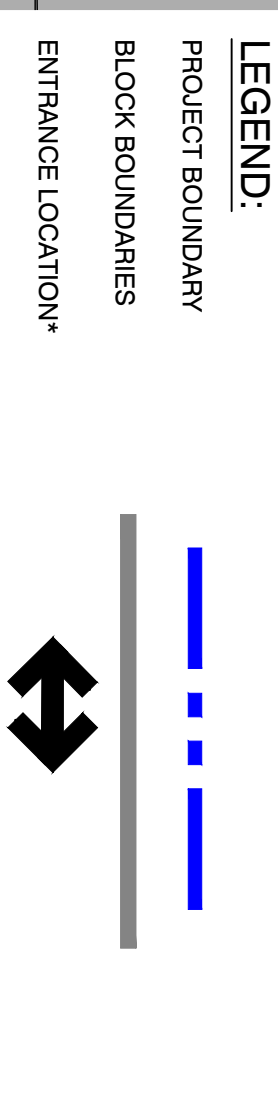
5375 HERRHOTE BLVD.  
GAINESVILLE, VA 20155-6893  
PHONE: 703.468.2211  
FAX: 703.468.2272

# MARINA OVERLOOK LANDBAY LAYOUT

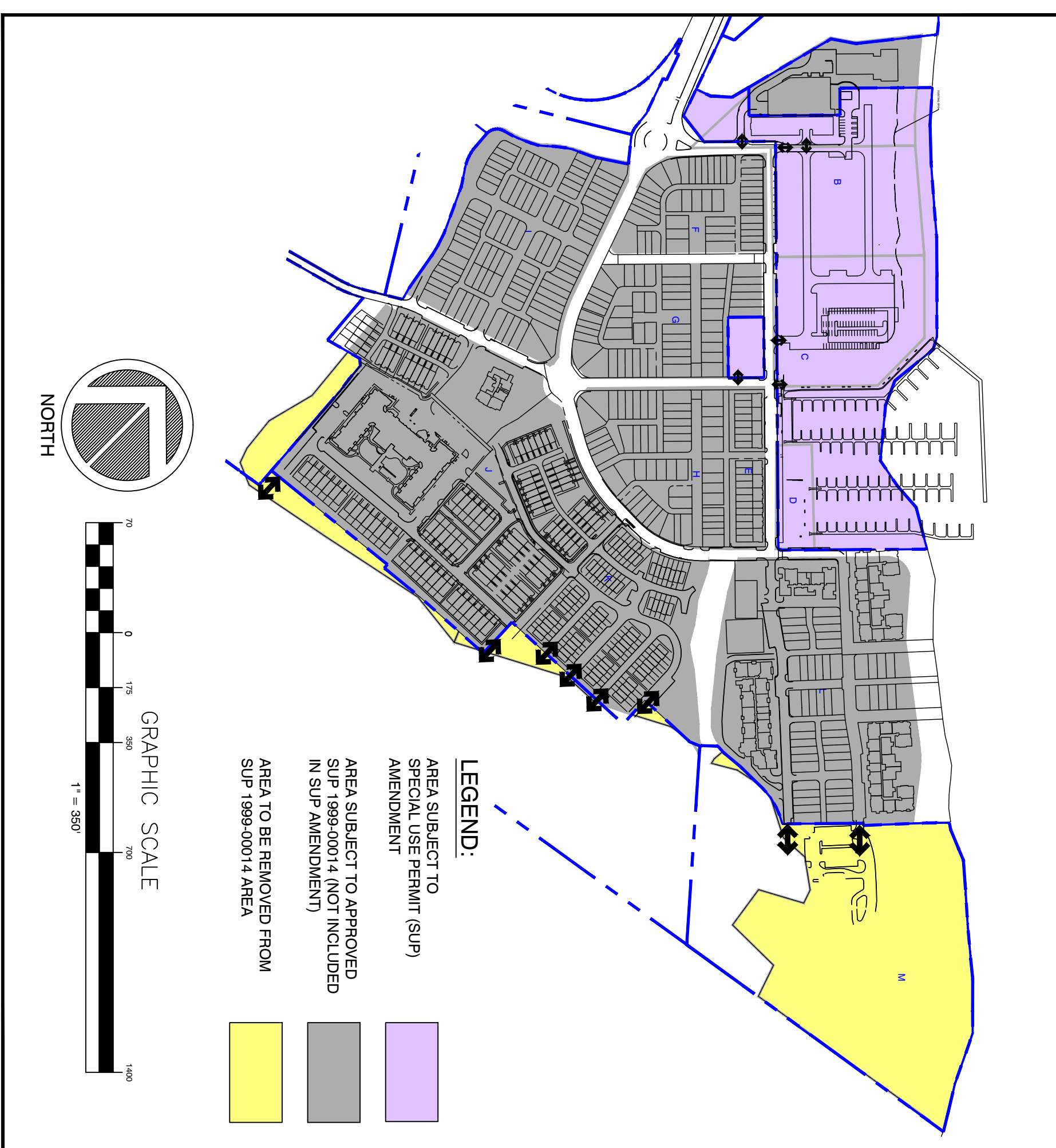
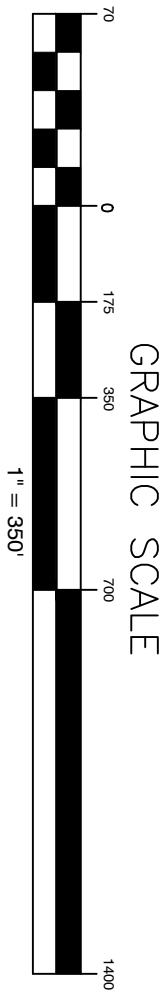


## DEVELOPMENT CODE

PROPOSED LAND BAY DESIGNATION	FORMER LAND BAY DESIGNATION (REZ #89-0022)	LAND BAY SIZE (ACRES)	ZONING DESIGNATIONS	PROJECTED LAND USE <sup>1</sup>	RESIDENTIAL DENSITY BY USE		RESIDENTIAL DENSITY BY LAND BAY		MAXIMUM NON-RESIDENTIAL RESIDENTIAL (Gross Floor Area or Slips)	YARDS (MINIMUM) FRONT LOAD TOWNHOME			YARDS (MINIMUM) REAR LOAD TOWNHOME			YARDS (MAXIMUM)			LOT WIDTH		LOT DEPTH		BUILDING HEIGHT		LOT COVERAGE <sup>3</sup> MAX	PARKING REQUIREMENTS MIN. OFF-STREET PER DC5M	BUFFER YARD
					MIN DUs/Acre	MAX DUs/Acre	MAX DUs/Acre	MAX DUs		FRONT	SIDE	REAR	FRONT	SIDE	REAR	FRONT	SIDE	REAR	MIN	MAX	MIN	MAX	PER DC5M				
MARINA OVERLOOK	PART LAND BAY 6 PART LAND BAY 14	16.22	MDR B-1	SINGLE FAMILY ATTACHED RESTAURANTS, RETAIL, MARINA BOAT SALES, PARKING, MIXED USE, COMMUNITY RECREATION AND/OR CIVIC FACILITIES, PUBLIC USES AND FACILITIES	4	6	6	74	20,200 SF* 163 SLIPS + 37 LET SKI TENDERS	18'	4'	6'	8'	4'	4'	30'	20'	25'	20'	35'	50'	20'	50'	45'	85%	PER DC5M	0'



# SUP AREA



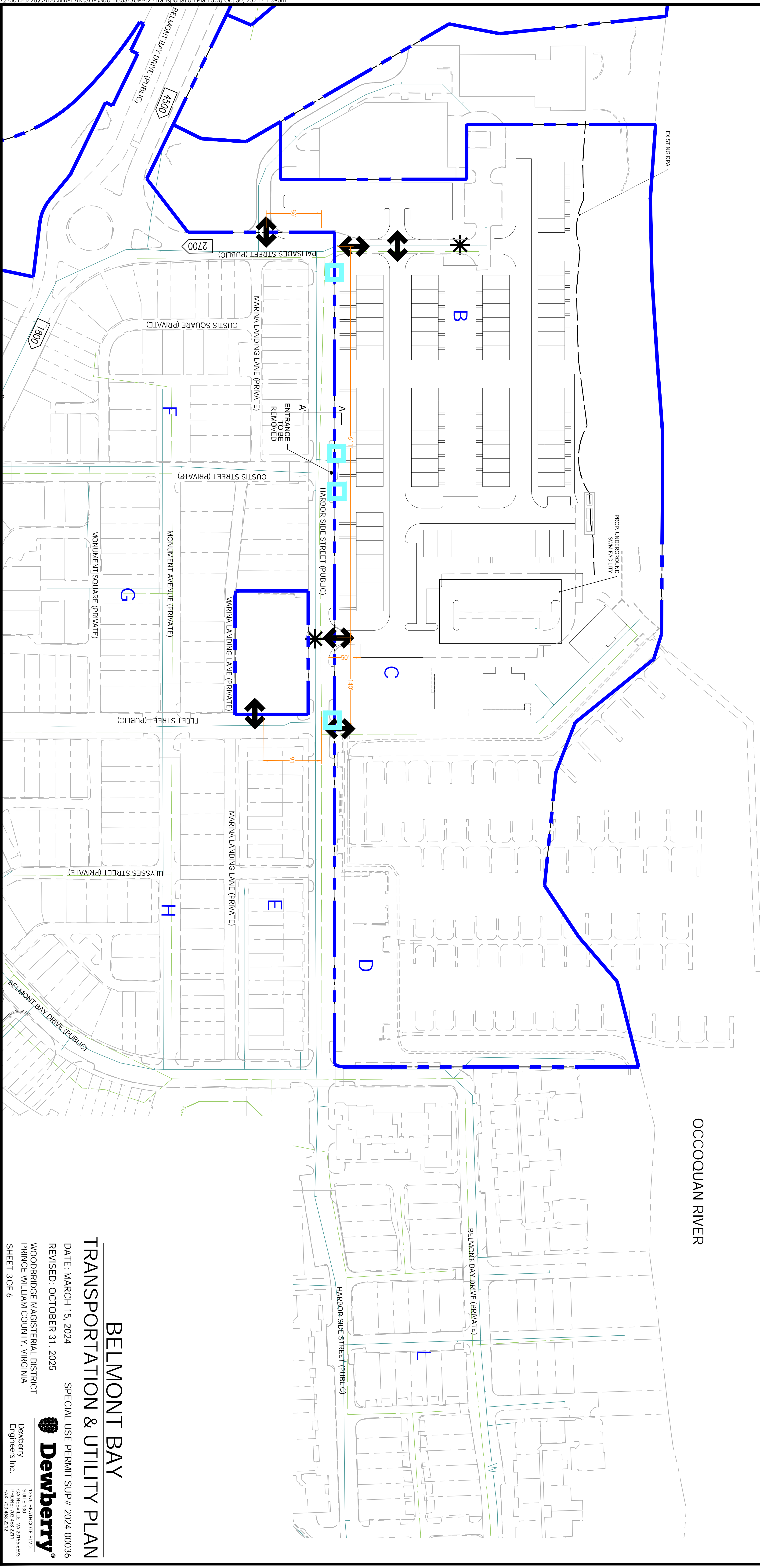
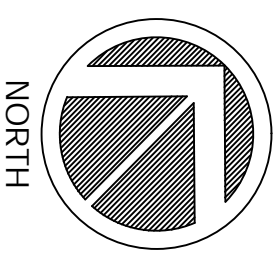
- NOTES:**
1. Projected Land Uses are not intended to preclude other uses permitted by land bay zoning designations.
  2. Lot Coverages and open space requirements for all land bays shall be satisfied by all of the Application Property as a whole per Zoning Ordinance Section 32.250.82.

## BELMONT BAY SPECIAL USE PERMIT PLAN

DATE: MARCH 15, 2024  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 2 OF 6

SPECIAL USE PERMIT SUP # 2024-00036





- LEGEND:**
- PROJECT BOUNDARY
  - ENTRANCE LOCATION\*
  - AVERAGE DAILY TRIPS (ADT)
  - CONNECTIONS TO EX UTILITIES
  - EXISTING ADA RAMP TO BE REVIEWED AND UPGRADED AT TIME OF FINAL SITE PLAN IF REQUIRED TO MEET CURRENT PROVING STANDARDS
  - EX. SANITARY SEWERLINE
  - EX. SANITARY SEWER FORCE MAIN
  - EX. WATERLINE

\* THE APPLICANT SHALL HAVE THE RIGHT TO CONSTRUCT VEHICULAR ENTRANCES AND ACCESS POINTS IN THE LOCATIONS GENERALLY SHOWN ON THIS SHEET. THE APPLICANT RESERVES THE RIGHT TO ADD, SUBTRACT, MODIFY OR MOVE AN ENTRANCE OR ACCESS POINT AT THE TIME OF FINAL SITE PLAN.

- NOTES:**
1. POSSIBLE WATER AND SANITARY SEWER CONNECTIONS SHOWN HEREON ARE PROPOSED TO PROVIDE SERVICE TO THE PROJECT AREA.
  2. ALIGNMENT AND SIZE OF PROPOSED WATER AND SANITARY SEWER LINES WILL BE DETERMINED WITH FINAL ENGINEERING AND WILL BE BASED UPON CAPACITY NEEDED TO SERVE THE PROJECT.
  3. ALIGNMENT OF EXISTING WATER AND SANITARY SEWER LINES MAY BE REVISED WITH FINAL ENGINEERING.
  4. ALL DEVELOPMENT WITHIN THE PROJECT LIMITS REQUIRING PUBLIC WATER OR SANITARY SEWER SHALL BE SERVED WITH PUBLIC WATER AND SANITARY SEWER.
  5. PROPOSED UTILITY CONNECTION POINTS ARE PRELIMINARY, SUBJECT TO CHANGE WITH FINAL ENGINEERING, AND ARE SUBJECT TO APPROVAL BY THE PRINCE WILLIAM COUNTY SERVICE AUTHORITY.
  6. OFF-SITE UTILITY CONNECTIONS SHALL REQUIRE APPLICABLE EASEMENT ACQUISITION PRIOR TO FINAL SITE PLAN APPROVAL, PROPOSING SUCH UTILITY.
  7. STORMWATER MANAGEMENT AND BMPs FOR MARINA OVERLOOK WILL BE PROVIDED IN ACCORDANCE WITH PRINCE WILLIAM COUNTY AND THE COMMONWEALTH OF VIRGINIA REQUIREMENTS OR AS APPROVED WITH FINAL SITE PLANS.
  8. ALL EXISTING STREETS WITHIN THE BELMONT BAY DEVELOPMENT AREA ARE CLASSIFIED AS LOCAL AND ALL PROPOSED STREETS WITHIN THE DEVELOPMENT WILL ALSO HAVE THE LOCAL CLASSIFICATION.
  9. SWM/BMP SHALL BE IN ACCORDANCE WITH THE SUP CONDITIONS.
  10. PRIVATE ENTRANCES ON PUBLIC ROADS WILL BE CONSIDERED COMMERCIAL ENTRANCES AND WILL MEET VDOT COMMERCIAL ENTRANCE REQUIREMENTS INCLUDING CG-11 STANDARDS.

OCCOQUAN RIVER

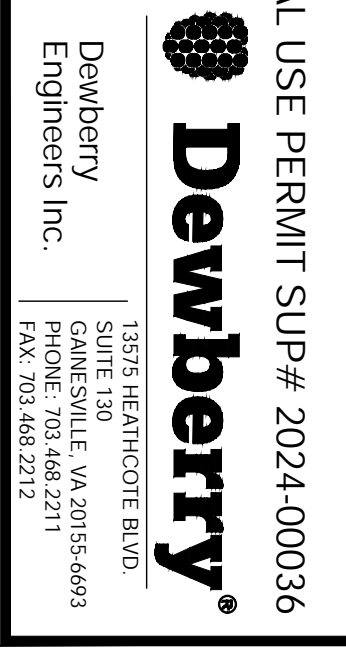
**BELMONT BAY**

**TRANSPORTATION & UTILITY PLAN**

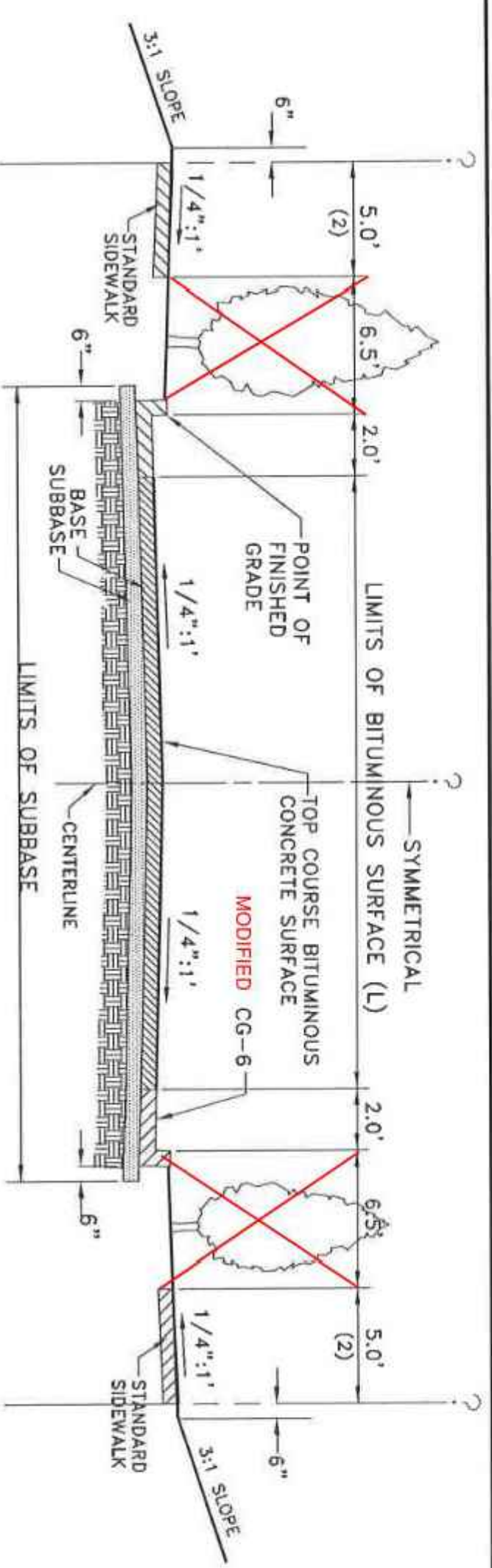
DATE: MARCH 15, 2024      SPECIAL USE PERMIT SUP# 2024-00036  
 REVISED: OCTOBER 31, 2025

WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 3 OF 6



# MODIFIED TS-1 ROAD SECTION

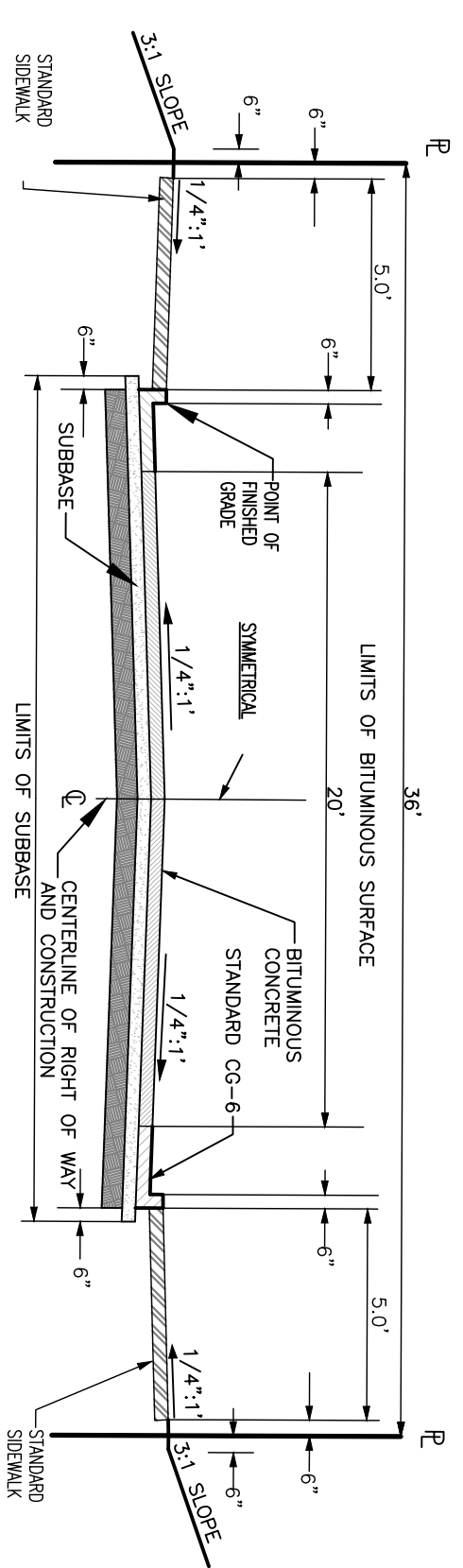


TRAFFIC VOLUME (VPD)	PARKING	L	PAVEMENT SECTION
0 TO 250	No Parking Allowed	20 ft.	CATEGORY I
0 TO 250	Parallel (one side)	20 ft.	CATEGORY I
0 TO 250	Parallel (both sides)	20 ft.	CATEGORY I
0 TO 250	Perpendicular	20 ft.	CATEGORY I
251 TO 400	Perpendicular (both sides)	20 ft.	CATEGORY II
251 TO 400	No Parking Allowed	20 ft.	CATEGORY II
401 TO 800	Parallel (one side)	20 ft.	CATEGORY III
401 TO 800	Parallel (both sides)	20 ft.	CATEGORY III
601 TO 2000	No Parking Allowed	20 ft.	CATEGORY III

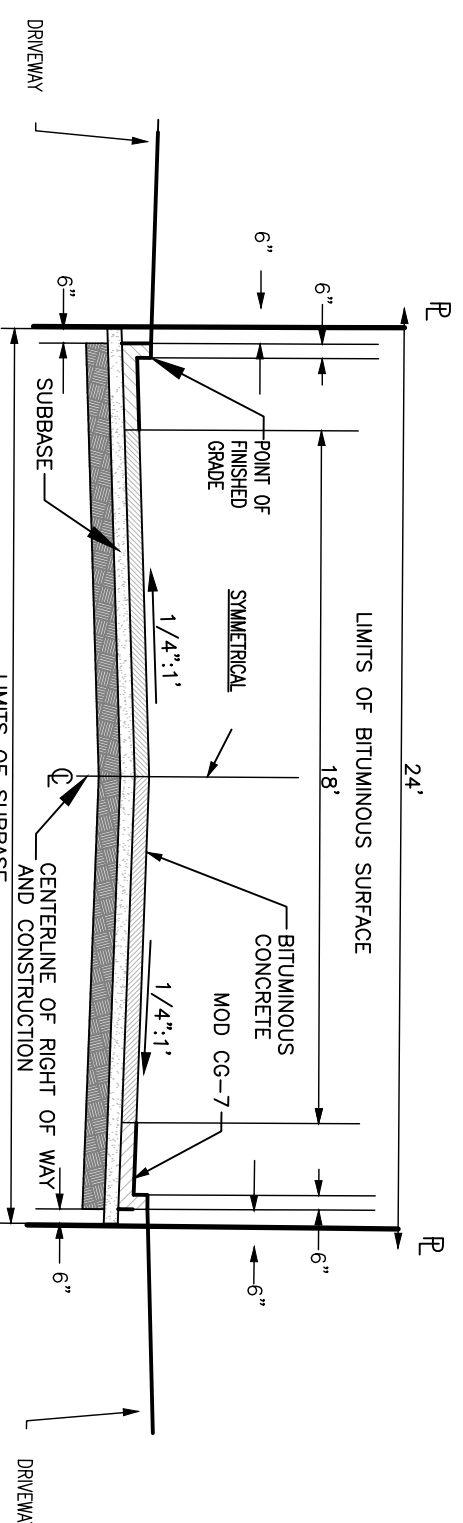
- GENERAL NOTES:
- These dimensions are for travelways only, exclusive of parking spaces.
  - Sidewalk location to be determined during final site plan review.
  - Stone material shall be extended under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The aggregate thickness under the curb and gutter shall be in excess of the depth of the gutter face or a minimum of four inches (4 in.), whichever is greater.
  - Refer to Detail 850.01 for alternative pavement sections.
  - Maximum grade eight percent (8%).
  - Design of this type of roadway shall conform to current VDOT requirements.
  - If optional street tree plantings are not provided per Section 802.45, the right-of-way may be reduced by 5 feet.

Detail No.	TS-1	COUNTY OF PRINCE WILLIAM VIRGINIA	TRAVELWAY STANDARDS FOR INDUSTRIAL, INSTITUTIONAL, OFFICE, COMMERCIAL, SINGLE FAMILY ATTACHED, AND MULTIFAMILY DEVELOPMENTS	Date 7/15/14
------------	------	-----------------------------------	---	--------------

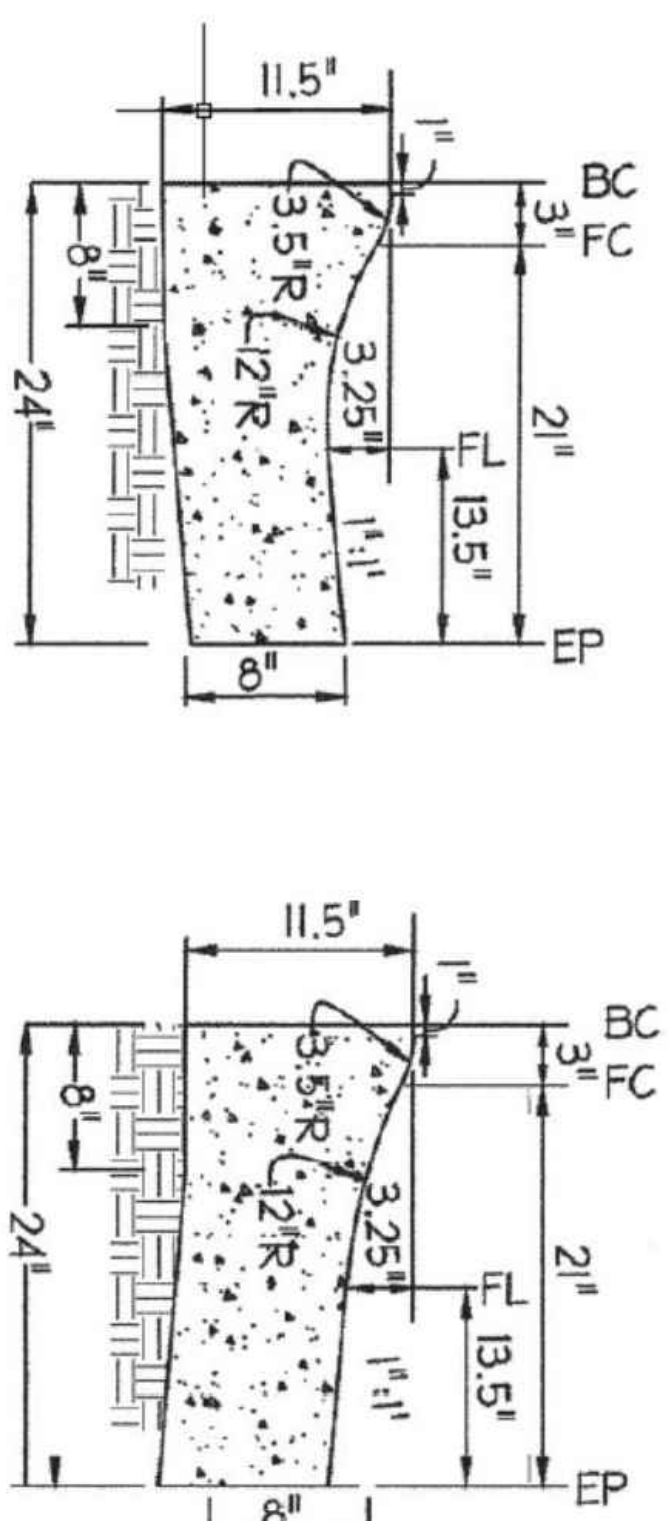
## TYPICAL PRIVATE STREET



## TYPICAL PRIVATE ALLEY

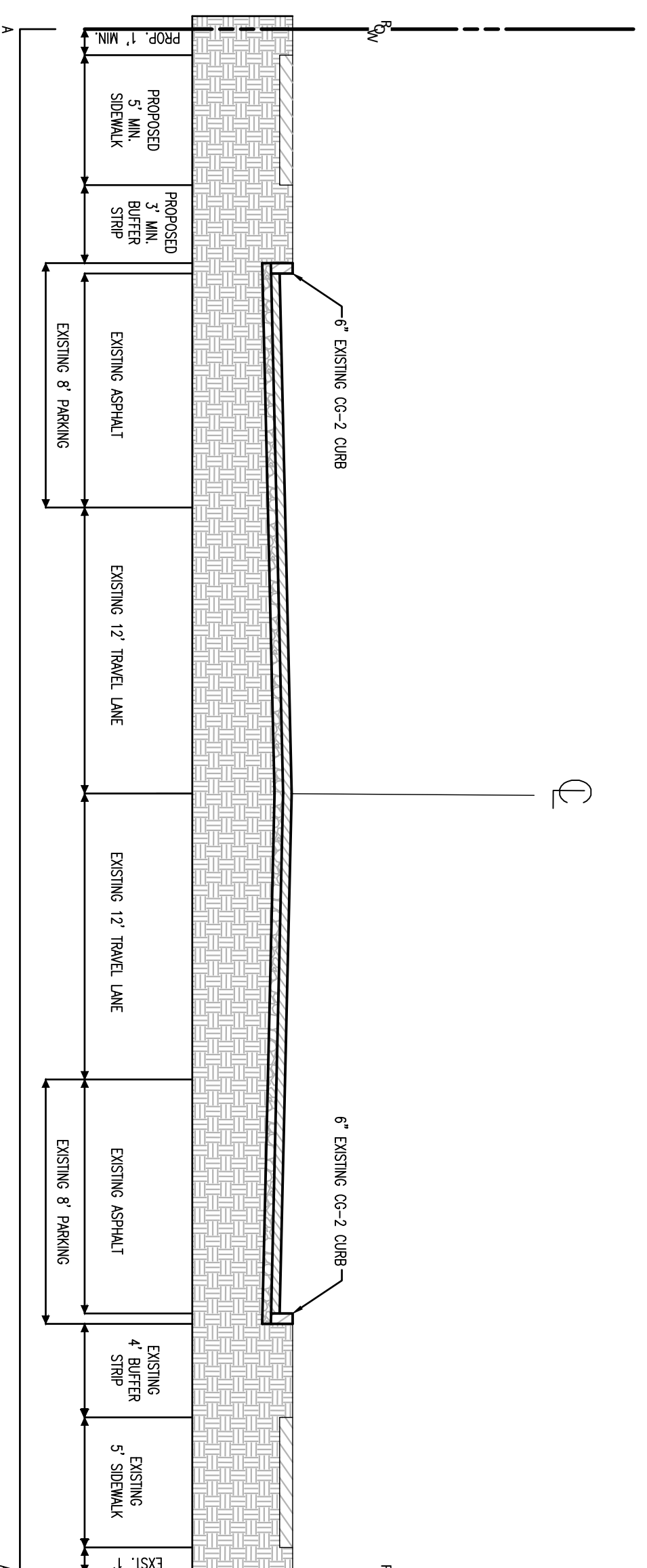


# MODIFIED CG-6



# HARBOR SIDE STREET SECTION A-A'

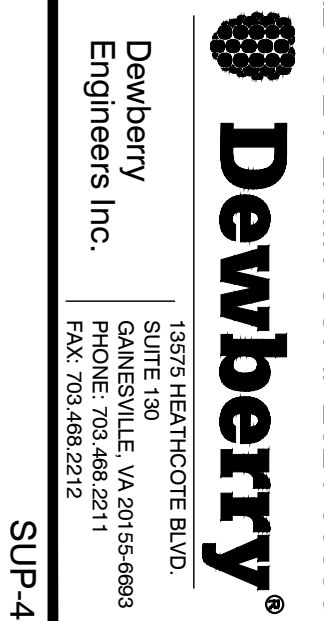
BETWEEN PALISADES STREET AND FLEET STREET

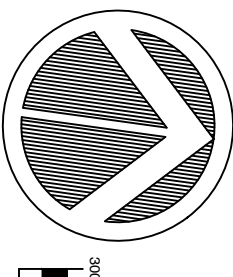


- NOTE:
- STREET TREES OR OTHER LANDSCAPE MATERIAL PROPOSED WITHIN THE RIGHT-OF-WAY SHALL REQUIRE VDOT APPROVAL.
  - ANY MODIFICATIONS OR ADDITIONS WITHIN THE RIGHT-OF-WAY SHALL REQUIRE VDOT APPROVAL.

# BELMONT BAY STREET SECTIONS

DATE: MARCH 15, 2024  
 REVISED: OCTOBER 31, 2025  
 SPECIAL USE PERMIT SUP # 2024-00036  
 WOODBRIDGE MASTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 4 OF 6





NORTH



GRAPHIC SCALE

1" = 300'



**LEGEND:**

- EXISTING PUBLIC ROADS
- EXISTING PRIVATE ROADS
- PROPOSED PRIVATE ROADS
- EXISTING PUBLIC ROAD ENTRANCE/  
INTERSECTION TO PROJECT AREA

# BELMONT BAY ROAD DESIGNATIONS

SPECIAL USE PERMIT SUP # 2024-00036

DATE: MARCH 15, 2024

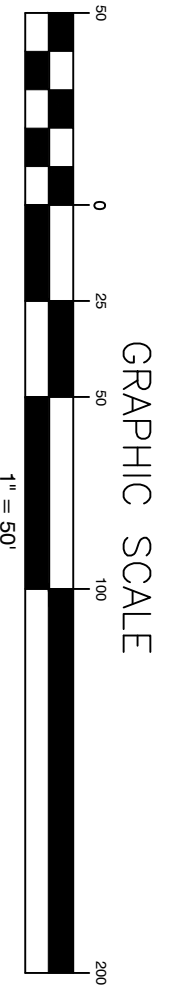
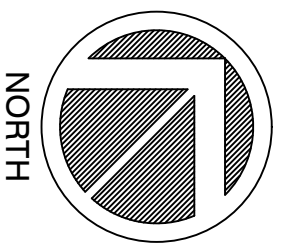
REVISED: OCTOBER 31, 2025

WOODBRIDGE MAGISTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 5 OF 6



Dewberry  
Engineers, Inc.  
5375 HIGHTHOTE BLDG  
GAINESVILLE, VA 20155-6893  
PHONE: 703.468.2211  
FAX: 703.468.2272



**LEGEND:**

- RESIDENTIAL PARKING SPACE •
- VEHICLES PER DAY

# BELMONT BAY

## MARINA OVERLOOK ILLUSTRATIVE

DATE: MARCH 15, 2024 SPECIAL USE PERMIT SUP # 2024-00036

REVISED: OCTOBER 31, 2025

WOODBRIDGE MASTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA

SHEET 6 OF 6



Dewberry  
Engineers, Inc.  
1370 HENRYHOTE BLVD.  
GAINESVILLE, VA 20155-6833  
PHONE: 703.468.2211  
FAX: 703.468.2272



***FRONT ELEVATION***



***RIGHT SIDE ELEVATION***



***REAR ELEVATION***



***LEFT SIDE ELEVATION***



PINNACLE DESIGN & CONSULTING INC.

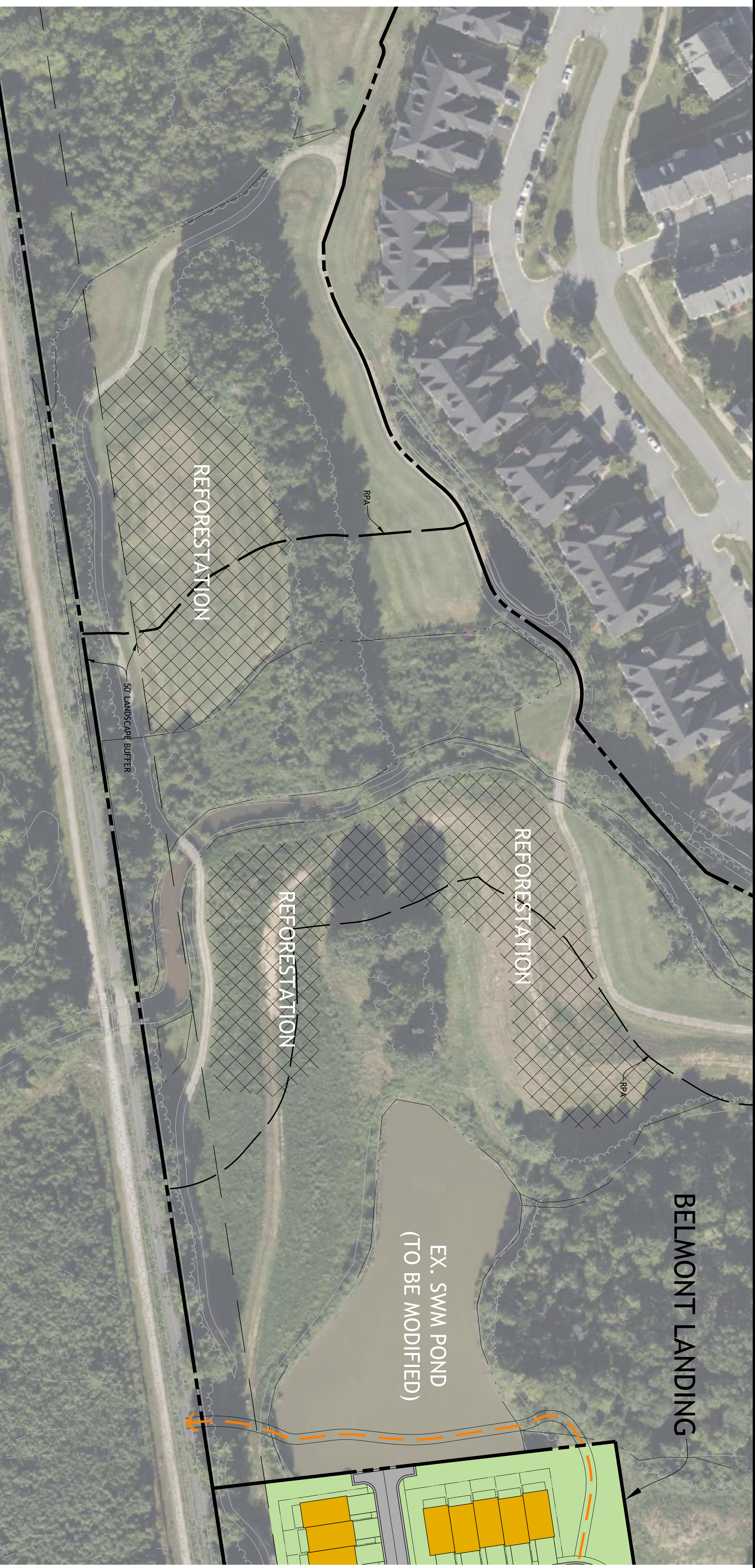
***BELMONT BAY COMMUNITY BUILDING***  
***MILLER AND SMITH HOMES, INC.***

1/8"=1'-0"

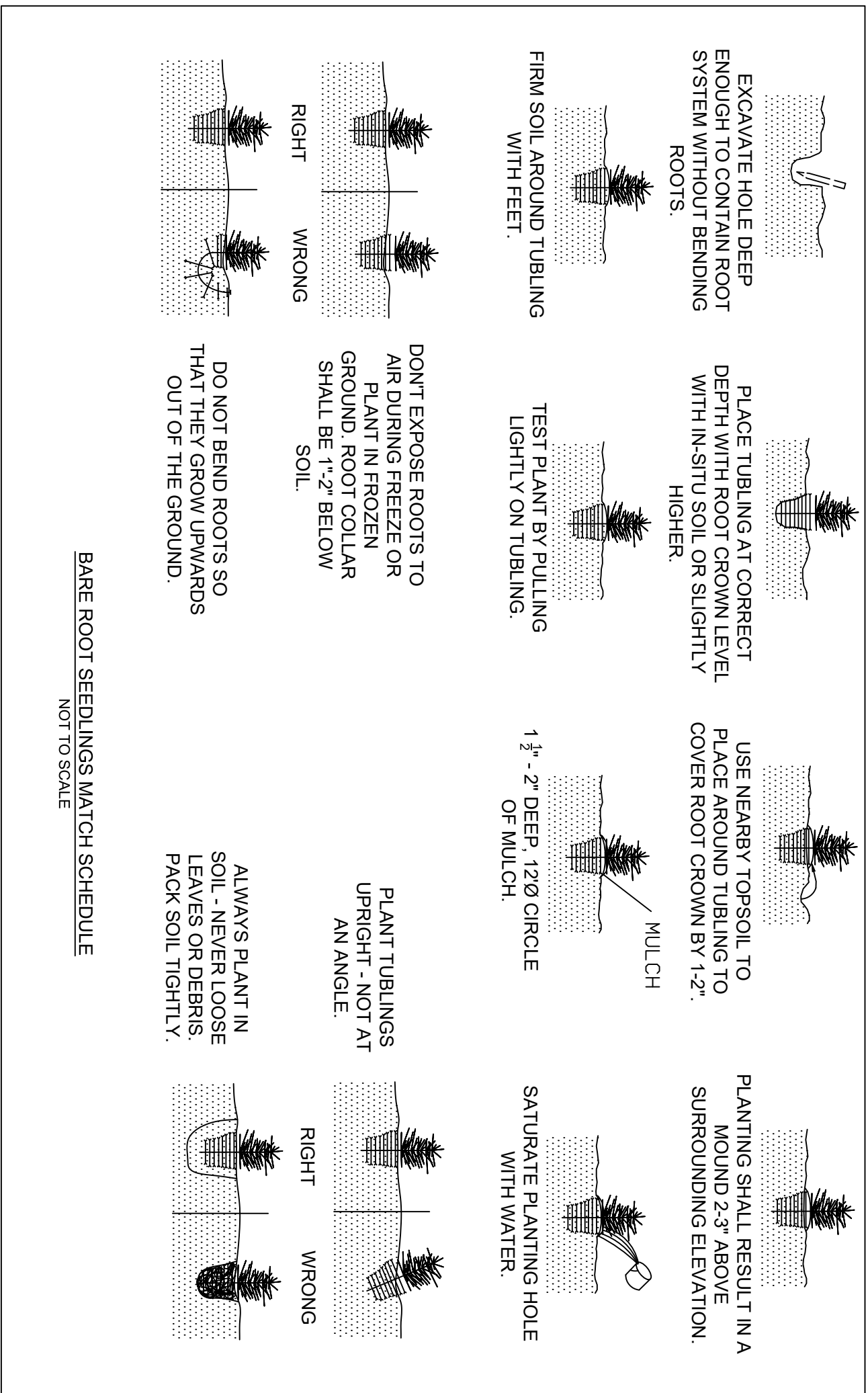
02/14/2024

Mixed-Use Amenity Building





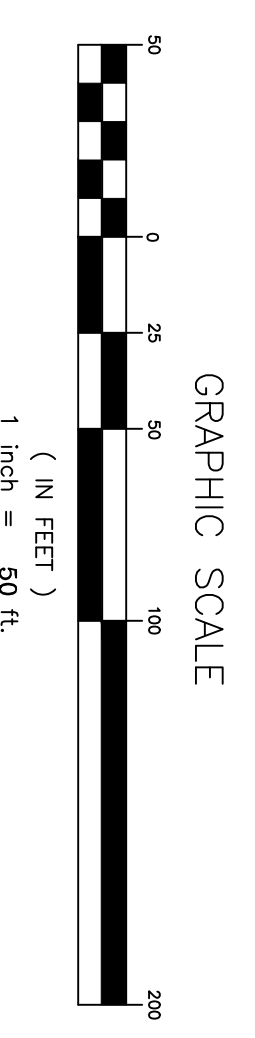
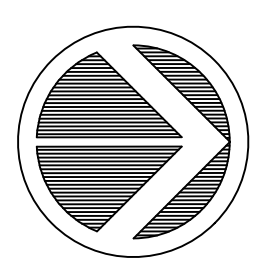
**TREE BAREROOT SEEDLING PLANTING DETAIL:**



**RIPARIAN BUFFER - SCHEDULE**

LEGEND	PLANTING ZONE	SPECIES NAME	COMMON NAME	CONTAINER SIZE	PLANT HEIGHT (MIN)	PLANT SPACING (MIN)	PLANTING RATIO	QUANTITY	REMARKS
RIPARIAN FOREST PLANTING AREA: 115,172 SF: 2.64 AC	RIPARIAN FOREST (OVERSTORY TREES)	<i>Acer rubrum</i>	Red Maple	N/A	12' - 18'	MIN. 10' O.C	450 Bare Root Seedling Per Acre	75	BARE ROOT SEEDLING TO PLANTED AT ONE (1) PER 97 SQUARE FEET IN RIPARIAN RESTORATION AREA.
		<i>Betula nigra</i>	River Birch	N/A	12' - 18'			75	
RIPARIAN FOREST (UNDERSTORY TREES)	RIPARIAN FOREST (UNDERSTORY TREES)	<i>Liriodendron tulipifera</i>	Tulip Poplar	N/A	12' - 18'	MIN. 10' O.C	450 Bare Root Seedling Per Acre	75	BARE ROOT SEEDLING TO PLANTED AT ONE (1) PER 97 SQUARE FEET IN RIPARIAN RESTORATION AREA.
		<i>Nyssa sylvatica</i>	Black Gum	N/A	12' - 18'			75	
		<i>Pinus taeda</i>	Loblolly Pine	N/A	12' - 18'			74	
		<i>Palmetus occidentalis</i>	Sycamore	N/A	12' - 18'			74	
		<i>Quercus bicolor</i>	Swamp White Oak	N/A	12' - 18'			74	
		<i>Quercus phellos</i>	Willow Oak	N/A	12' - 18'			74	
		<i>Amenocleber canadensis</i>	Sandcherry	N/A	12' - 18'			74	
		<i>Asimina triloba</i>	Paw Paw	N/A	12' - 18'			74	
		<i>Cornus canadensis</i>	Redbud	N/A	12' - 18'			74	
		<i>Chionanthus virginicus</i>	Fringetree	N/A	12' - 18'			74	
<i>Liriodendron tulipifera</i>	Spicebush	N/A	12' - 18'	74					
<i>Magnolia virginiana</i>	Sweetbay Magnolia	N/A	12' - 18'	74					
<i>Sambucus pubens</i>	Elderberry	N/A	12' - 18'	74					
<i>Sassafras albidum</i>	Sassafras	N/A	12' - 18'	74					
BARE ROOT SEEDLING QUANTITY SUBTOTALS:								1,188	

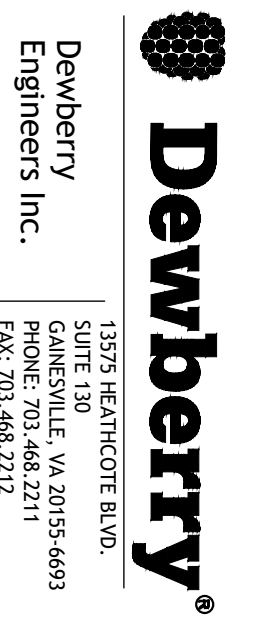
1. Spacing and species distribution on site shall be determined based on field conditions to achieve a natural random nature and to avoid a plantation appearance.
2. This plan is conceptual in nature and will be revised as necessary and appropriate according to the Prince William County CDSM Standards and the Riparian Buffers Guidance Manual during the site plan approval process in conjunction with County staff review.
3. Species selection will be further developed with County staff review.
4. Any plant substitutions or changes must be approved by project engineer prior to planting.
5. All bare root seedlings shall not remain unplanted for more than two weeks. Refer to Landscape Planting Schedule & Notes Sheet for unplanted shrub requirements.
6. Project engineer shall be notified at least three days prior to installation of trees and shrubs. Project engineer shall review location and layout of plantings prior to installation.
7. Tubelings and other small container grown material may be substituted based on availability and expected survival.
8. Planting rates may exceed 450 per acre at time of planting to account for survival so long as 338 trees per acre are present and the time of review for escrow release.



**BELMONT BAY**  
CONCEPTUAL REFORESTATION PLAN

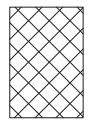
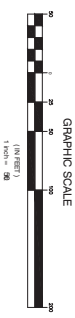
DATE: FEBRUARY 10, 2025

WOODBRIDGE MAGISTERIAL DISTRICT  
PRINCE WILLIAM COUNTY, VIRGINIA  
SHEET 1 OF 1





REVEGETATION AREA  
±0.50 AC



LEGEND  
RIPARIAN VEGETATION

**BELMONT RPA RESTORATION PLANTING NOTES:**

1. LARGE TREES AND UNDERSTORY TREES PLANTED IN THE RPA BUFFER WILL BE ARRANGED IN CLUSTERS AND LOCATED TO MINIMIZE THE OBSTRUCTION OF PRIMARY VIEWS AND VISAS ENJOYED BY THE RESIDENTS OF THE COMMUNITY TO THE GREATEST EXTENT POSSIBLE WITHOUT REDUCING THE QUANTITIES OR TYPES OF VEGETATION PROVIDED OR DIMINISHING THE EFFECTIVENESS OF MAINTAINING WATER QUALITY BENEFITS AND OBJECTIVES.
2. A MANAGEMENT PLAN FOR THE BENEFIT OF THE COMMUNITY WILL BE DEVELOPED AND APPROVED BY THE COUNTY AGENCIES AT THE TIME OF SITE PLAN APPROVAL. THIS PLAN WILL INCLUDE THE OBJECTIVES OF MAINTAINING THE SUCCESS OF THE PLANTING, PROVIDING FOR THE MANAGEMENT OF NATIVE SPECIES AND MANAGEMENT OF THE BUFFER TO PREVENT THE INTRODUCTION OF INVASIVE SPECIES AND TO PREVENT THE INTRODUCTION OF ALIEN AND IMPROVING WATER QUALITY SHALL BE ACKNOWLEDGED AND INCLUDED WITH ALL MANAGEMENT EFFORTS.
3. THE EXACT QUANTITIES OF SPECIES MAY CHANGE DUE TO SPECIES AVAILABILITY AT THE TIME OF PLAN APPROVAL. THE EXACT QUANTITIES OF SPECIES MAY CHANGE DUE TO SPECIES AVAILABILITY AT THE TIME OF PLAN APPROVAL. TO MAINTAIN VEGETATION, THE LINK OF CANOPY TREES, UNDERSTORY TREES, LARGE SHRUBS AND SMALL SHRUBS SHALL BE GENERALLY THE SAME AS PRESENTED ON THIS PLAN.

**RIPARIAN BUFFER PLANTING SCHEDULE**

Symbol	Species (Common name)	Quantity	Planting Size	Height at Planting	Types	Comments
<b>Waterline area = 10,880 sq. ft.</b>						
<b>B. Canopy Trees @ 14,120 sq. ft. = 29 Understory Trees</b>						
AT	Red maple (Liquidambar styraciflua)	31	12" cal.	N/A	B&B or Container	
BN	Black Birch (Betula nigra)	51	12" cal.	N/A	B&B or Container	Single stem preferred
DV	Downy Woodpecker (Picoides pubescens)	1	12" cal.	N/A	B&B or Container	
DB	Swamp white oak (Quercus bicolor)	51	12" cal.	N/A	B&B or Container	
CC	American hornbeam (Corylus cornuta)	29	12" cal.	N/A	B&B or Container	
<b>Total</b>						
<b>B. Understory Trees @ 14,120 sq. ft. = 29 Understory Trees</b>						
AT	Red maple (Liquidambar styraciflua)	31	12" cal.	N/A	B&B or Container	
BN	Black Birch (Betula nigra)	51	12" cal.	N/A	B&B or Container	
DV	Downy Woodpecker (Picoides pubescens)	1	12" cal.	N/A	B&B or Container	
DB	Swamp white oak (Quercus bicolor)	51	12" cal.	N/A	B&B or Container	
CC	American hornbeam (Corylus cornuta)	29	12" cal.	N/A	B&B or Container	
<b>Total</b>						
<b>B. Large Shrubs @ 2,680 sq. ft. = 58 Large shrubs</b>						
LB	Shrub Dogwood (Cornus alternifolia)	101	12" cal.	15-18"	Container	
SC	Shrub Dogwood (Cornus alternifolia)	101	12" cal.	15-18"	Container	
HO	Witch Hazel (Hamamelis virginica)	101	12" cal.	15-18"	Container	
CS	Red Twig Dogwood (Cornus sericea)	101	12" cal.	15-18"	Container	
<b>Total</b>						
<b>B. Strubs @ 2,680 sq. ft. = 58 Strubs</b>						
CF	Eastern white pine (Pinus strobus)	101	12" cal.	15-18"	Container	
CA	Redstart (Cardinalis rubra)	101	12" cal.	15-18"	Container	
SA	White Pine (Pinus strobus)	101	12" cal.	15-18"	Container	
VA	White Pine (Pinus strobus)	101	12" cal.	15-18"	Container	
VX	White Pine (Pinus strobus)	101	12" cal.	15-18"	Container	
CS	White Pine (Pinus strobus)	101	12" cal.	15-18"	Container	
<b>Total</b>						
<b>B. Landuse area = 10,880 sq. ft. - Primarily shrub planting (Modification from seeding planting)</b>						
AT	Red maple (Liquidambar styraciflua)	31	12" cal.	N/A	B&B or Container	
BN	Black Birch (Betula nigra)	51	12" cal.	N/A	B&B or Container	
CC	American hornbeam (Corylus cornuta)	29	12" cal.	N/A	B&B or Container	
CR	Redstart (Cardinalis rubra)	29	12" cal.	N/A	B&B or Container	
SA	White Pine (Pinus strobus)	29	12" cal.	N/A	B&B or Container	
<b>Total</b>						
<b>Small Shrubs</b>						
CF	Eastern white pine (Pinus strobus)	141	12" cal.	15-18"	Container	
CA	Redstart (Cardinalis rubra)	141	12" cal.	15-18"	Container	
SA	White Pine (Pinus strobus)	141	12" cal.	15-18"	Container	
VA	White Pine (Pinus strobus)	141	12" cal.	15-18"	Container	
VX	White Pine (Pinus strobus)	141	12" cal.	15-18"	Container	
CS	White Pine (Pinus strobus)	141	12" cal.	15-18"	Container	
<b>Total</b>						
<b>481</b>						
<b>Total</b>						
<b>120</b>						
<b>190</b>						

**BELMONT BAY**  
**CONCEPTUAL MARINA OVERLOOK**  
**RPA REVEGETATION PLAN**  
 DATE: FEBRUARY 10, 2025  
 REVISED: OCTOBER 31, 2025  
 WOODBRIDGE MAGISTERIAL DISTRICT  
 PRINCE WILLIAM COUNTY, VIRGINIA  
 SHEET 1 OF 1

Dowberry  
 11251 BELMONT BLVD.  
 SUITE 100  
 FARMERSVILLE, VA 22434-5893  
 PHONE: 703.961.2722



## HISTORICAL COMMISSION RESOLUTION

**MOTION: PORTA**

**June 11, 2024  
Regular Meeting  
Res. No. 24-039**

**SECOND: BURGESS**

**RE: LAND DEVELOPMENT RECOMMENDATIONS**

**ACTION: APPROVED**

**WHEREAS**, the Prince William County Historical Commission seeks to identify, preserve and protect historic sites and structures in Prince William County; and

**WHEREAS**, the Prince William County Historical Commission's review of pending land development applications assists in determining the necessity for cultural resource surveys and other research and evaluations; and

**WHEREAS**, the Prince William County Historical Commission believes that the identification, preservation and protection of historic sites and structures throughout Prince William County is well served by this action;

**NOW, THEREFORE, BE IT RESOLVED**, that the Prince William County Historical Commission does hereby recommend to the Prince William County Planning Commission the action(s) noted for the following properties:

<b>Case Number</b>	<b>Name</b>	<b>Recommendation</b>
REZ2024-00034	Hawthorn at Kettle Run	No Further Work
REZ2024-00035	Alderwood at Kettle Run	No Further Work
REZ2024-00002	Balls Ford Road Crossing 2 <sup>nd</sup> Submission	Table
REZ2023-00031	Thomas Farm at Bristow Station 3 <sup>rd</sup> Submission	Table
REZ2024-00039	Manassas Mall Mixed Use Rezoning	Phase I Archaeology Survey and Phase II Level Historical Research on all three rezoning parcels.
REZ2024-00036	Longleaf at Kettle Run	Table

<b><u>Case Number</u></b>	<b><u>Name</u></b>	<b><u>Recommendation</u></b>
REZ2024-00030	Heathcote Marketplace Residential	Phase I study and Metal Detector Survey, and if warranted, Phase II evaluation and Phase III data recovery study. Artifacts to be donated to and curated with the County.
REZ2024-00037	North Carters Grove	No Further Work
REZ2024-00040	Belmont Bay Proffer Amendment	No Further Work
SUP2024-00036	Belmont Bay SUP Amendment	No Further Work
SUP2024-00038	Chase Bank at Stonewall Shops Square	No Further Work

**Votes:**

**Ayes:** by acclamation

**Nays:** None

**Absent from Vote:** None

**Absent from Meeting:** Brace, Brickley, Kulick

**MOTION CARRIED**

ATTEST:



Secretary to the Commission